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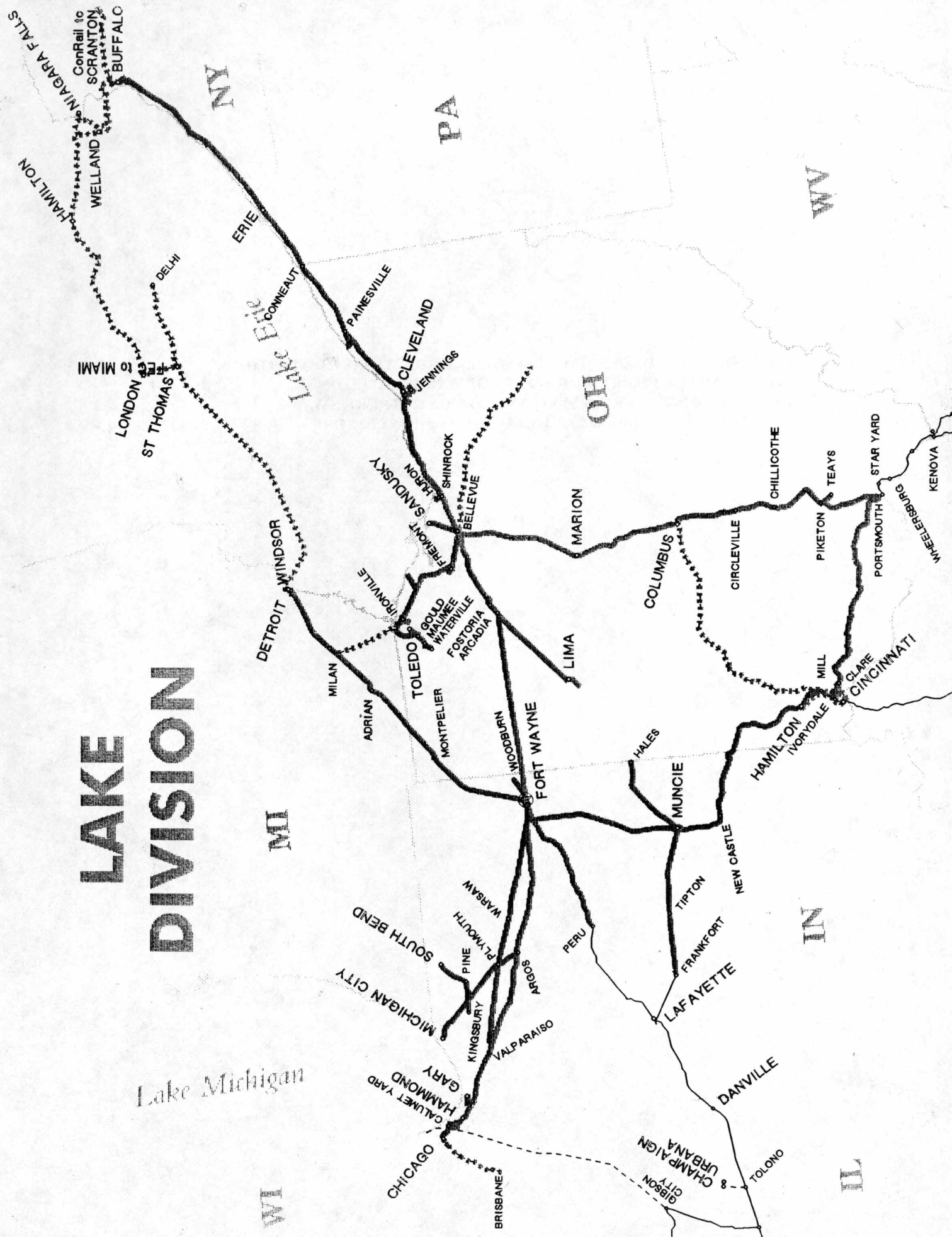
**NORFOLK  
SOUTHERN**

# **Lake Division**

**1996**



# LAKE DIVISION



**INFORMATION CONCERNING CHANGES, CORRECTIONS OR ADDITIONS  
TO THIS TRACK CHART SHOULD BE FORWARDED TO THE  
OFFICE OF ENGINEERING SYSTEMS, 99 SPRING STREET, SW,  
ROOM 513, NS BOX 139, ATLANTA, GA 30303 - PHONE 529-1314**

## LAKE DIVISION

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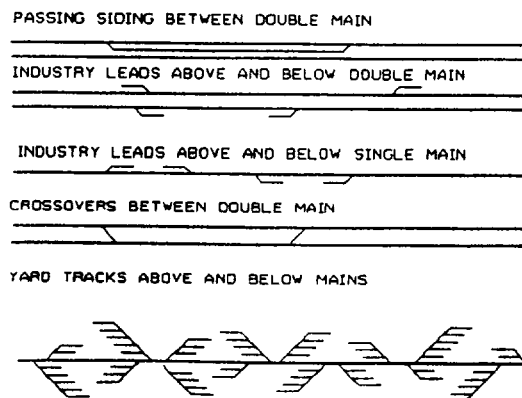
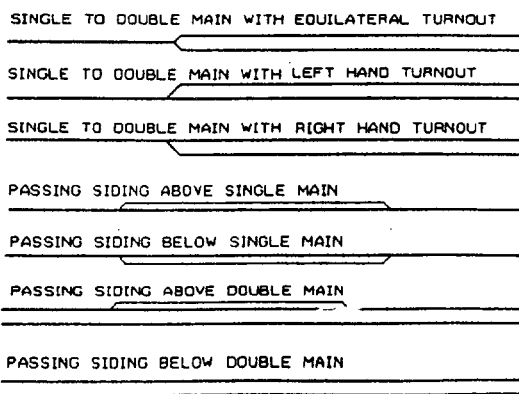
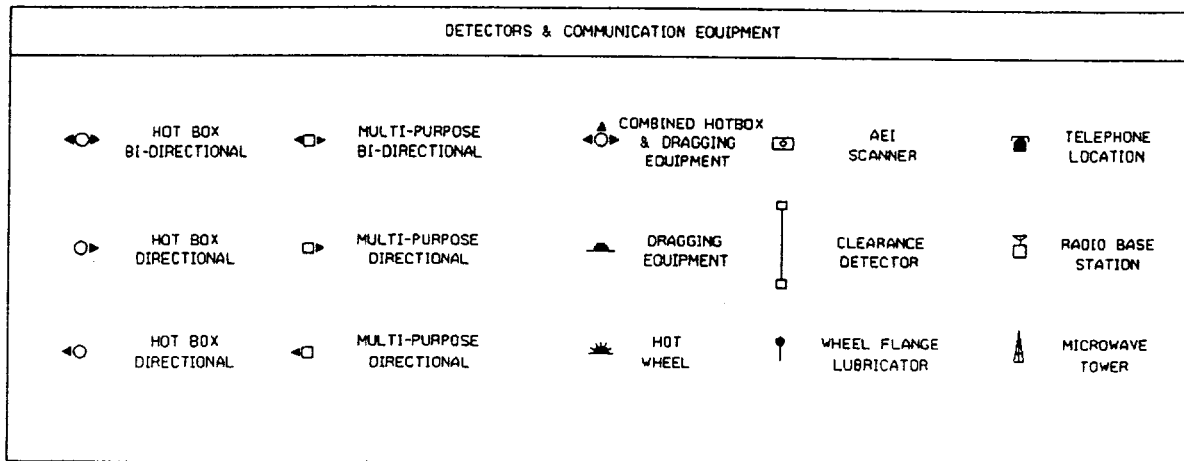
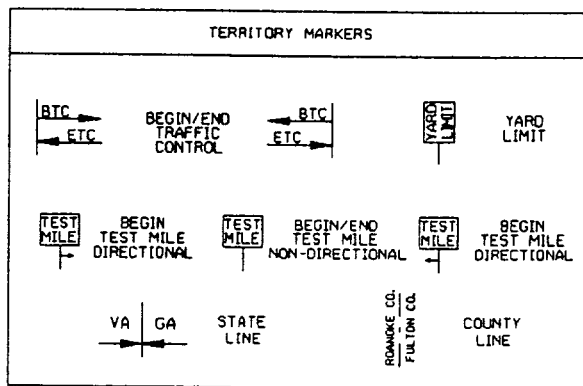
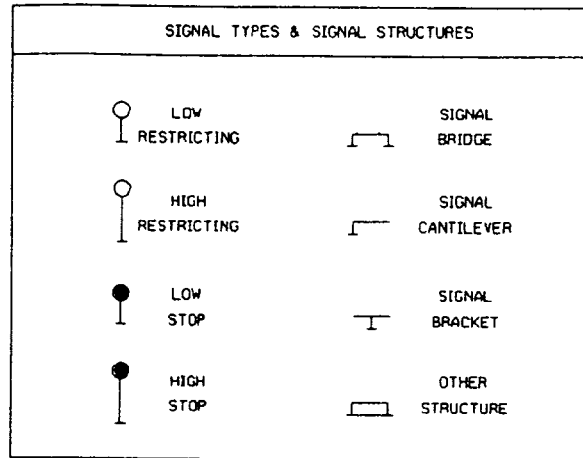
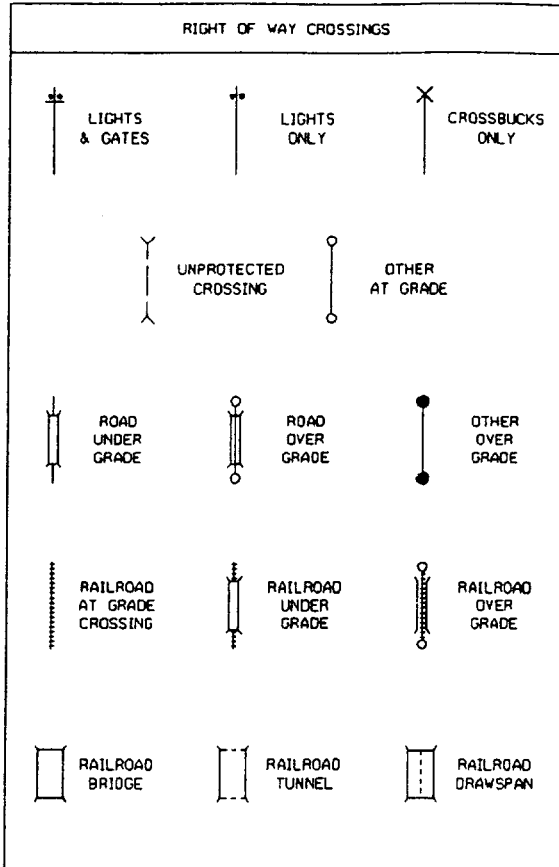
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# TRACK CHART SYMBOL LEGEND



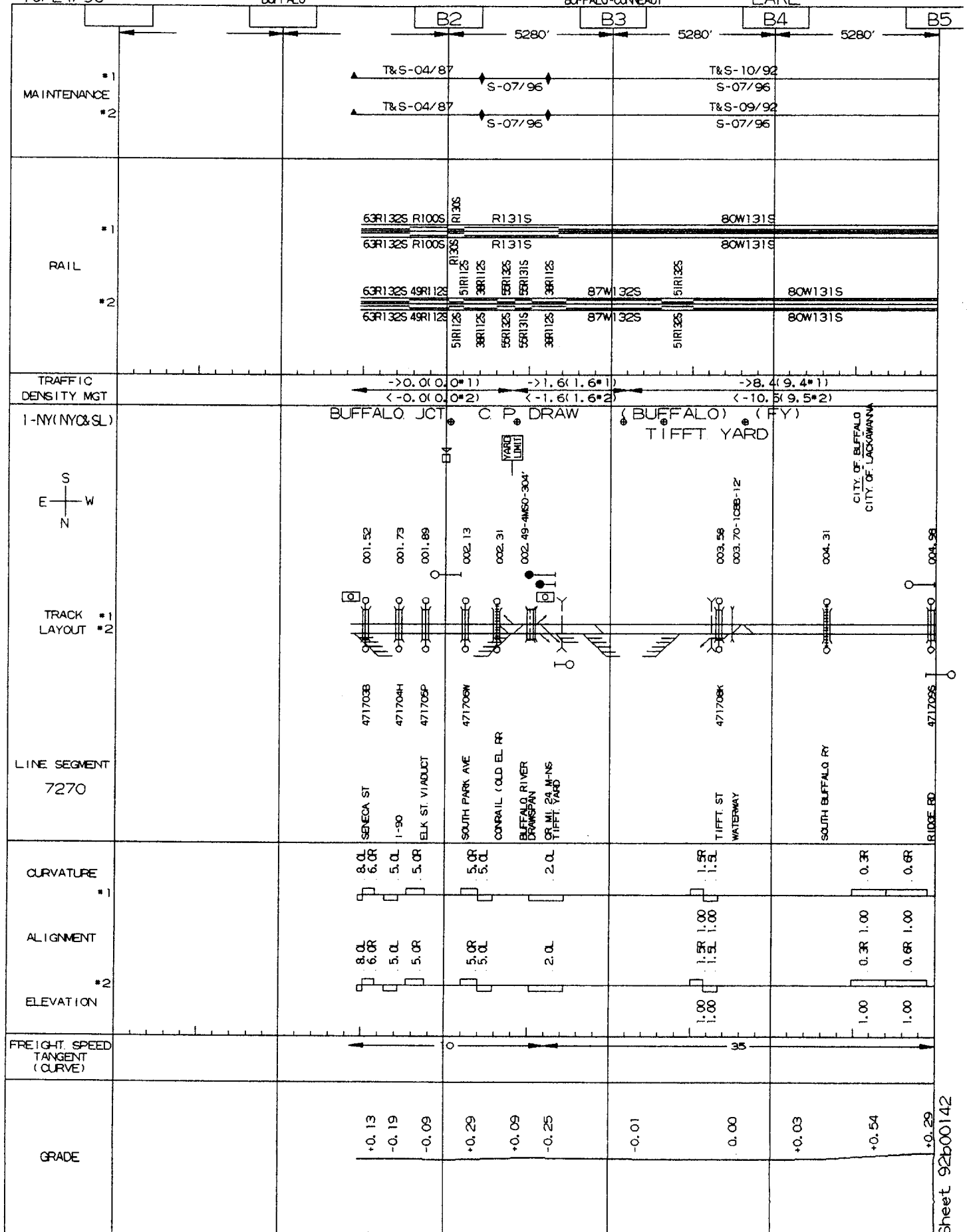
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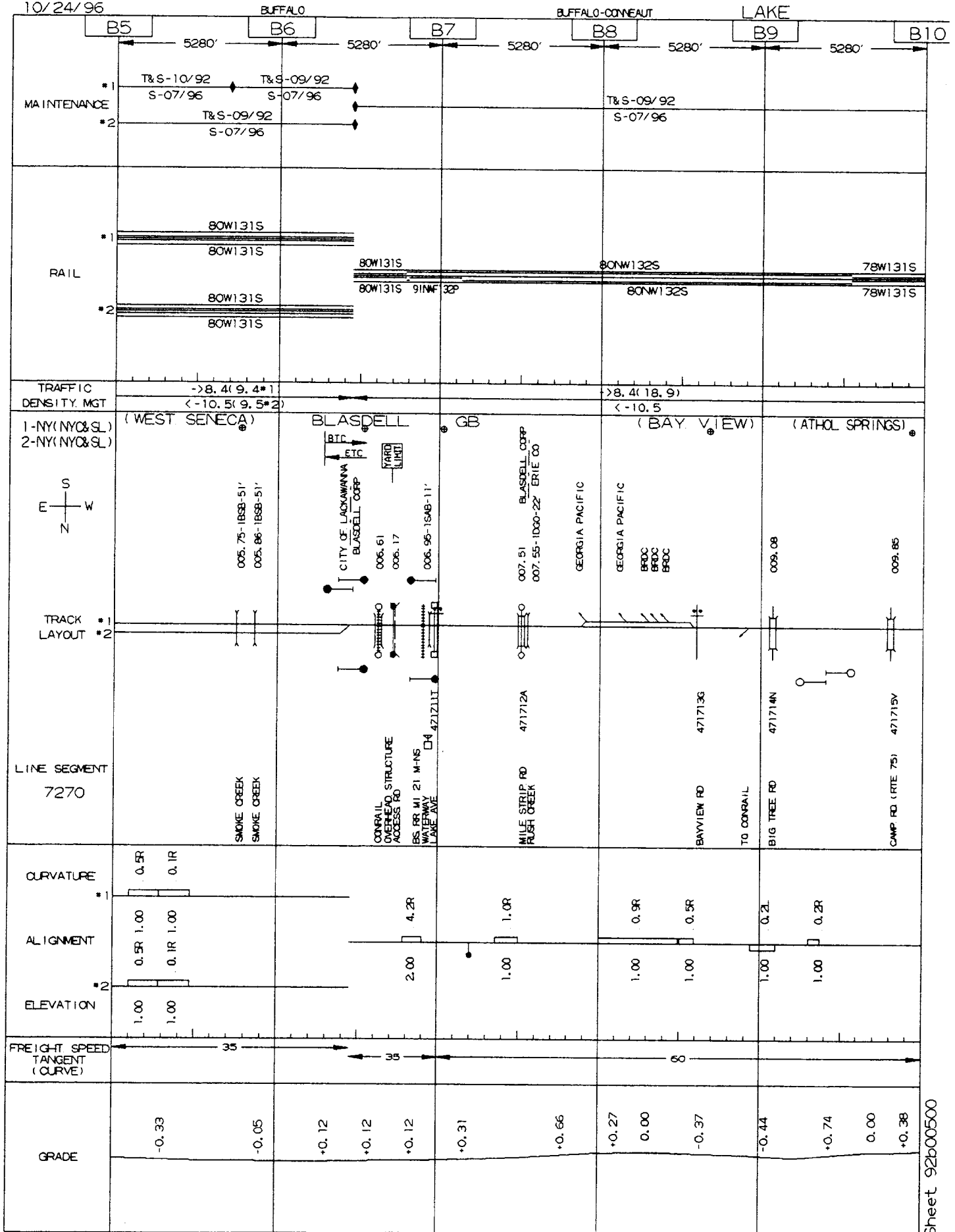
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BUFFALO-CORNEAULT

LAKE



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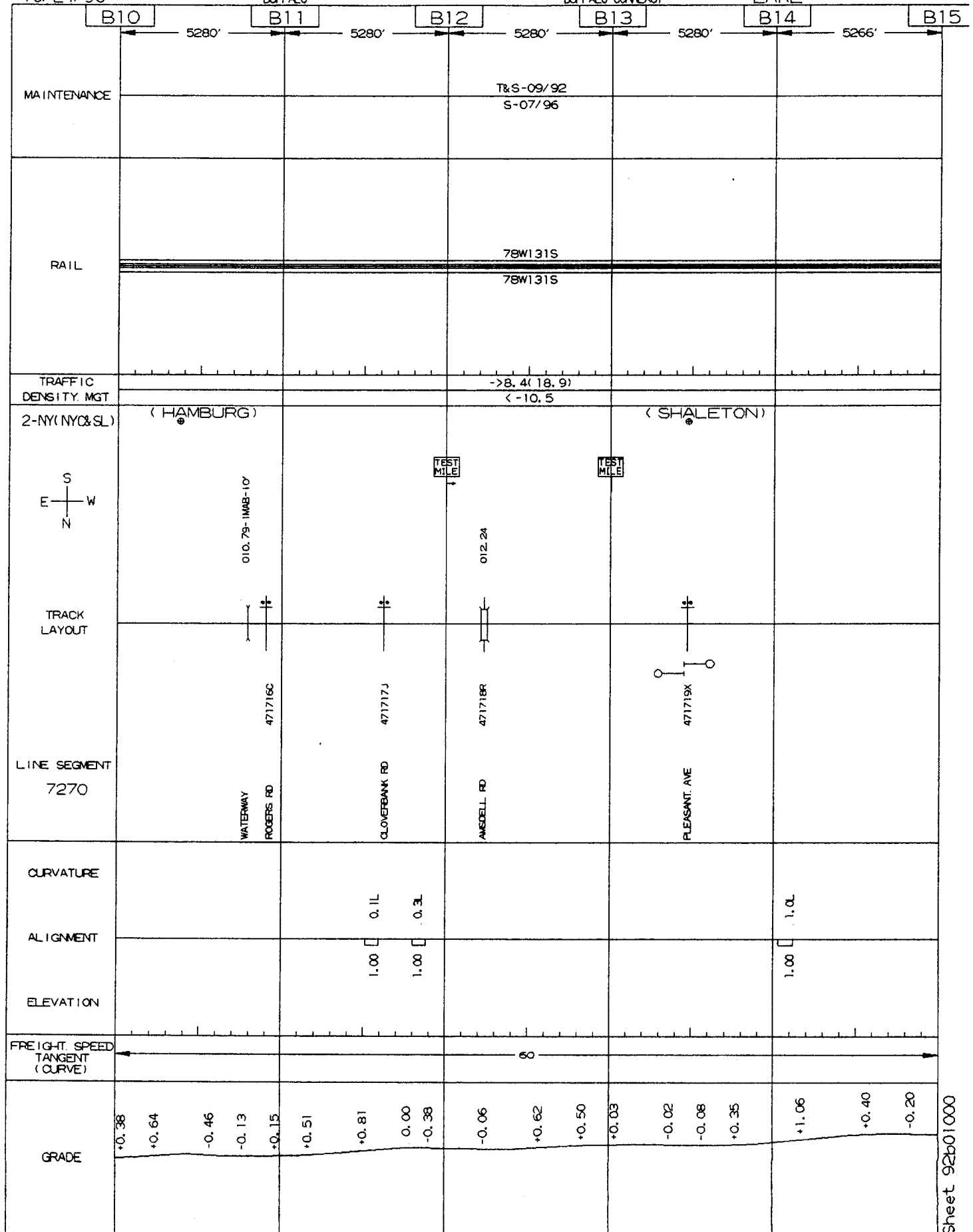


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BUFFALO-CONNEAUT

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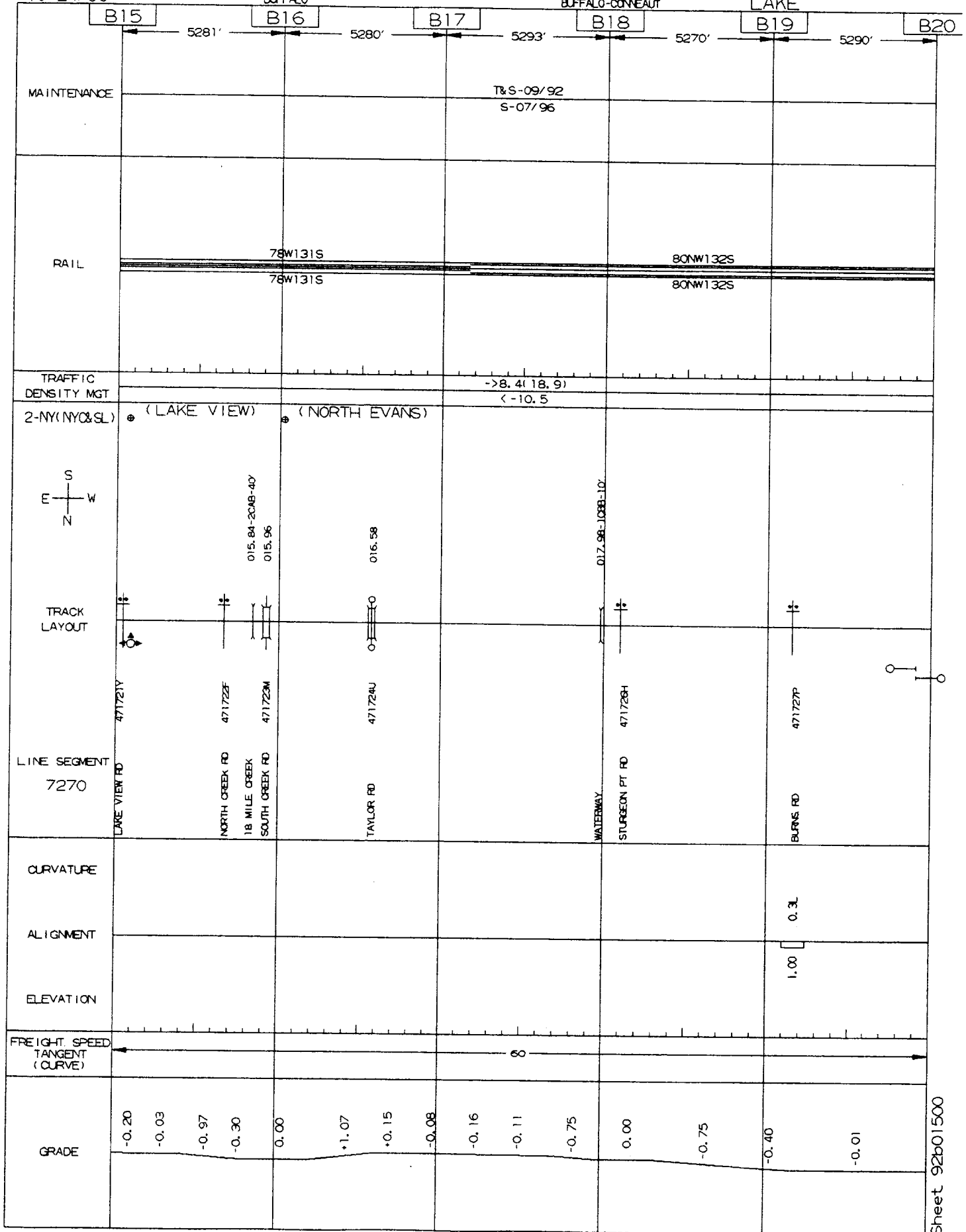


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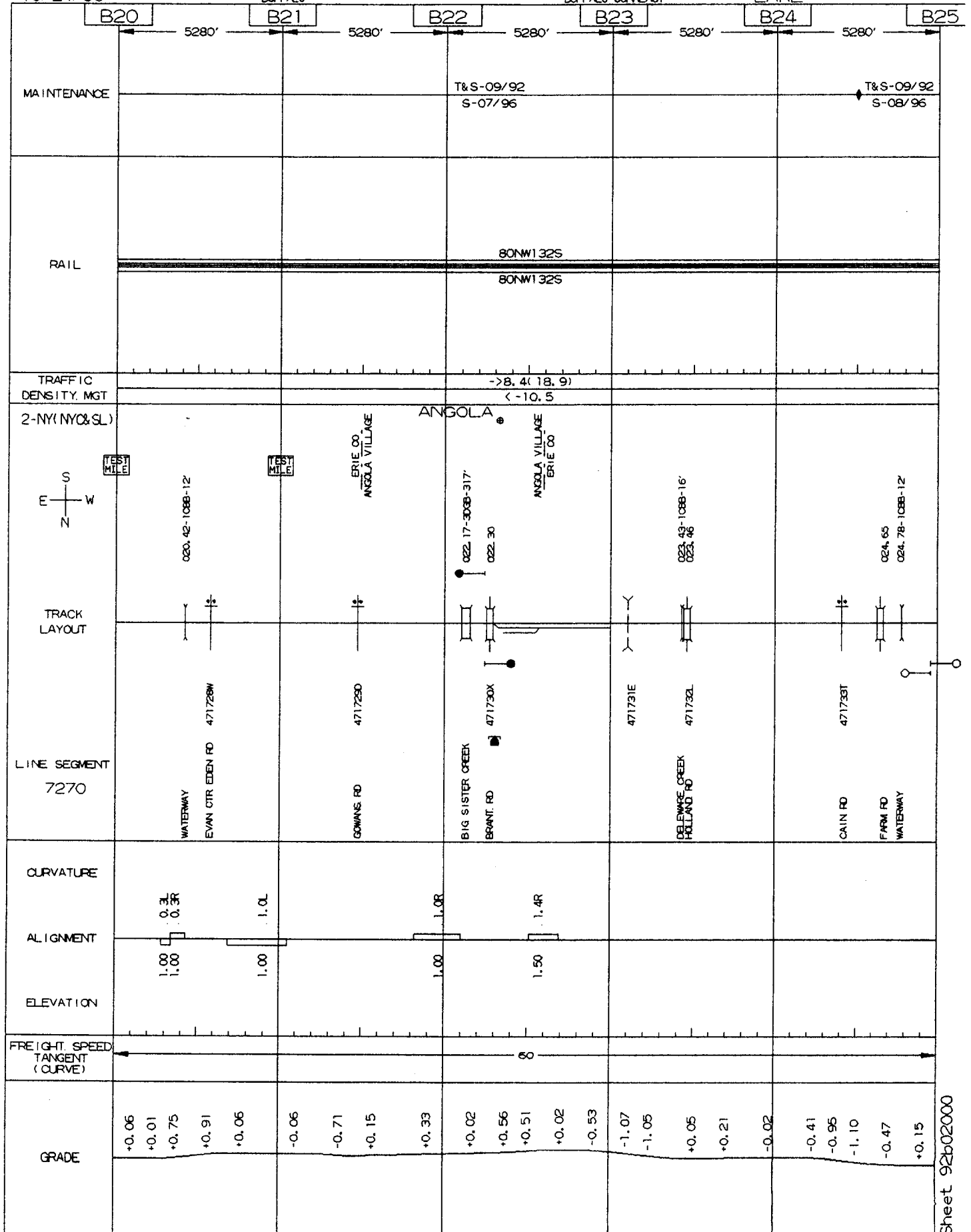


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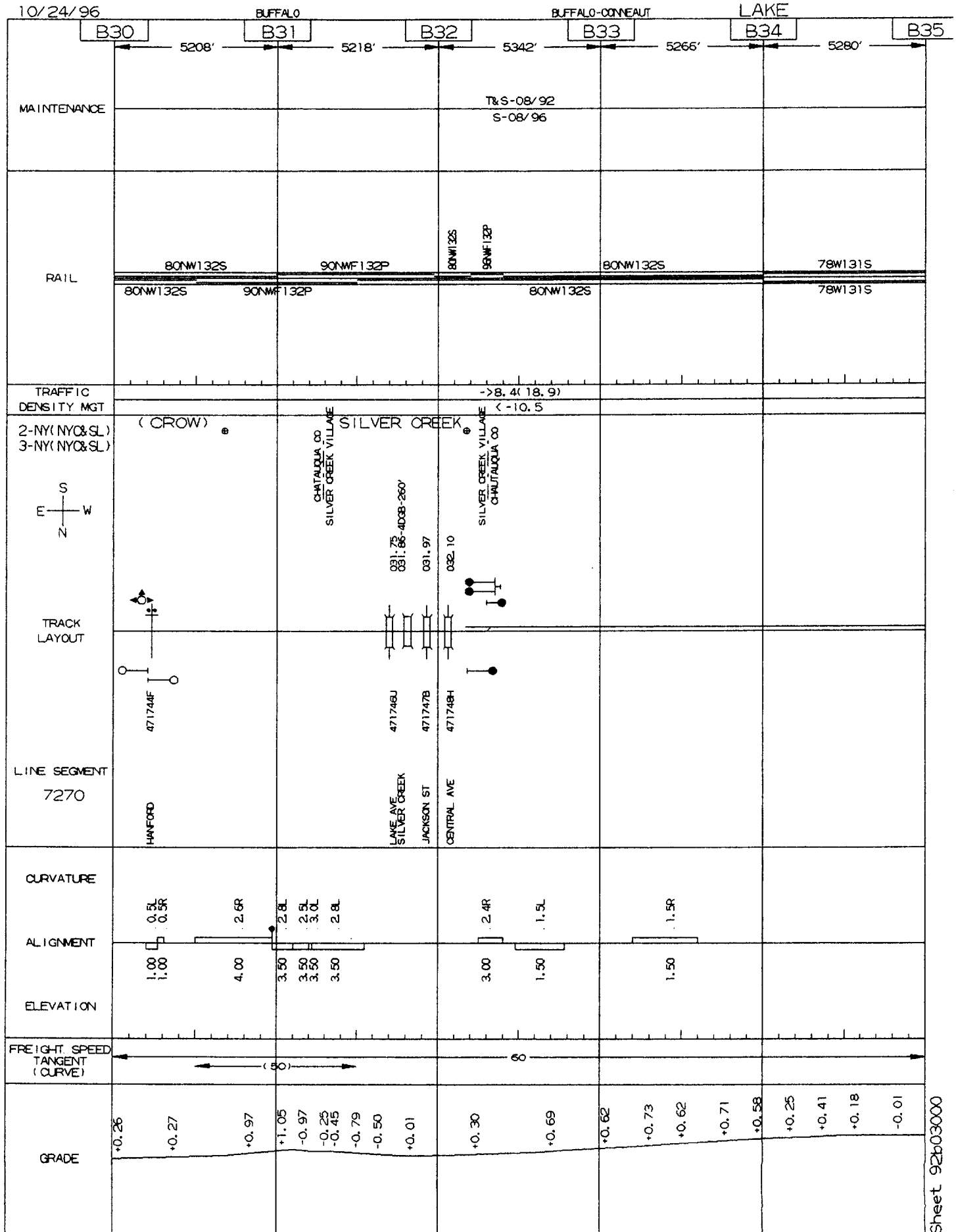
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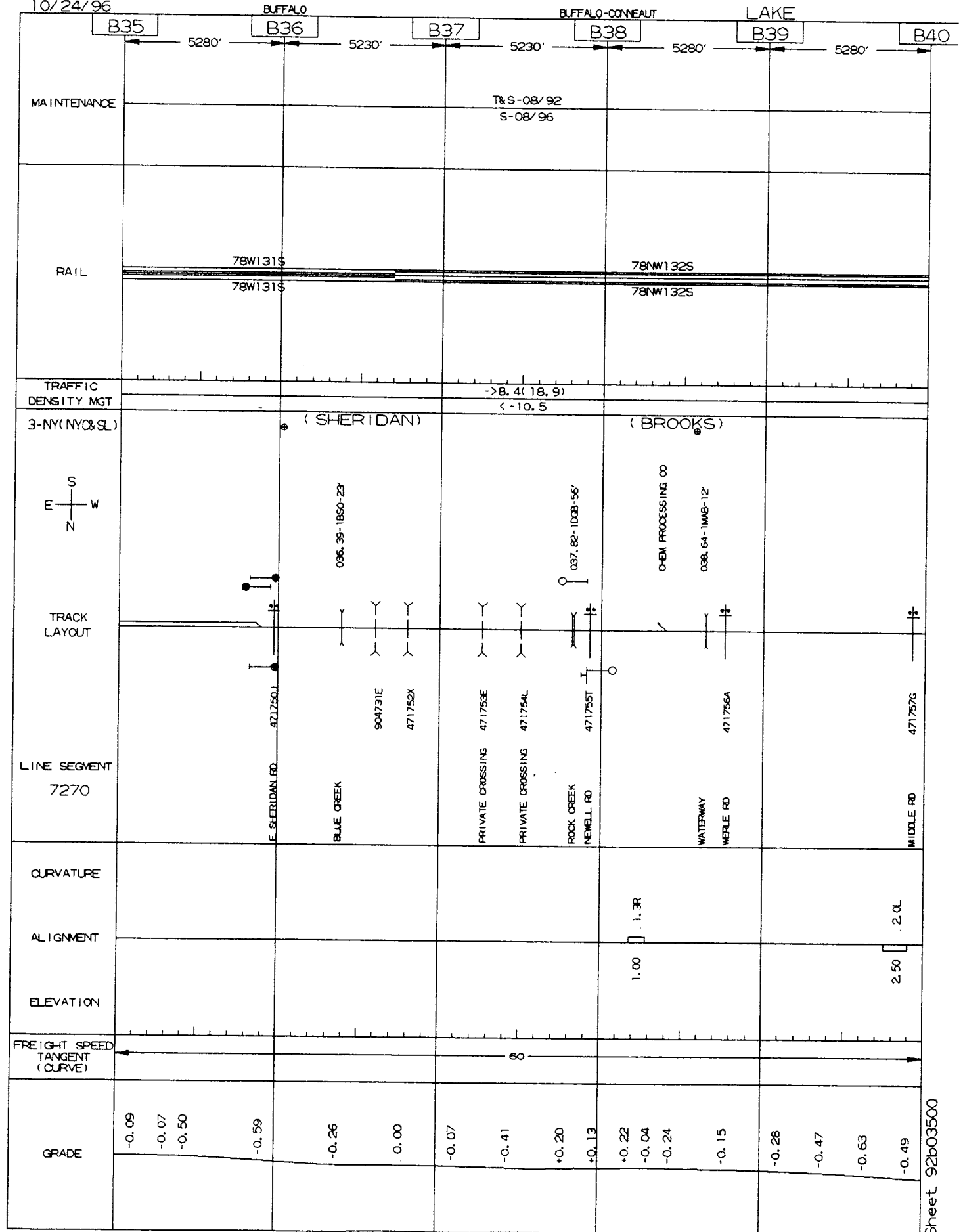
LAKE

	B25	B26	B27	B28	B29	B30
	5376'	5296'	5275'	5275'	5264'	
MAINTENANCE		T&S-09/92 S-08/96		T&S-08/92 S-08/96		
RAIL		80NW132S		80N132S 90NW132S	80NW132S	
TRAFFIC DENSITY MGT			-> 8, 4(18, 9) < -10, 5			
2-NY(NY&SL)		ERIE CO FARNHAM VILLAGE	FARNHAM VILLAGE ERIE CO	(IRVING) ERIE CO CATALPA CREEK		
TRACK LAYOUT	876694B	876695H 471734A 471735G	471736N 876695D 876697W 471737V 471738C 471739J	471740D	471743Y	
LINE SEGMENT	7270	MUD CREEK	FARNHAM RD ERIE RD	SR 5	CATTARAUGUS CREEK	DEAD CREEK ALLEGHENY RD
CURVATURE	0.7L			1.0L	2.5R	1.0L
ALIGNMENT	1.00			1.00	3.50	1.00
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			80			
GRADE	-0.55 -0.72 -1.11 -0.96 -0.70 -0.16	+1.30 +0.60 +0.90 +0.97 -0.30 -0.37 -0.70	-0.67 -0.56 -0.62 -0.54 -0.38 -0.32 -0.25 0.00 -0.22 -0.30 -0.17	+0.01 +0.19 +0.26		

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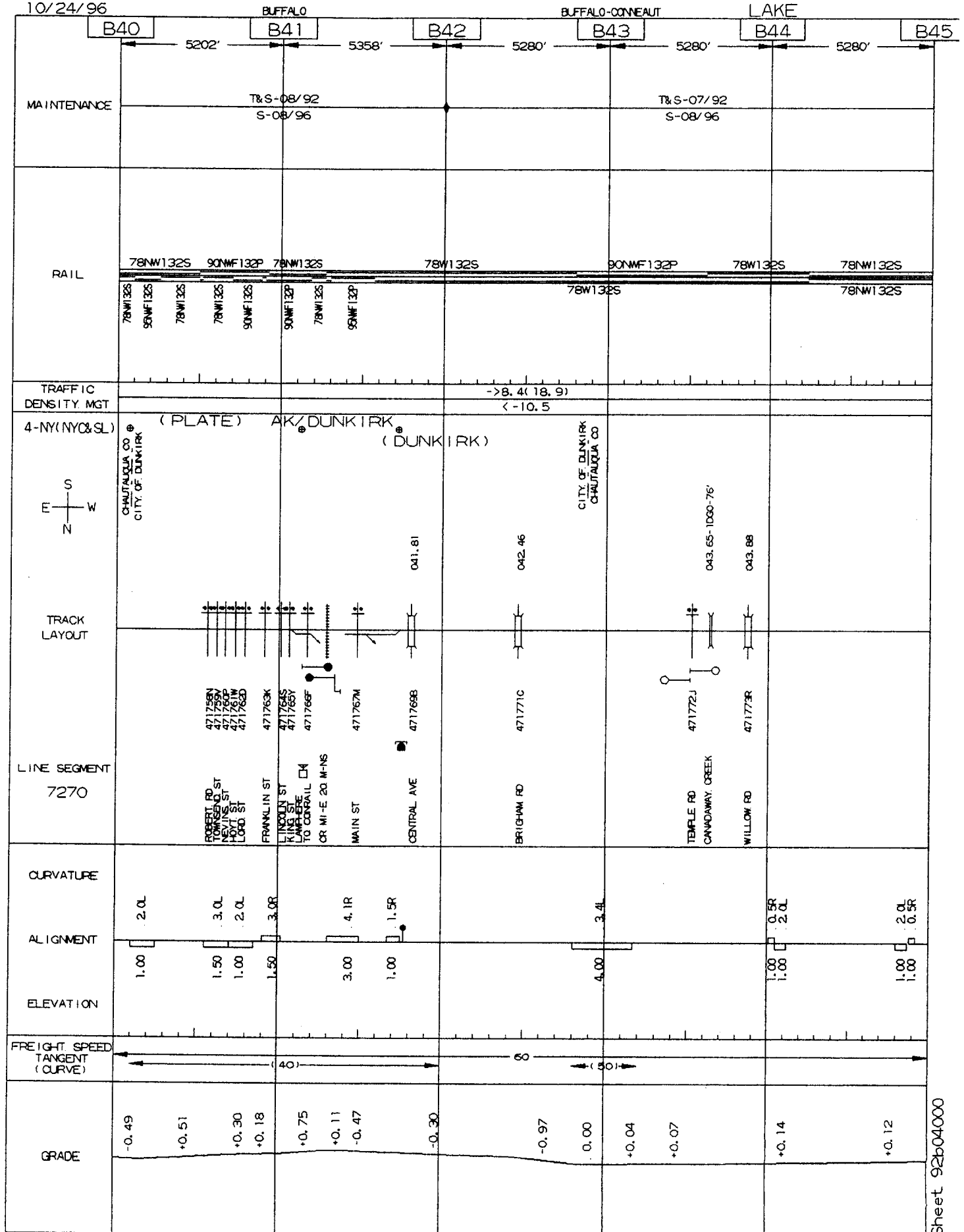


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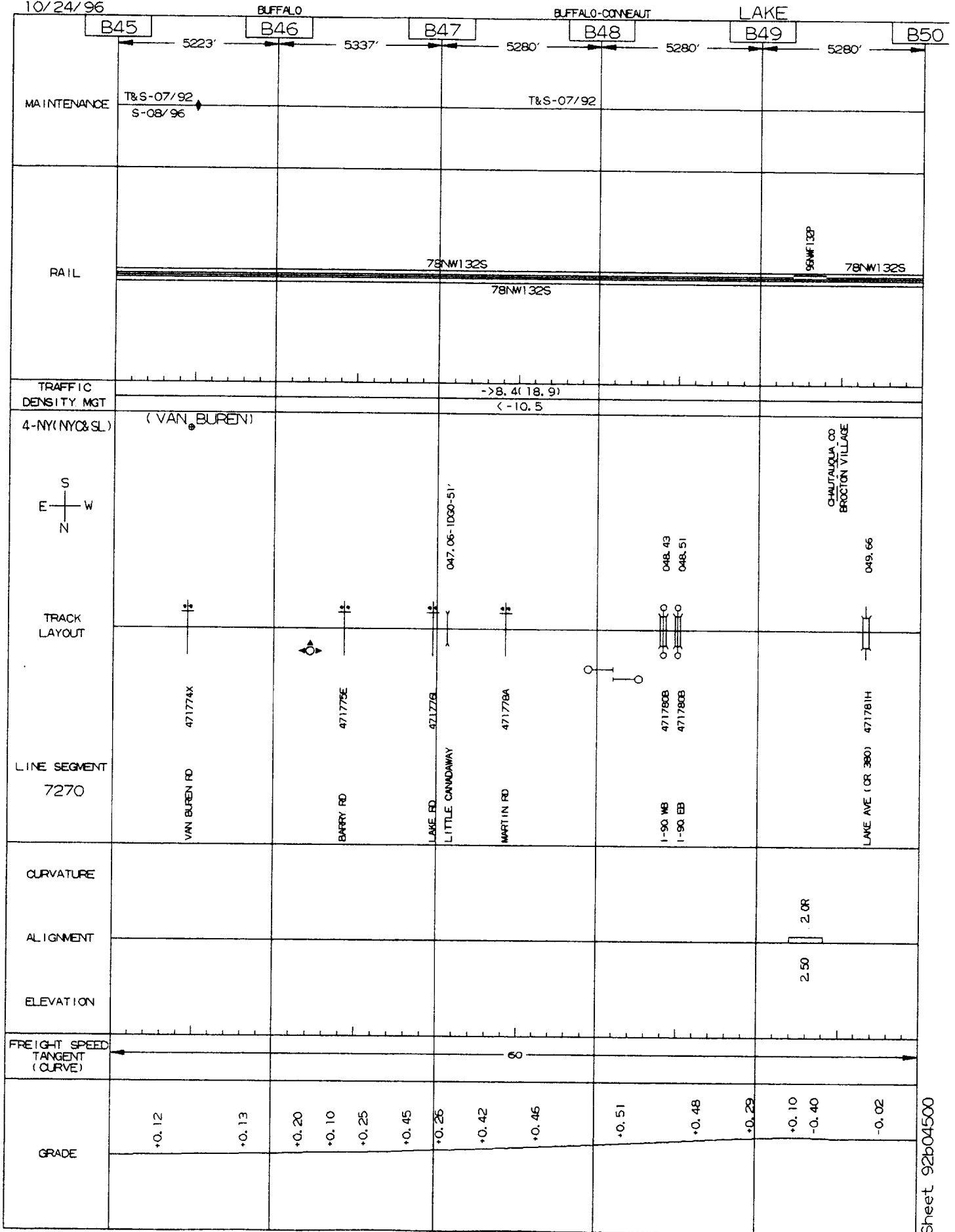




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BUFFALO

BUFFALO-CORNEAULT

LAKE

	B50	B51	B52	B53	B54	B55
MAINTENANCE	5280'	5280'	5266'	5326'	5248'	
RAIL	78NW132S 91NMF132P 78NW132S	78NW132S		79NW132S 79NW132S		
TRAFFIC DENSITY MGT			>8, 4(18, 9) <-10.5			
4-NY(NY&SL)	(BROCTON) BROCTON VILLAGE CHAUTAUQUA CO	(PORTLAND)	EE POMFRET		POMFRET	
TRACK LAYOUT	050.14-114B-10' 471782P	051.26-108B-12' 471784D	052.37-108B-12' 471786S	053.91-108B-12' 471788F	471791N	
LINE SEGMENT	7270					
CURVATURE						
ALIGNMENT	3.3R 3.3L 2.00 1.00 1.00					
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	(40)		60			
GRADE	-0.11 -0.10 +0.10 -0.04	+0.17 +0.62 +0.35	+0.29 +0.01	-0.29 -0.08 +0.02 +0.26 +0.08	-0.20	

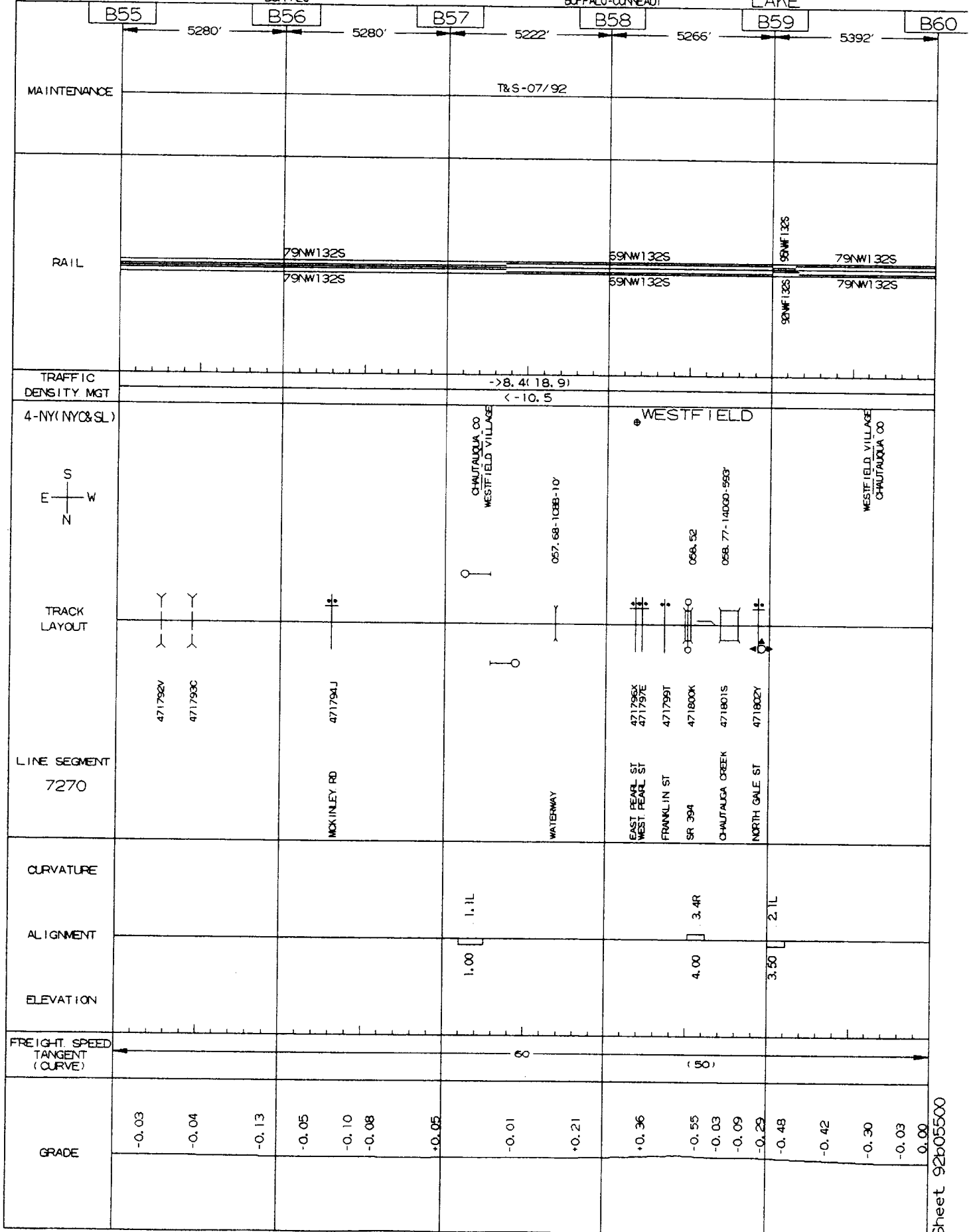


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BUFFALO-CONNEAUT

LAKE

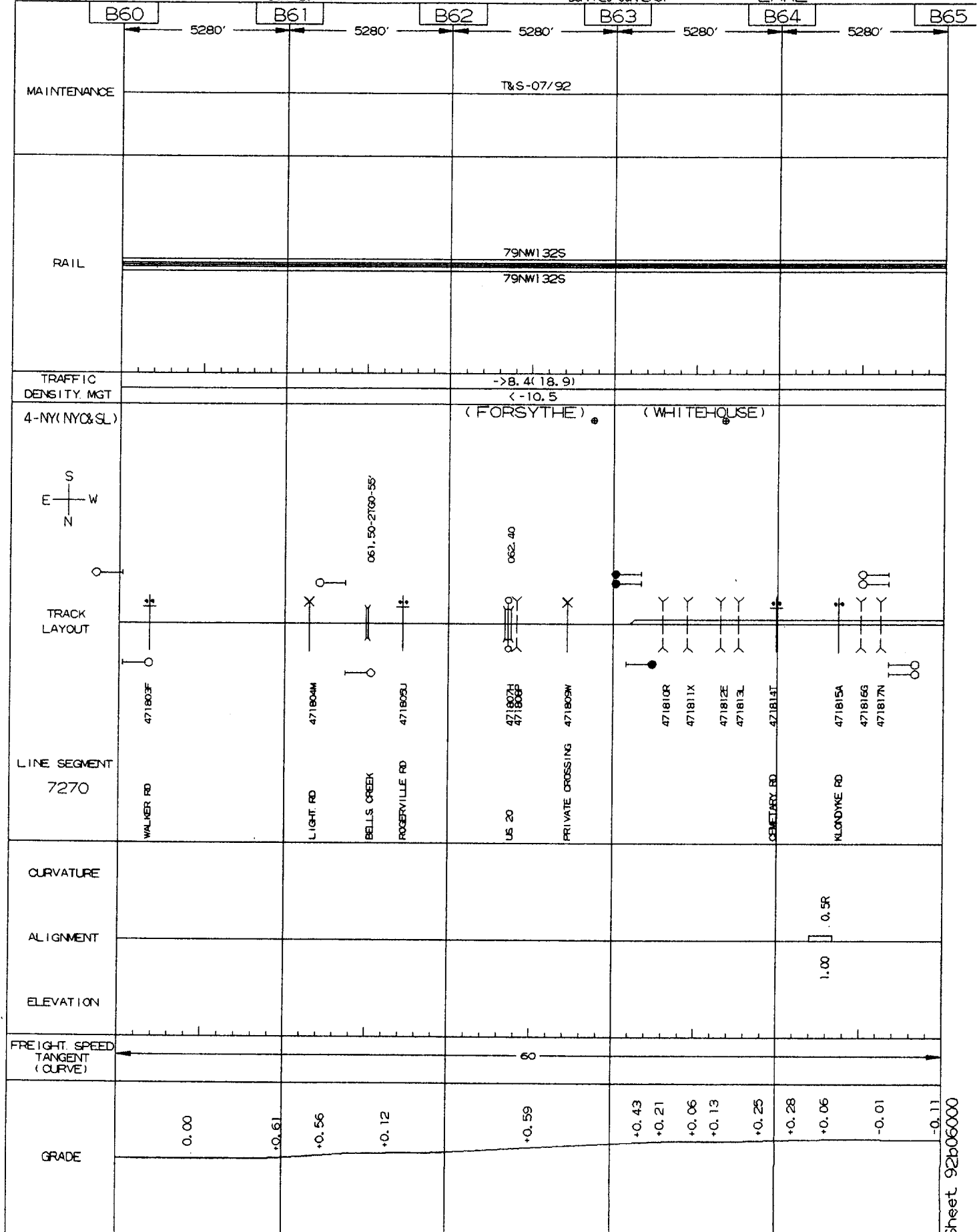


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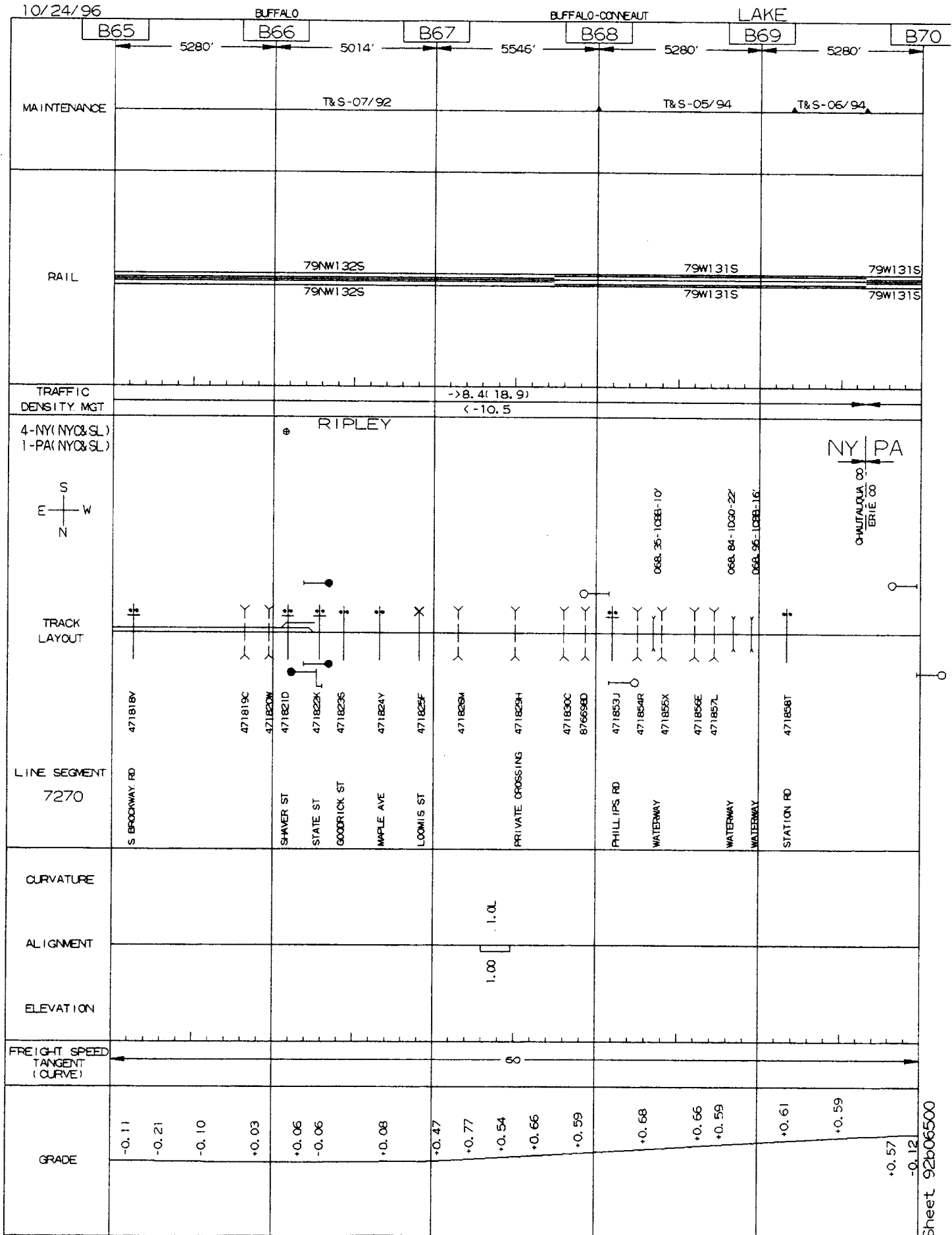
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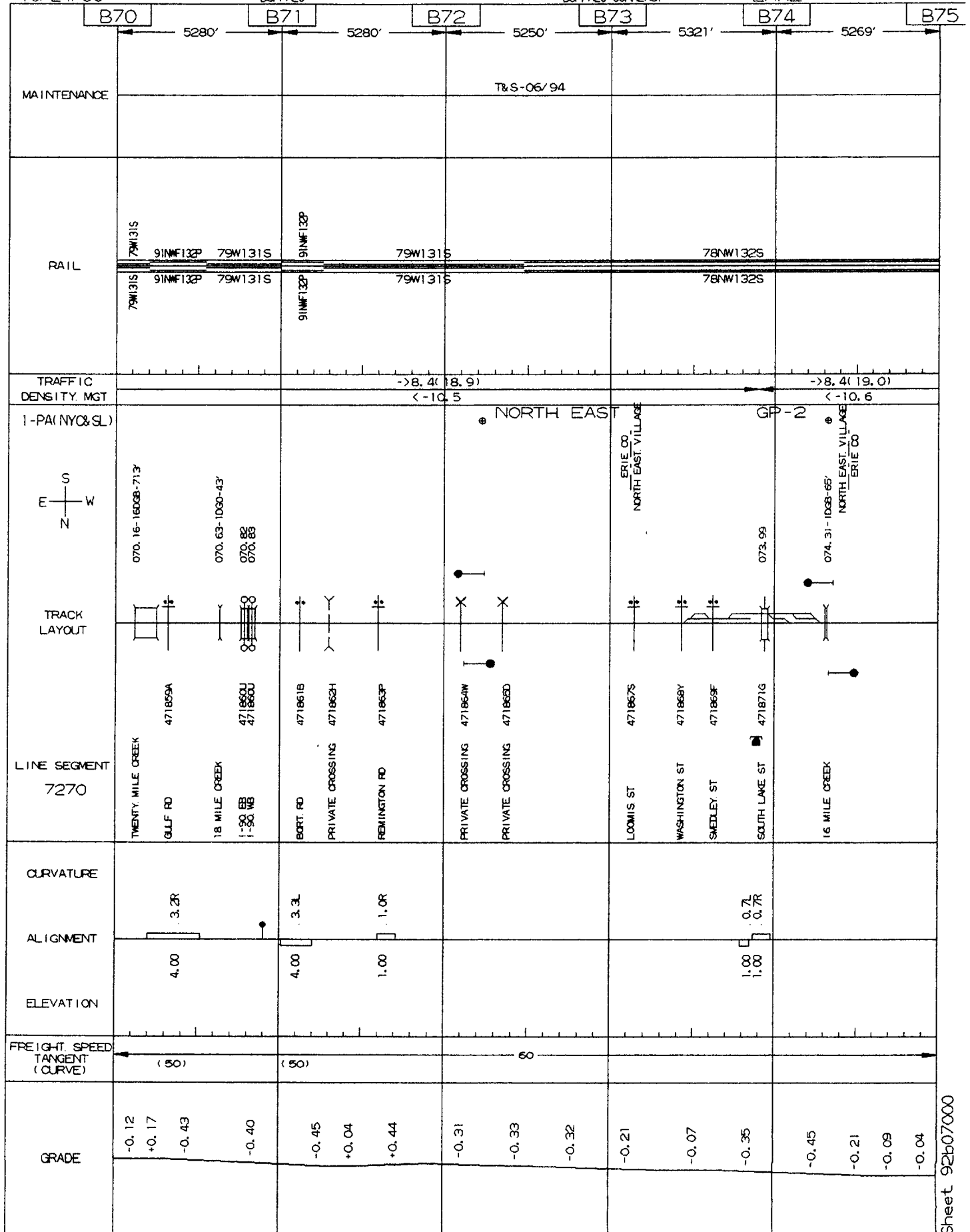


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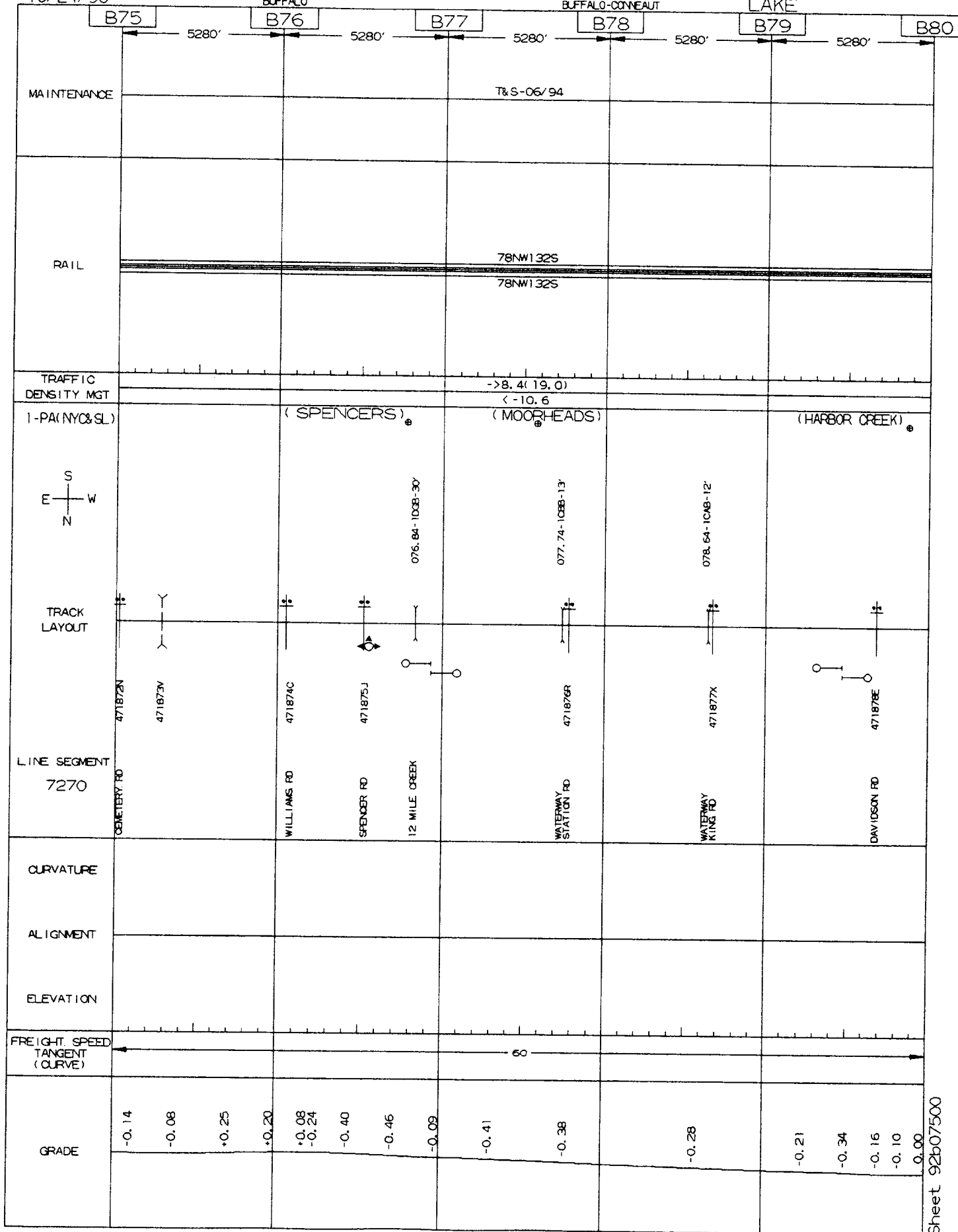


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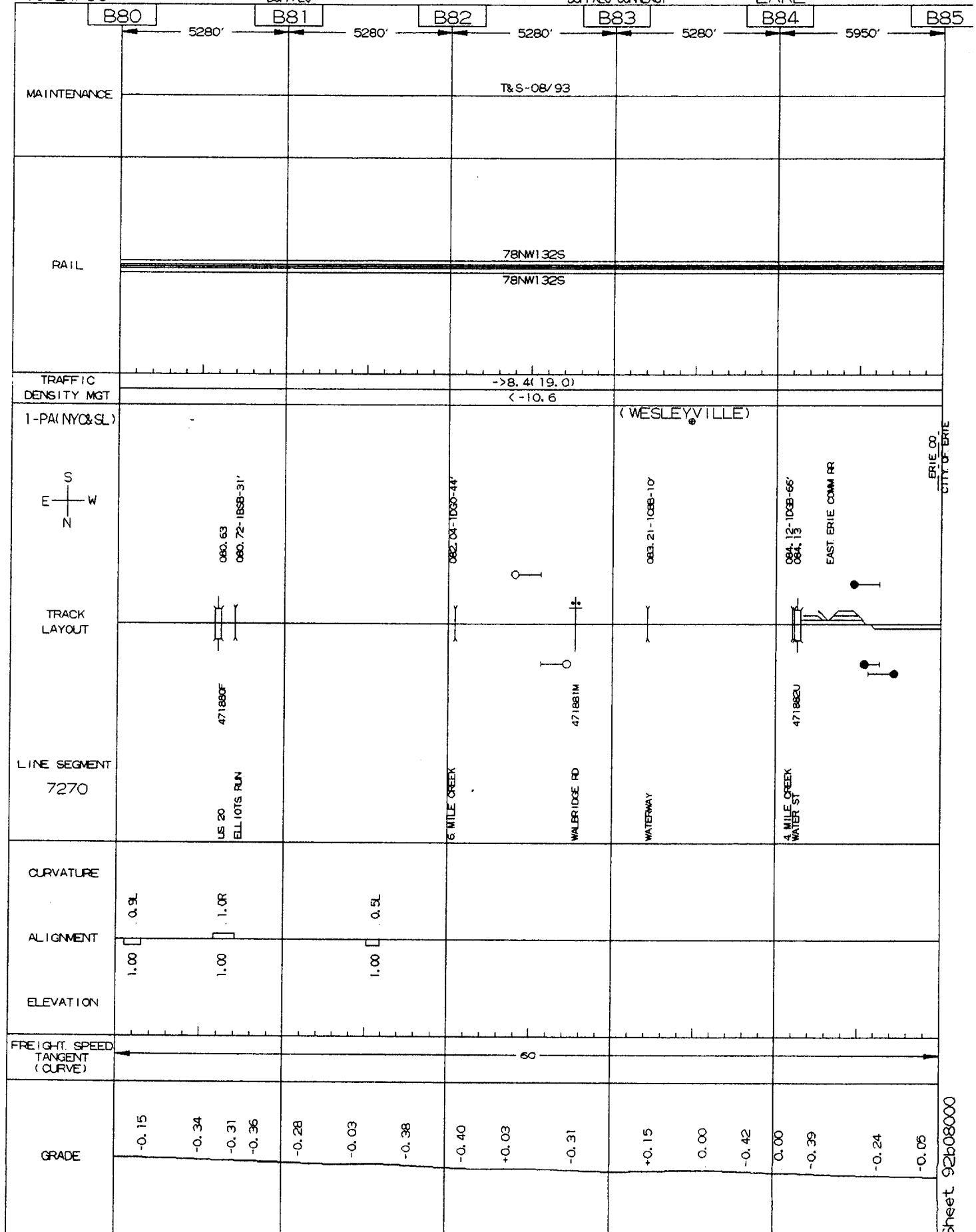


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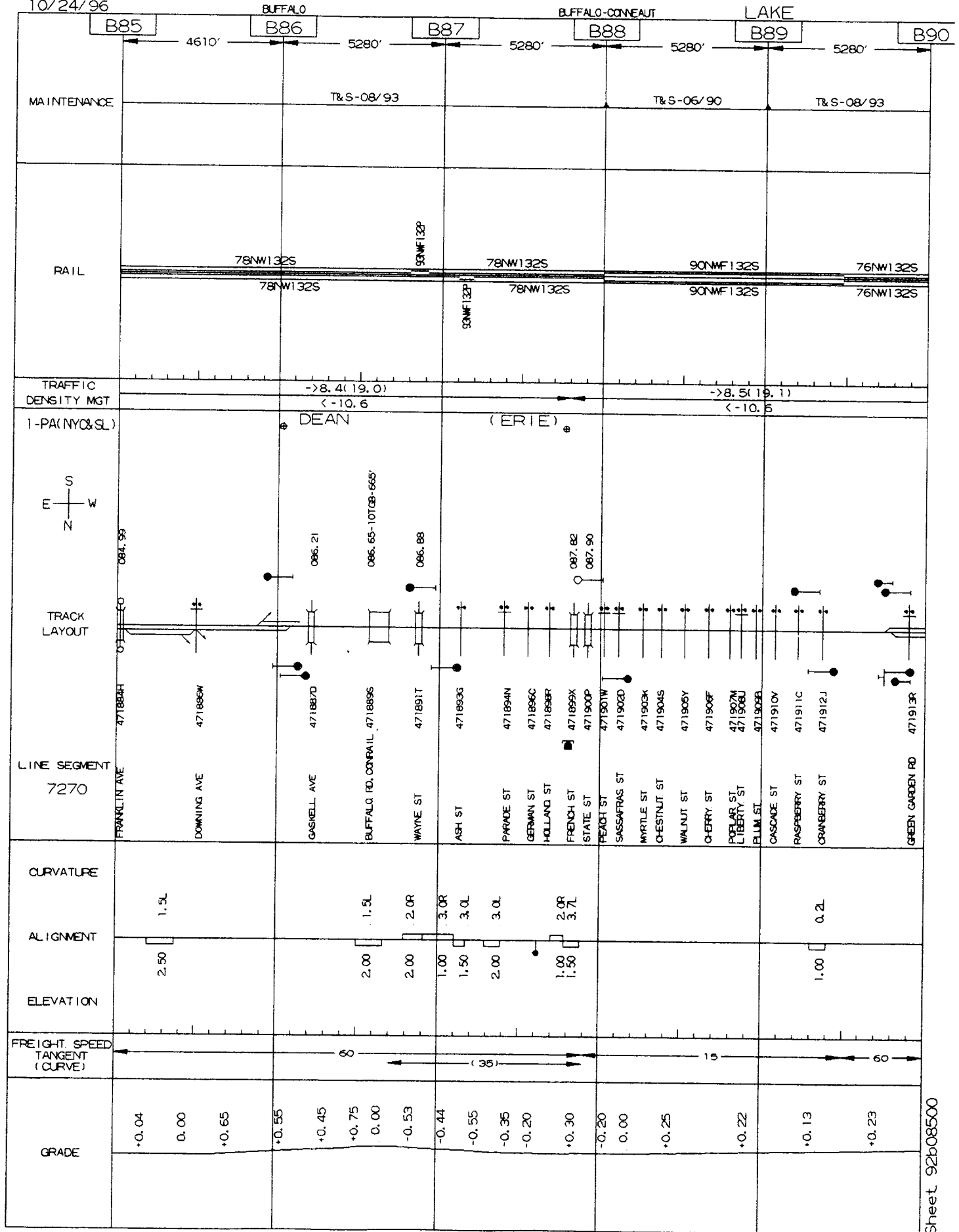
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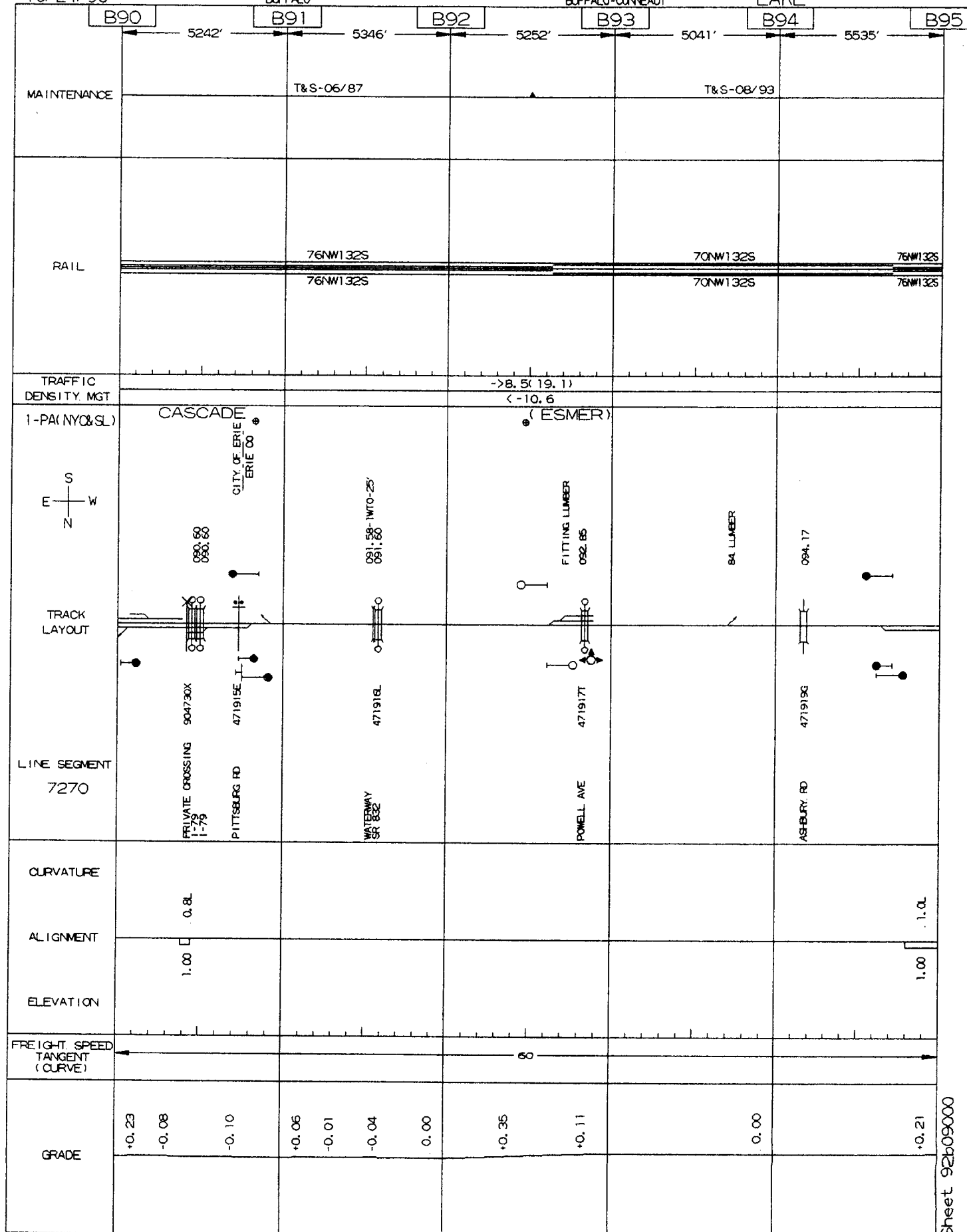


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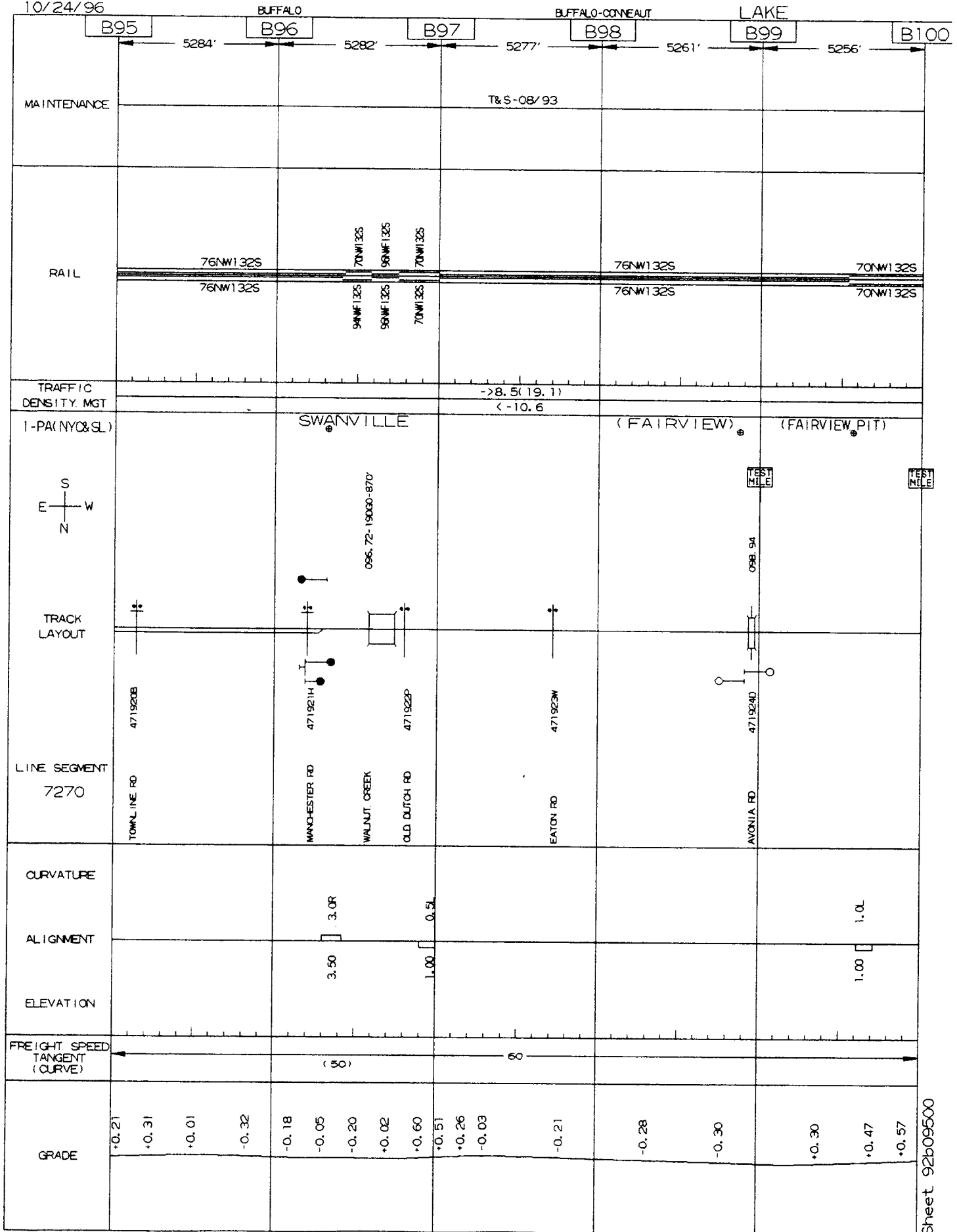
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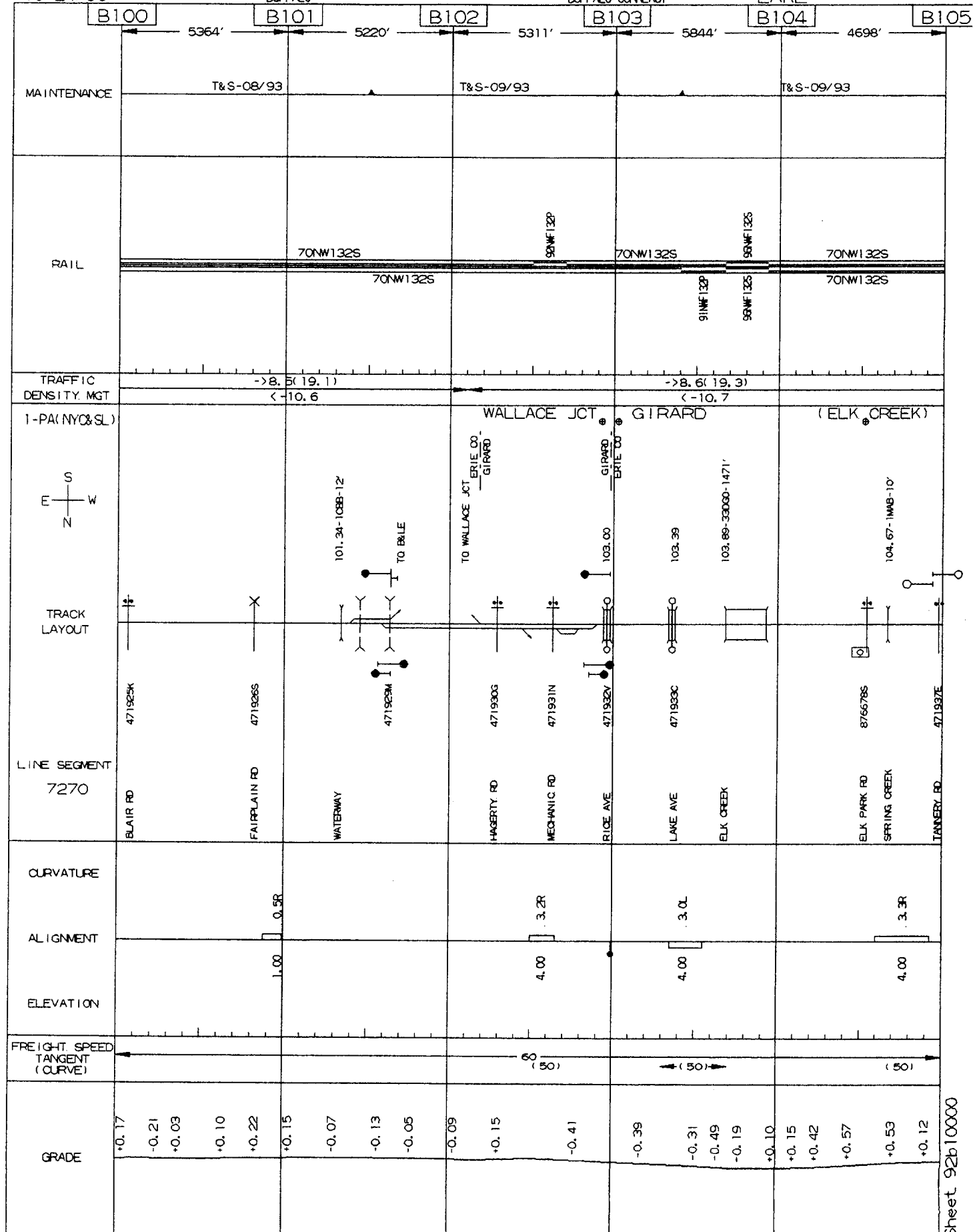


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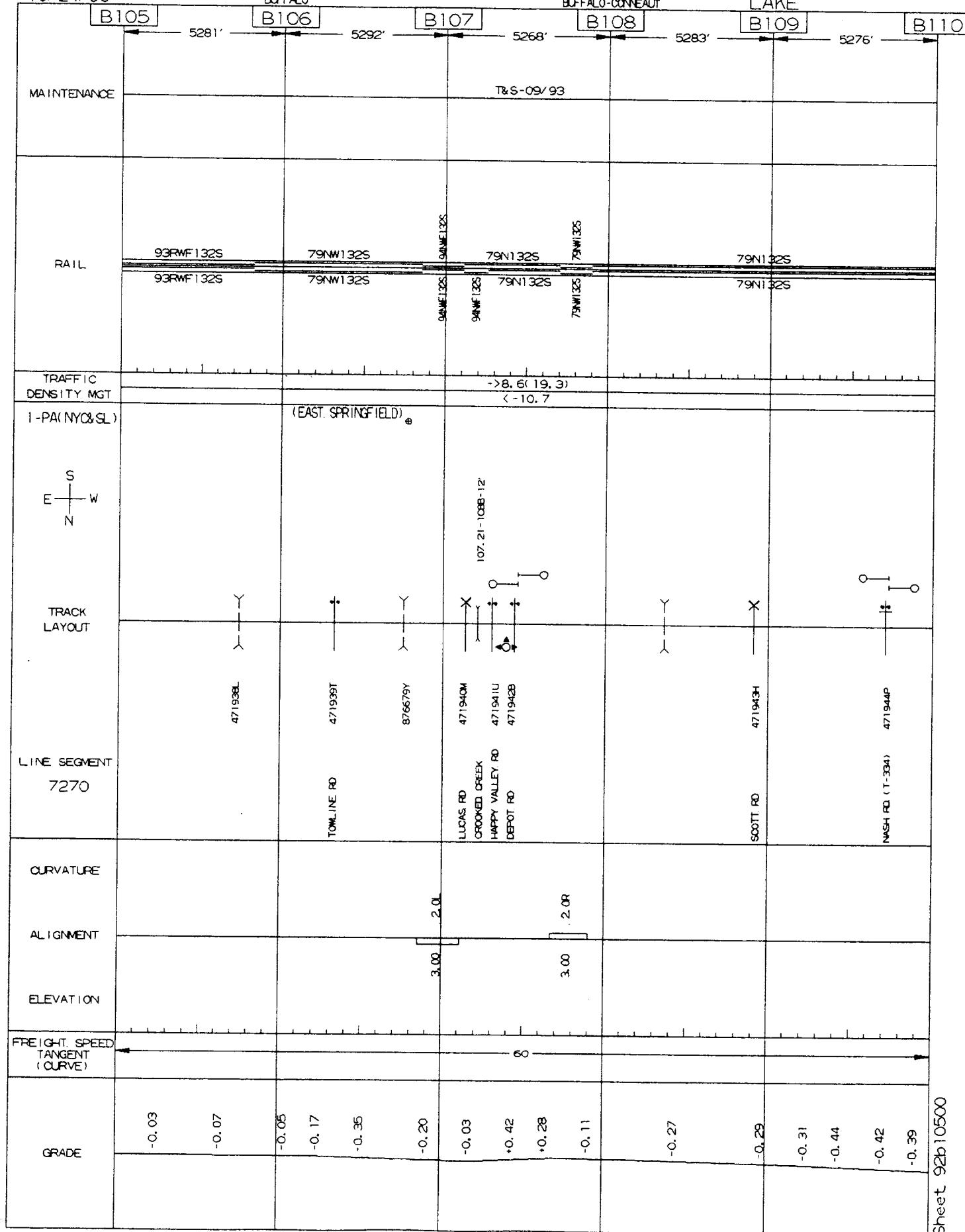


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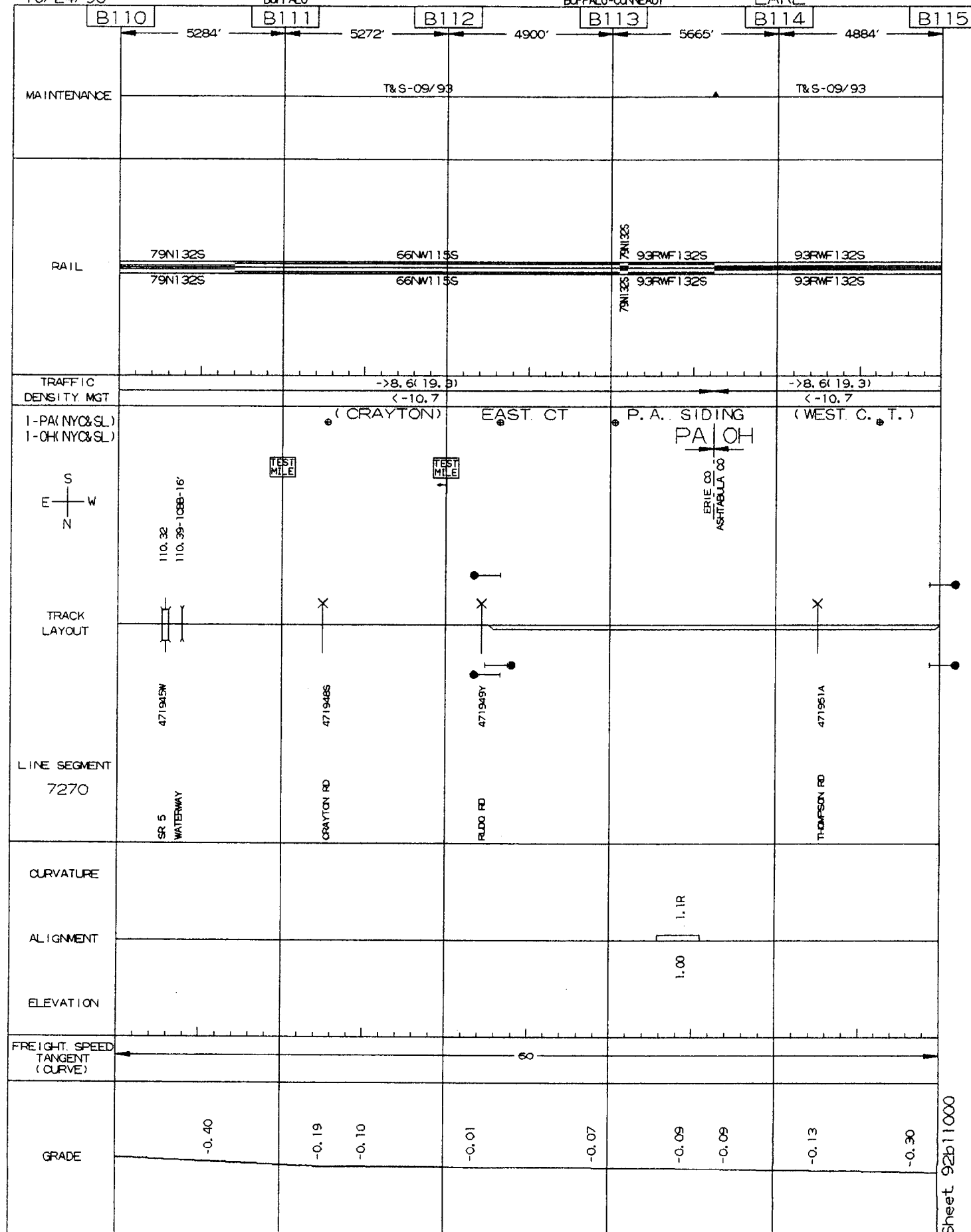


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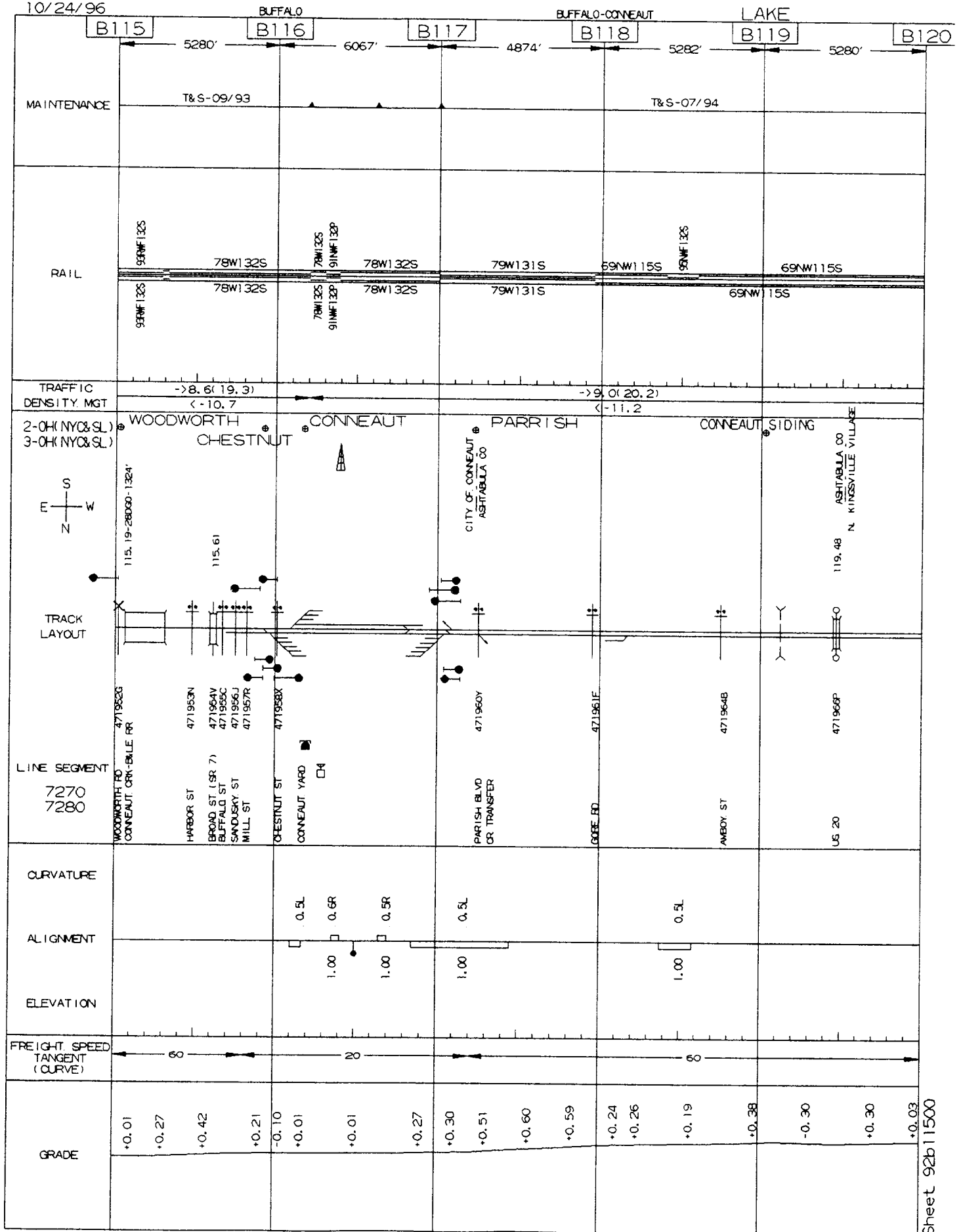
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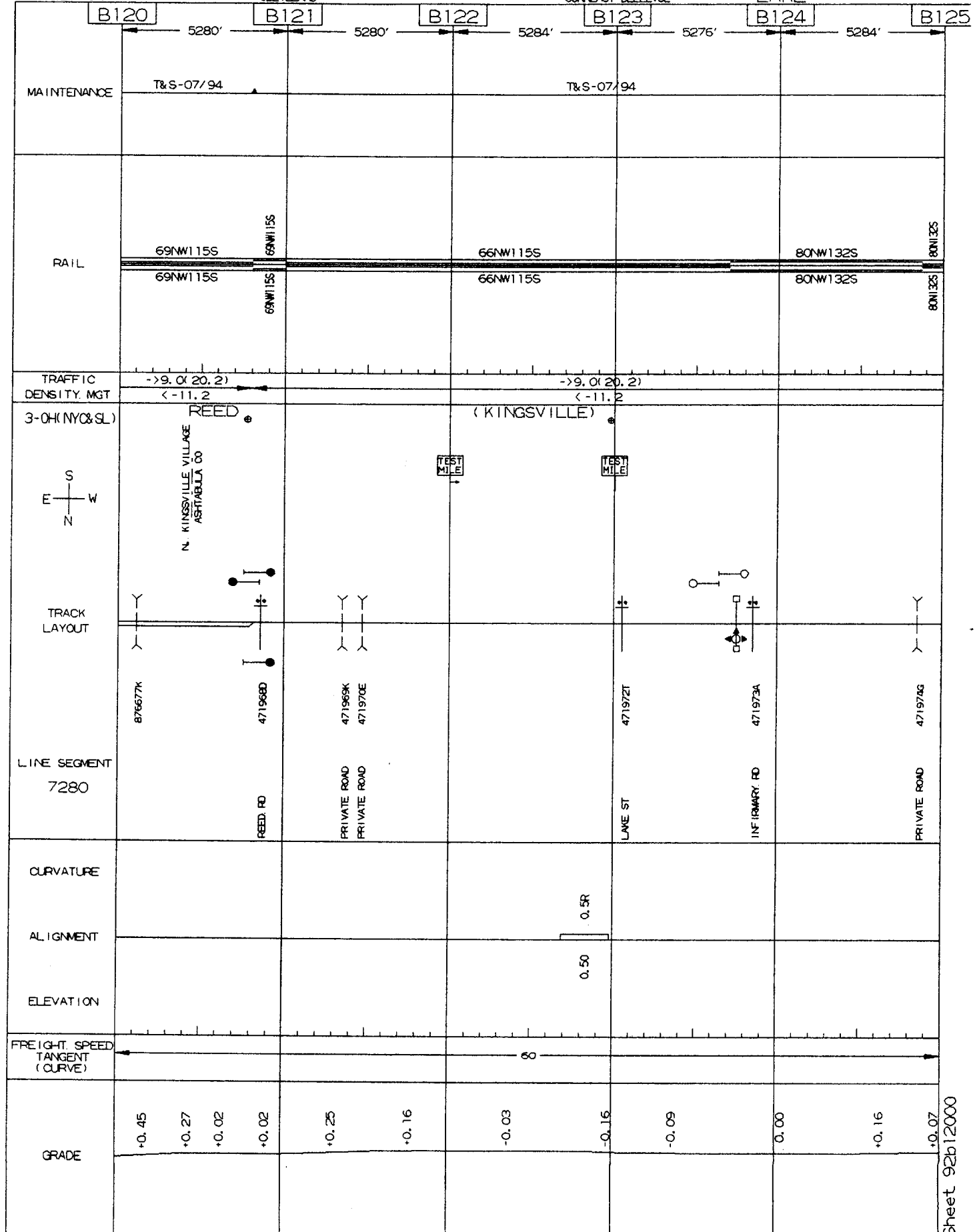


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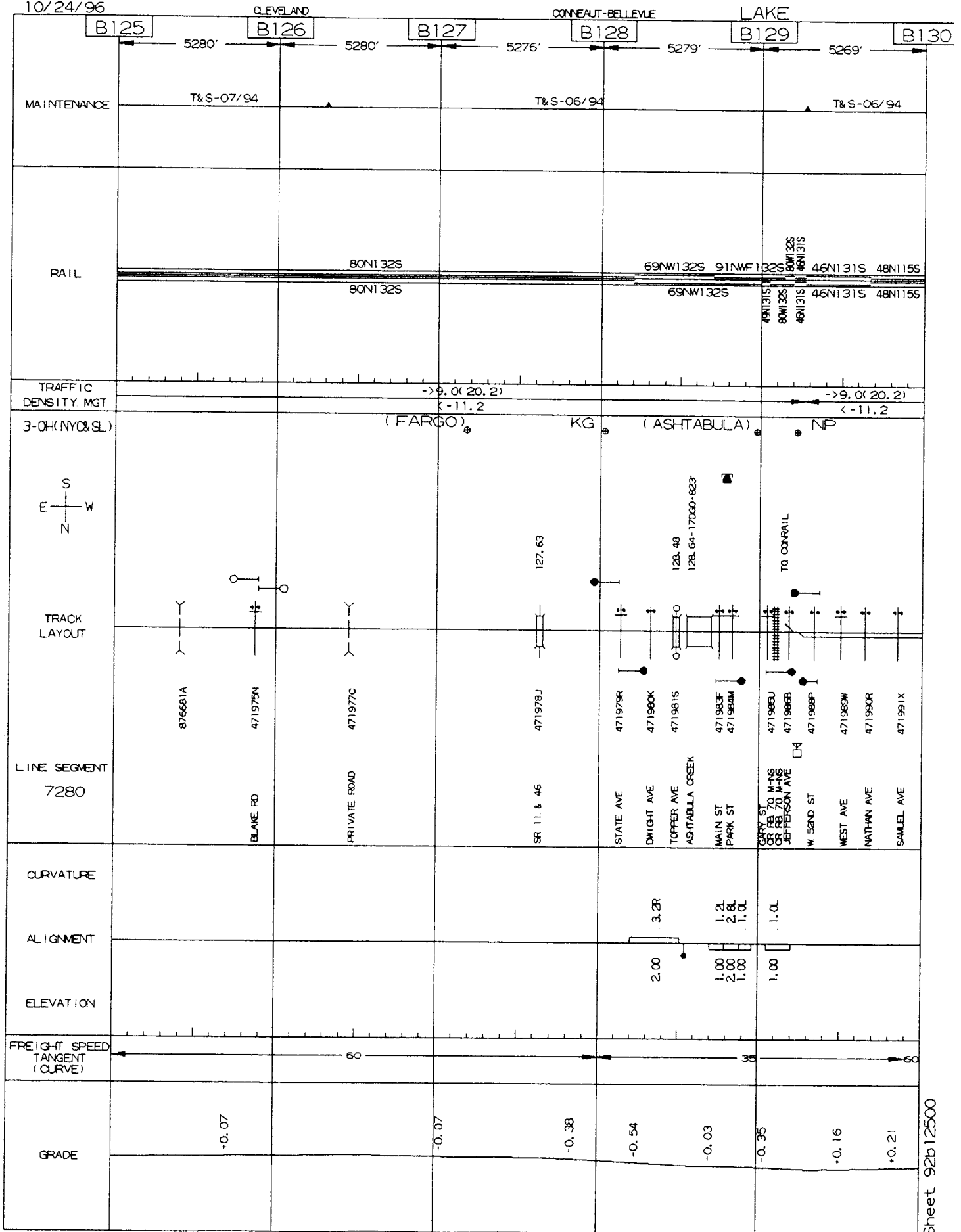
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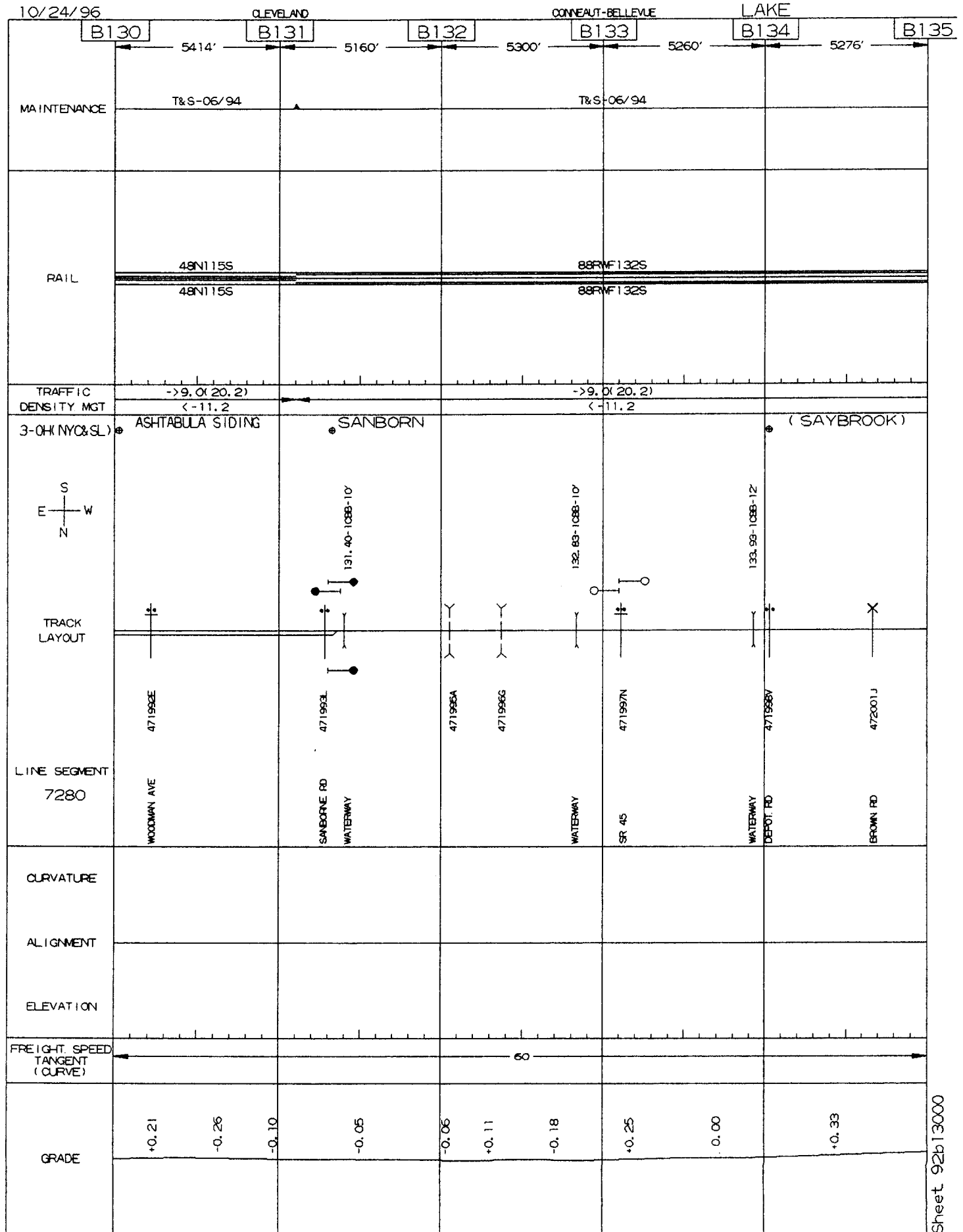
CONNEAUT-BELLEVUE

LAKE



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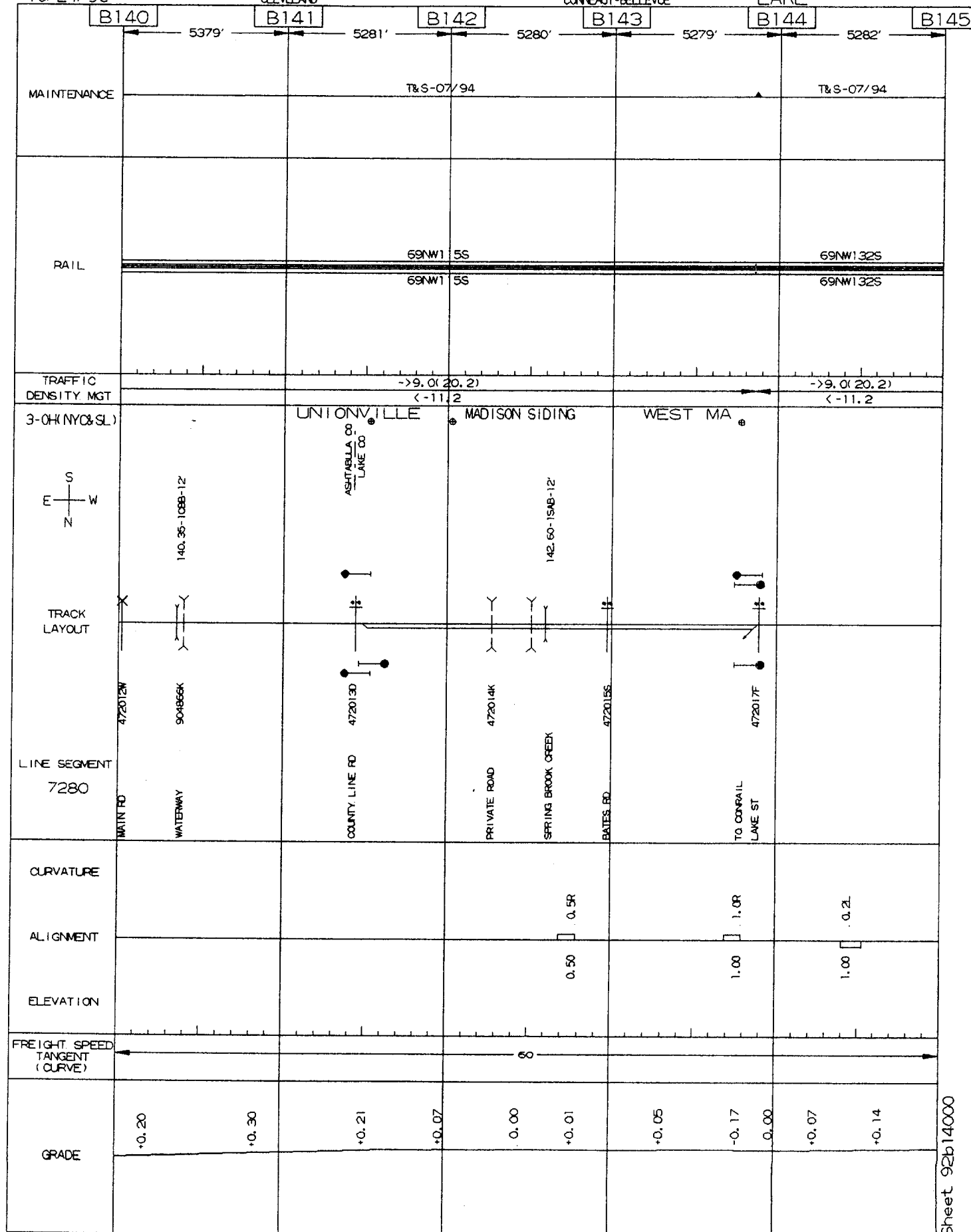
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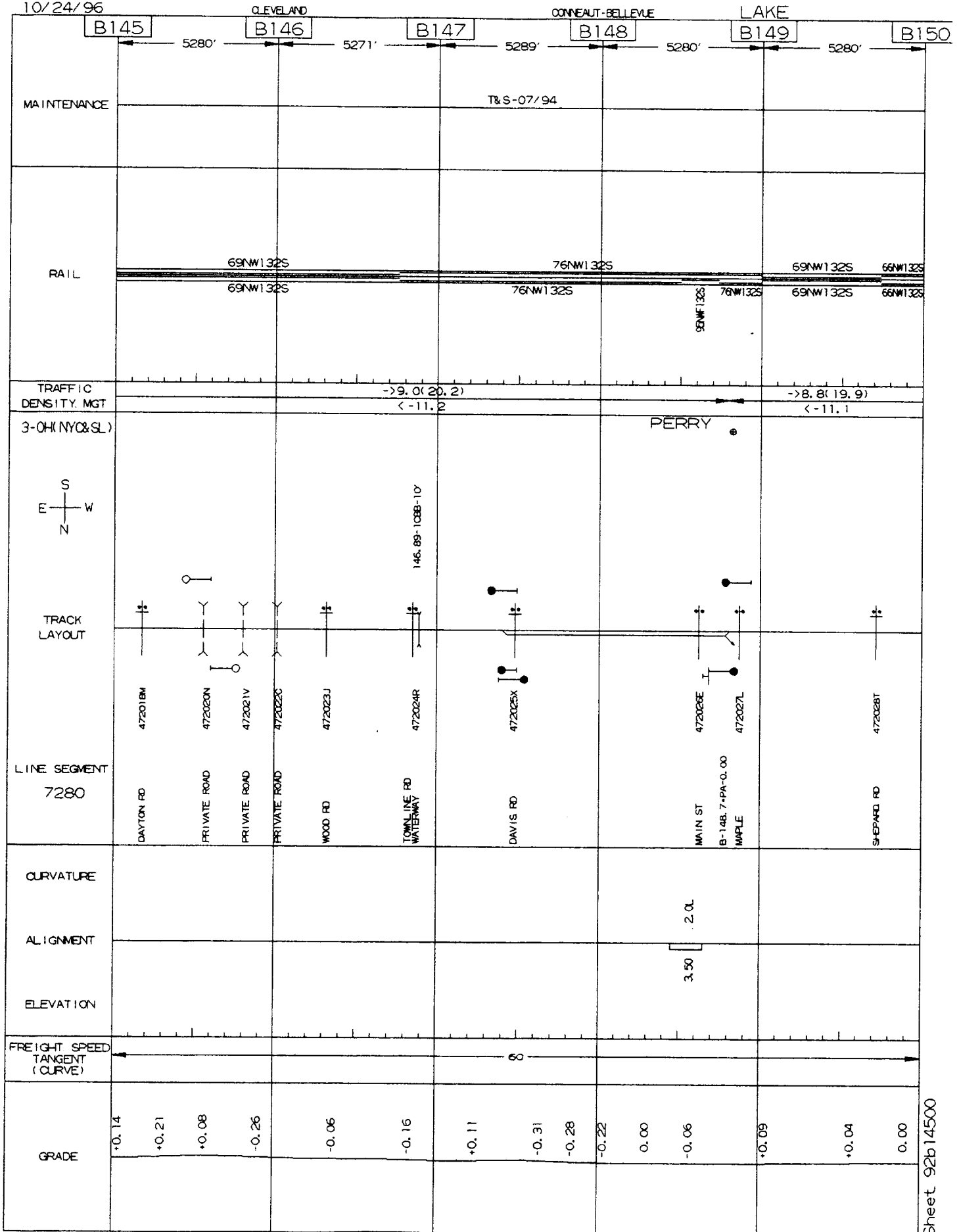
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LAKE



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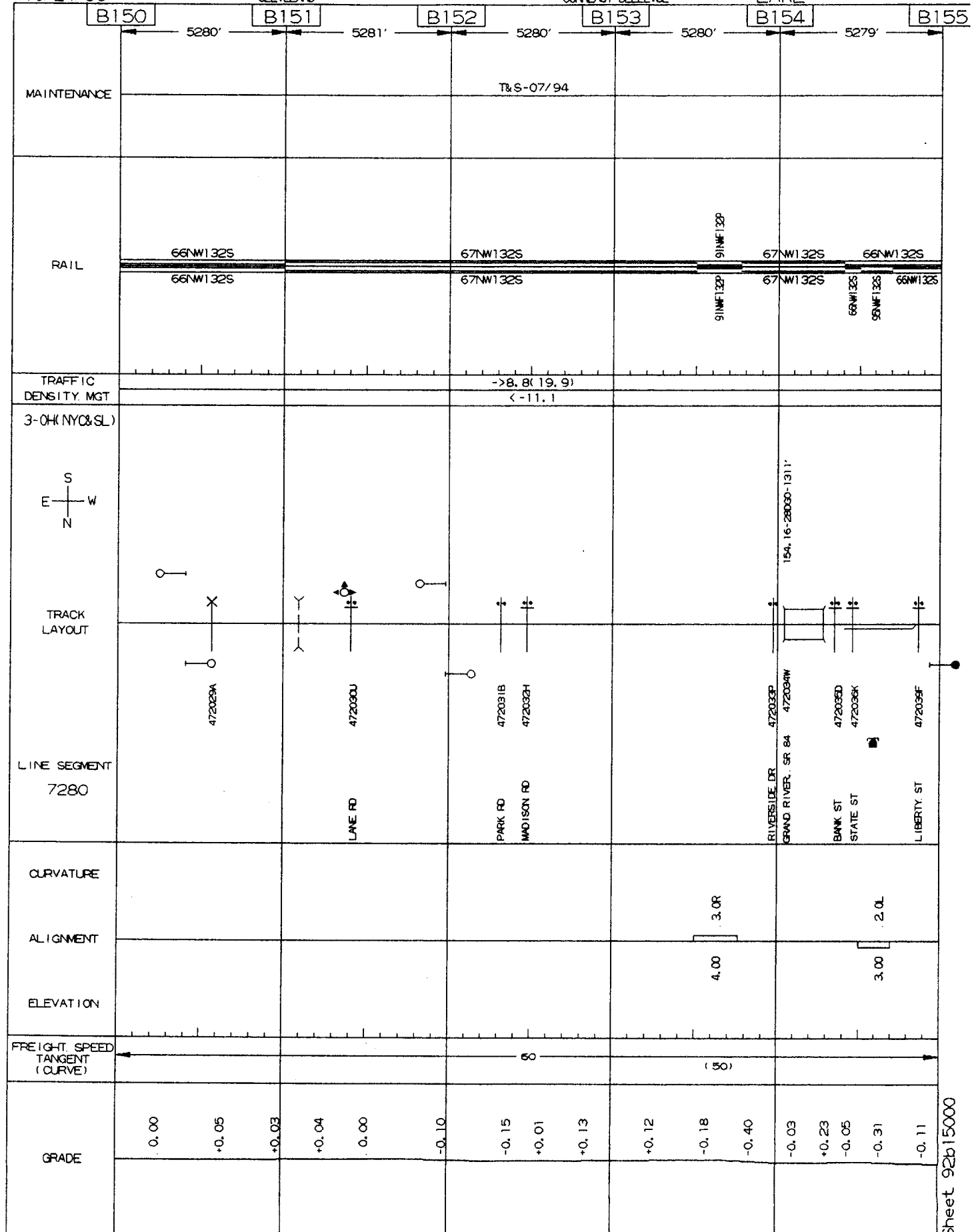
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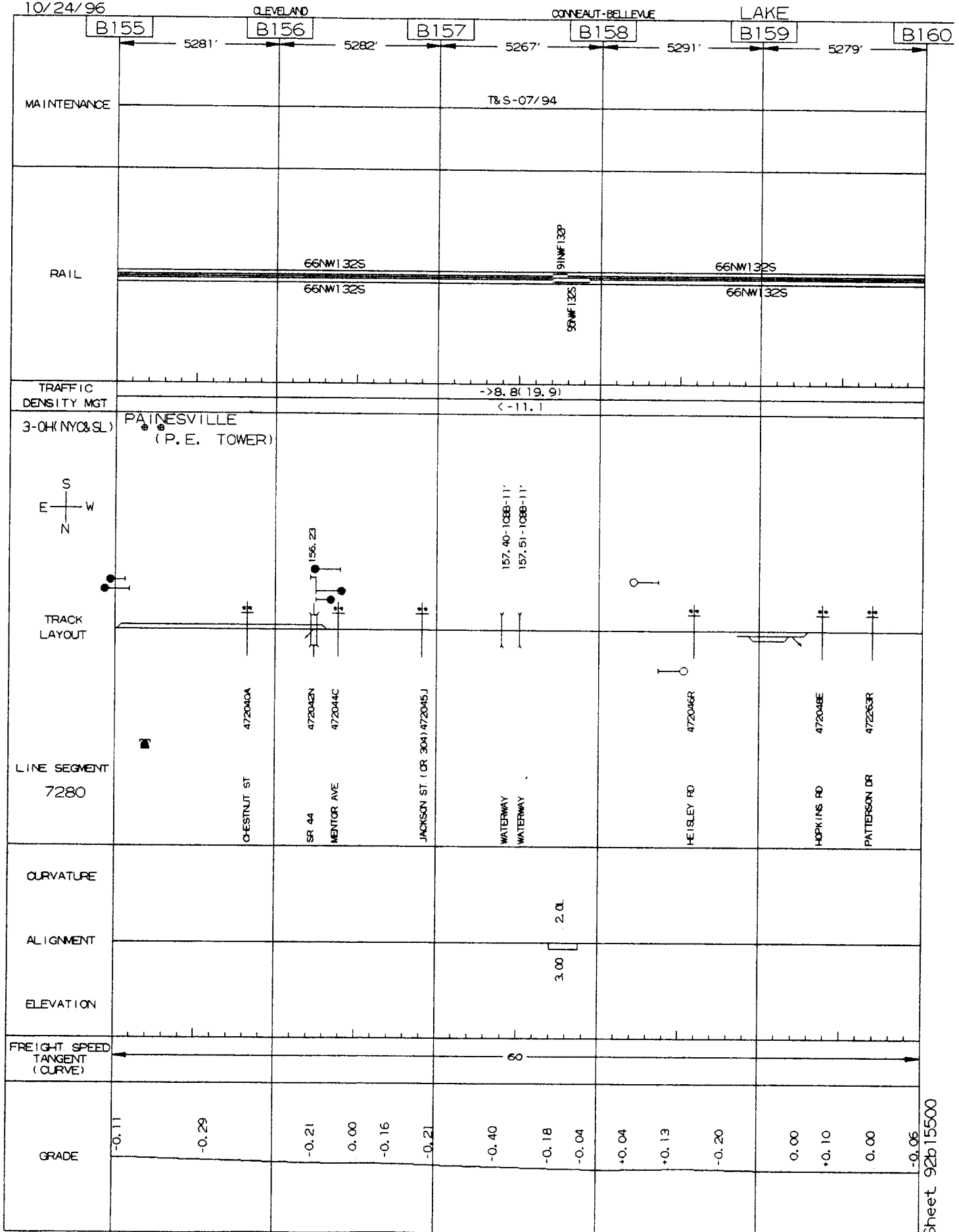
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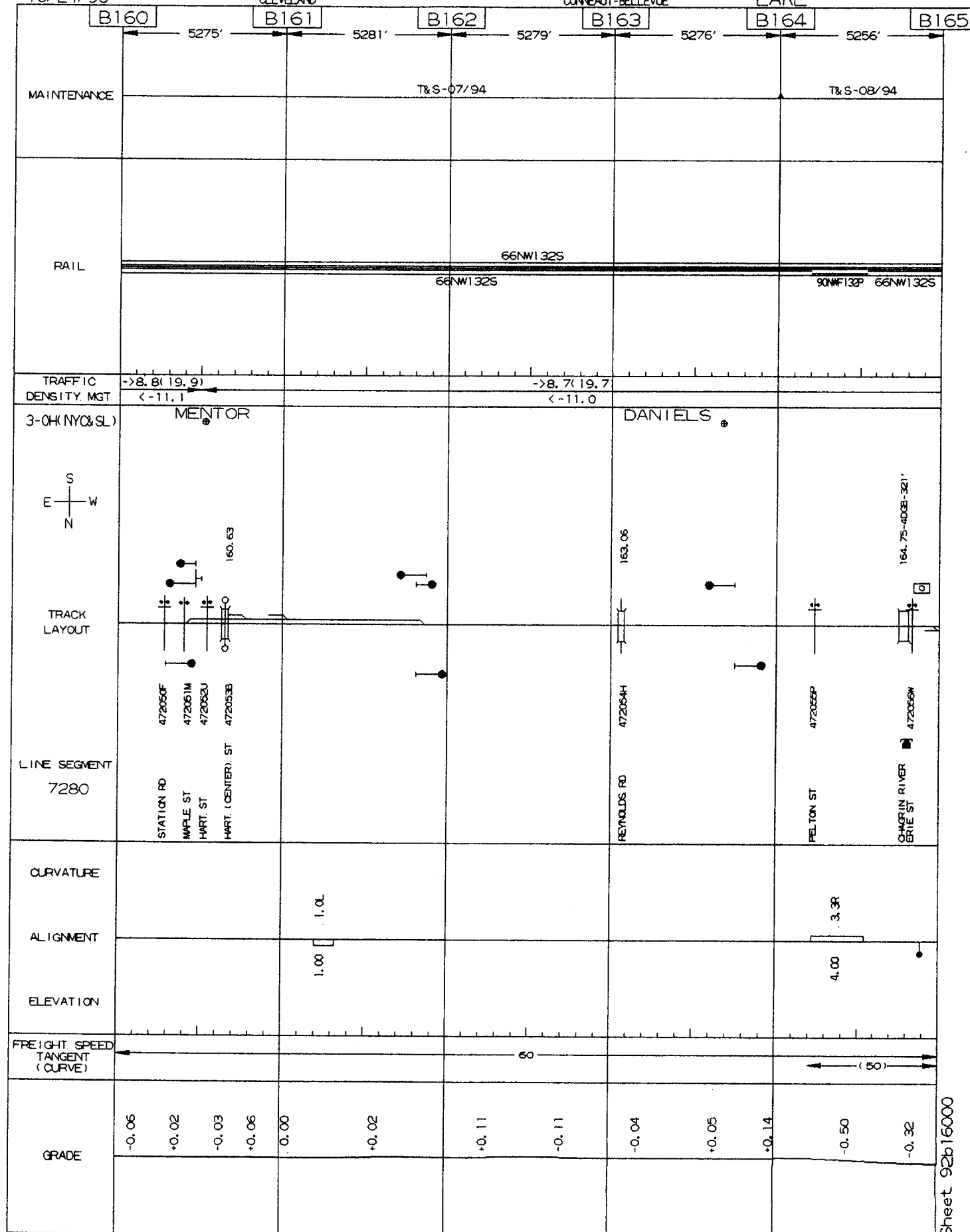


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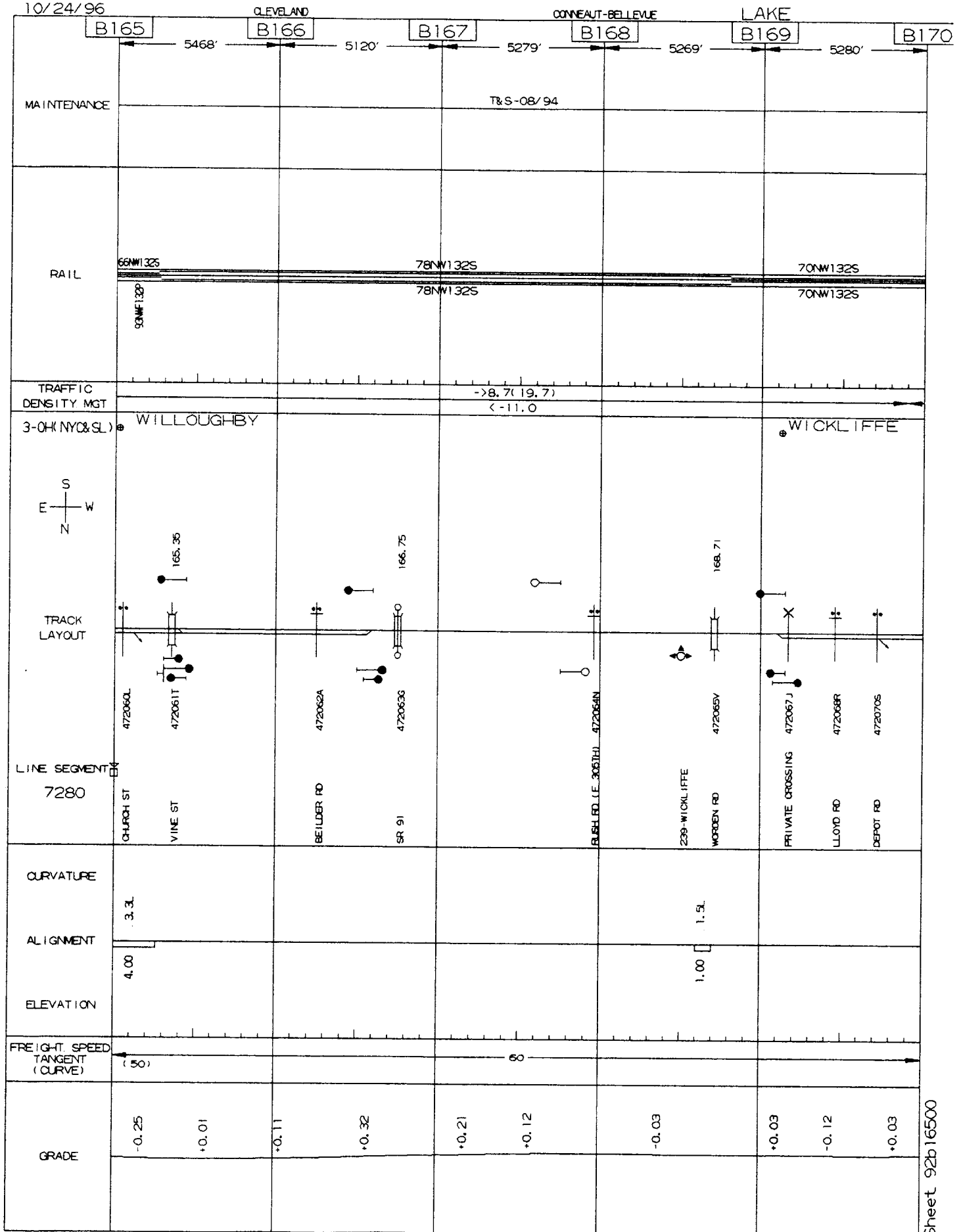
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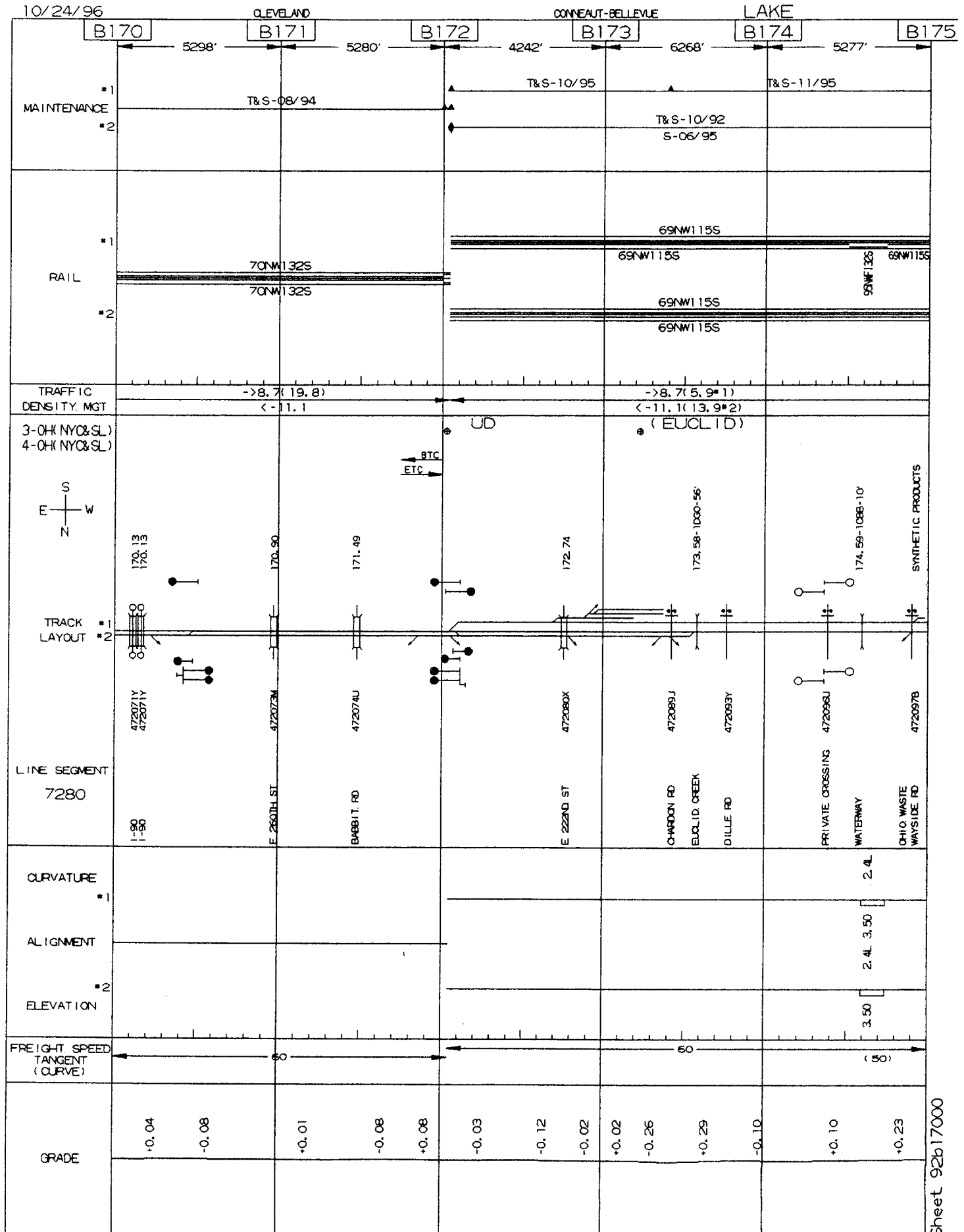
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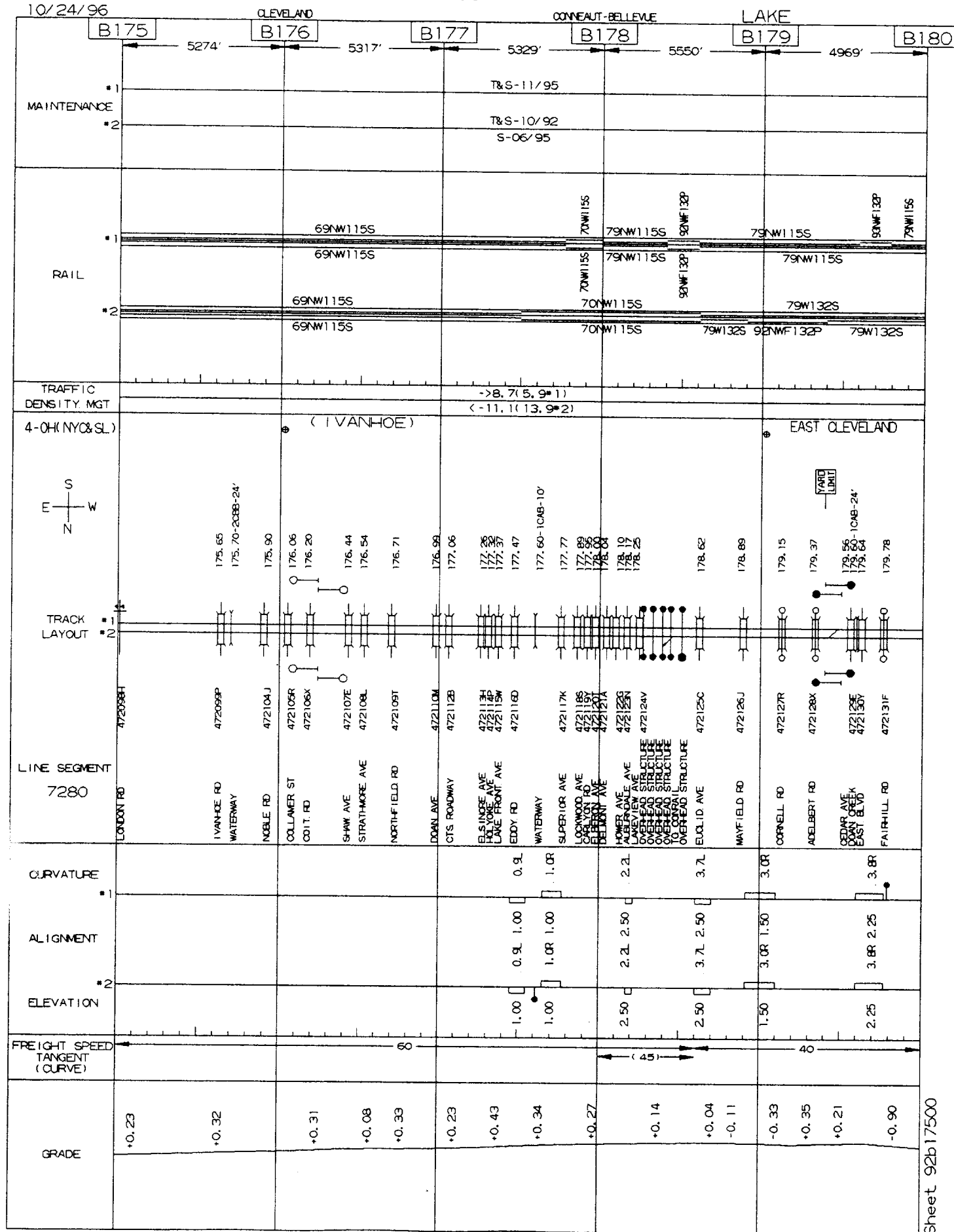


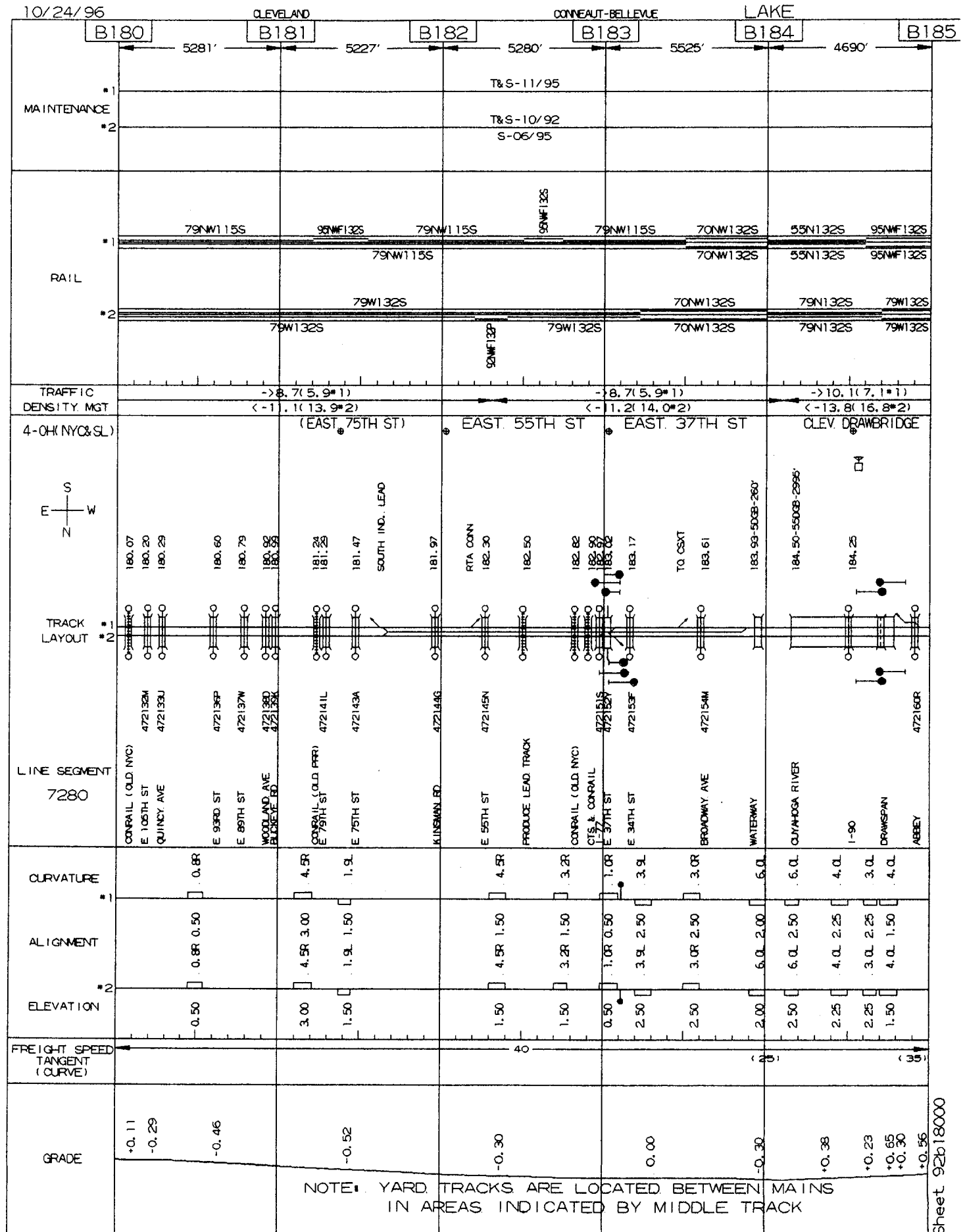
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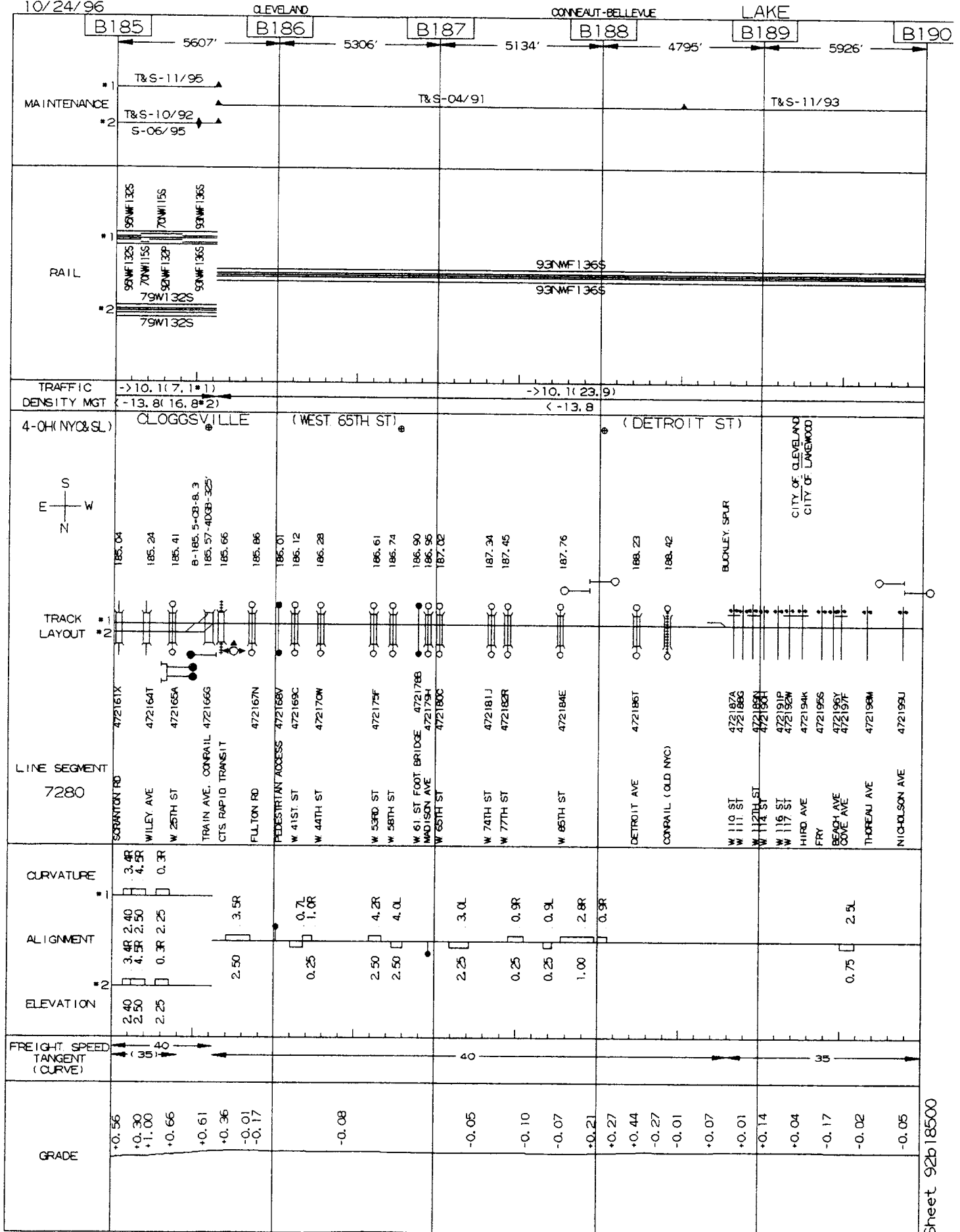


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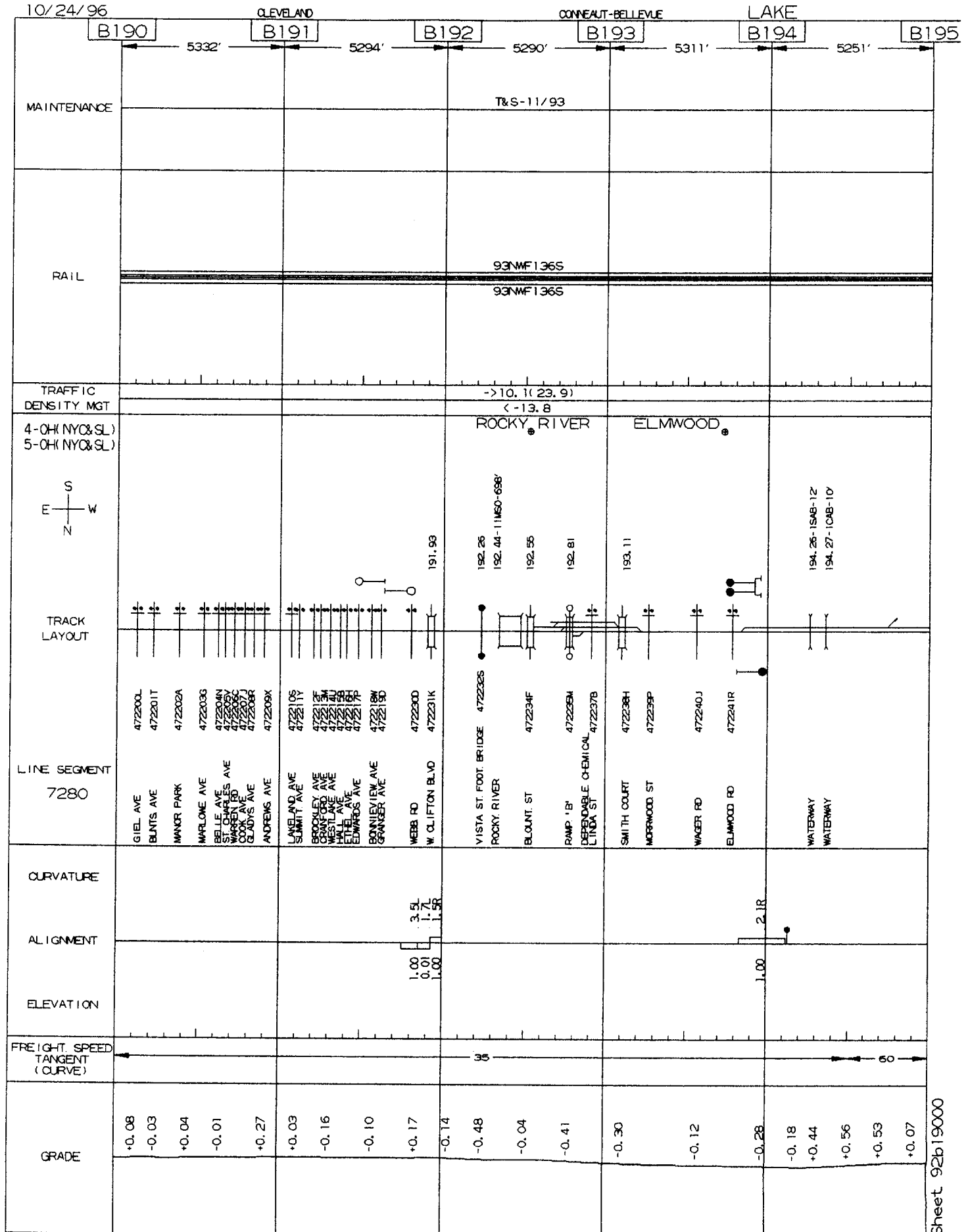




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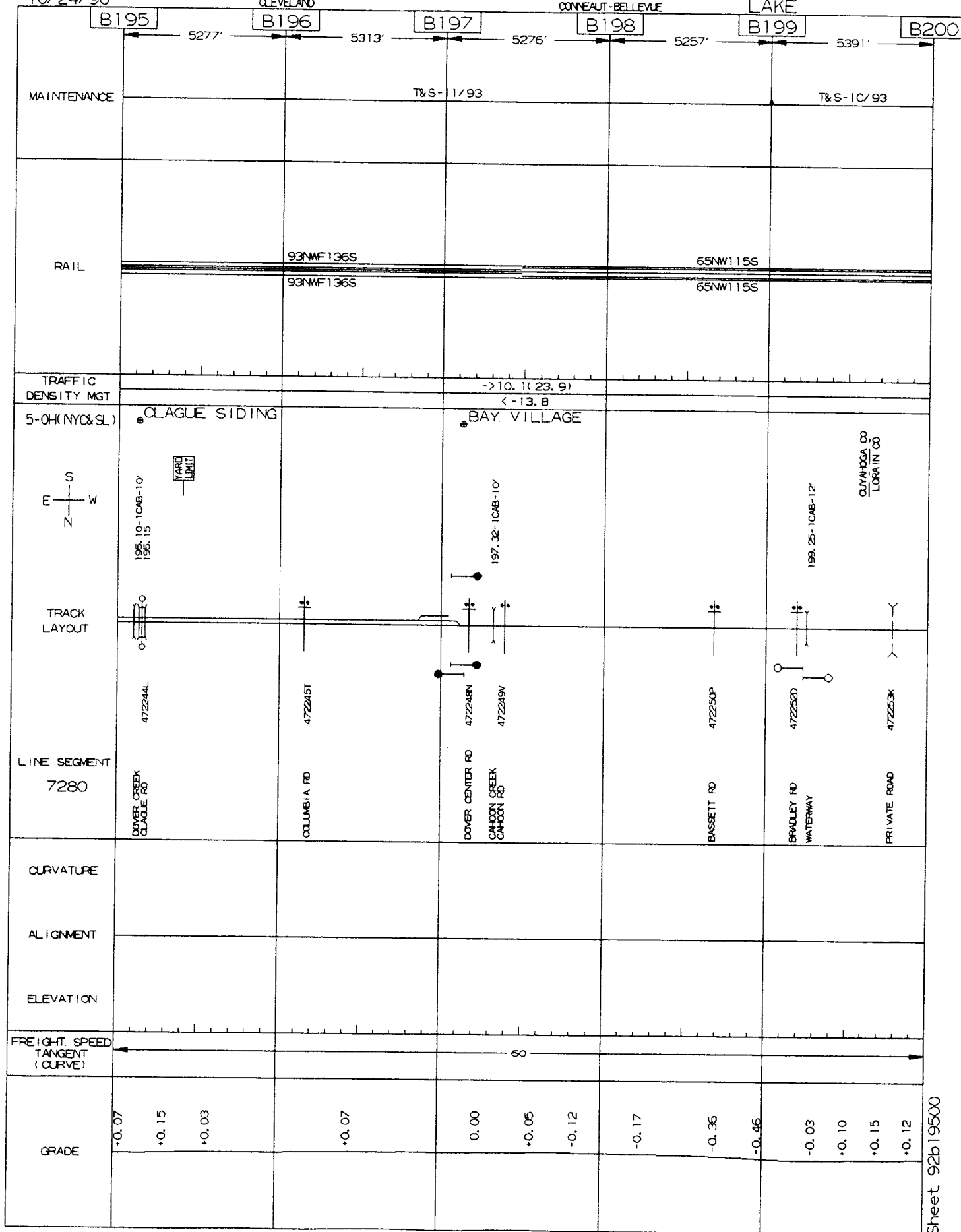


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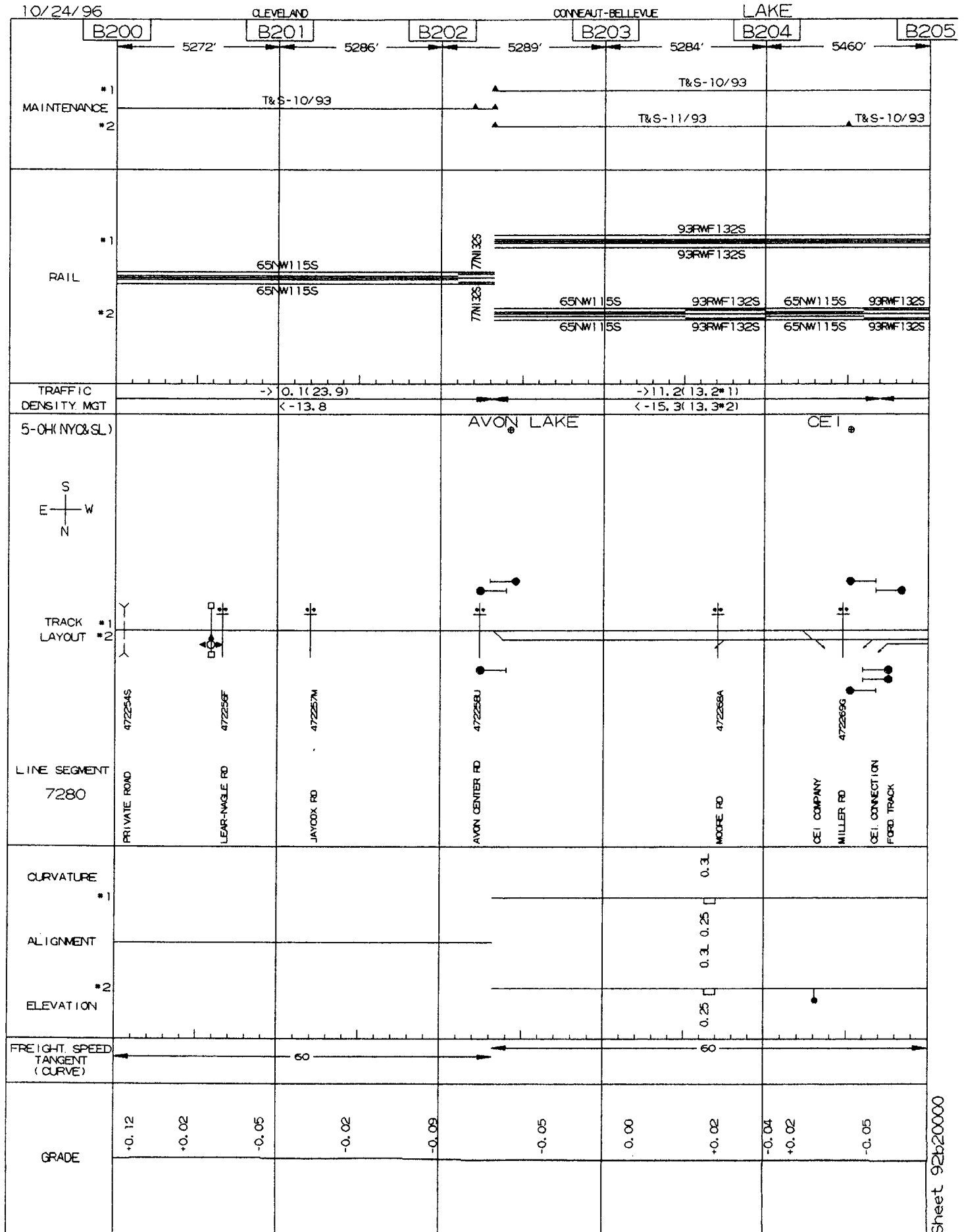
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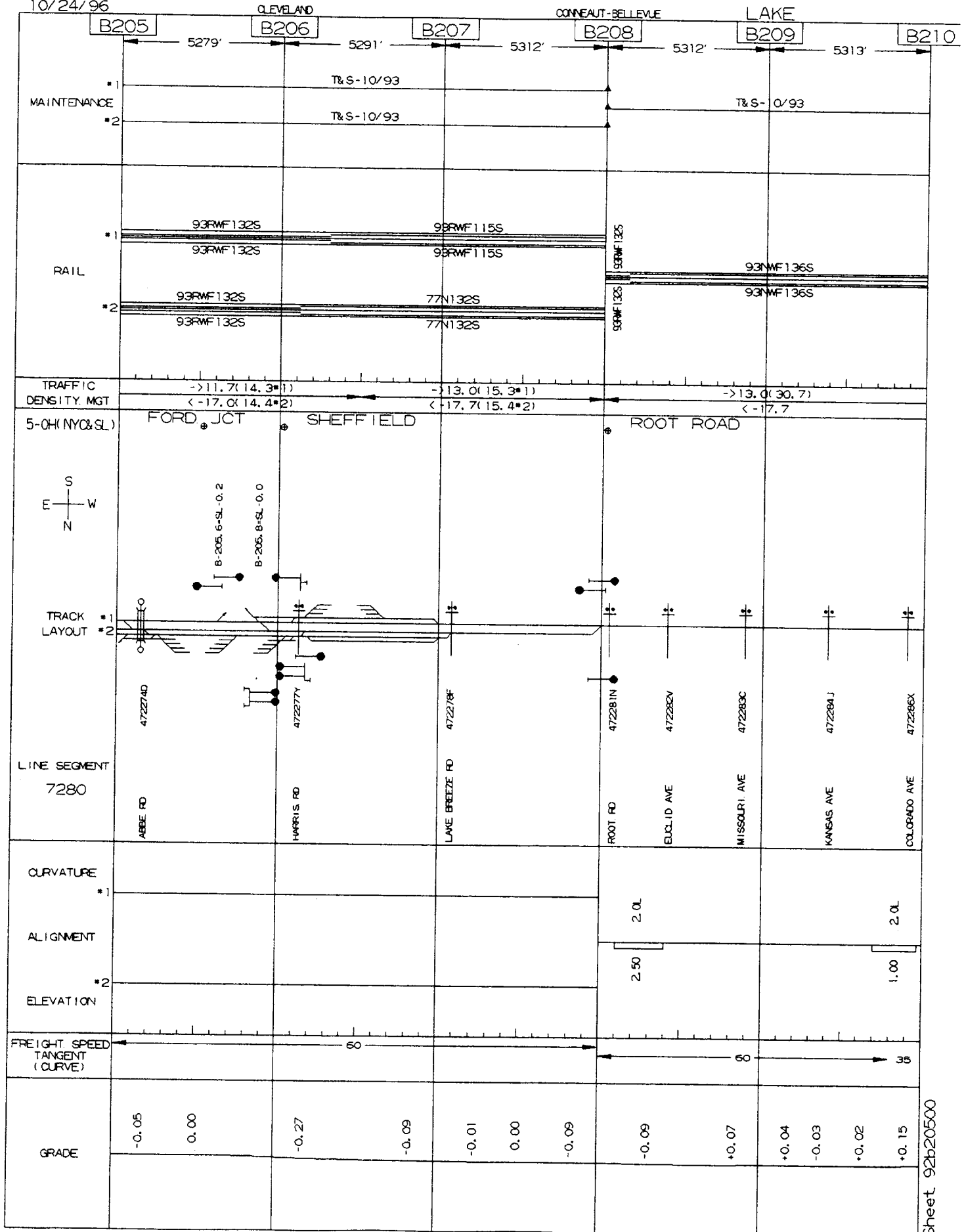
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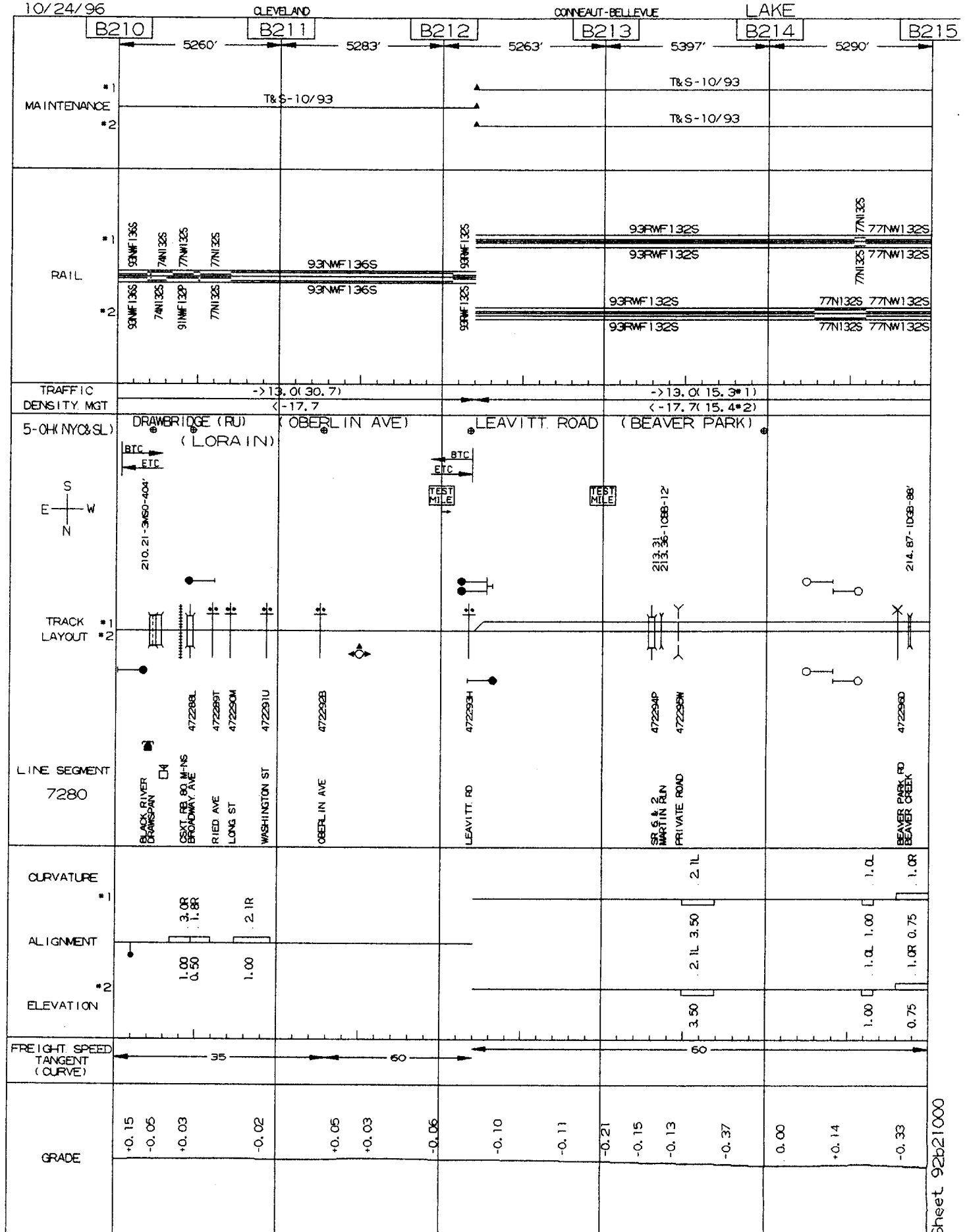
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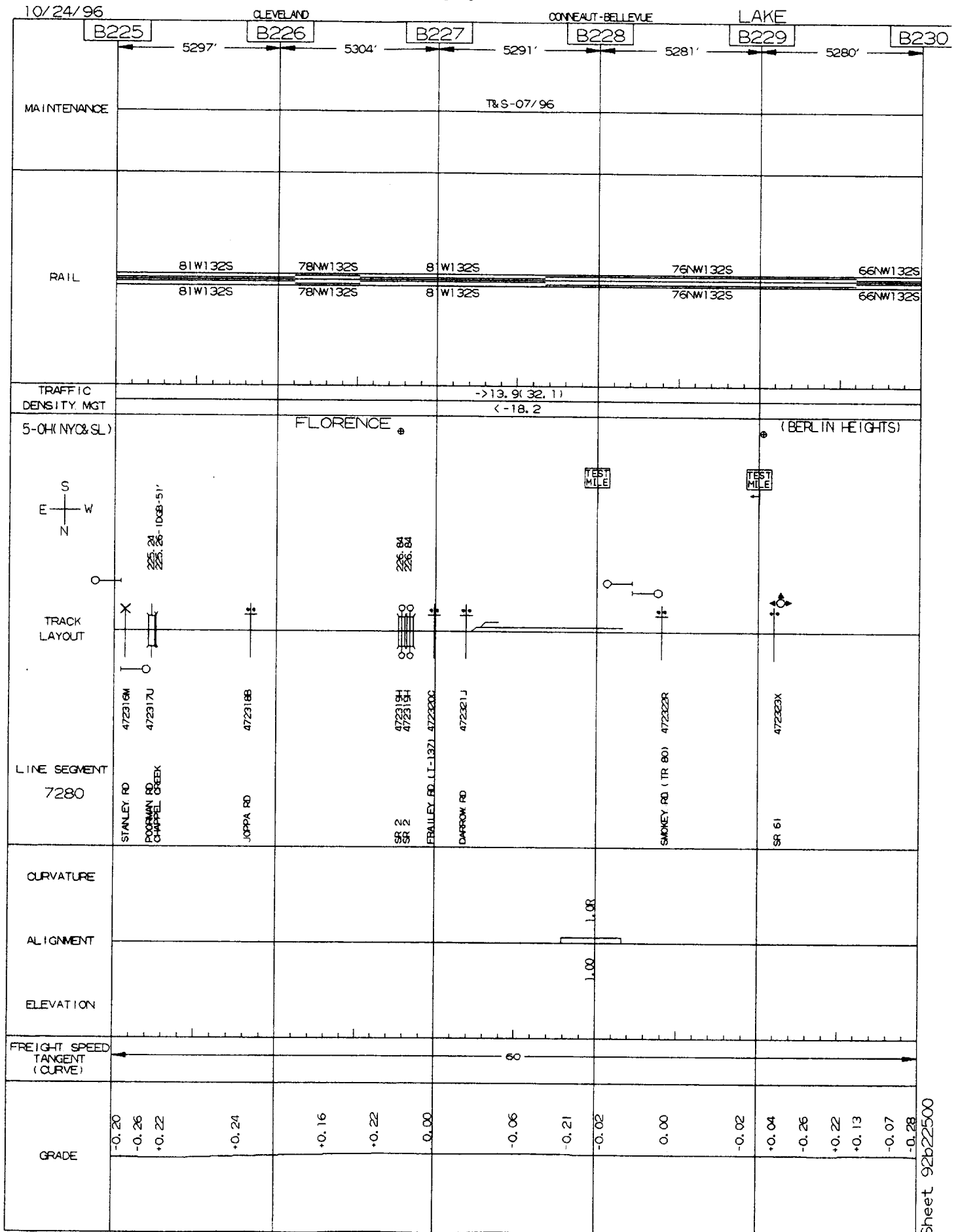
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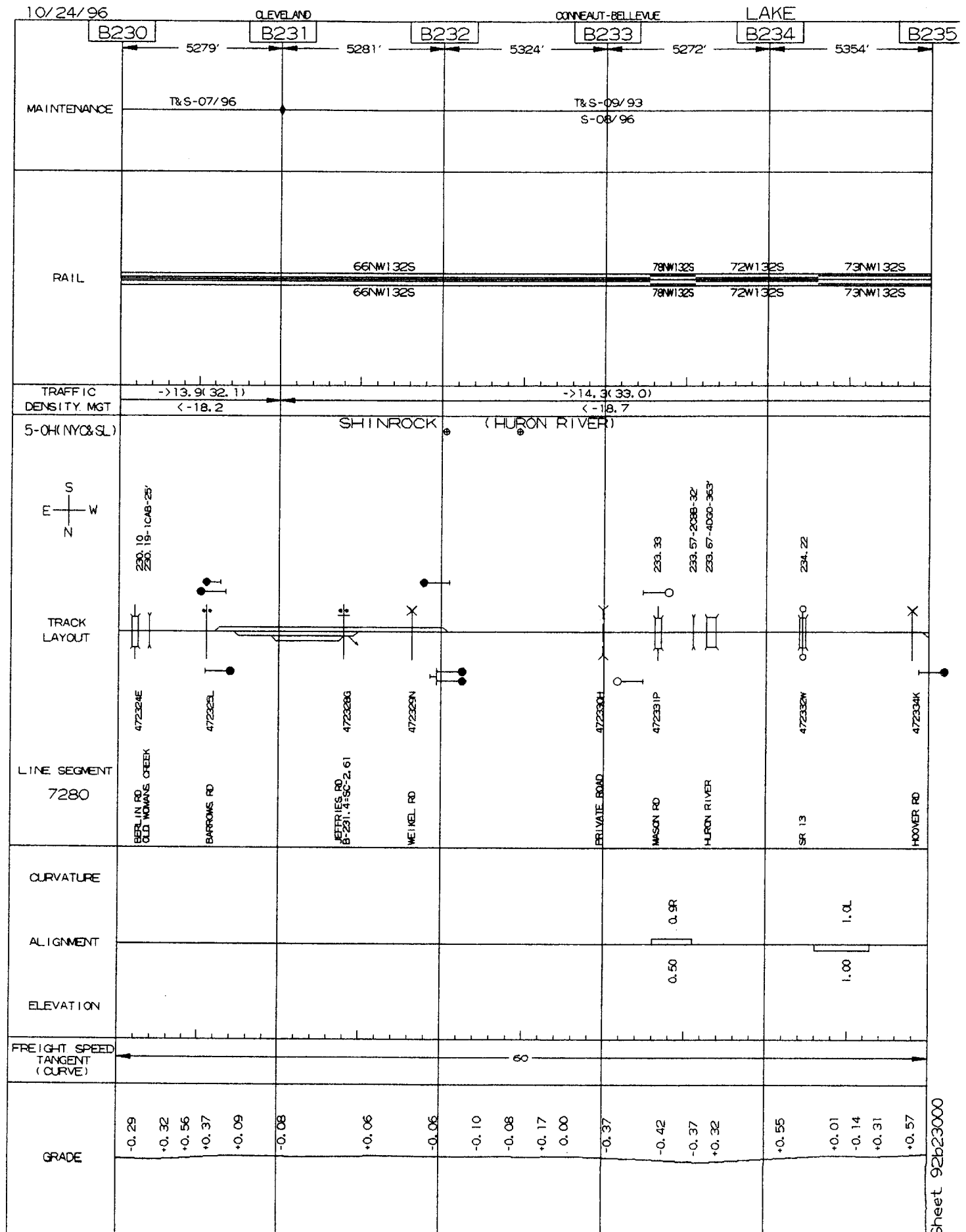
CLEVELAND

CONNEAUT-BELLEVUE

LAKE

	B220	B221	B222	B223	B224	B225
MAINTENANCE	5286'	5315'	5284'	5273'	5321'	
RAIL	73W131S 73W131S	76NW132S 76NW132S	76NW132S 76NW132S	76NW132S 76NW132S	81W132S 81W132S	
TRAFFIC DENSITY MGT			->13.9 (32.1) <-18.2			
5-OH (NYC&SL)		(VERMILION)	GC			
TRACK LAYOUT						
LINE SEGMENT 7280	CONRAIL (OLD NYC) EAST RIVER RD VERMILION RIVER WATER ST DOUGLASS ST STATE ST	PRIVATE ROAD	PRIVATE ROAD	PRIVATE ROAD	PRIVATE ROAD	SUGAR CREEK BARNES RD
CURVATURE	1.0L	2.4L				
ALIGNMENT	1.00	2.50				
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			60			
GRADE	-0.57 +0.31 +0.17 +0.63 +0.44 +0.17	0.00 -0.06 +0.05	+0.08	+0.11 +0.09	0.00 -0.08 -0.01	-0.25 +0.41 +0.09





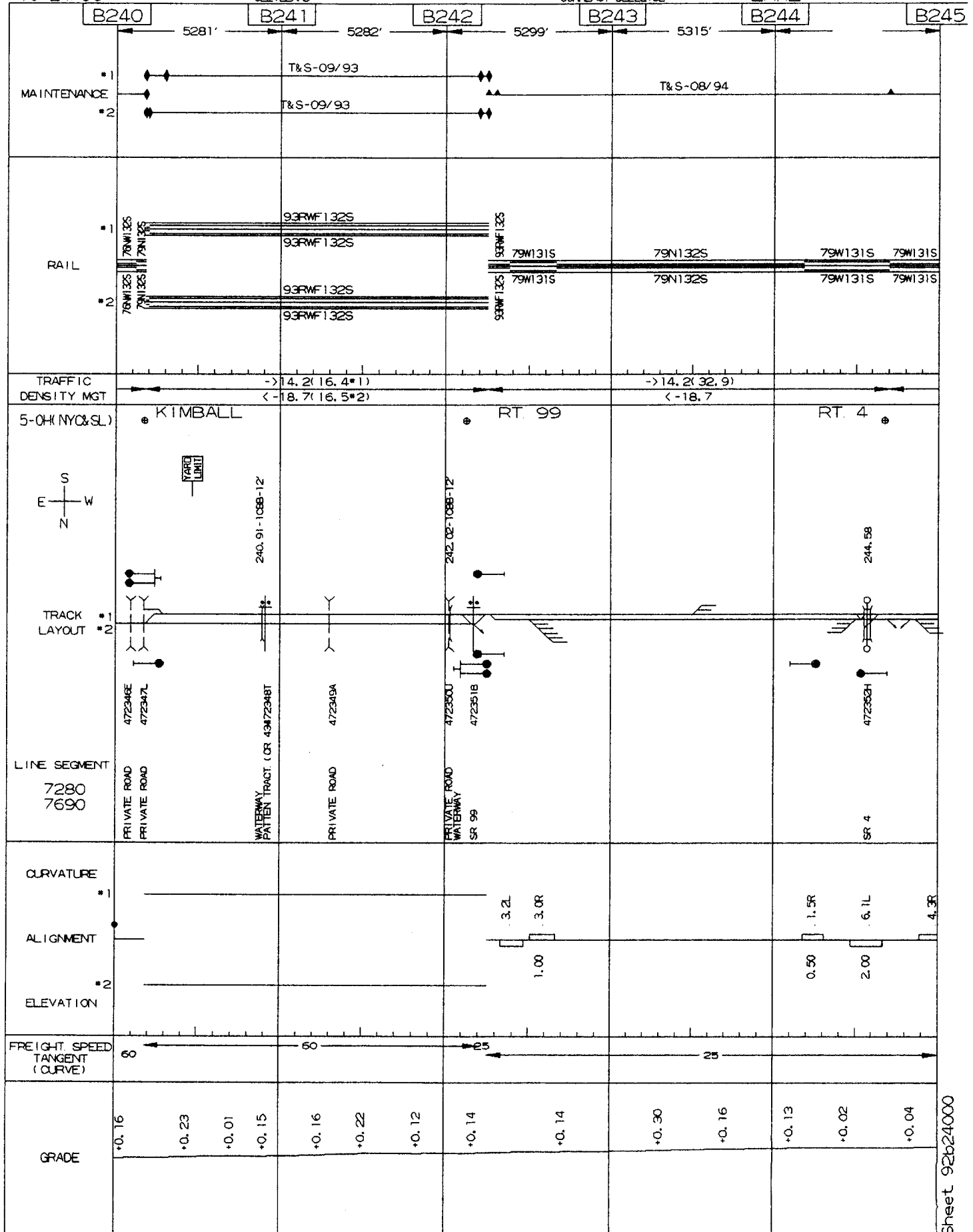
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CLEVELAND

CONNEAUT-BELLEVUE

LAKE



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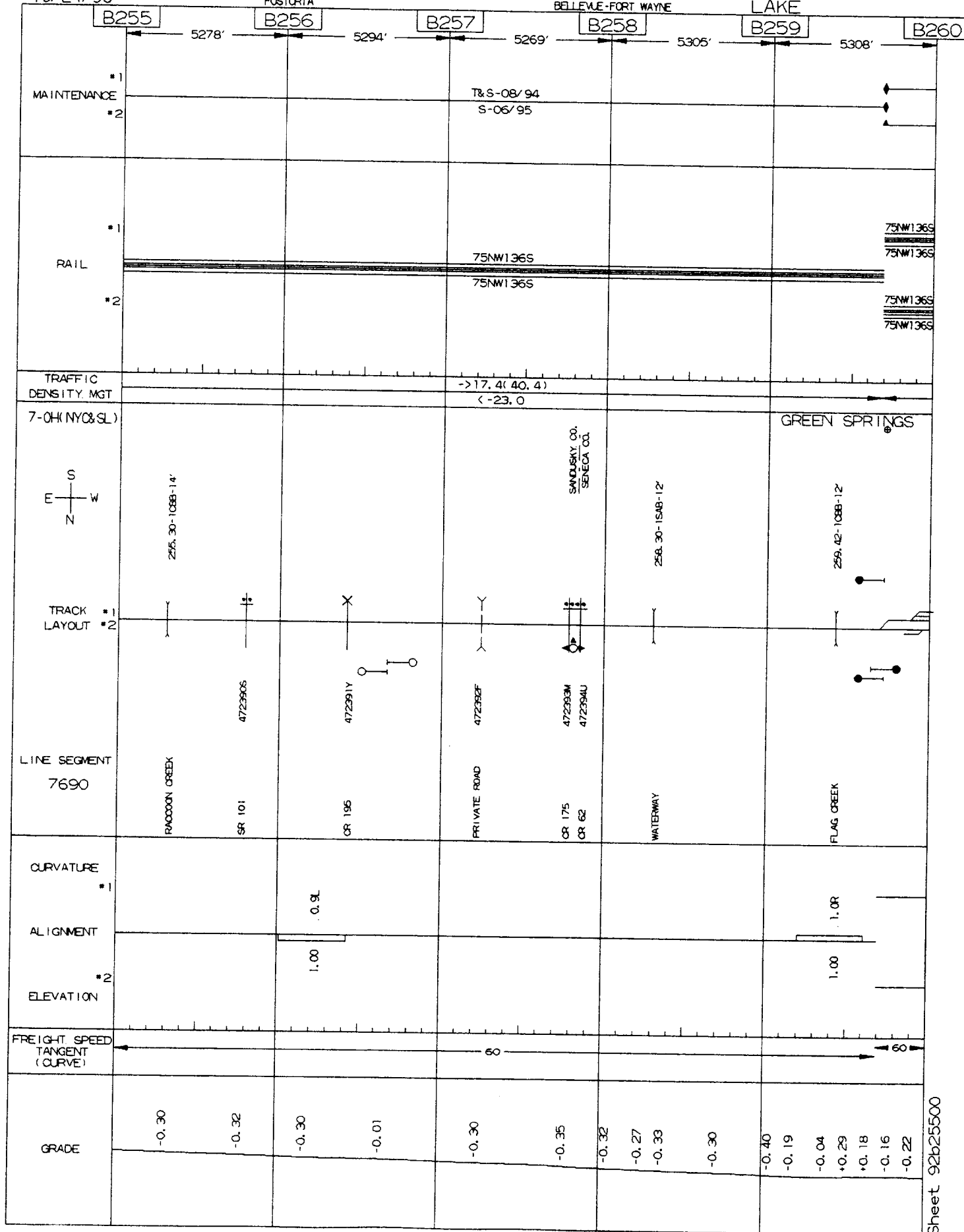


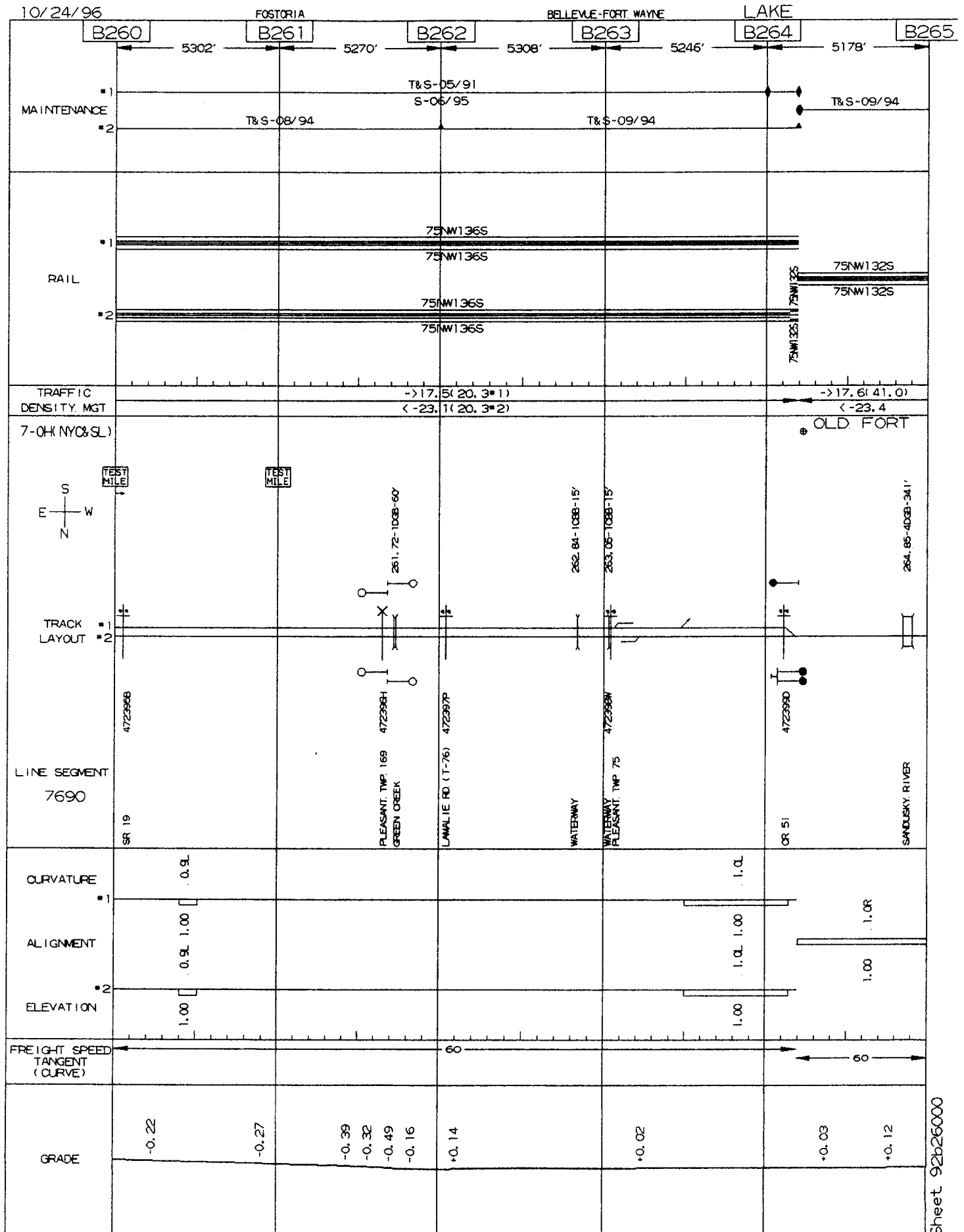
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FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



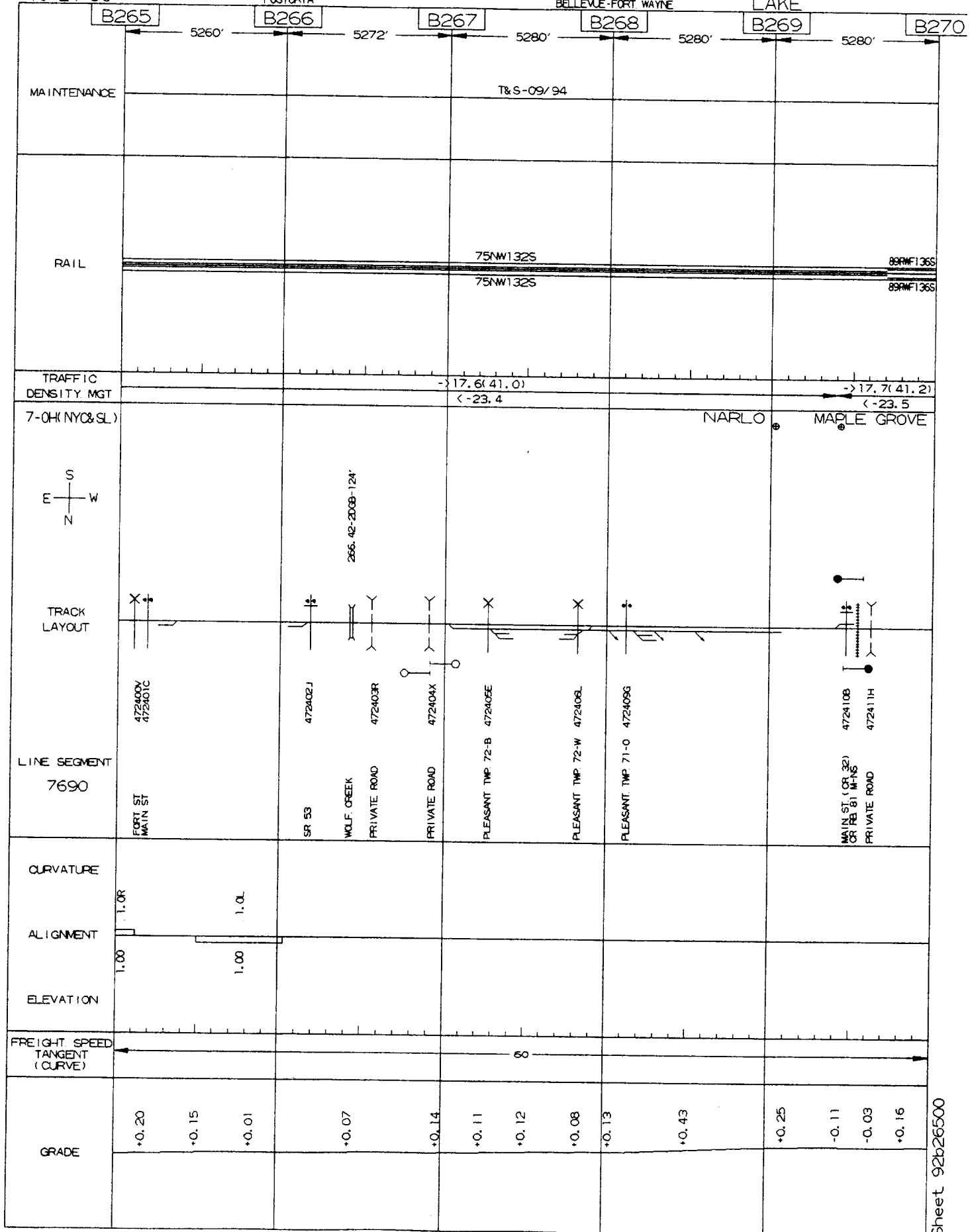


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FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



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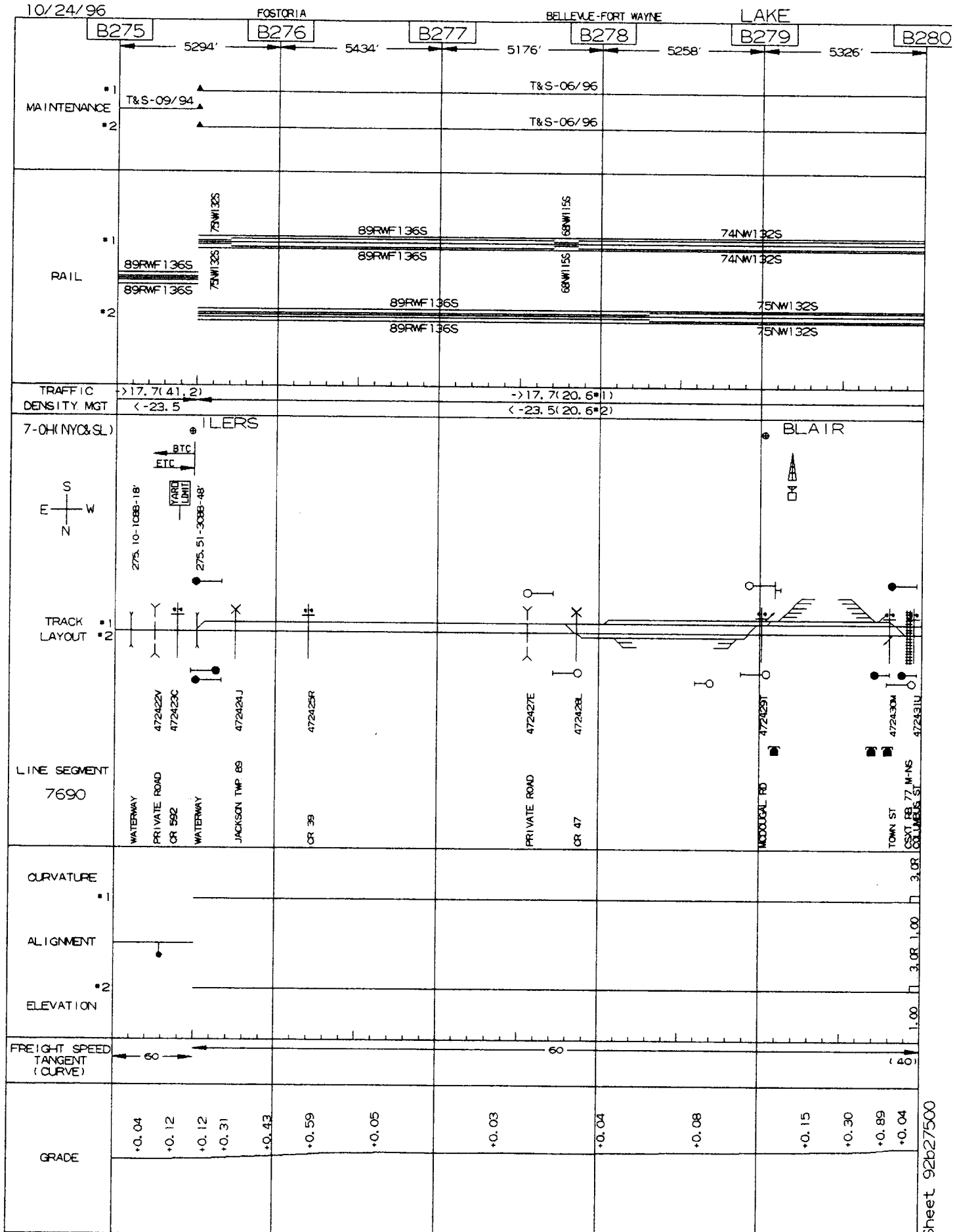
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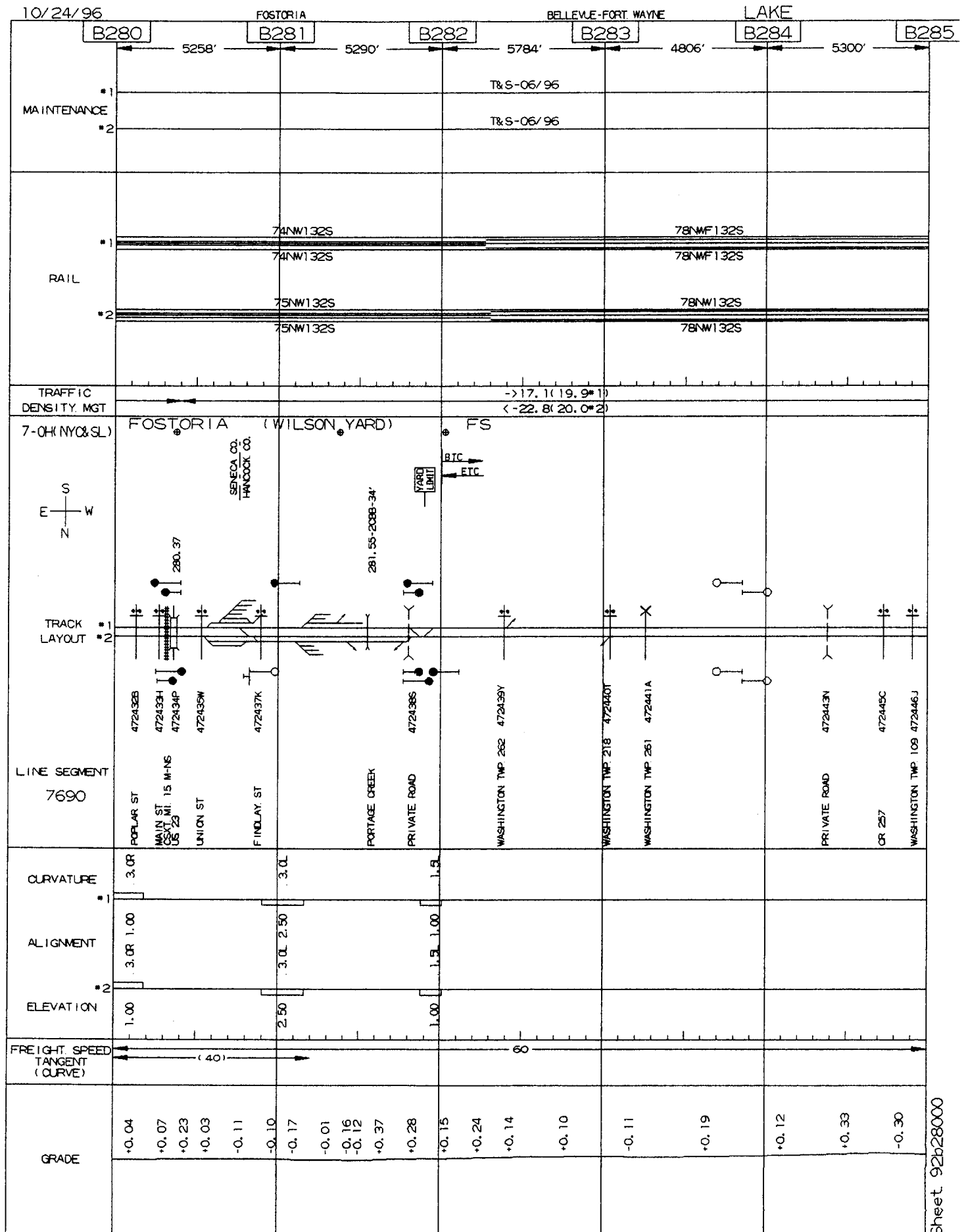
BELLEVUE-FORT WAYNE

LAKE

	B270	B271	B272	B273	B274	B275
	5348'	5298'	5296'	5306'	5280'	
MAINTENANCE			T&S-09/94			
RAIL	89RWF136S 89RWF136S		75NW136S 75NW136S		89RWF136S 89RWF136S	
TRAFFIC DENSITY MGT			-> 17, 7(41, 2) < -23.5			
7-OH(NY&SL)						
<div style="text-align: center;">           S            E — W            N         </div>		271.21-1088-15'		273.46-2088-24'		
TRACK LAYOUT	Y X X	Y Y	Y Y	Y Y	X	X
LINE SEGMENT 7690	PRIVATE ROAD 472413W OR 11 LIBERTY TWP 152-B 472415K	LOFT RD (T 69-C) CATTLE PASS 472416S PRIVATE ROAD 472417Y	PRIVATE ROAD 472418F SR 636 472419M	WATERWAY	LIBERTY TWP 101U 472420G	OR 5 472421N
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			60			
GRADE	+0.16 +0.34 +0.31 -0.01	-0.08 -0.04 -0.01	+0.09 +0.02	0.00	+0.17 +0.09	

10/24/96



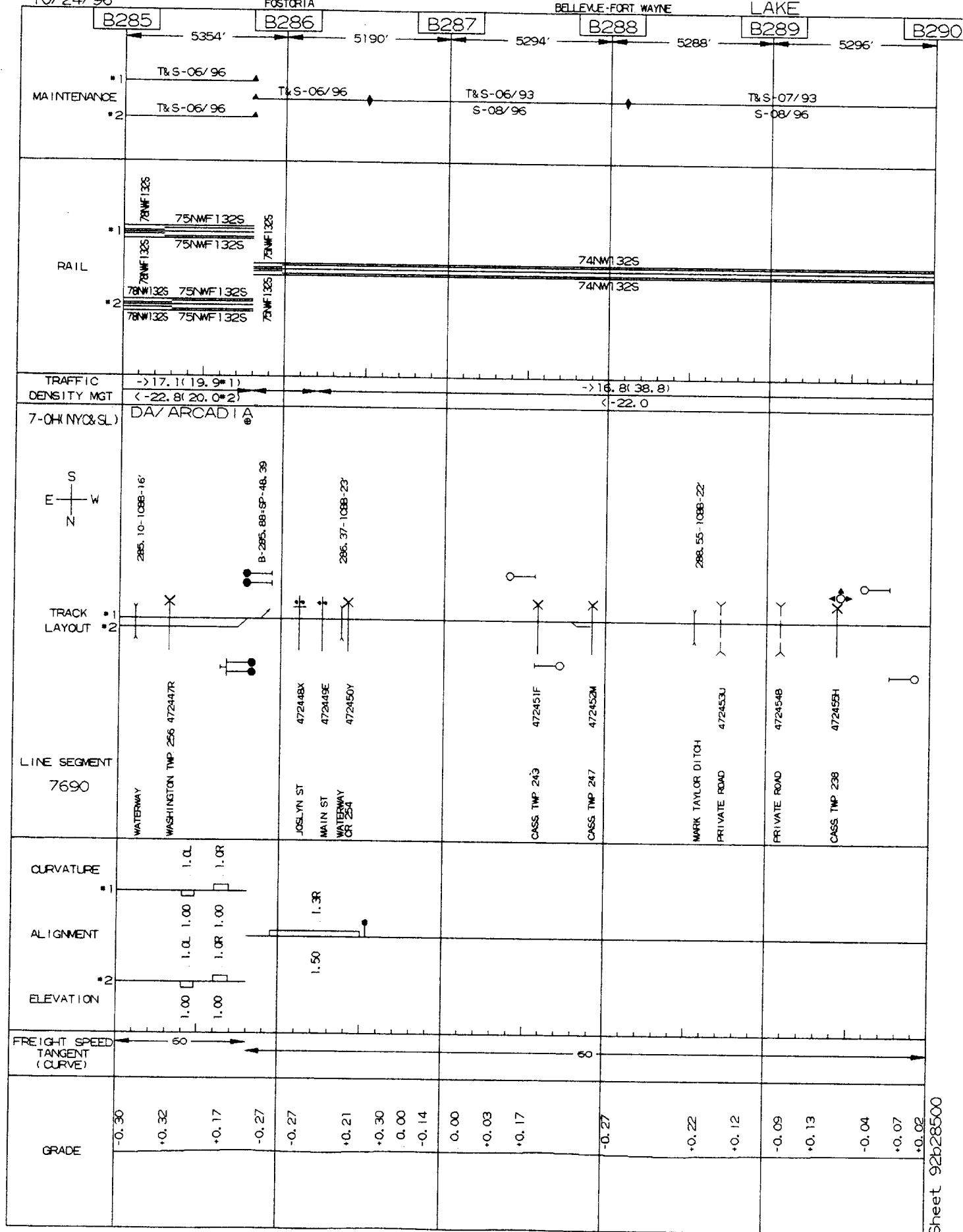


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



Sheet 92b29000

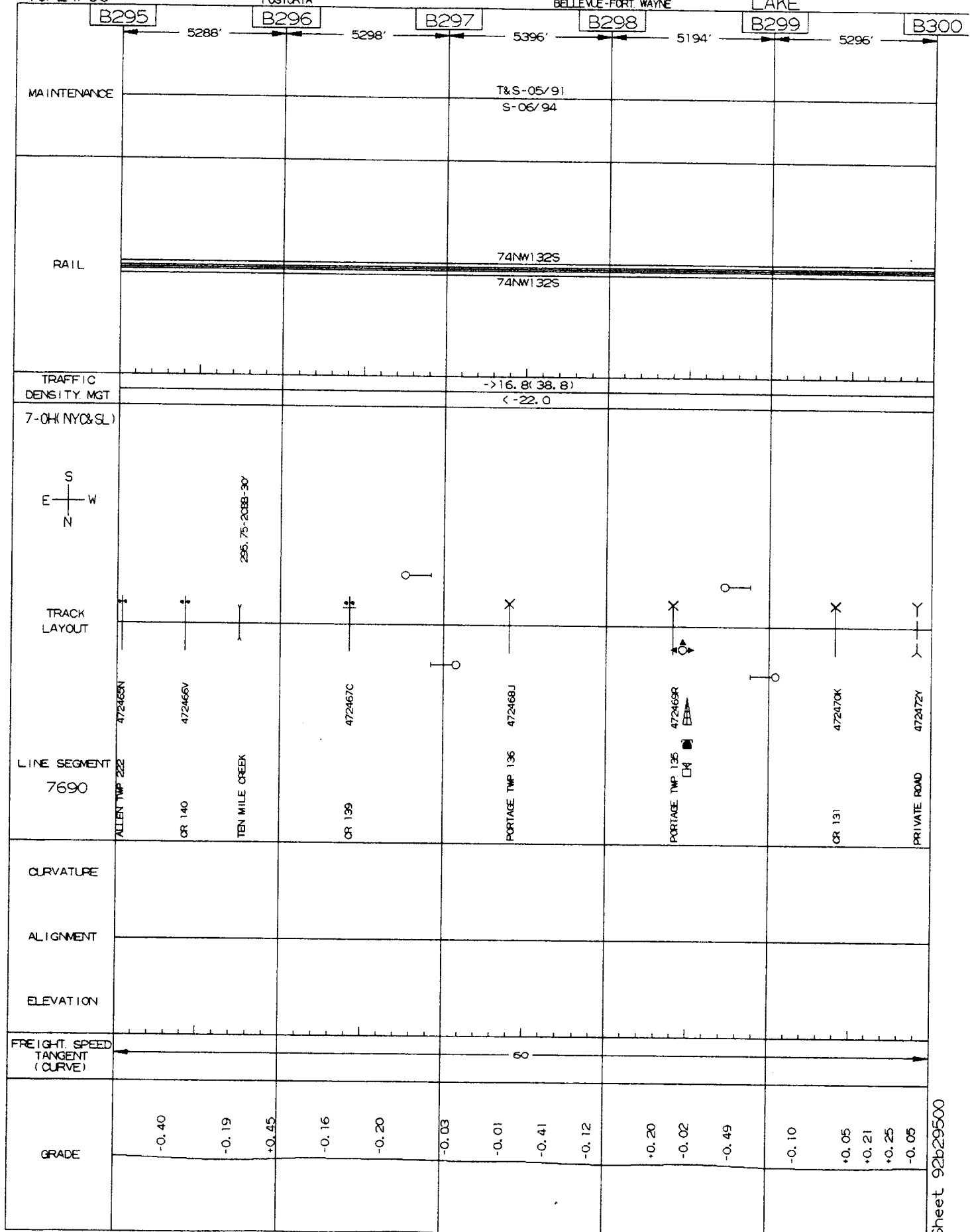


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE

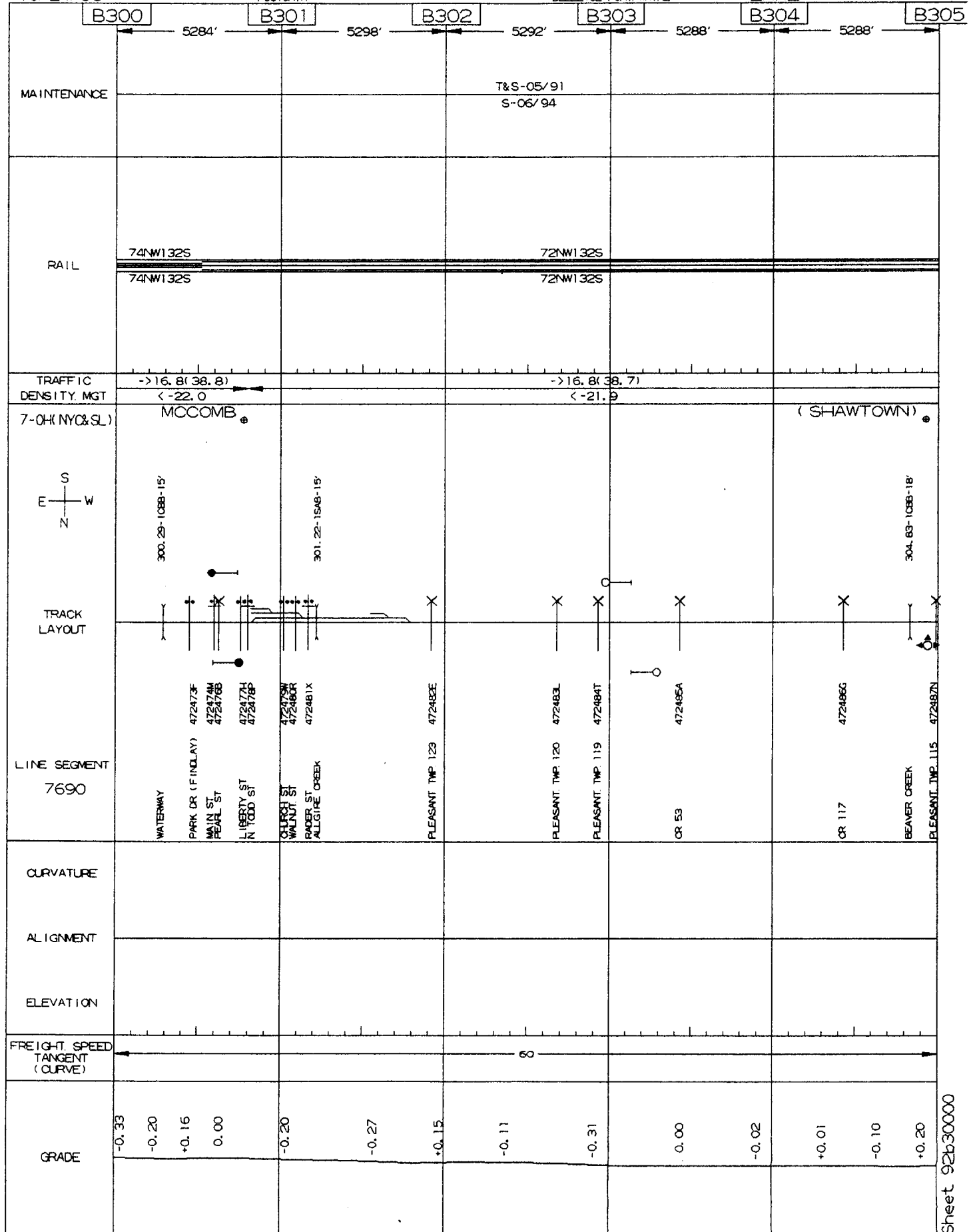


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

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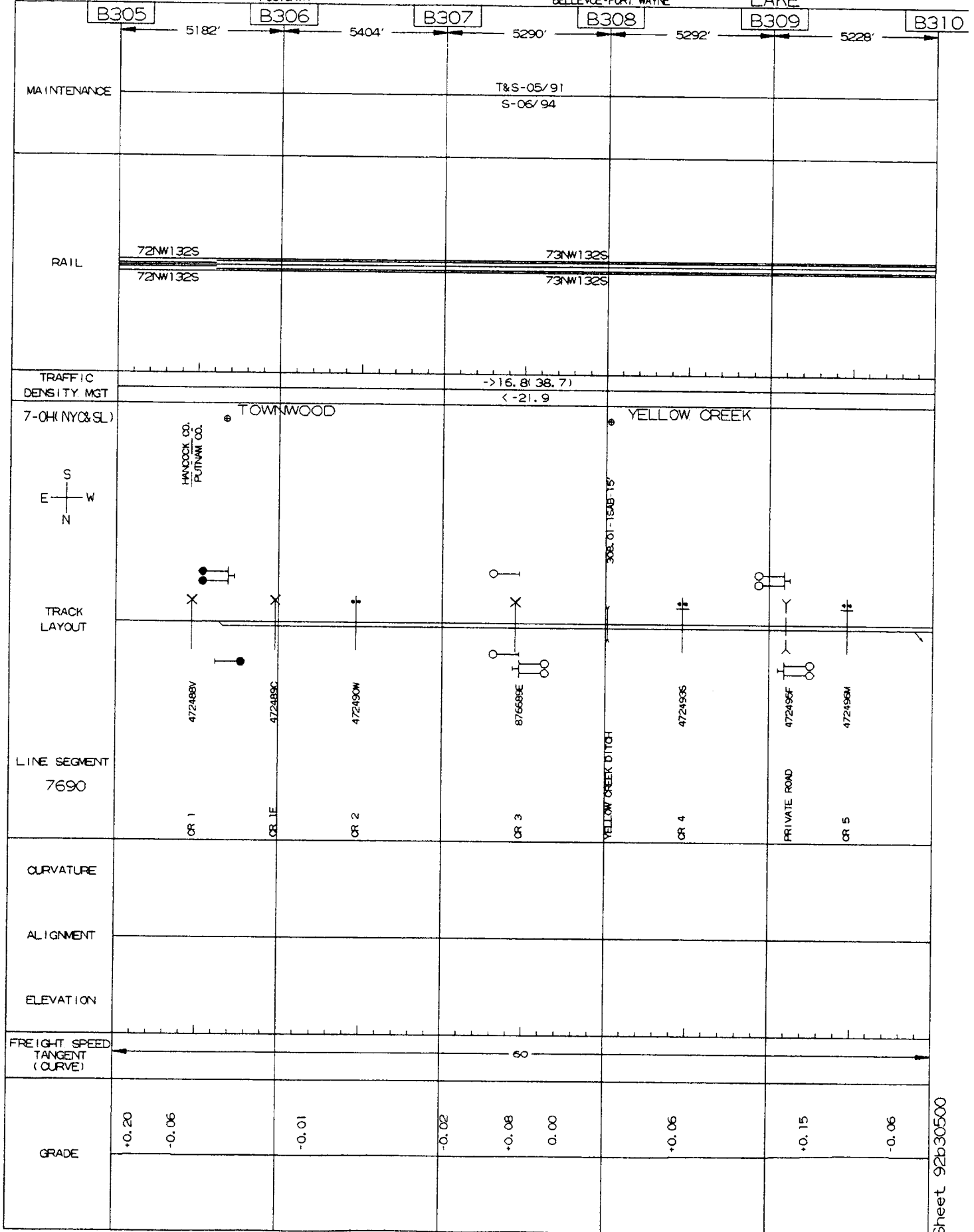


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE

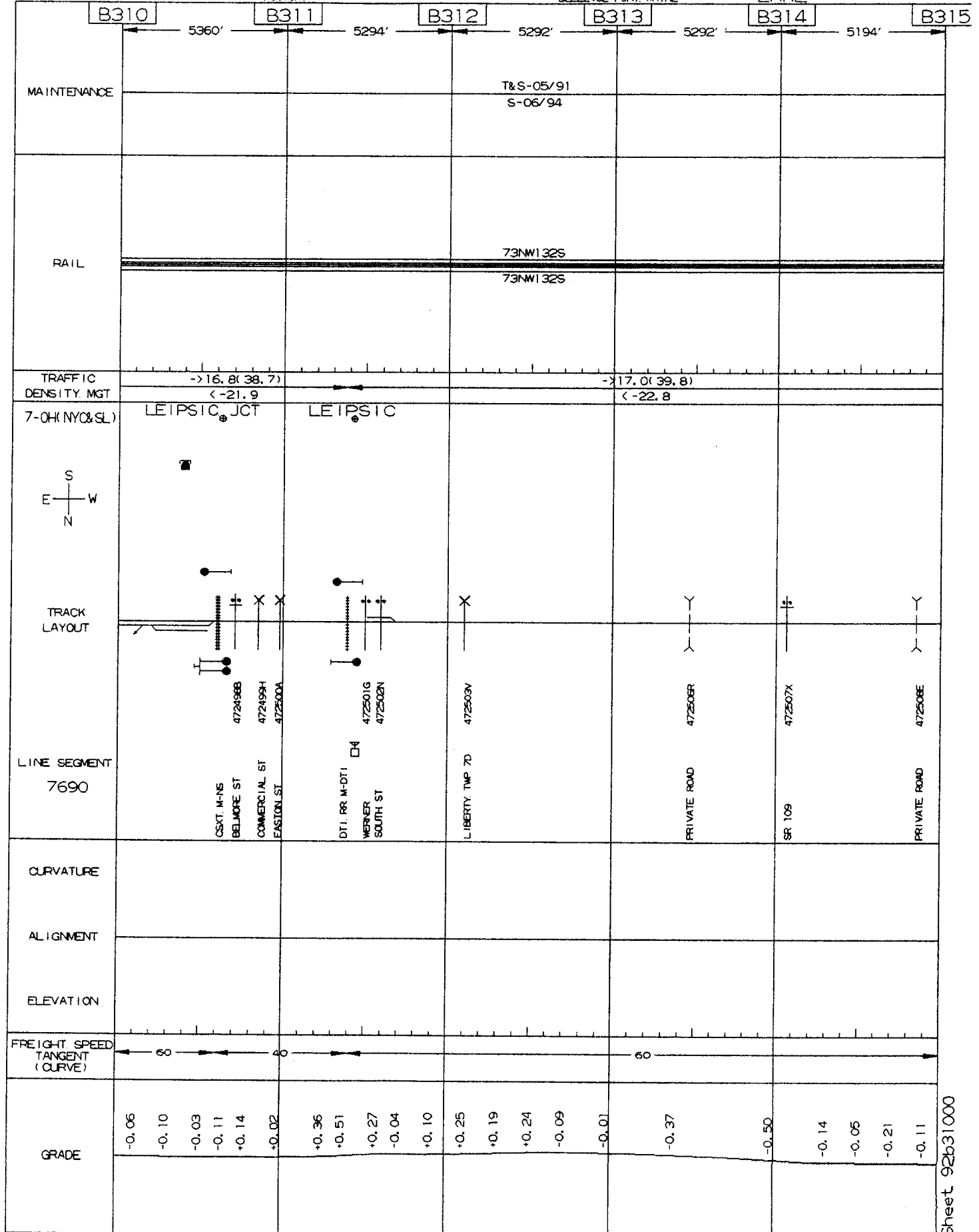


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE

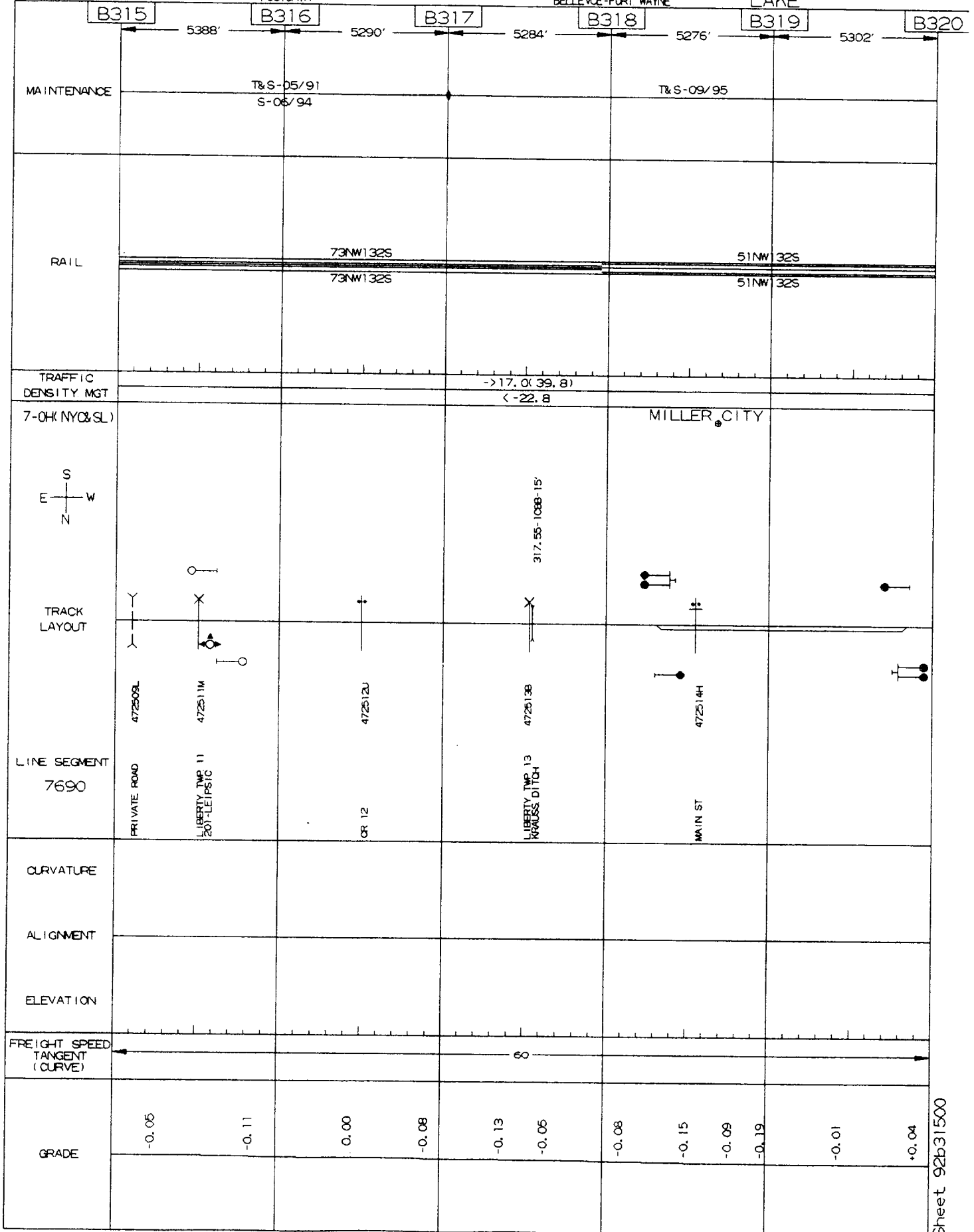


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



Sheet 92b32000

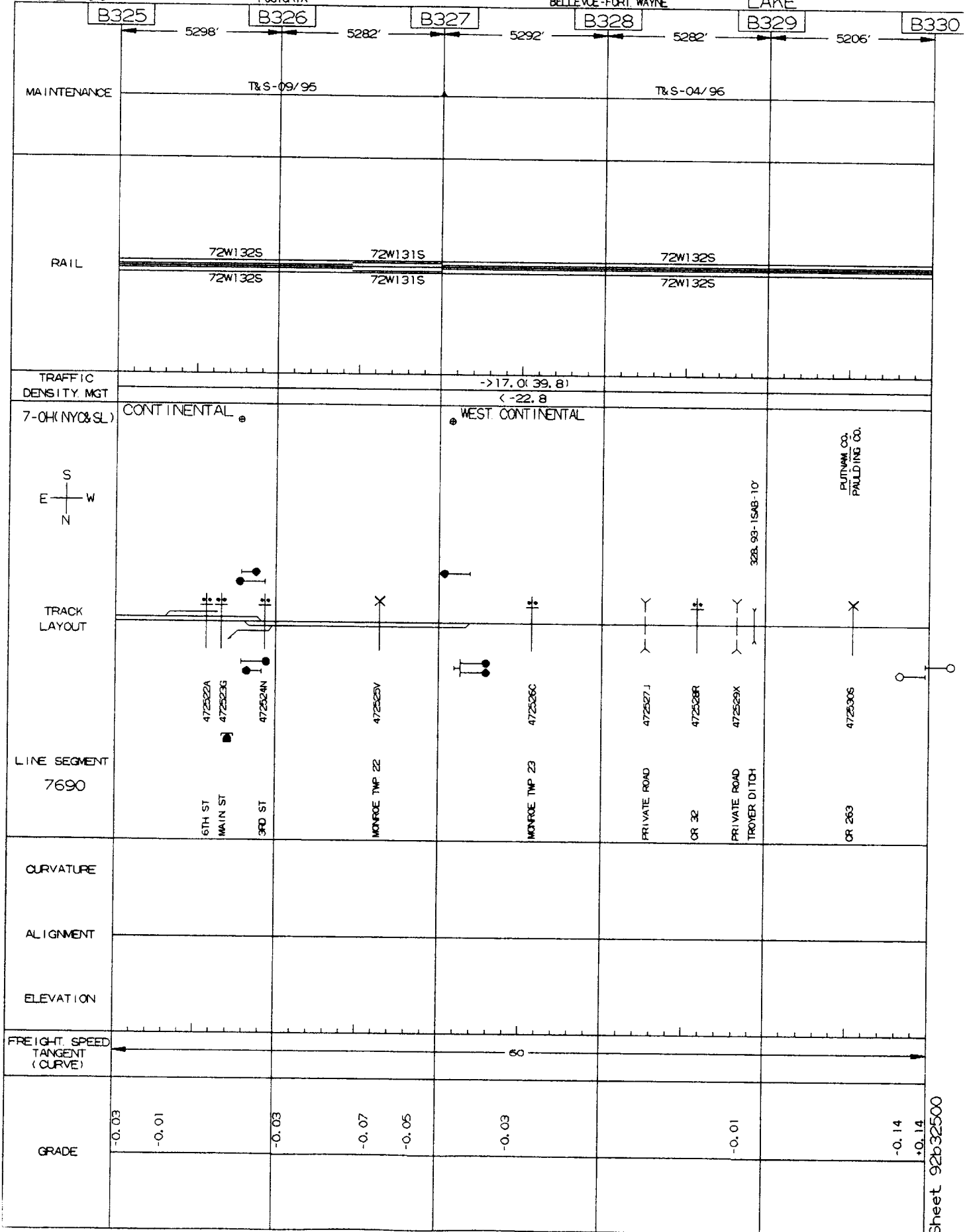
066

10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



Sheet 92b32500

10/24/96		FOSTORIA		BELLEVUE-FORT WAYNE		LAKE					
B330		B331		B332		B333		B334		B335	
5364'		5288'		5446'		5126'		5288'			
MAINTENANCE		T&S-04/96									
RAIL		72W131S 72W131S 73NW132S 73NW132S									
TRAFFIC DENSITY MGT		-> 17.0 (39.8) < -22.8									
7-OH (NYC&SL)		OAKWOOD (MELROSE)									
<div><div>S</div><div>E</div><div>W</div><div>N</div></div>		331.57 331.68-2TT0-278' 332.99-1TT0-137'									
TRACK LAYOUT											
LINE SEGMENT 7690		BROWN TWP. 211 472531Y DICKNEY RD. (OR 209) 472532F 876690Y 6TH ST 472533M 2ND ST 472534U SR 66 472535B AUGLAIZE RIVER STANLEY RD. (OR 171) 472536H BROWN TWP. 187 472537P PRIVATE ROAD 472538N LITTLE AUGLAIZE SR 613 472539D PARK ST 472541E PRIVATE ROAD 472540X BROWN TWP. 165 472542L									
CURVATURE											
ALIGNMENT											
ELEVATION											
FREIGHT SPEED TANGENT (CURVE)		80									
GRADE		+0.14 -0.01 -0.04 -0.16 +0.03 +0.14 +0.03 +0.04 +0.06 -0.25 -0.06 +0.10 +0.06 +0.02 +0.03									

Sheet 92b33000

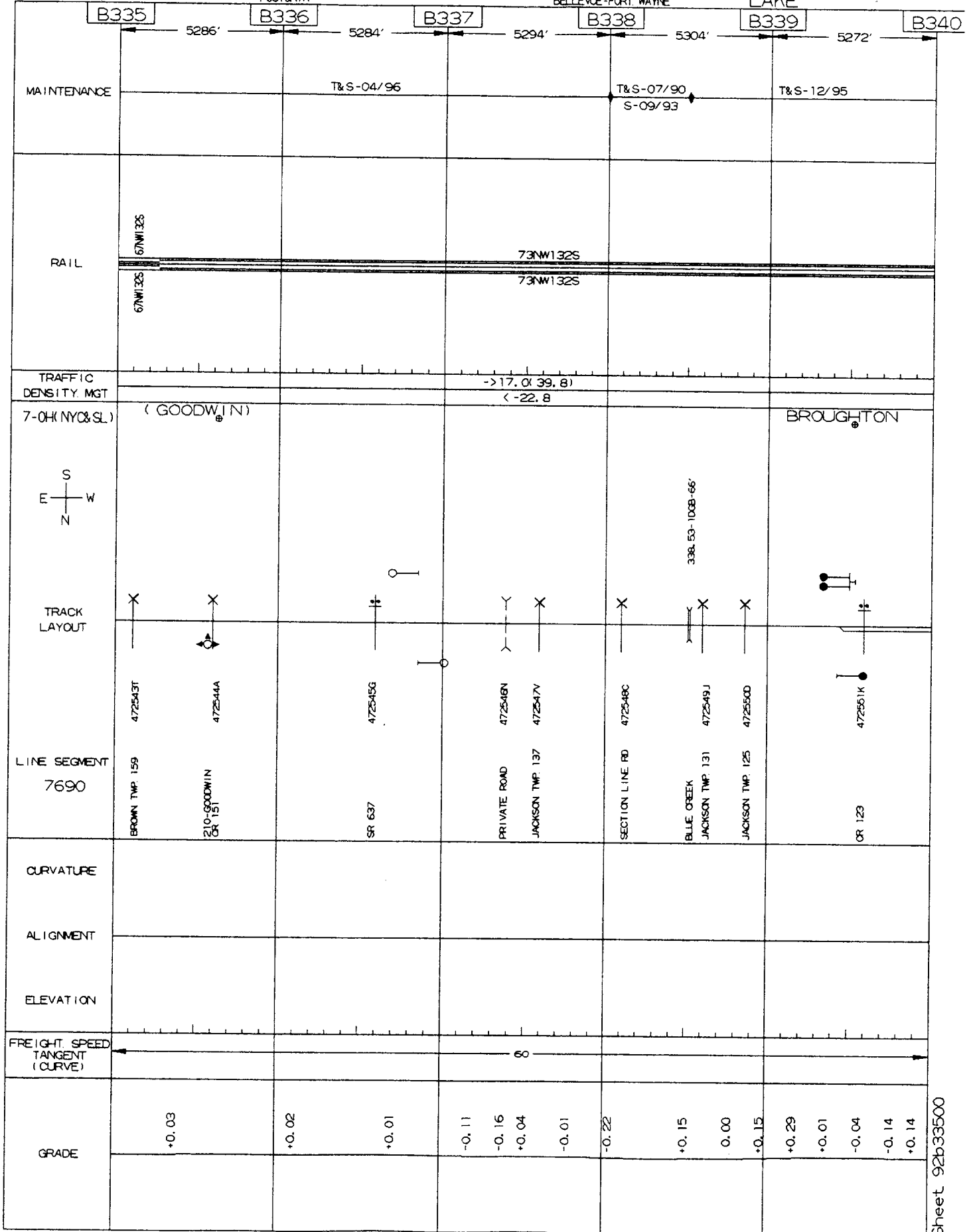


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FOSTORIA

BELLEVUE-FORT WAYNE

LAKE

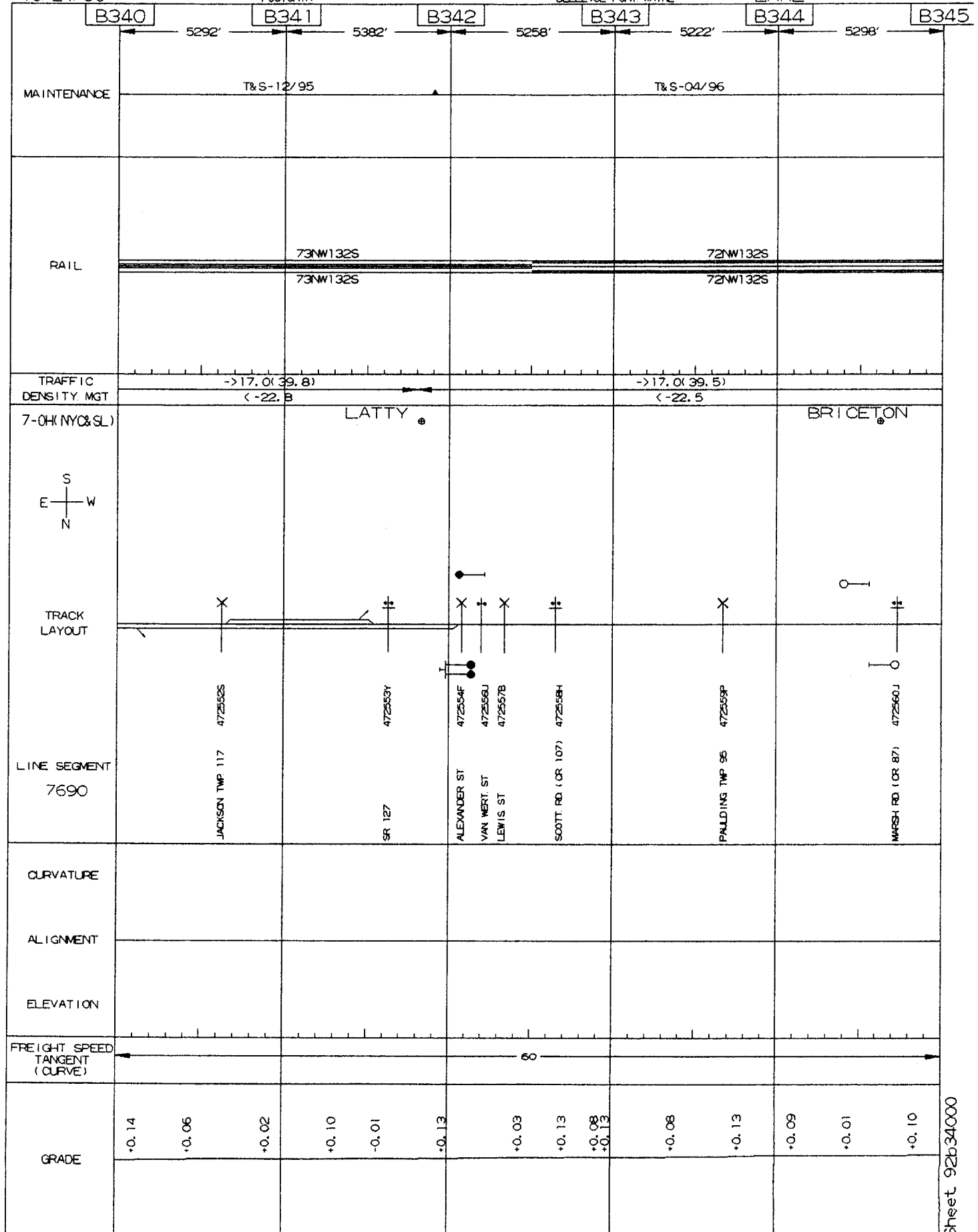


10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



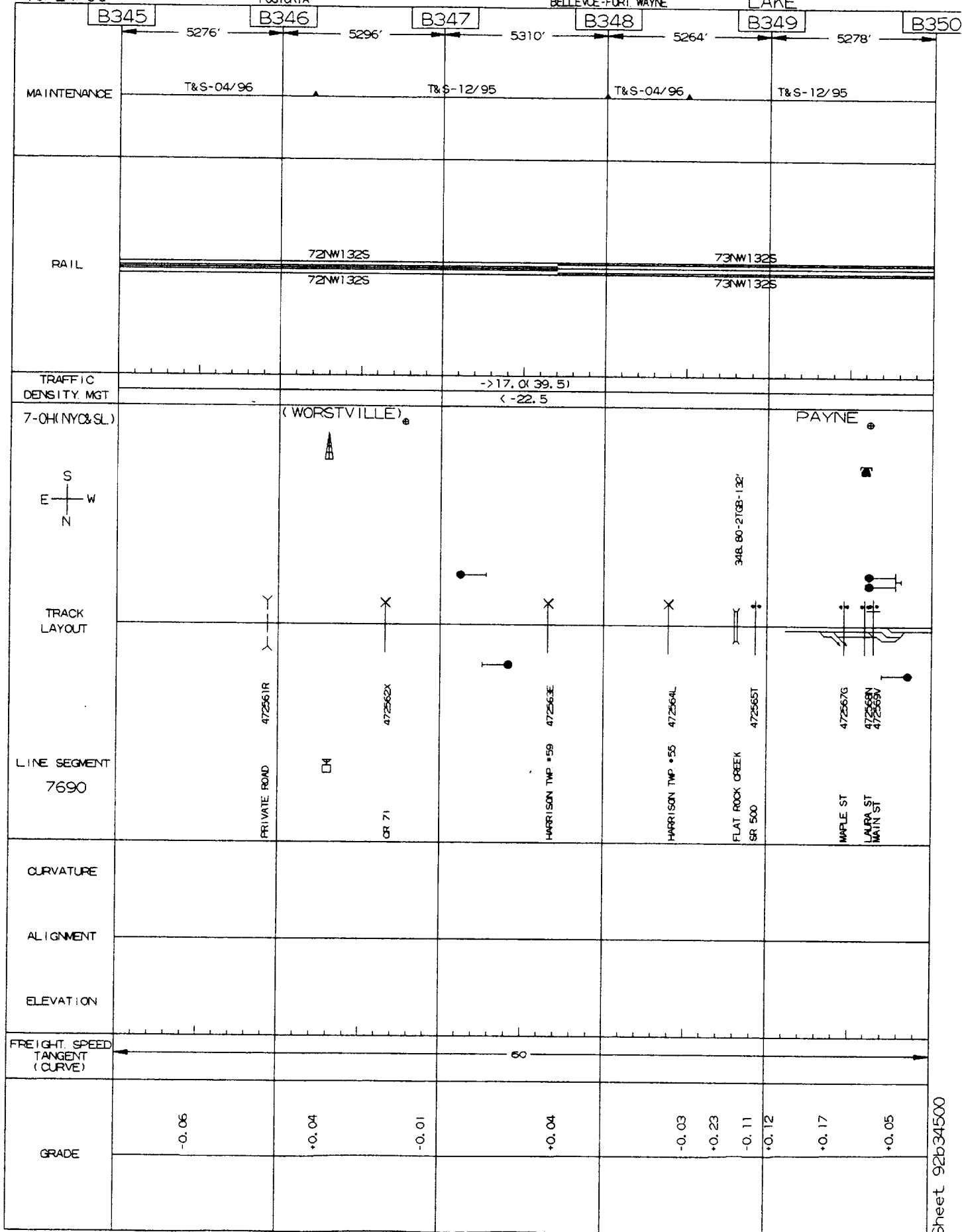
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10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



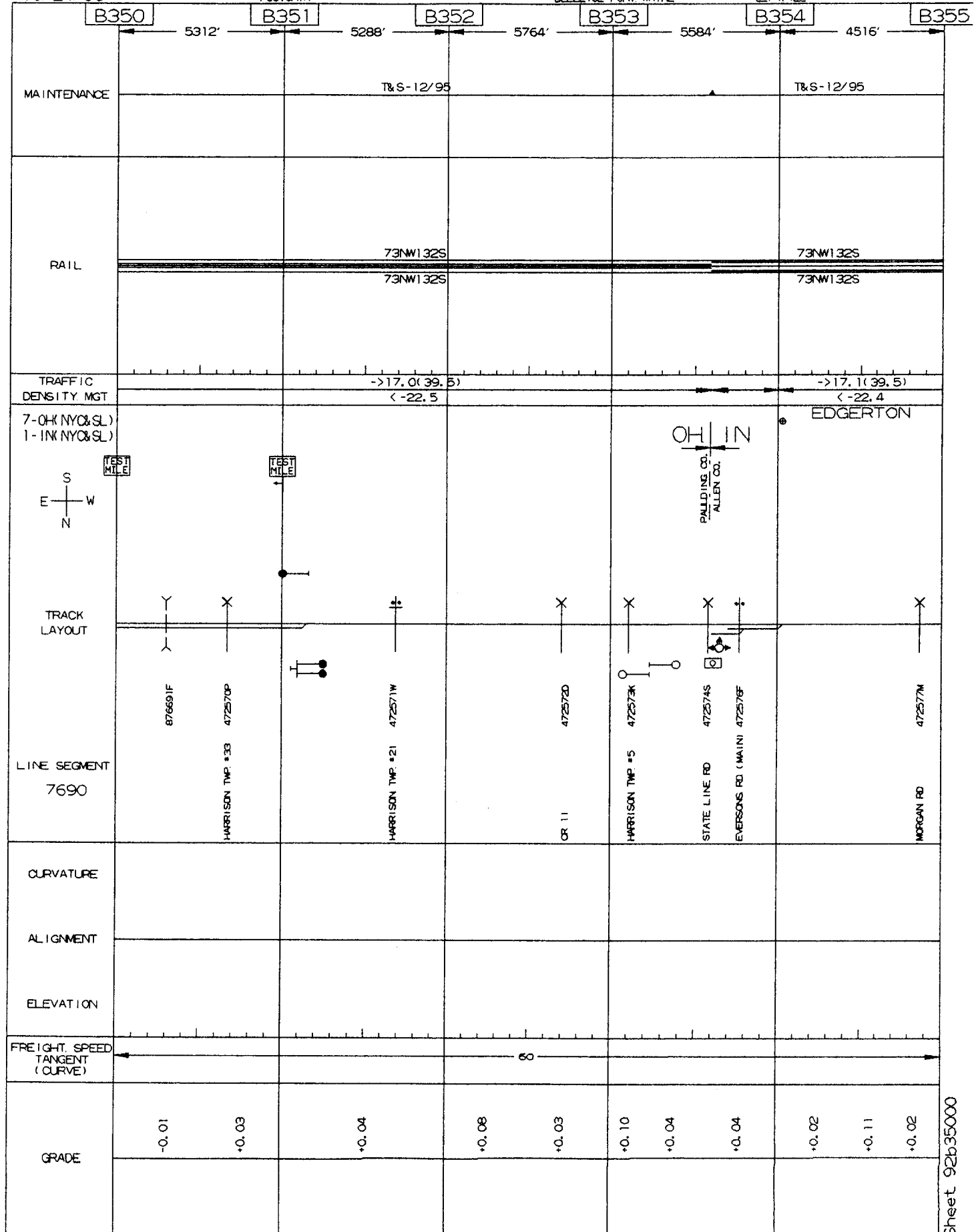
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10/24/96

FOSTORIA

BELLEVUE-FORT WAYNE

LAKE



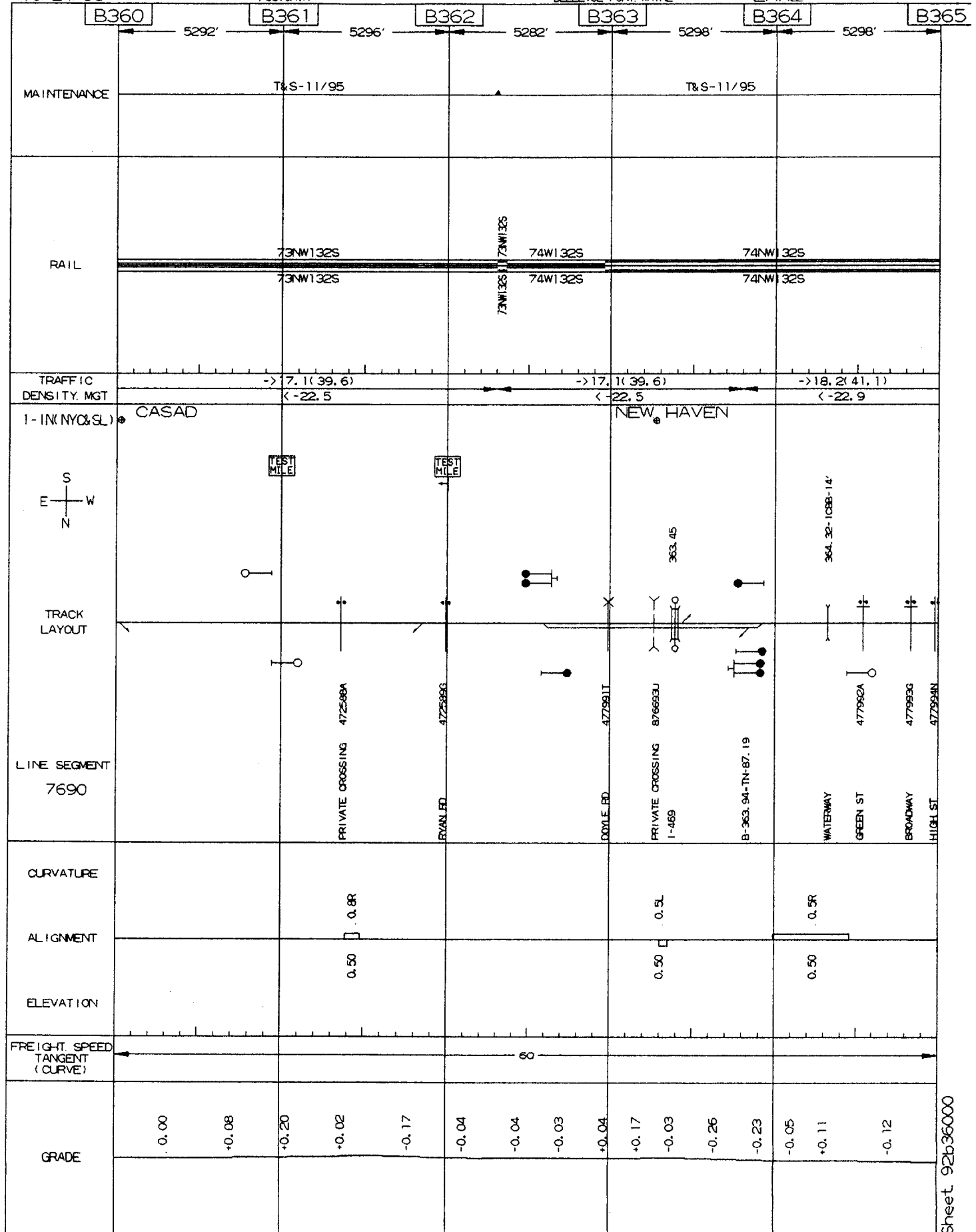
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10/24/96

FOSTORIA

BELLEVUE-FORT. WAYNE

LAKE



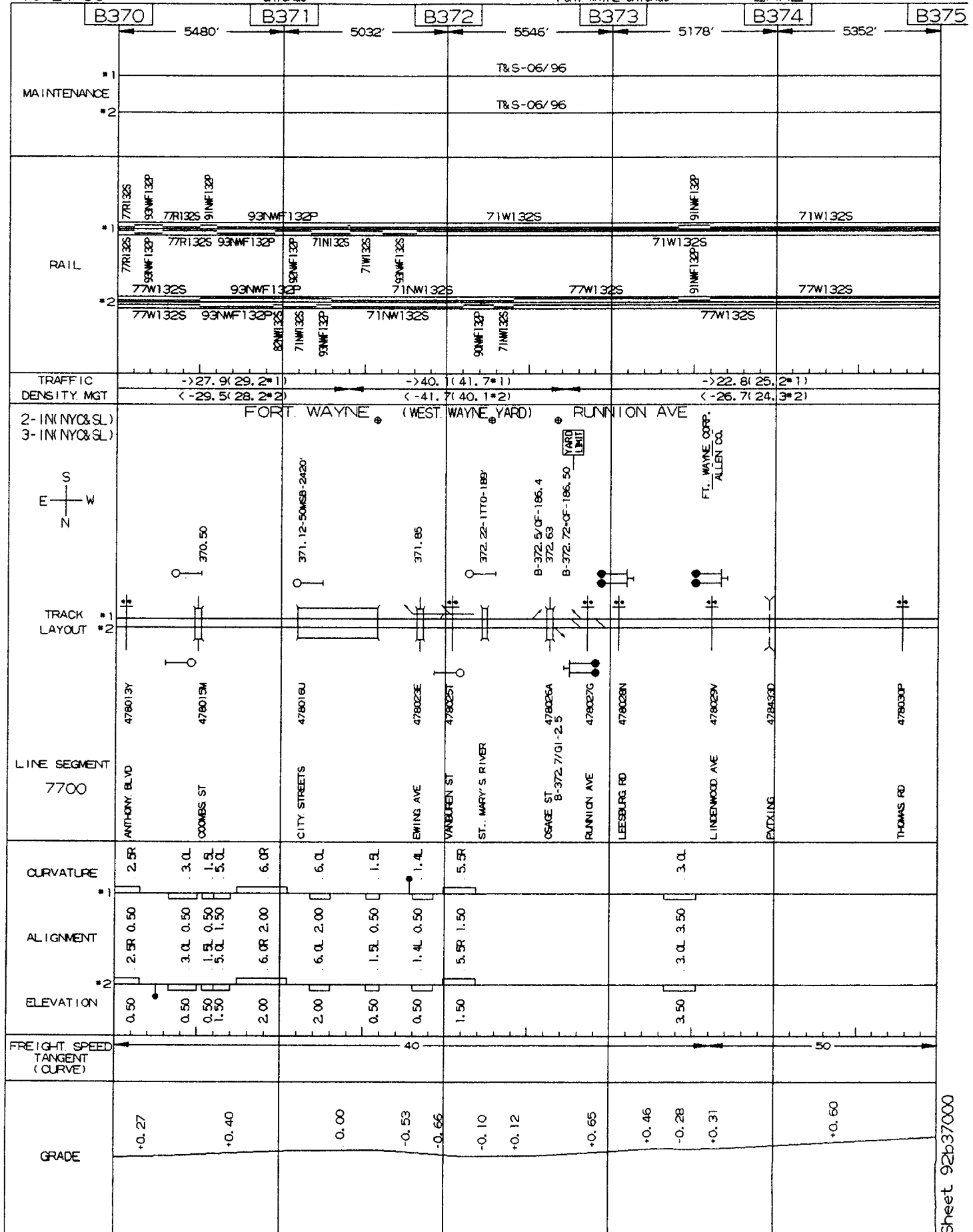
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10/24/96

CHICAGO

FORT WAYNE-CHICAGO

LAKE



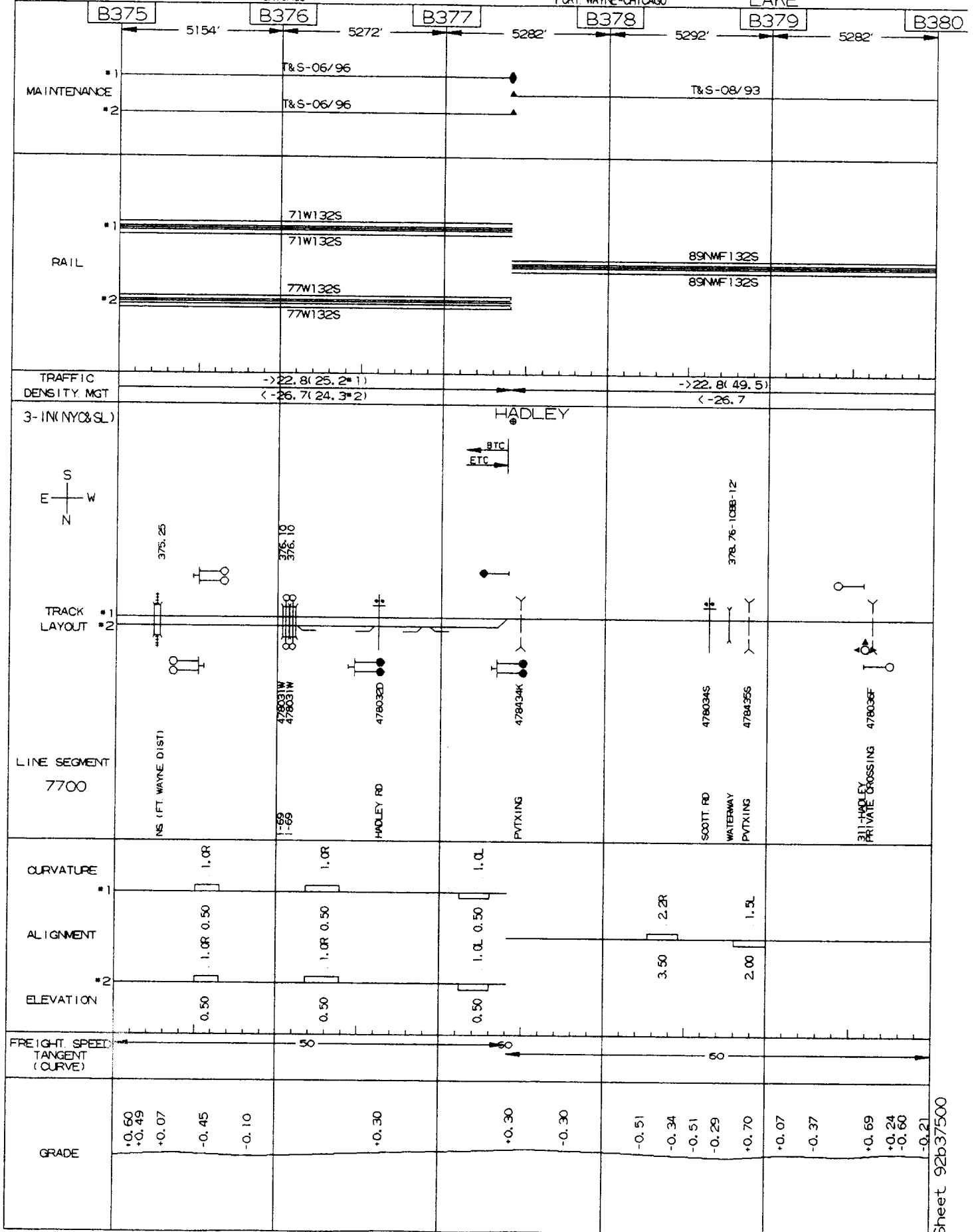


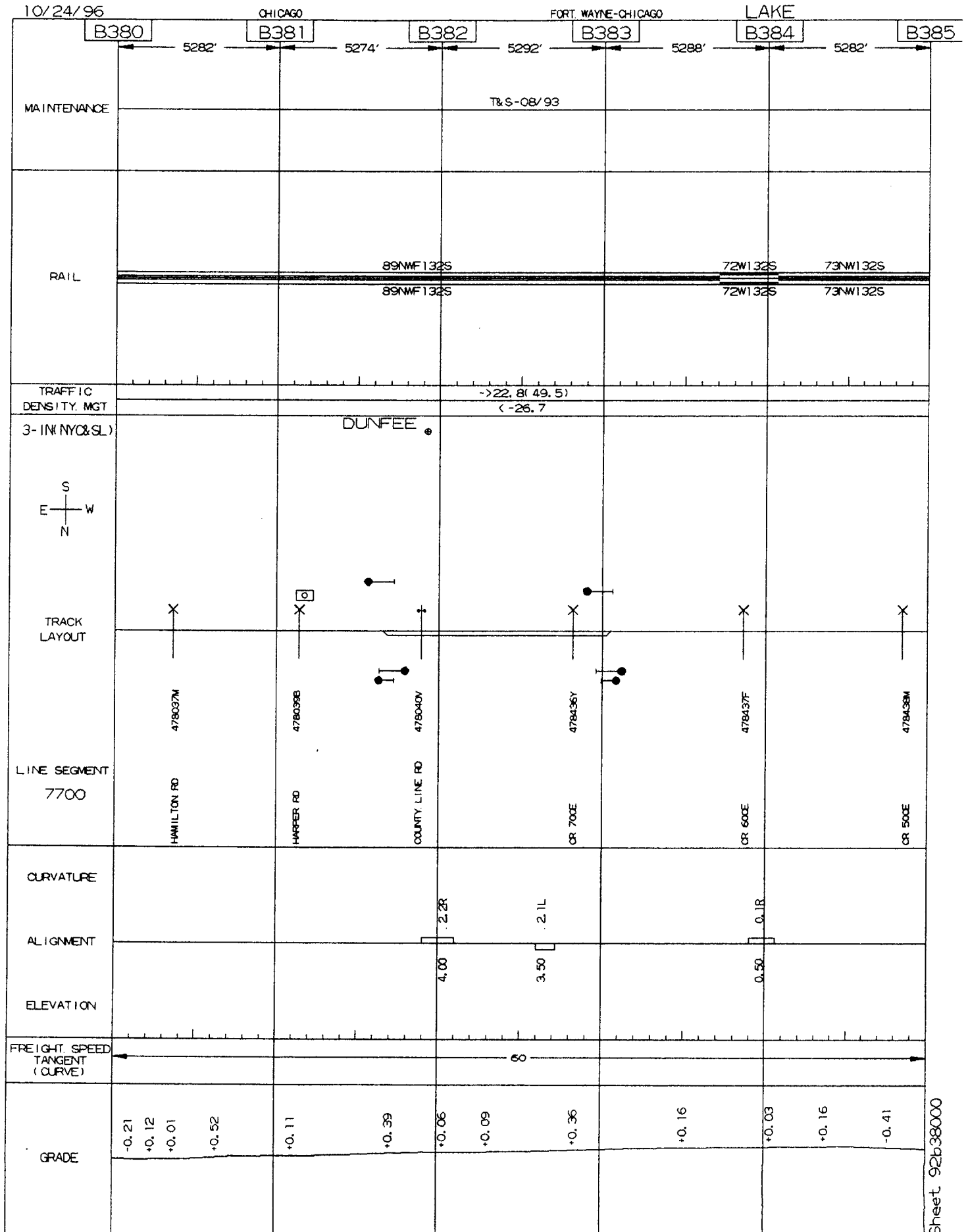
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CHICAGO

FORT WAYNE-CHICAGO

LAKE



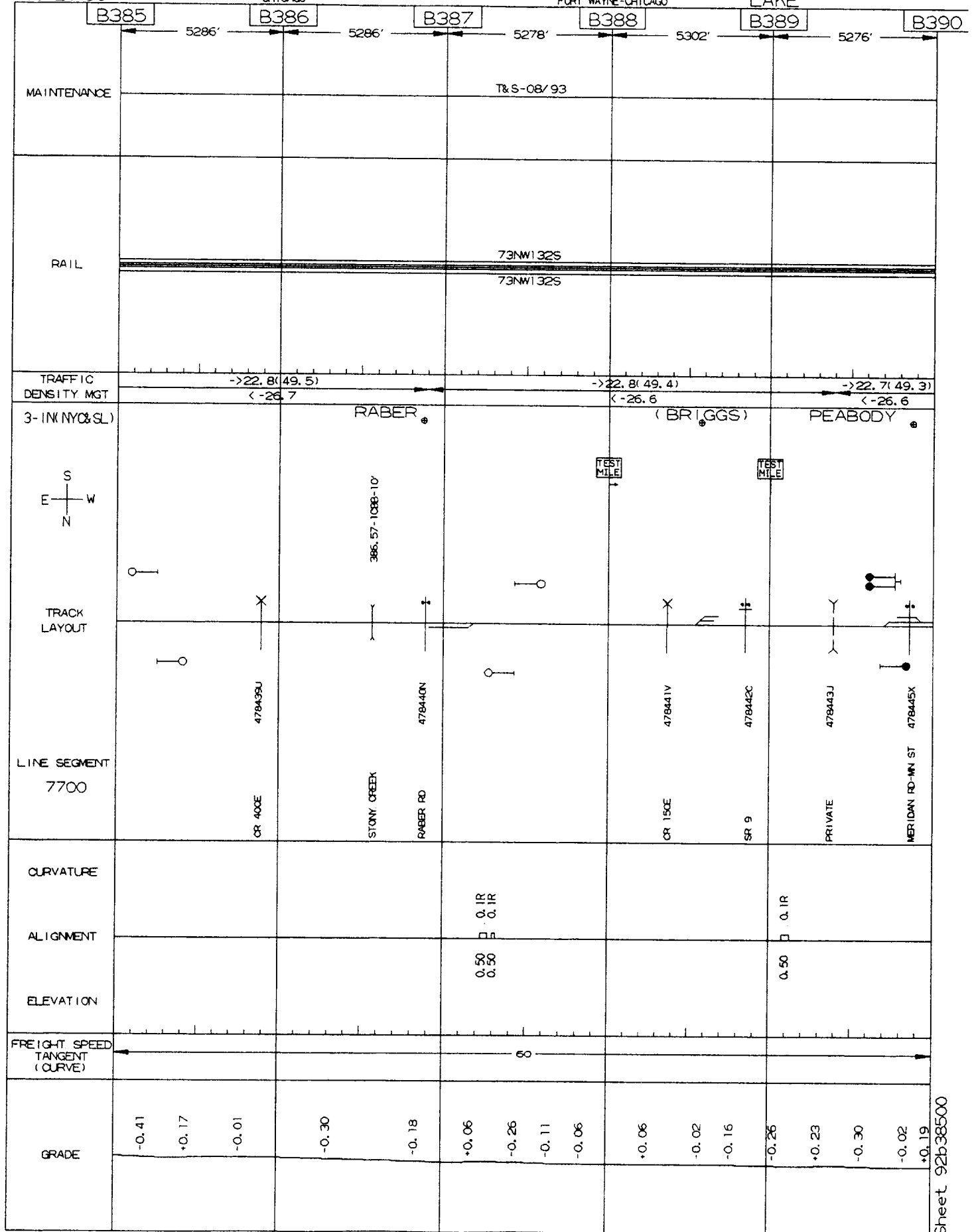


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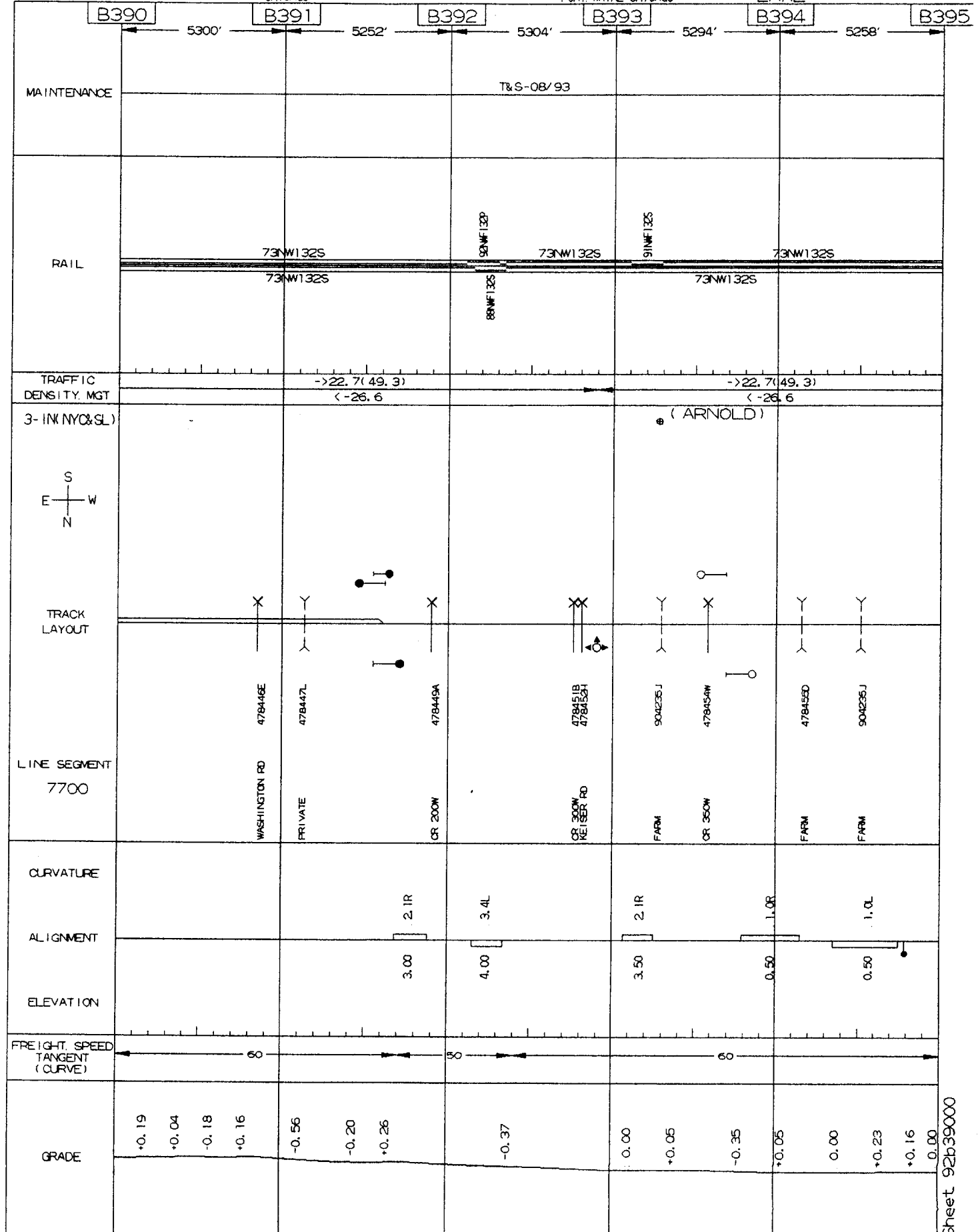


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FORT. WAYNE-CHICAGO

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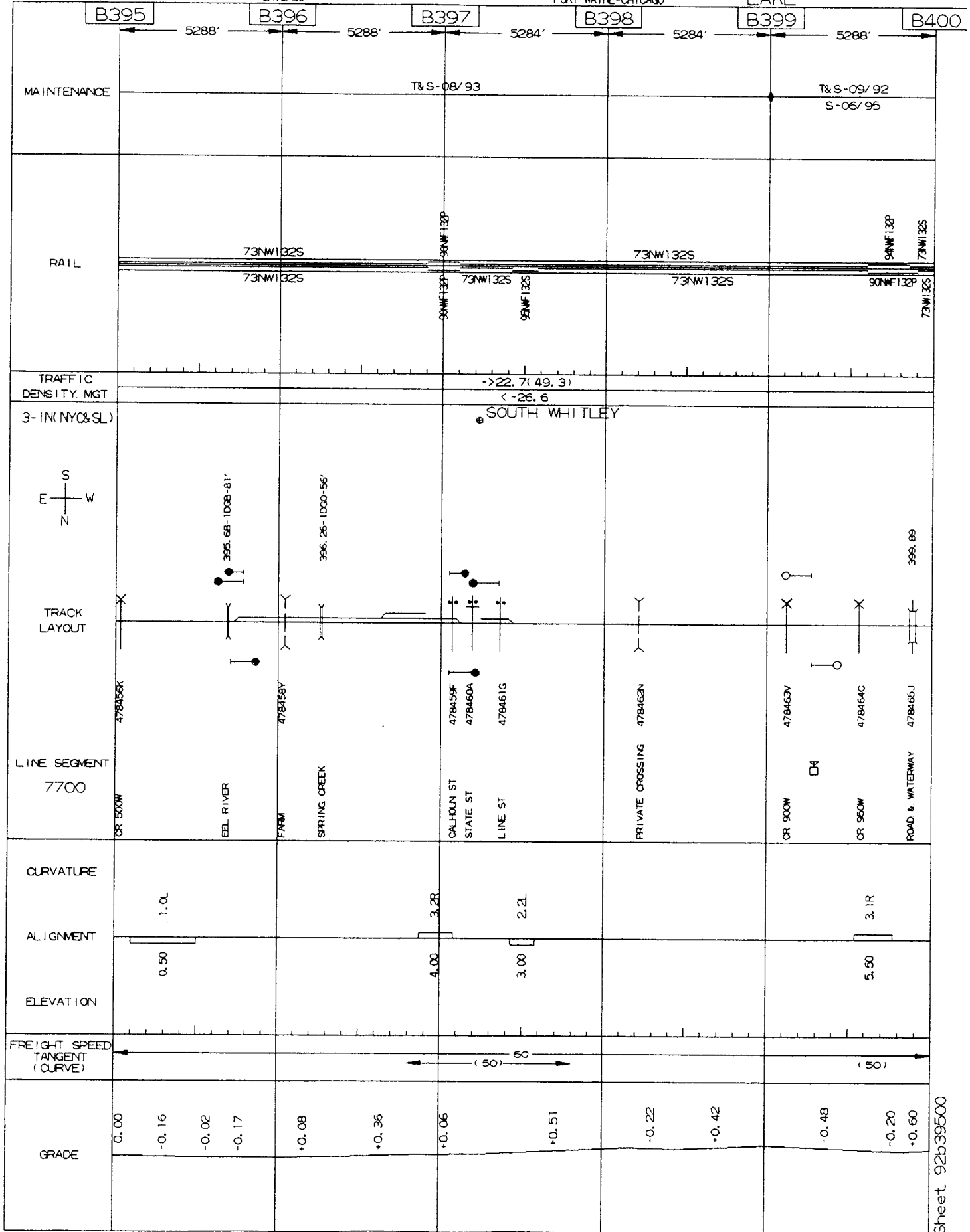


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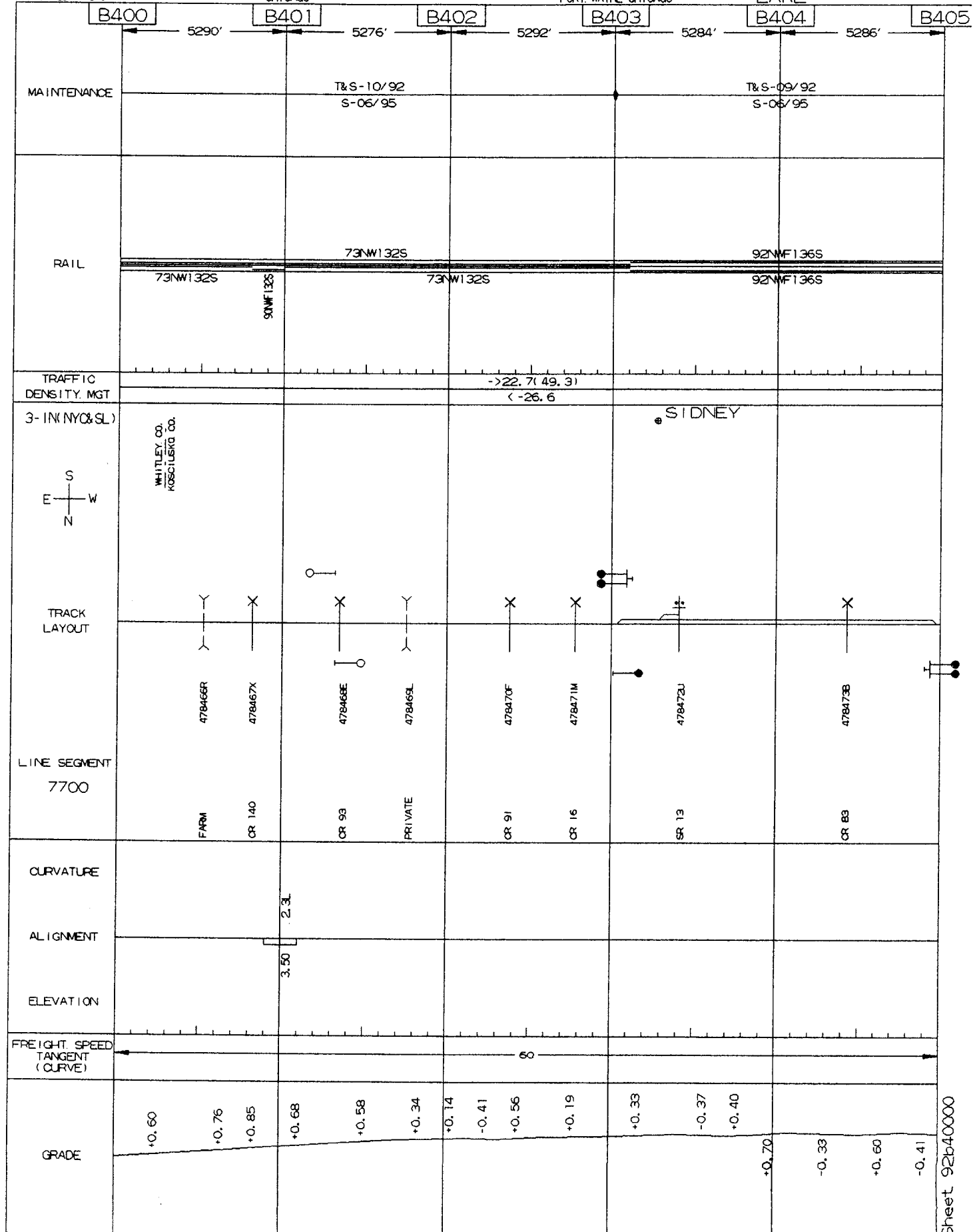


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FORT. WAYNE-CHICAGO

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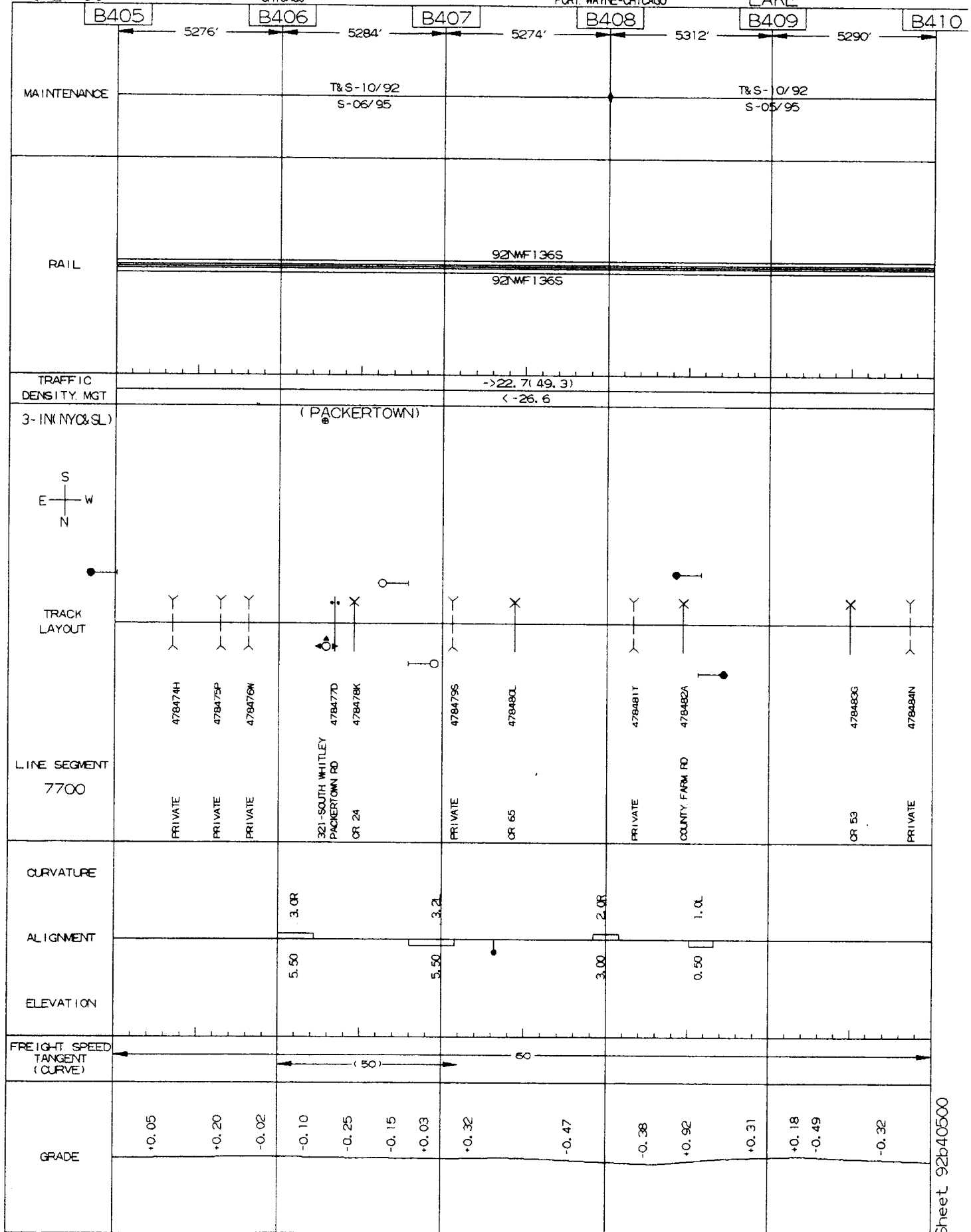


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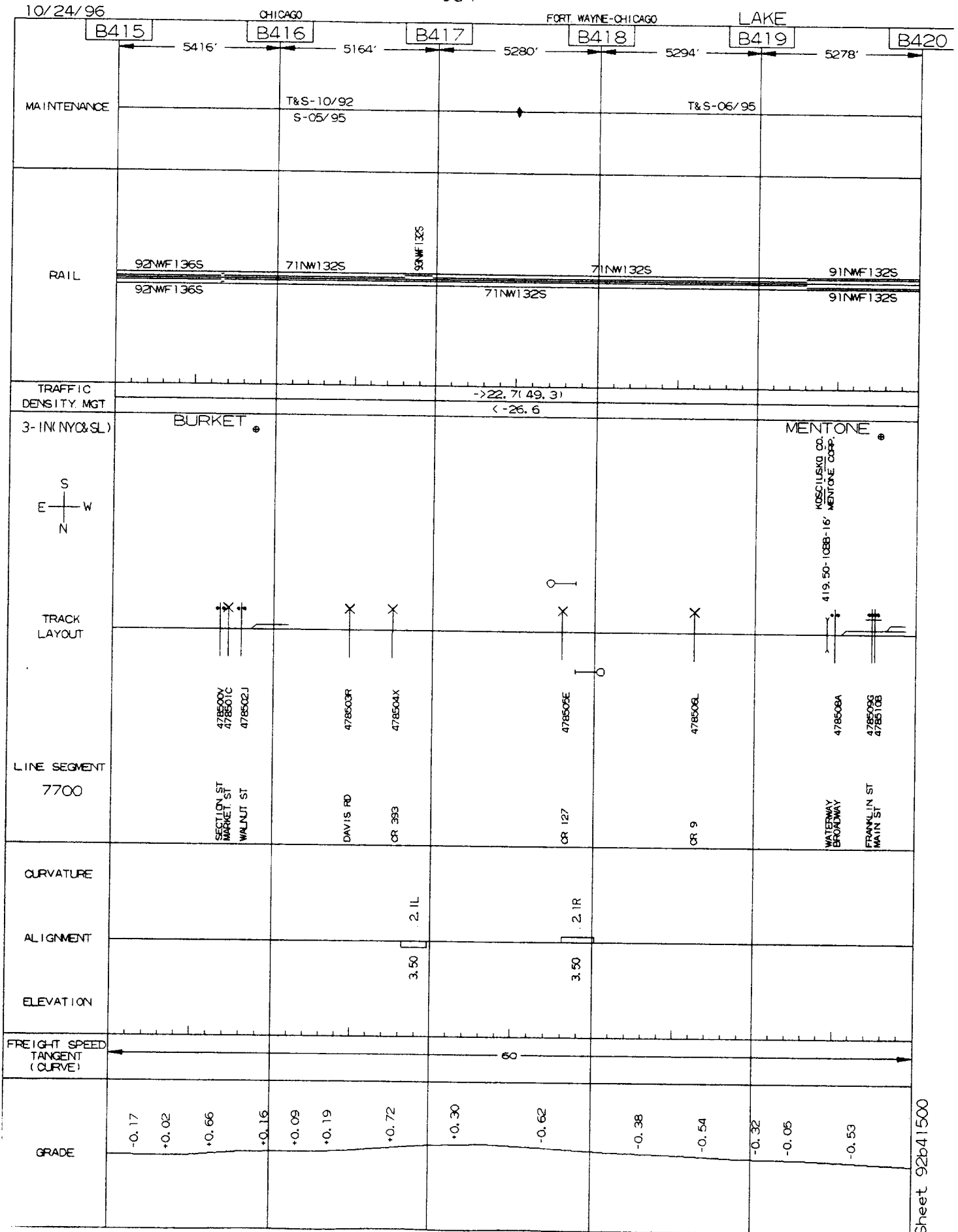
FORT WAYNE-CHICAGO

LAKE



Sheet 92b41000



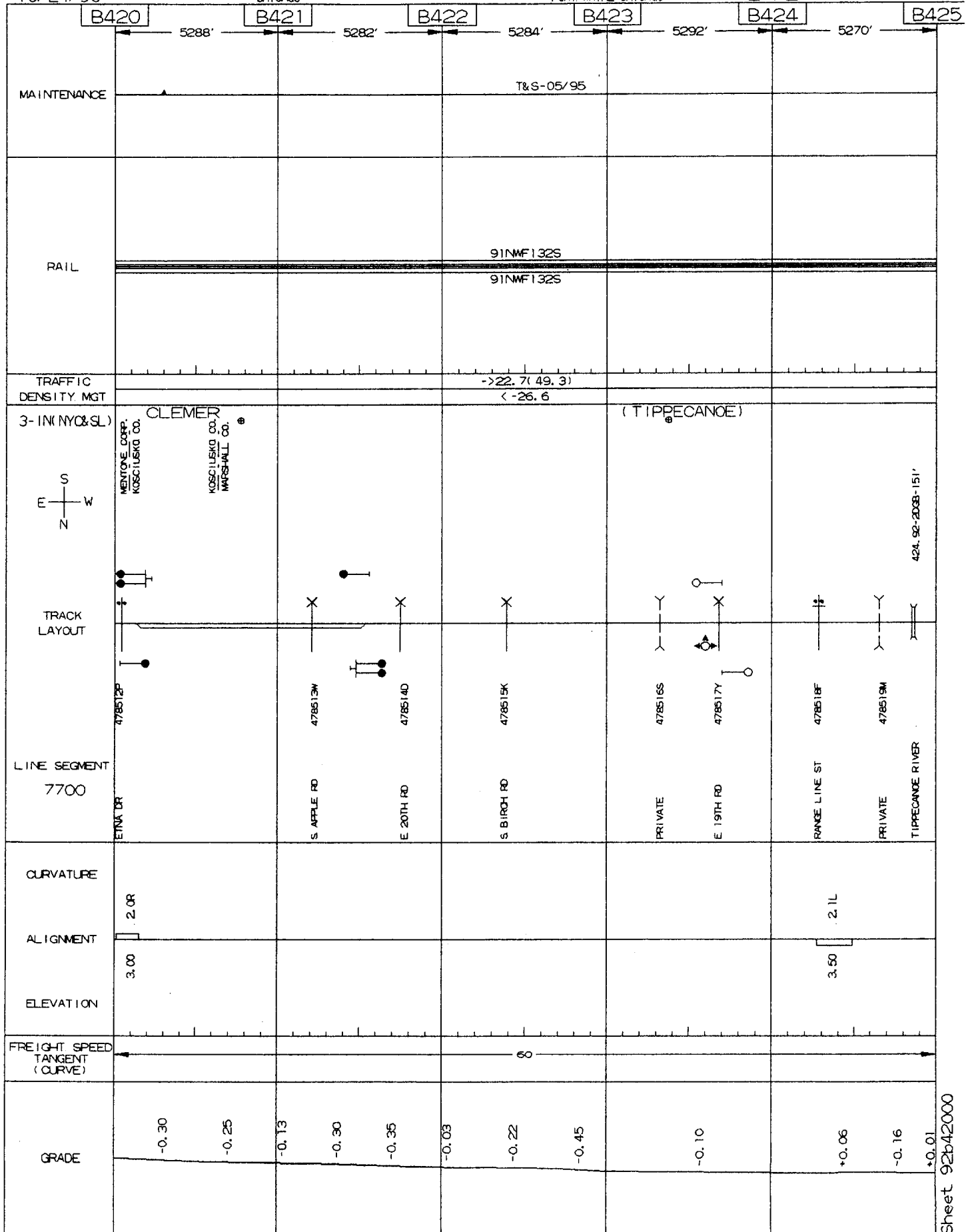


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CHICAGO

FORT WAYNE-CHICAGO

LAKE

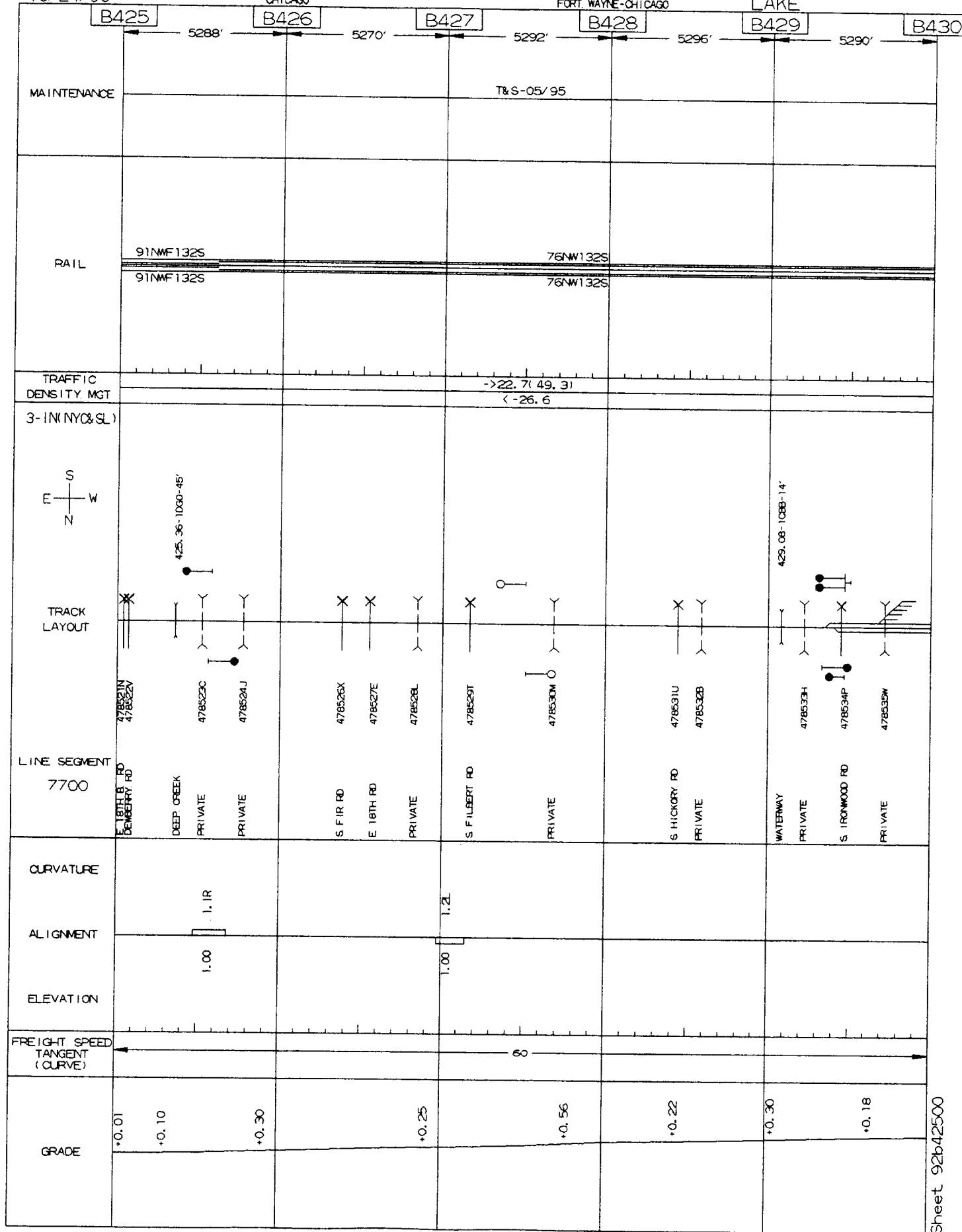


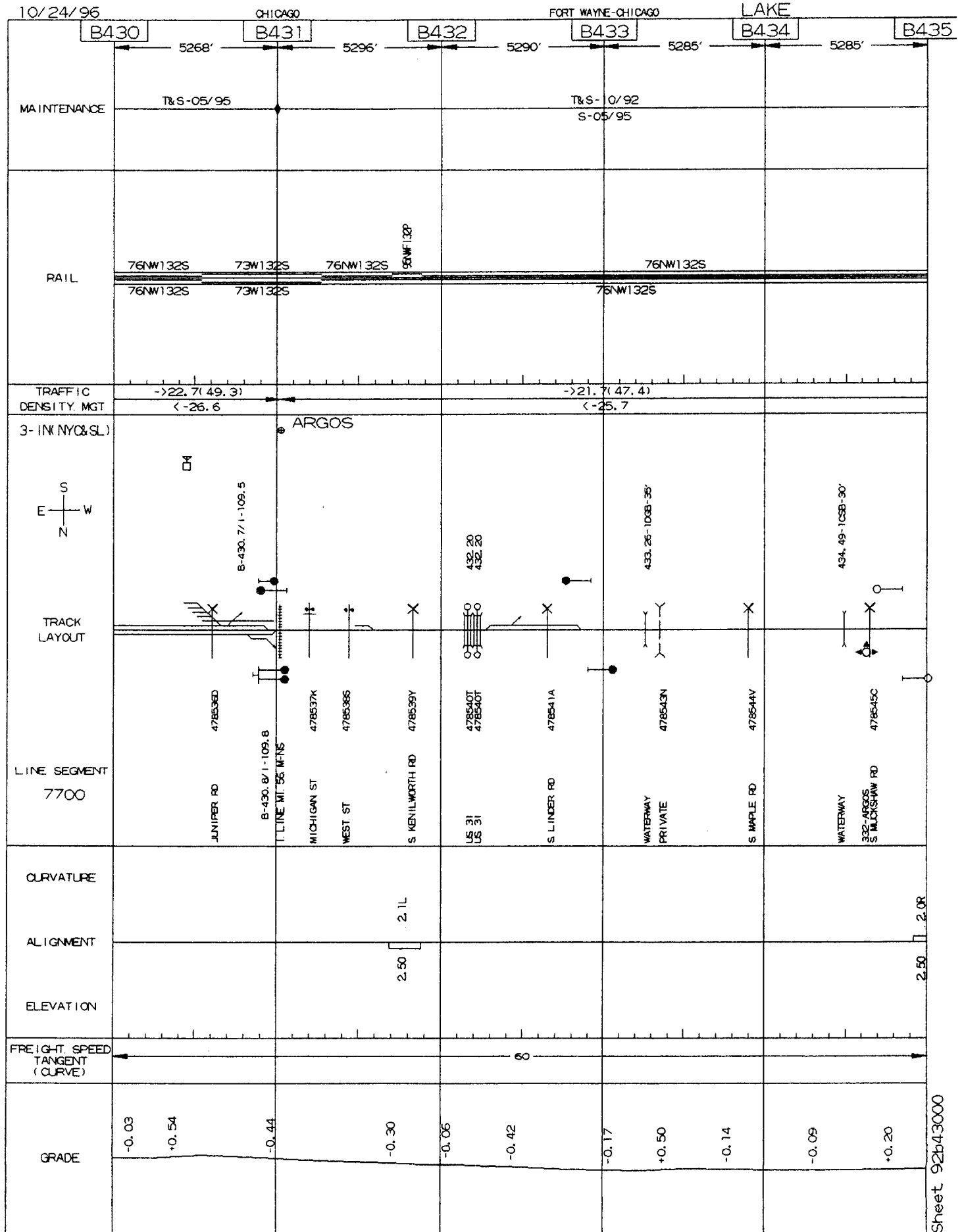
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FORT. WAYNE-CHICAGO

LAKE



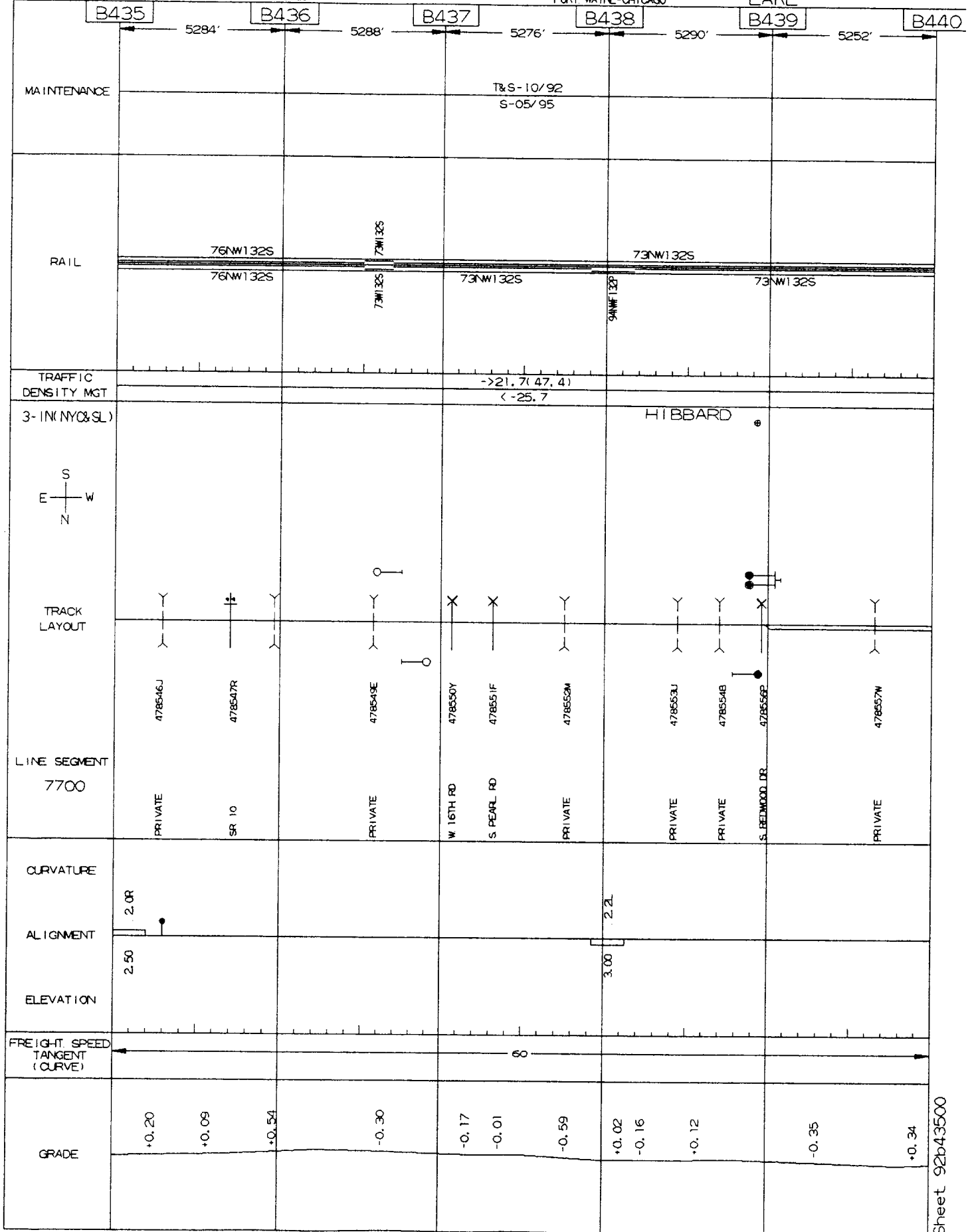


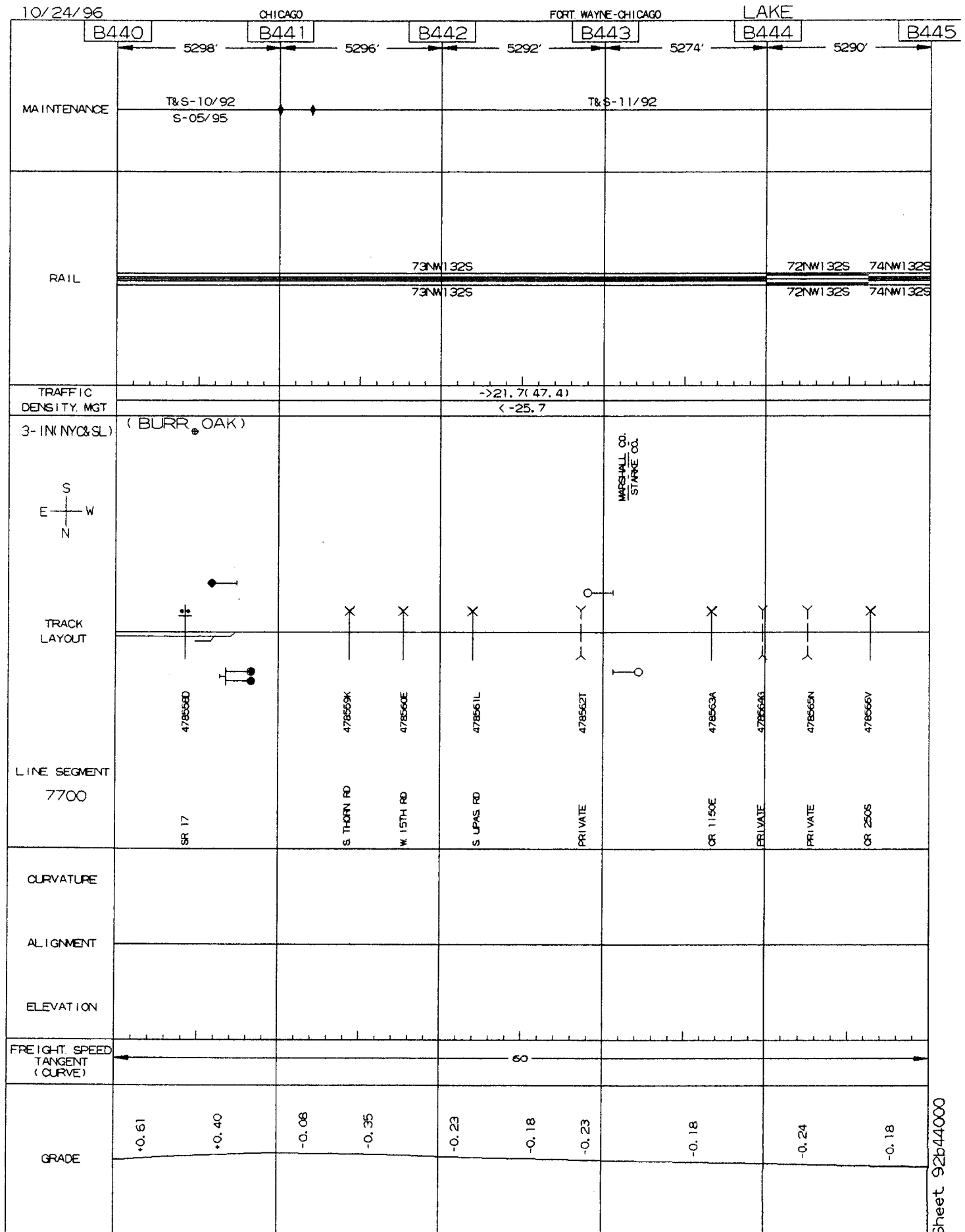
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CHICAGO

FORT WAYNE-CHICAGO

LAKE





090

10/24/96

CHICAGO

FORT WAYNE-CHICAGO

LAKE

	B445	B446	B447	B448	B449	B450
	5272'	5300'	5298'	5278'	5288'	
MAINTENANCE			T&S-11/92			
RAIL			74NW132S			
			74NW132S			
TRAFFIC DENSITY MGT			->21.7 (47.4)			
			< -25.7			
3-IN(NYC&SL)						
<div style="text-align: center;">           S                         E — W                         N         </div>						
TRACK LAYOUT	X	X	+	X	X	+
	478567C	478569R	478570K	478571S	478572Y	478573F
	478574M				478575J	478576B
LINE SEGMENT	7700					
	OR 1000E	OR 225S	SR 23	OR 200S	OR 300E	OR 150S
					OR 700E	OR 600E
						SR 8
CURVATURE						
ALIGNMENT	1.00					
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			60			
GRADE	-0.18	+0.01	-0.19	+0.37	+0.08	-0.11
				-0.37		-0.16
					+0.02	-0.15
						-0.04
						+0.07

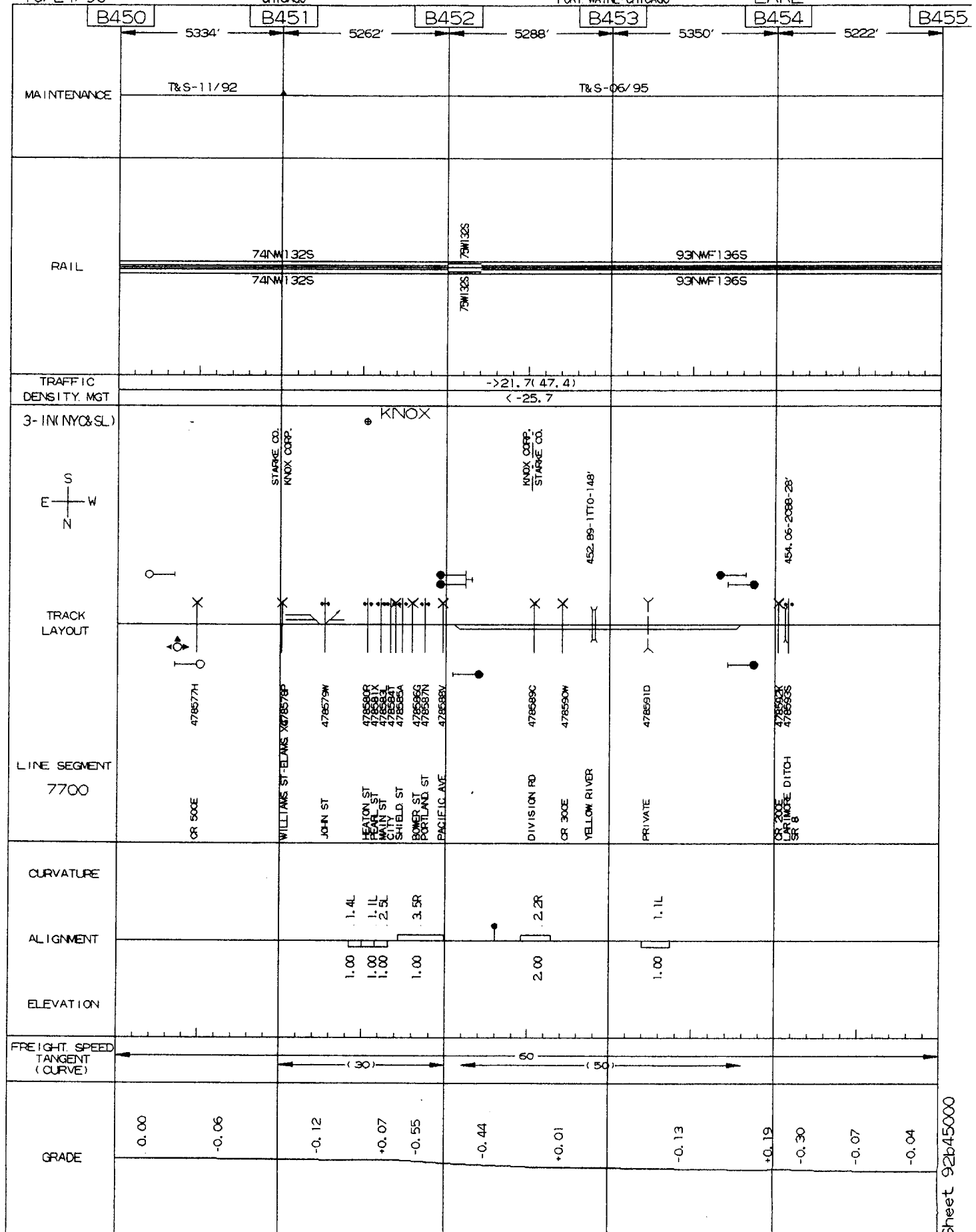
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10/24/96

CHICAGO

FORT WAYNE-CHICAGO

LAKE



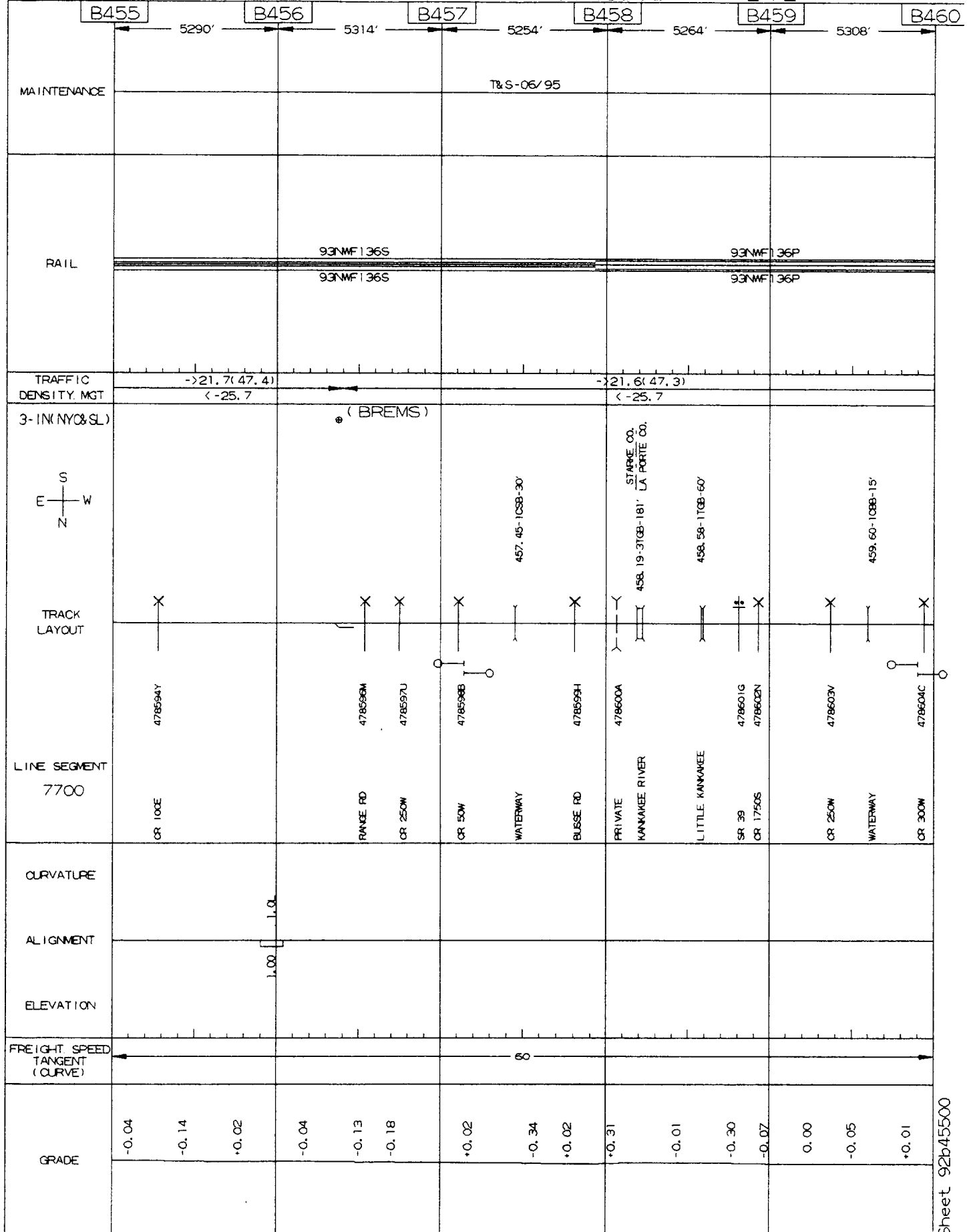


10/24/96

CHICAGO

FORT WAYNE-CHICAGO

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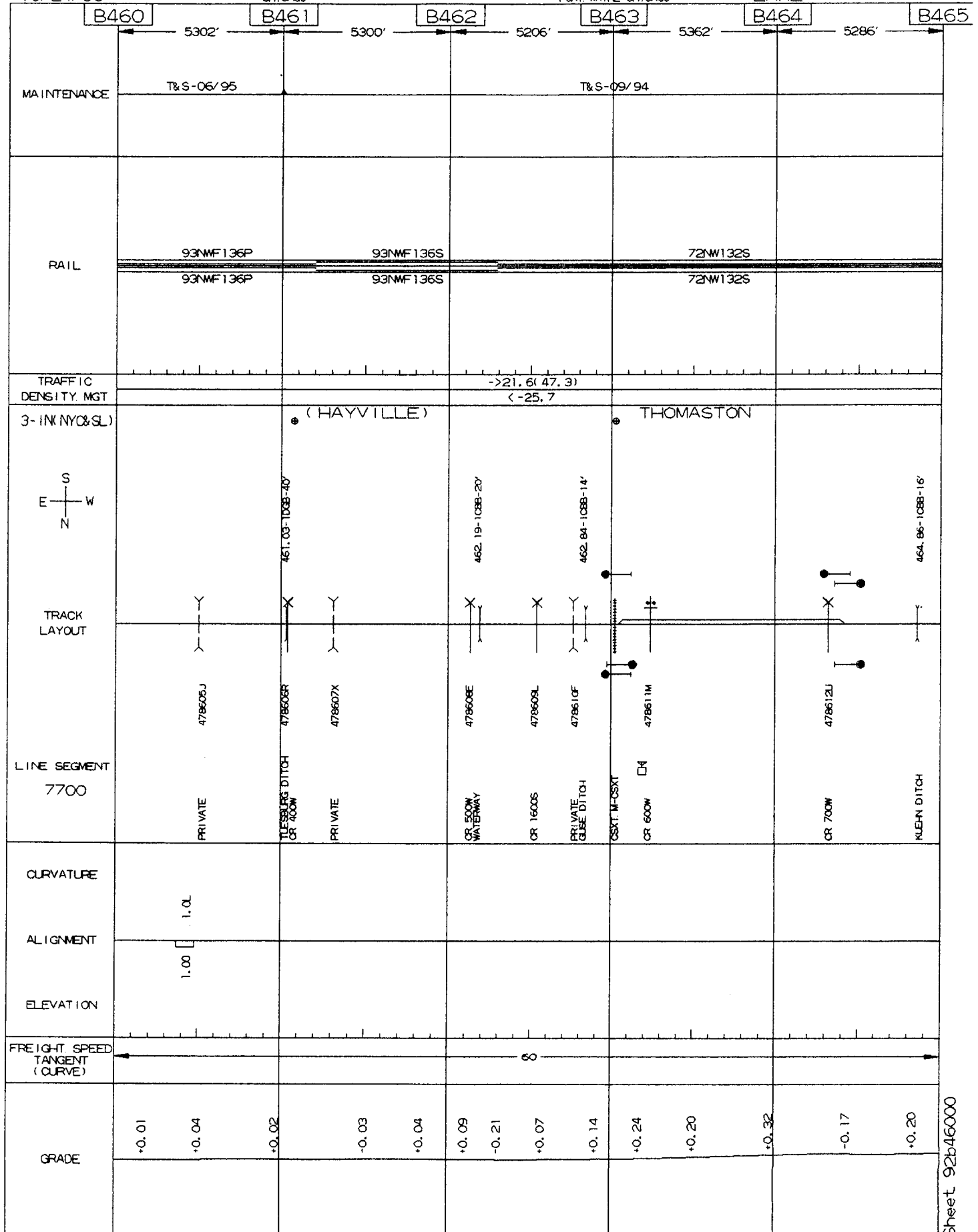


10/24/96

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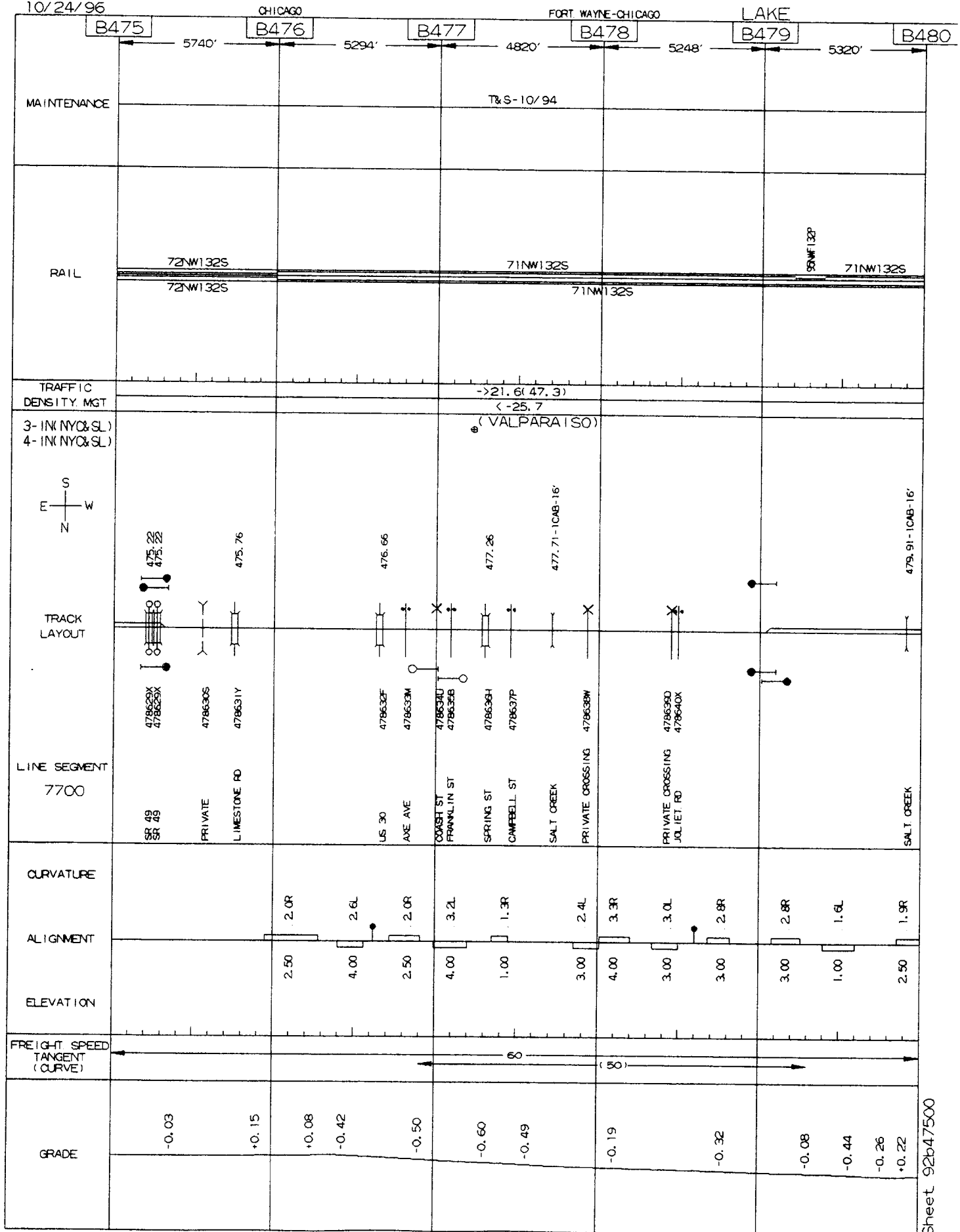
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Sheet 92b46500

Sheet 92b47000

10/24/96

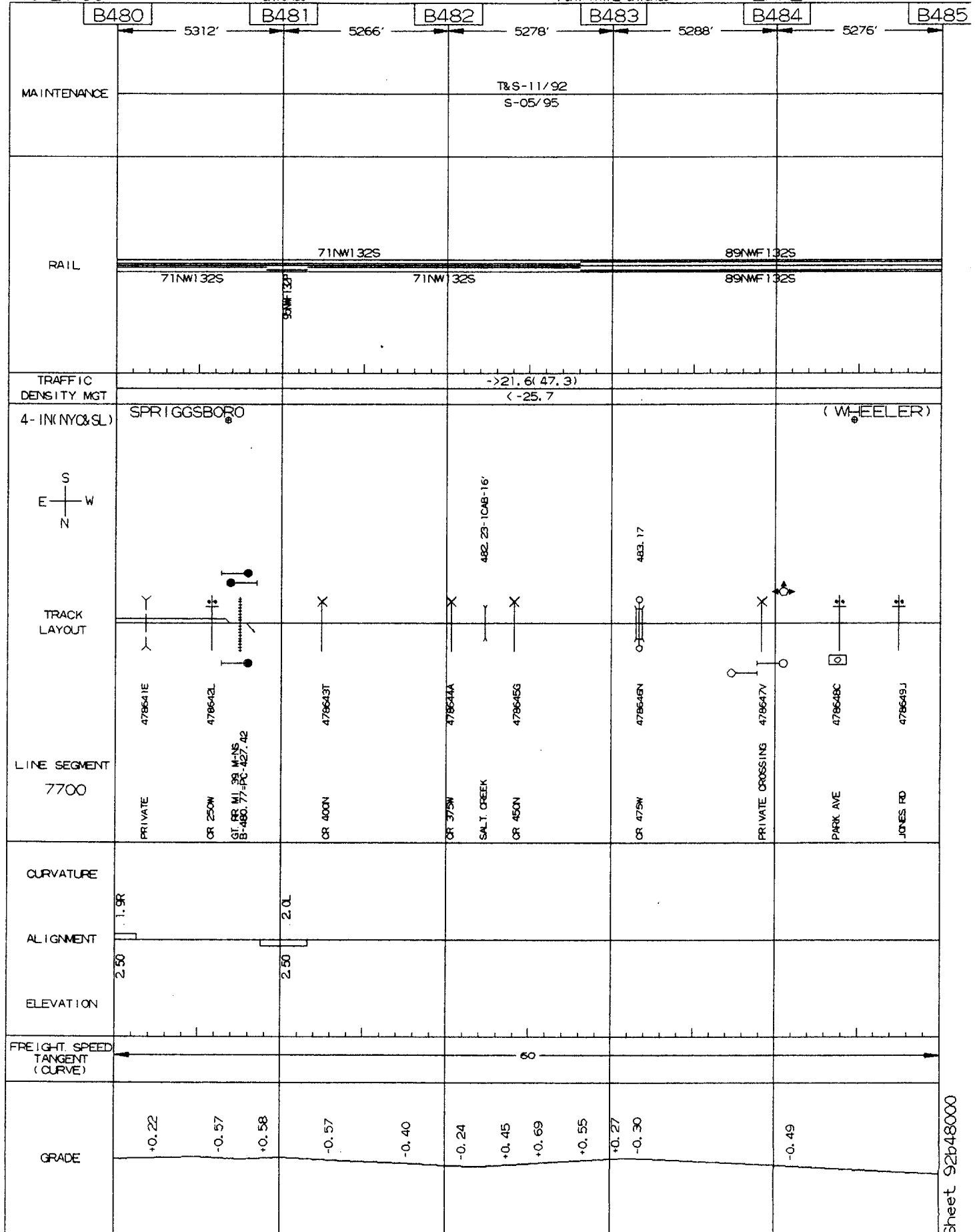


10/24/96

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FORT WAYNE-CHICAGO

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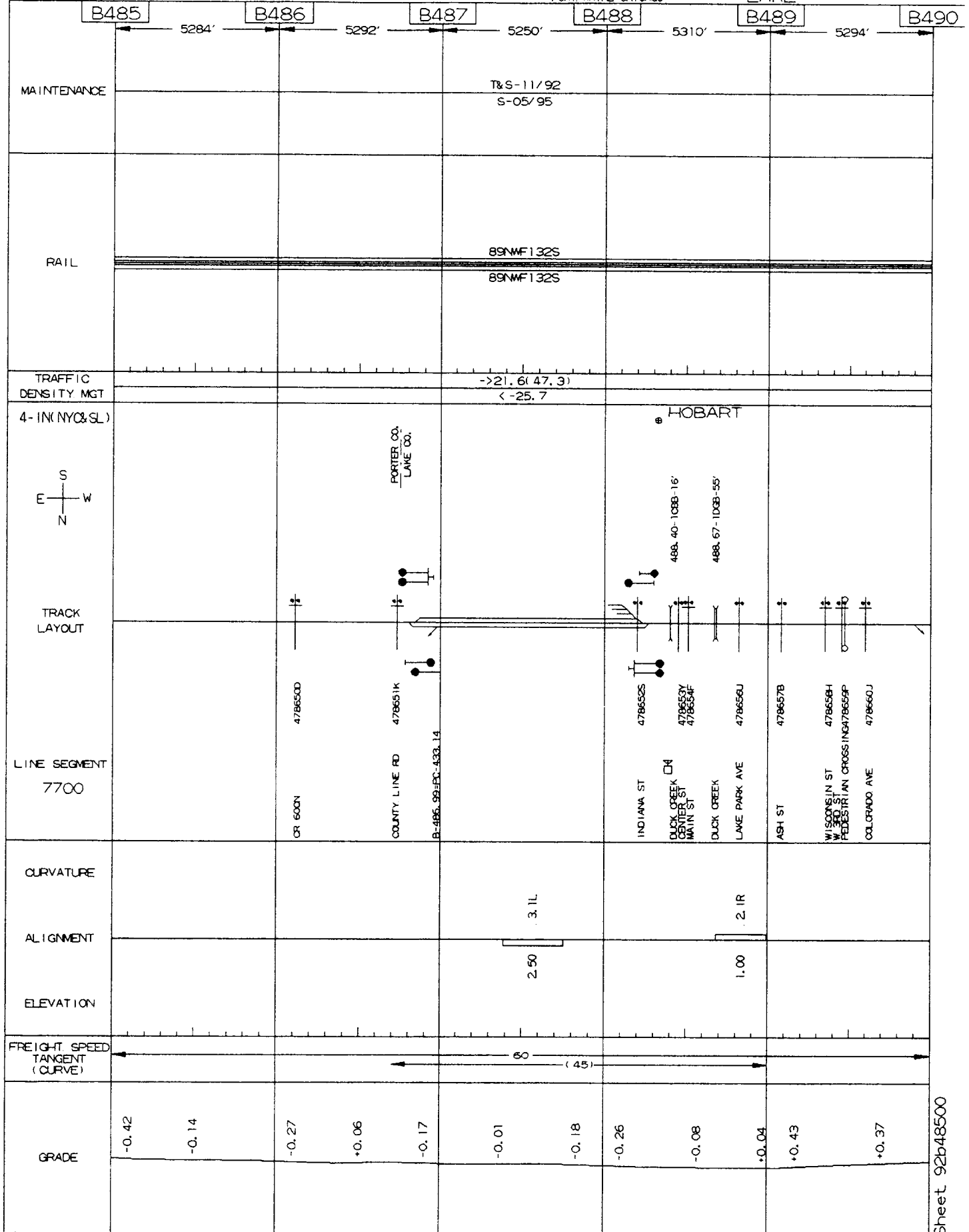


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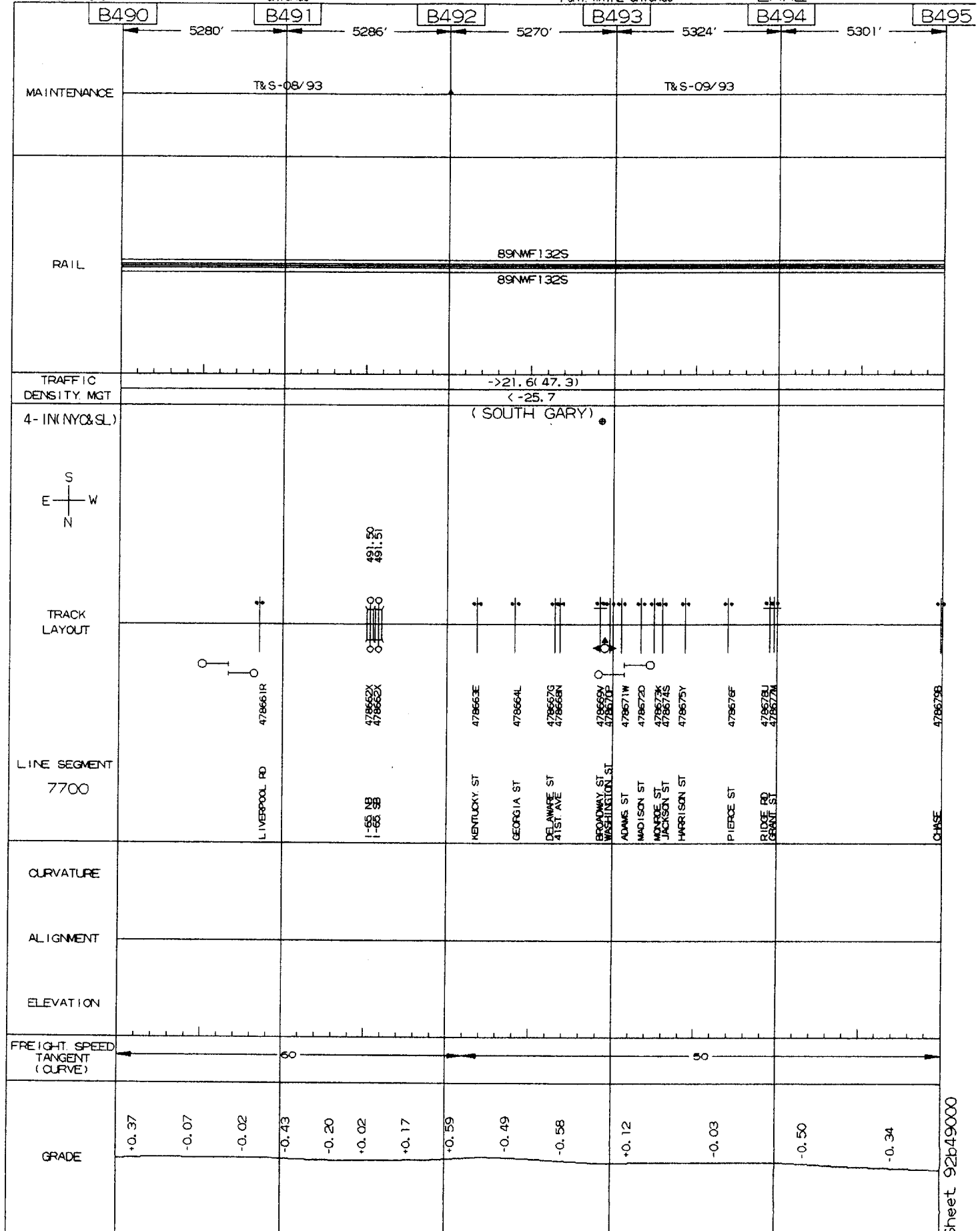


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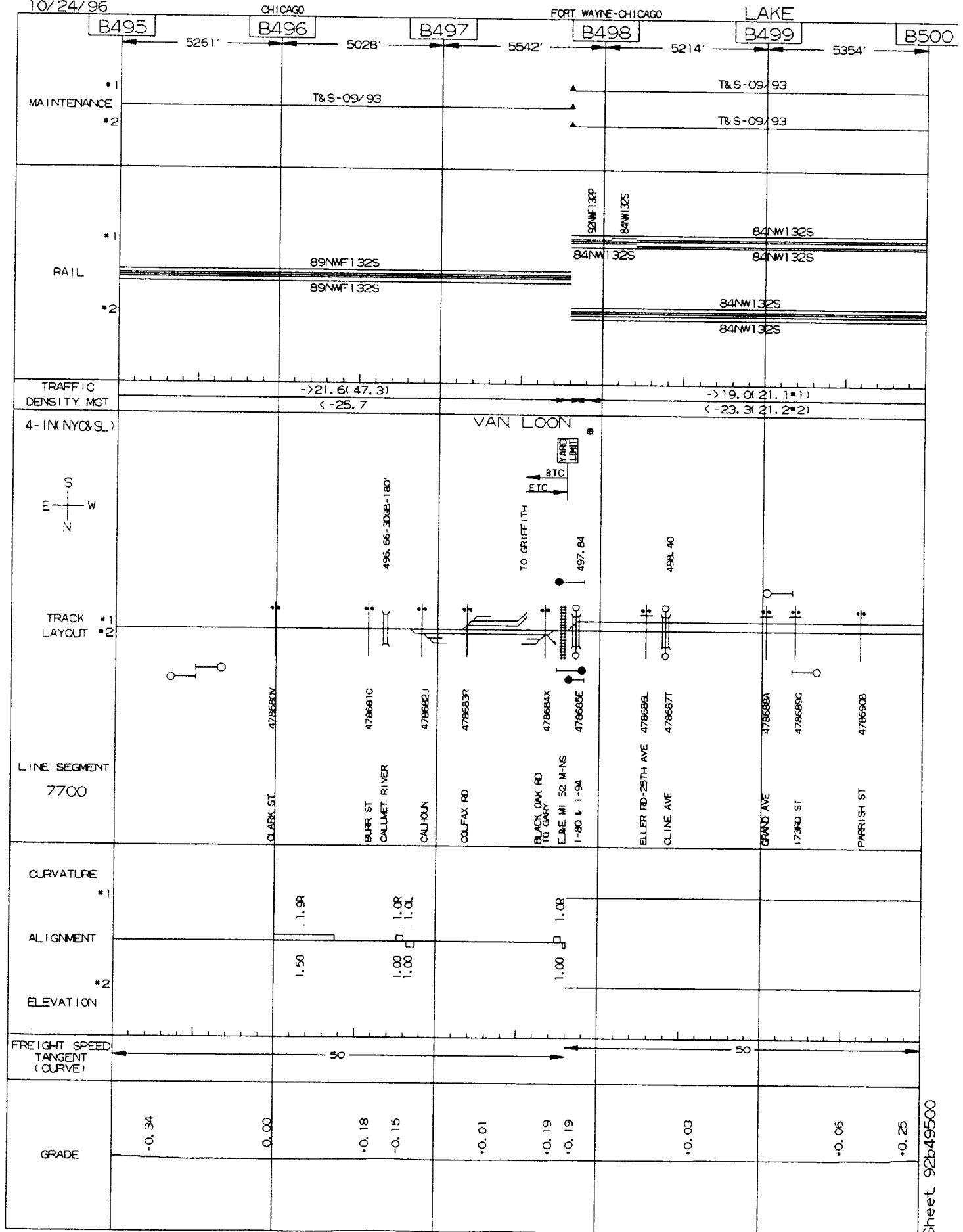
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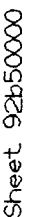
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10/24/96



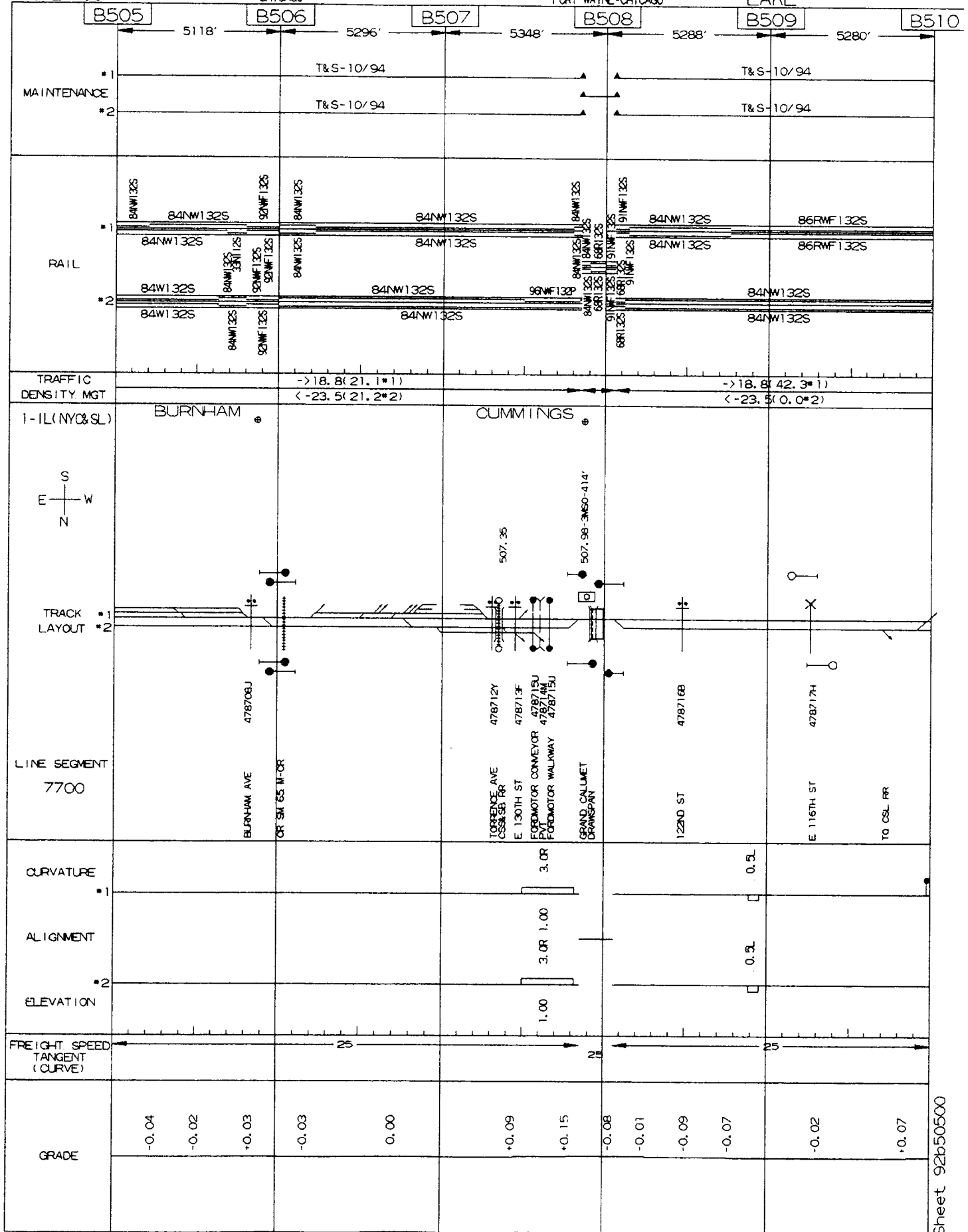


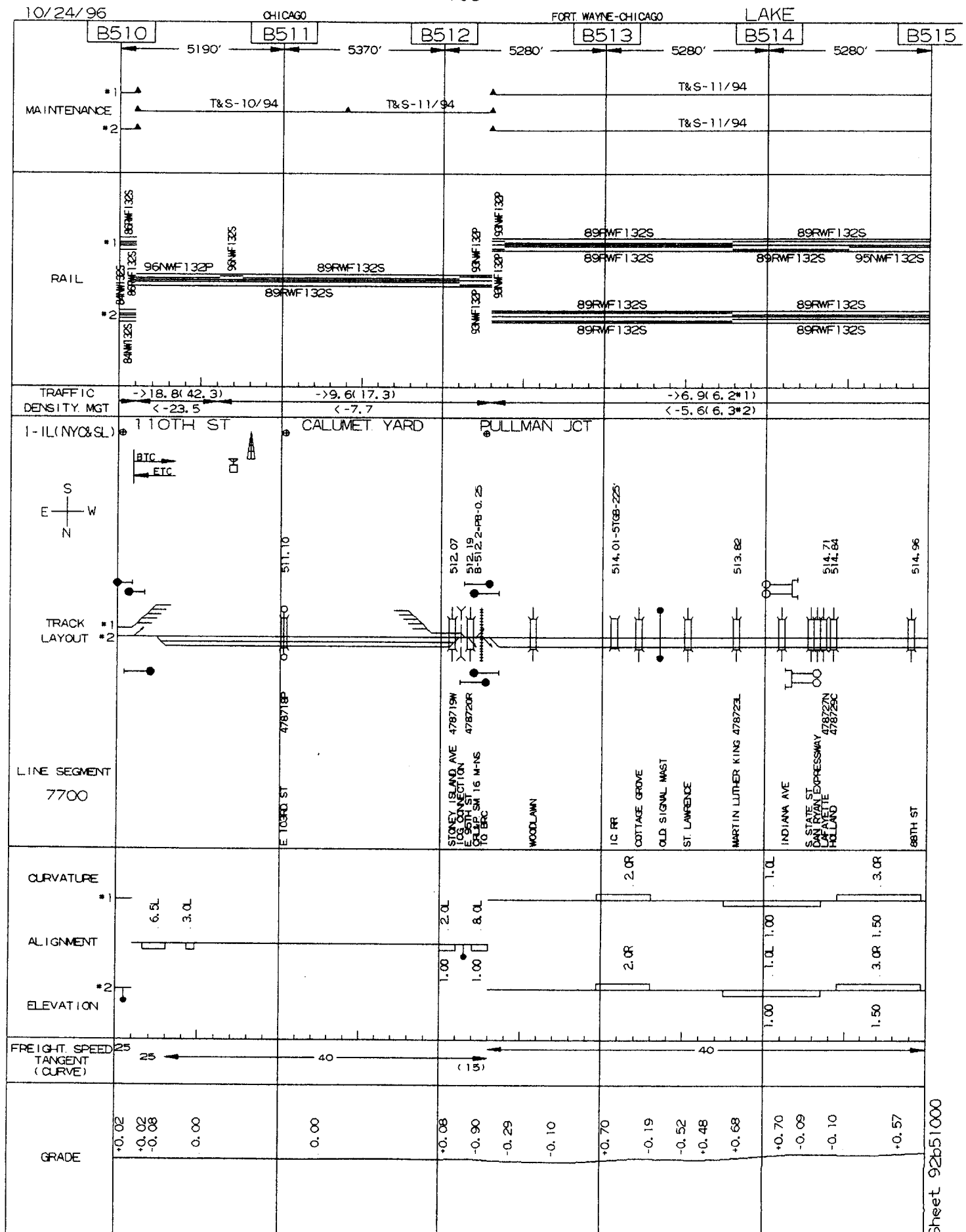
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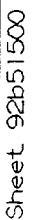
CHICAGO

FORT WAYNE-CHICAGO

LAKE







Sheet 92pb00000

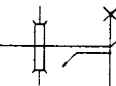
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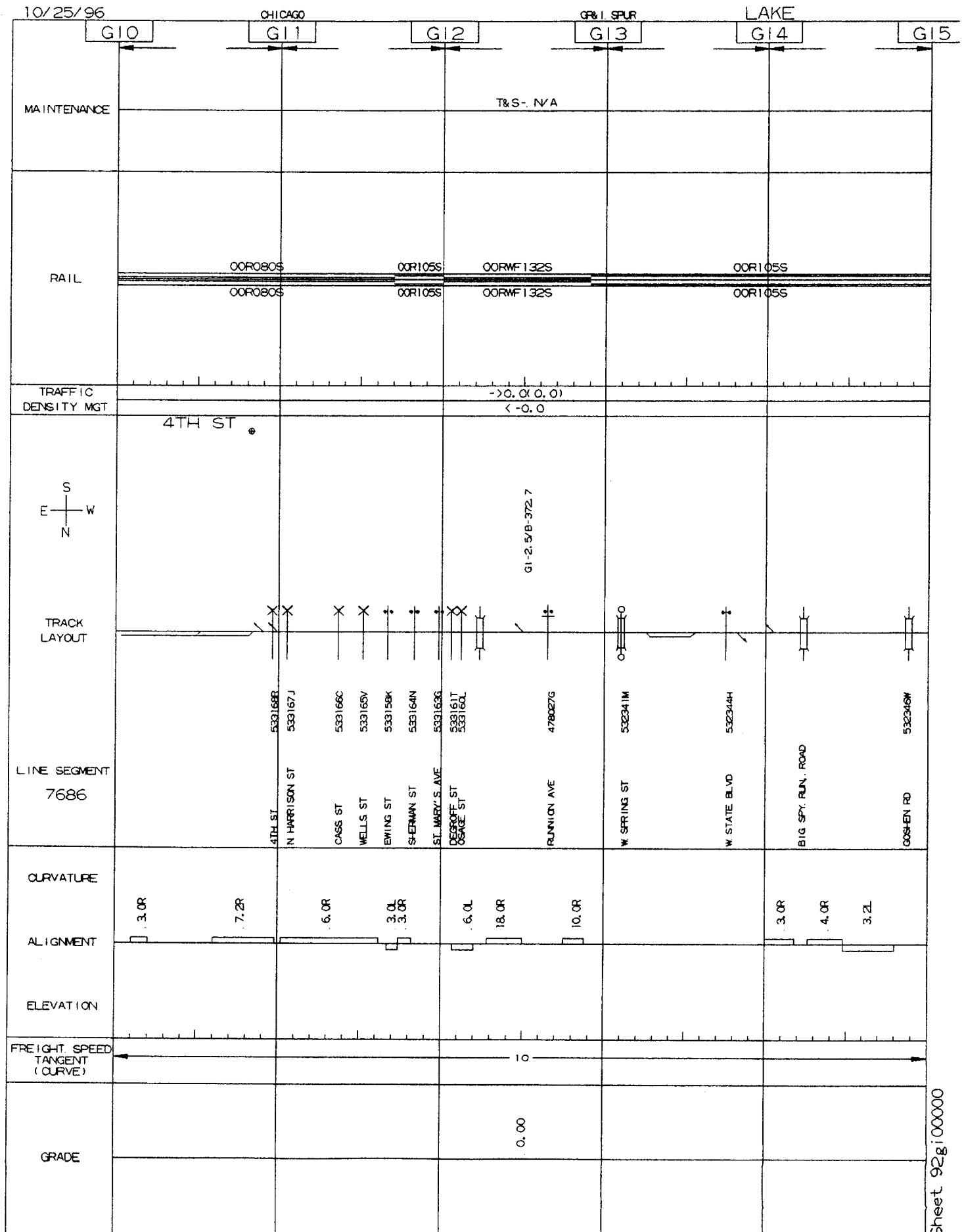
CHICAGO

PULLMAN BRANCH

PULLMAN JCT TO 130TH STR

LAKE

10/24/96	PB5	CHICAGO	PULLMAN BRANCH	PULLMAN JCT TO 130TH STR	LAKE
MAINTENANCE	T&S-12/85				
RAIL	<div data-bbox="232 533 348 638"> <div>00W115S</div> <div>00W115S</div> <div>00115S</div> </div>				
TRAFFIC DENSITY MGT	<div data-bbox="232 743 348 785"> <div>-&gt;0.0 (0.0)</div> <div>&lt;-0.0</div> </div>				
<div data-bbox="100 890 183 974"> <div>S</div> <div>E</div> <div>W</div> <div>N</div> </div> <div data-bbox="100 1100 166 1142">TRACK LAYOUT</div> <div data-bbox="67 1331 199 1373">LINE SEGMENT</div> <div data-bbox="100 1352 166 1373">7704</div>	<div data-bbox="249 785 431 827">(130TH ST)</div> <div data-bbox="315 848 348 1037">TO CHICAGO PORT AUTH</div> <div data-bbox="232 1079 348 1163">  </div> <div data-bbox="249 1415 282 1436">1-80</div> <div data-bbox="315 1373 348 1436">130TH ST</div>				
CURVATURE					
ALIGNMENT	<div data-bbox="232 1520 299 1562"> <div>12.0L</div> <div>12.0L</div> </div>				
ELEVATION					
FREIGHT SPEED TANGENT (CURVE)	<div data-bbox="232 1751 332 1772"> <div>10</div> </div>				
GRADE	<div data-bbox="249 1856 282 1877">8</div>				





10/24/96

CHICAGO

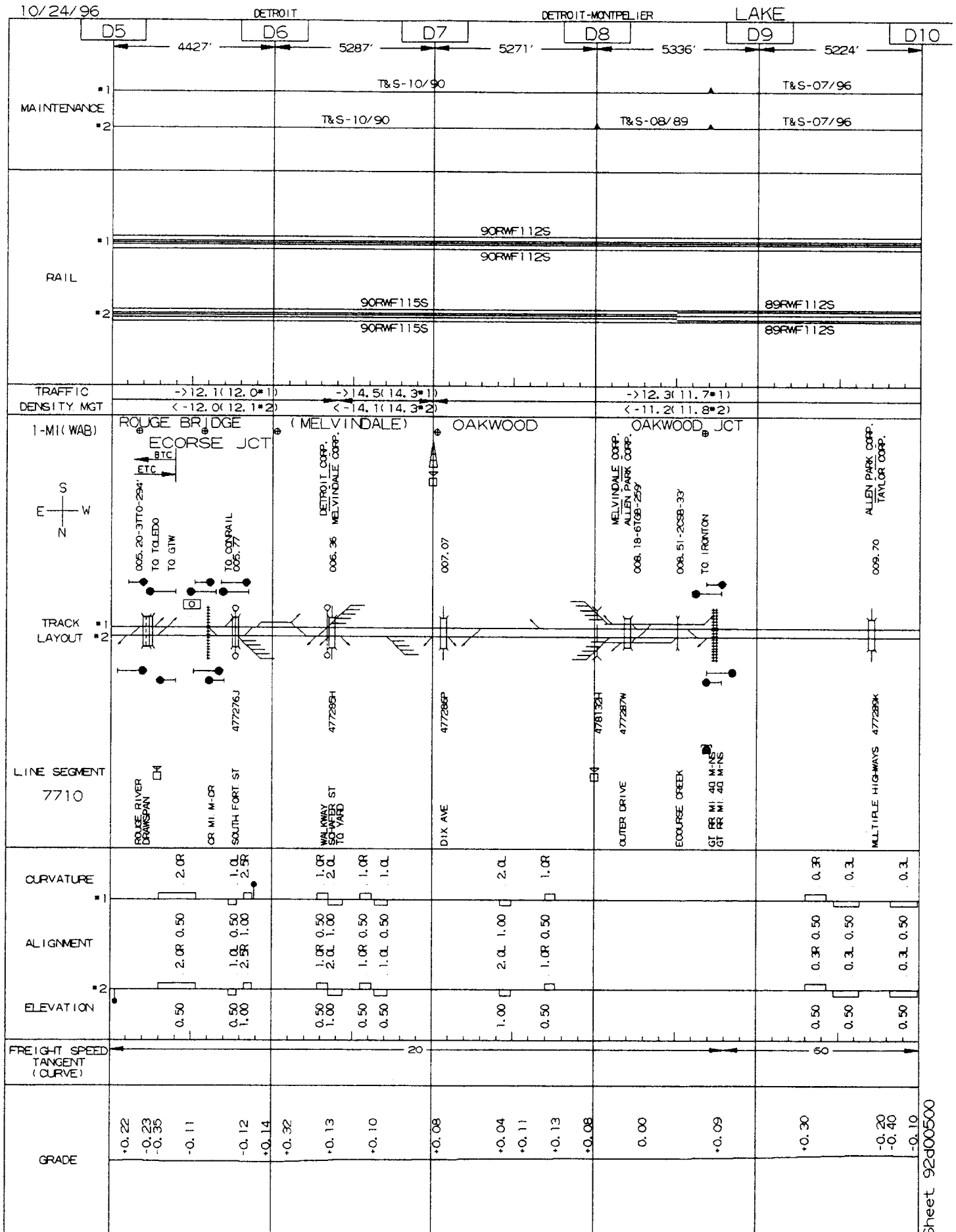
GR&I SPUR

LAKE

	G15	G16				
MAINTENANCE	T&S- N/A					
RAIL	<div> <div>OOR105S</div> <div>OOR105S</div> </div>					
TRAFFIC DENSITY MGT	<div> <div>-&gt; 0.0 (0.0)</div> <div>&lt;- 0.0</div> </div>					
TRACK LAYOUT	<div> <div>END OF TRACK</div> <div> <div> <div>S</div> <div>E</div> <div>N</div> <div>W</div> </div> </div> </div>					
LINE SEGMENT	<div> <div>532347D</div> <div>532354N</div> <div>COLISEUM BLVD</div> <div>I-69</div> </div>					
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	<div> <div>10</div> </div>					
GRADE	<div> <div>0.00</div> </div>					

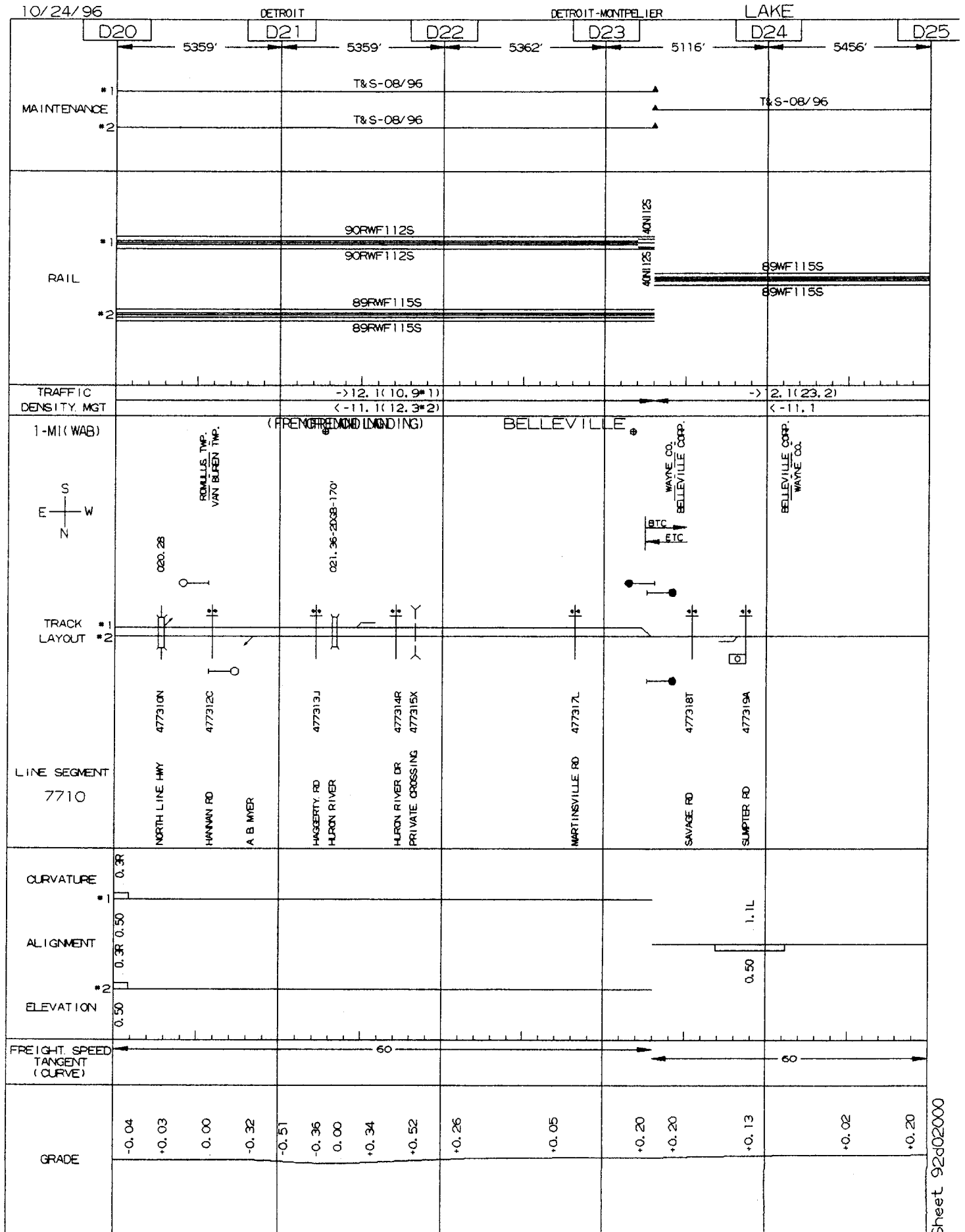
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10/24/96



Sheet 92d01000

Sheet 92d01500

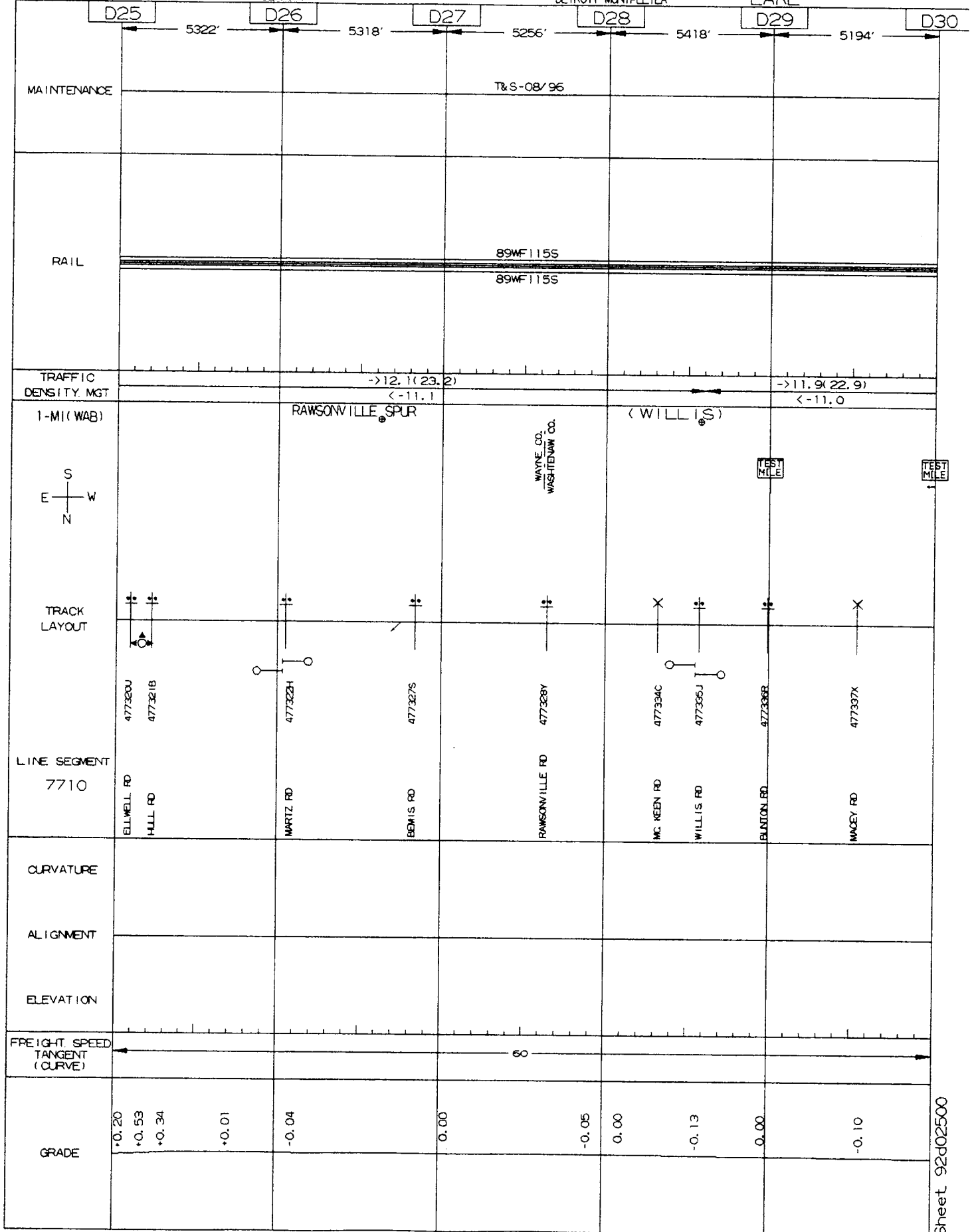


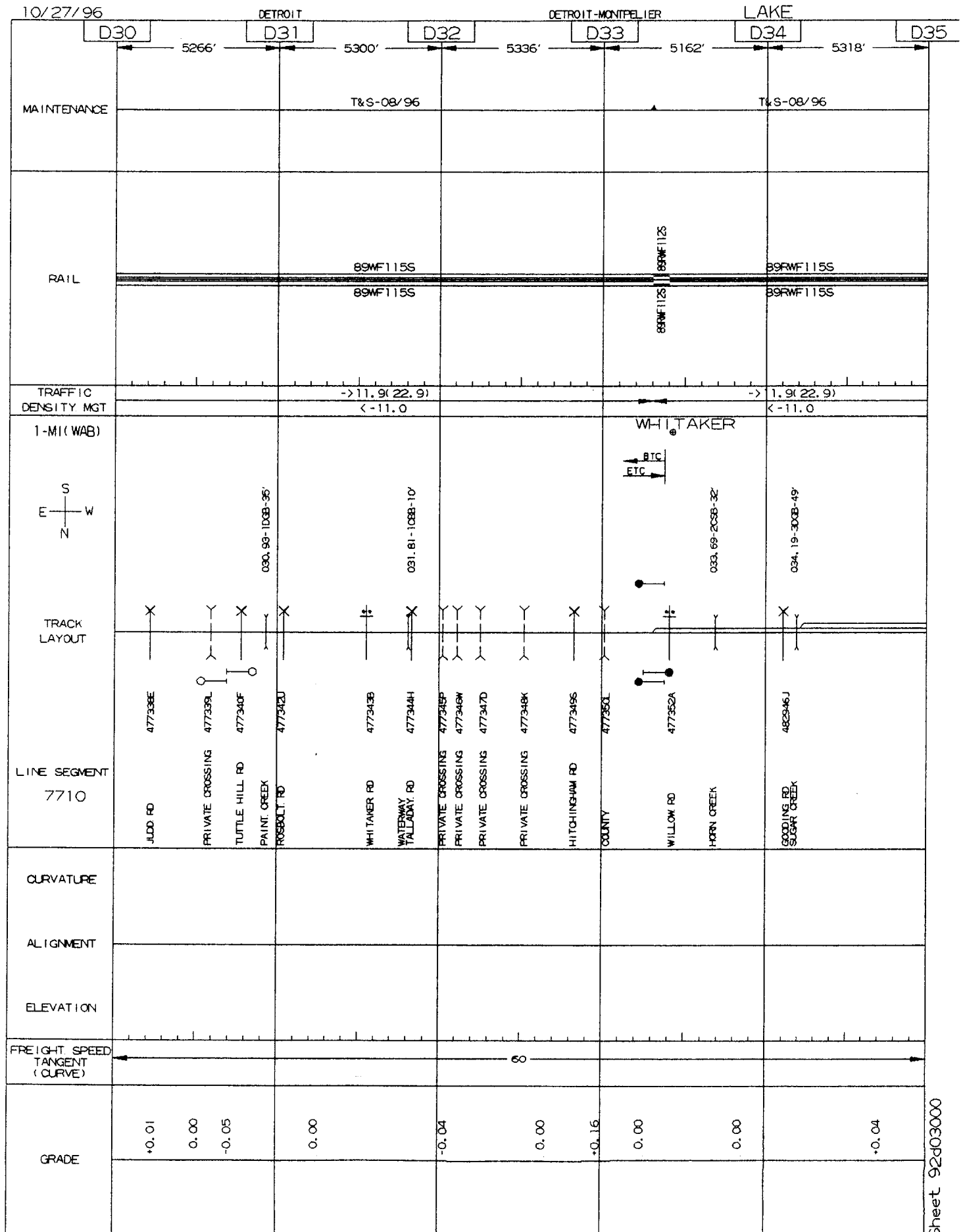
10/24/96

DETROIT

DETROIT-MONTPELIER

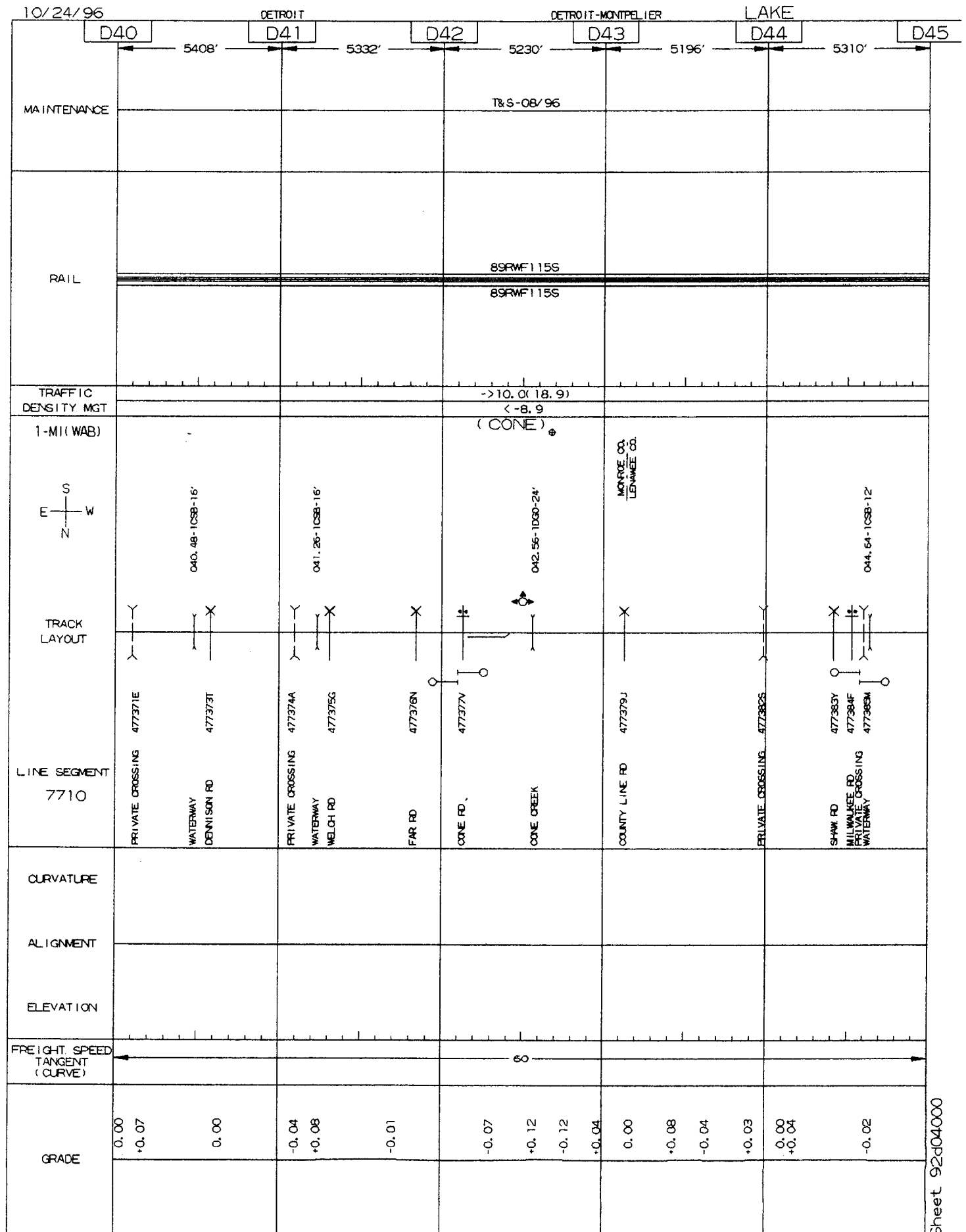
LAKE



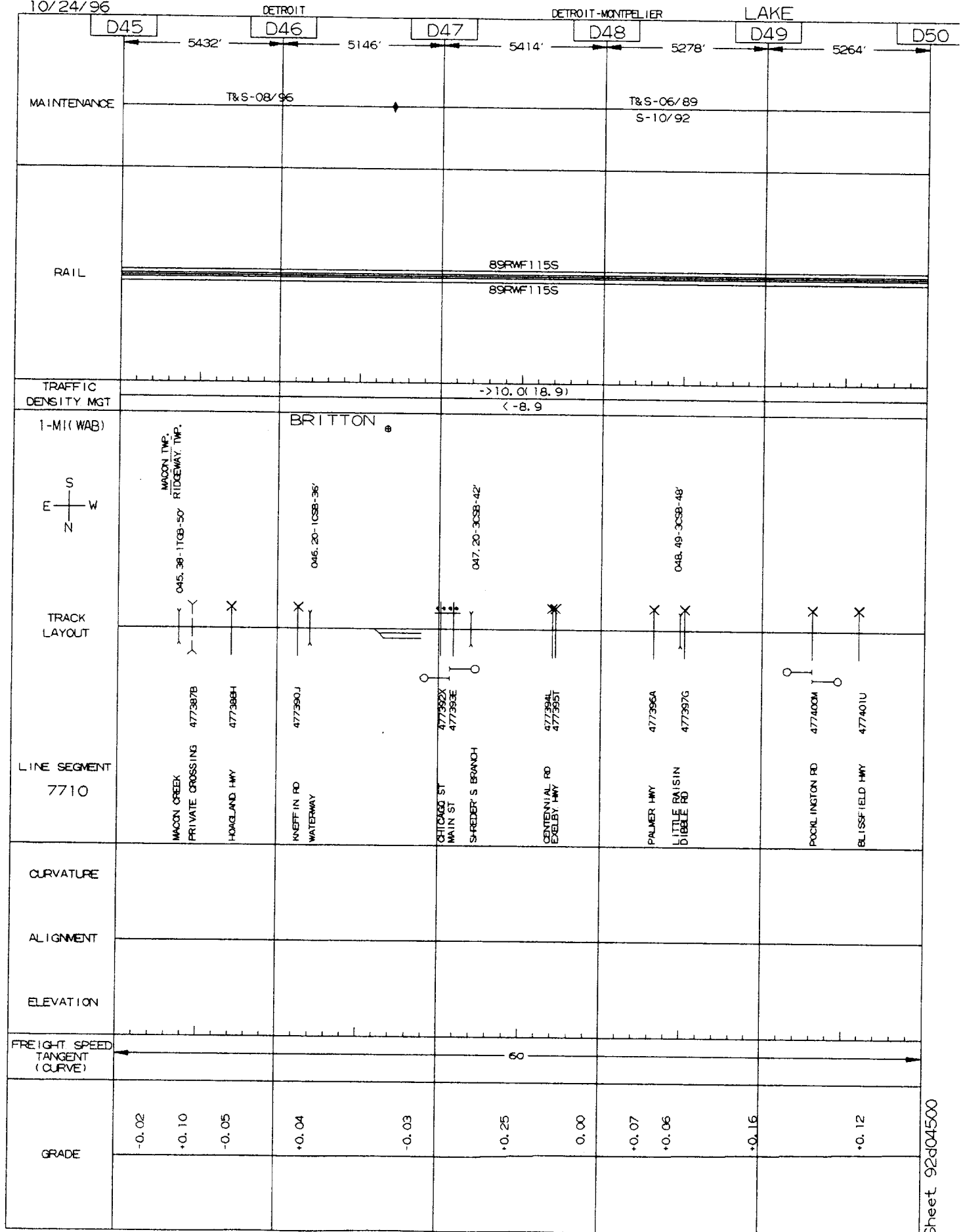


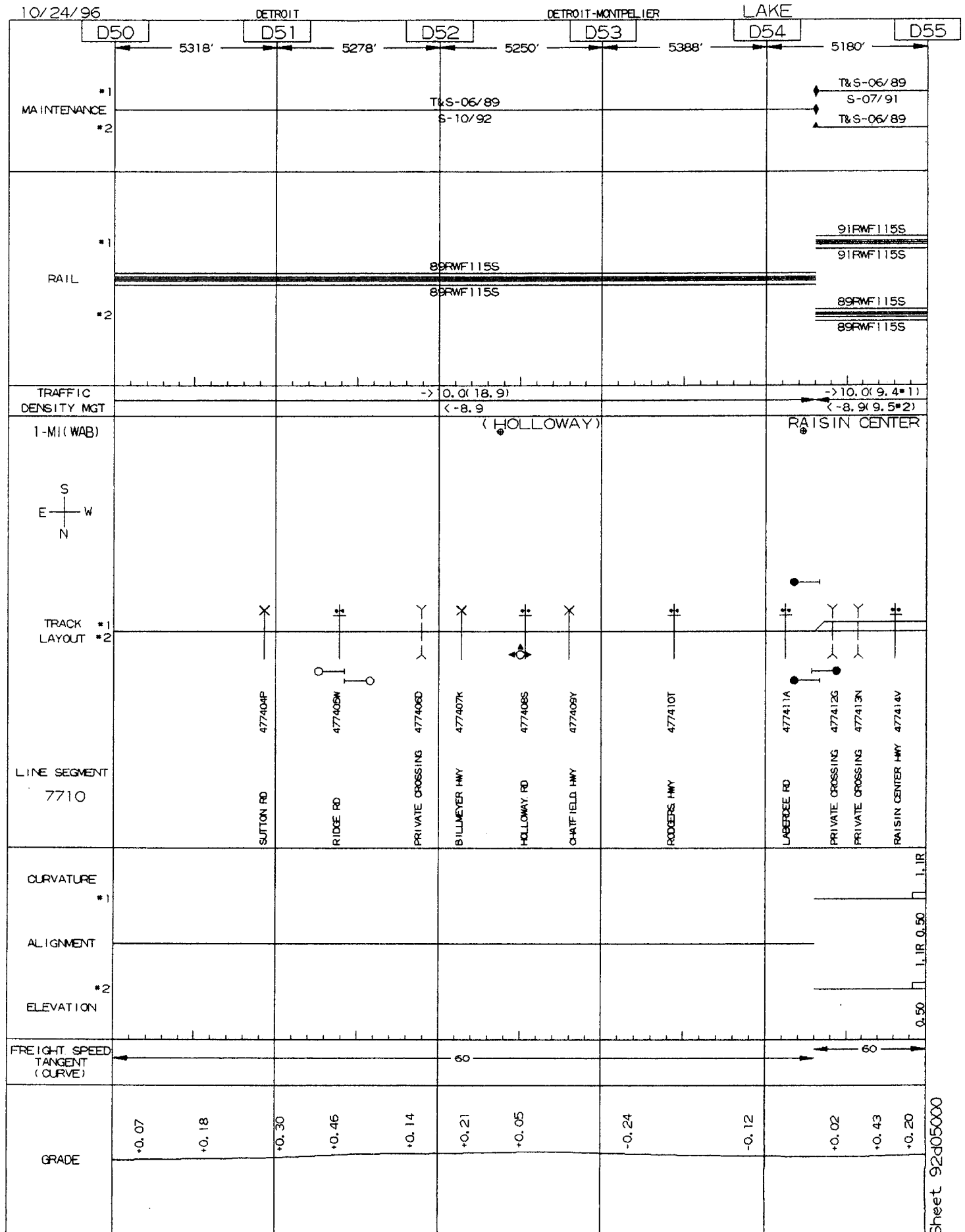


Sheet 92d03500



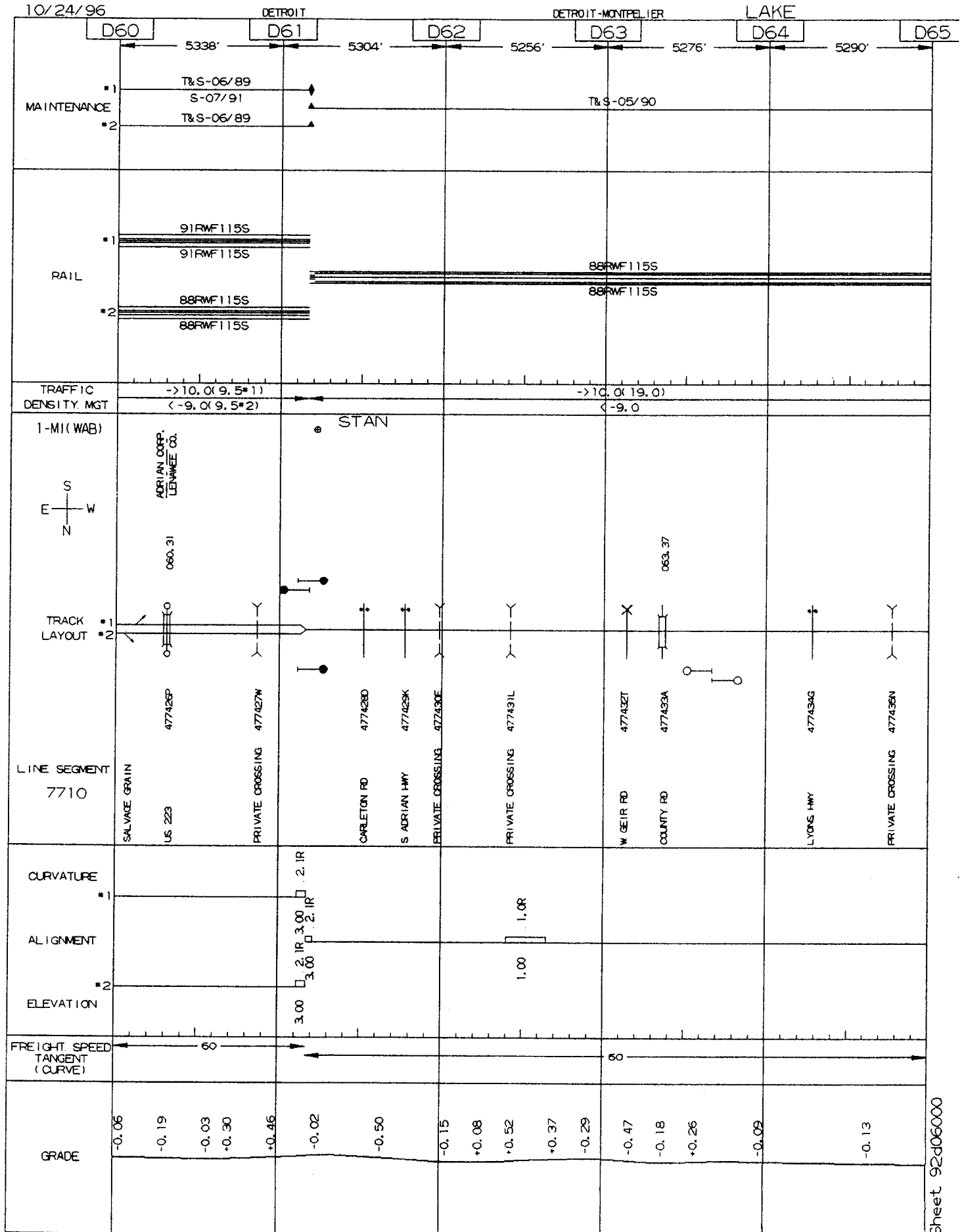
10/24/96





Sheet 92d05500

10/24/96



10/24/96		DETROIT		DETROIT-MONTPELIER		LAKE						
		D65	D66	D67	D68	D69	D70					
		5366'		5300'	5214'	5294'	5358'					
MAINTENANCE		T&S-05/90										
RAIL		88RWF115S 88RWF115S										
TRAFFIC DENSITY MGT		-> 10.0 (19.0) < -9.0										
1-MI (WAB)		(SAND CREEK)										
<div><div>S</div><div>E</div><div>N</div><div>W</div></div>		MADISON TWP. SENECA TWP.										
TRACK LAYOUT		<div><div><div>+</div><div>+</div></div><div>477436V</div><div>477437C</div><div>337-SAND CREEK</div><div>BRYANT RD</div><div>477440K</div><div>PRIVATE CROSSING</div><div>477442Y</div><div>PRIVATE CROSSING</div><div>477443F</div><div>BLACK CREEK</div><div>W HORTON RD</div><div>477445J</div><div>TURTLE HWY</div><div>477446B</div><div>PRIVATE CROSSING</div><div>477447H</div></div>										
LINE SEGMENT		7710										
CURVATURE												
ALIGNMENT		<div><div>0.7</div><div>0.58</div><div>1.00</div><div>1.00</div></div>										
ELEVATION												
FREIGHT SPEED TANGENT (CURVE)		60										
GRADE		+0.03	+0.11	0.00	+0.09	0.00	+0.08	+0.02	+0.07	0.00	+0.07	+0.17

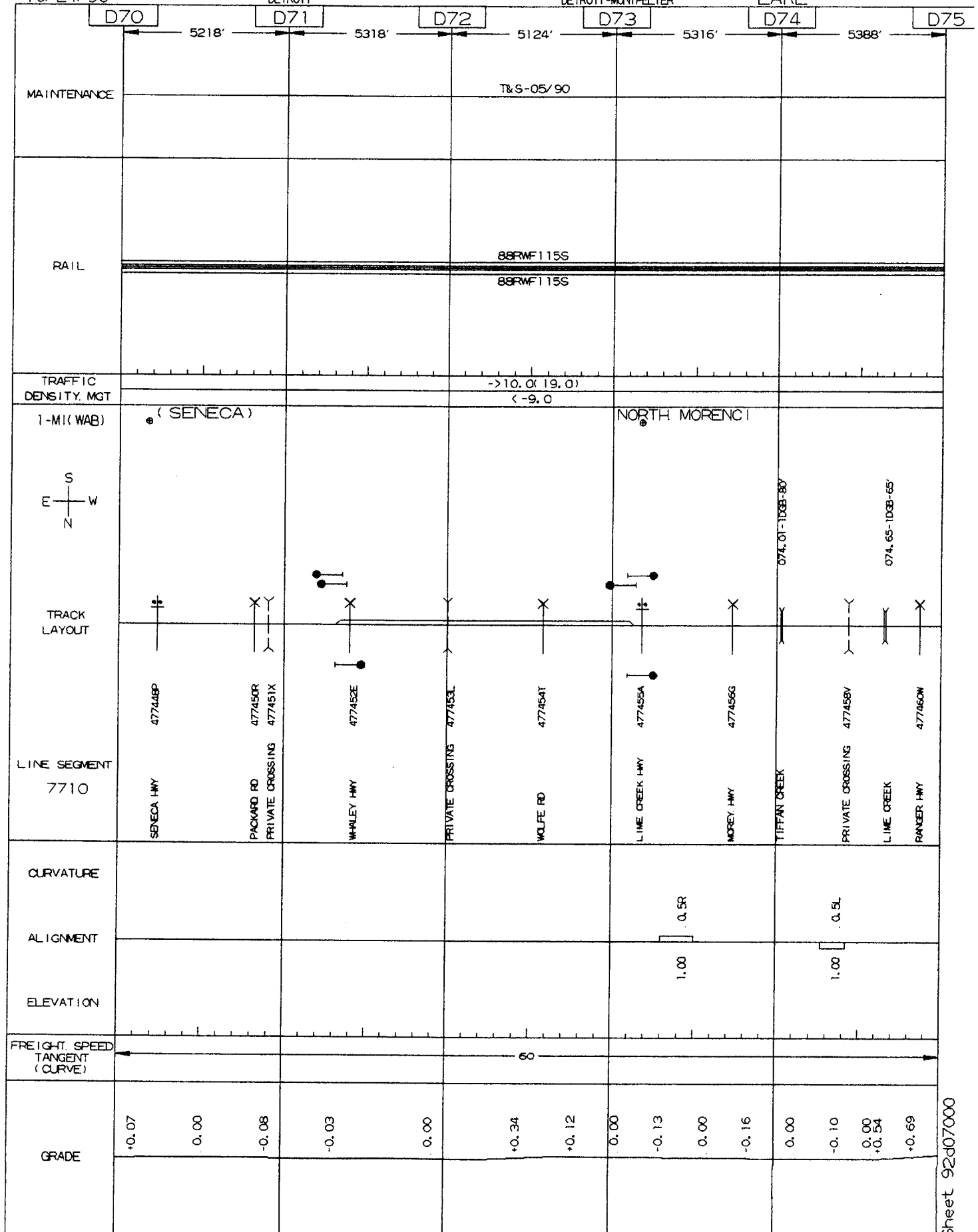
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10/24/96

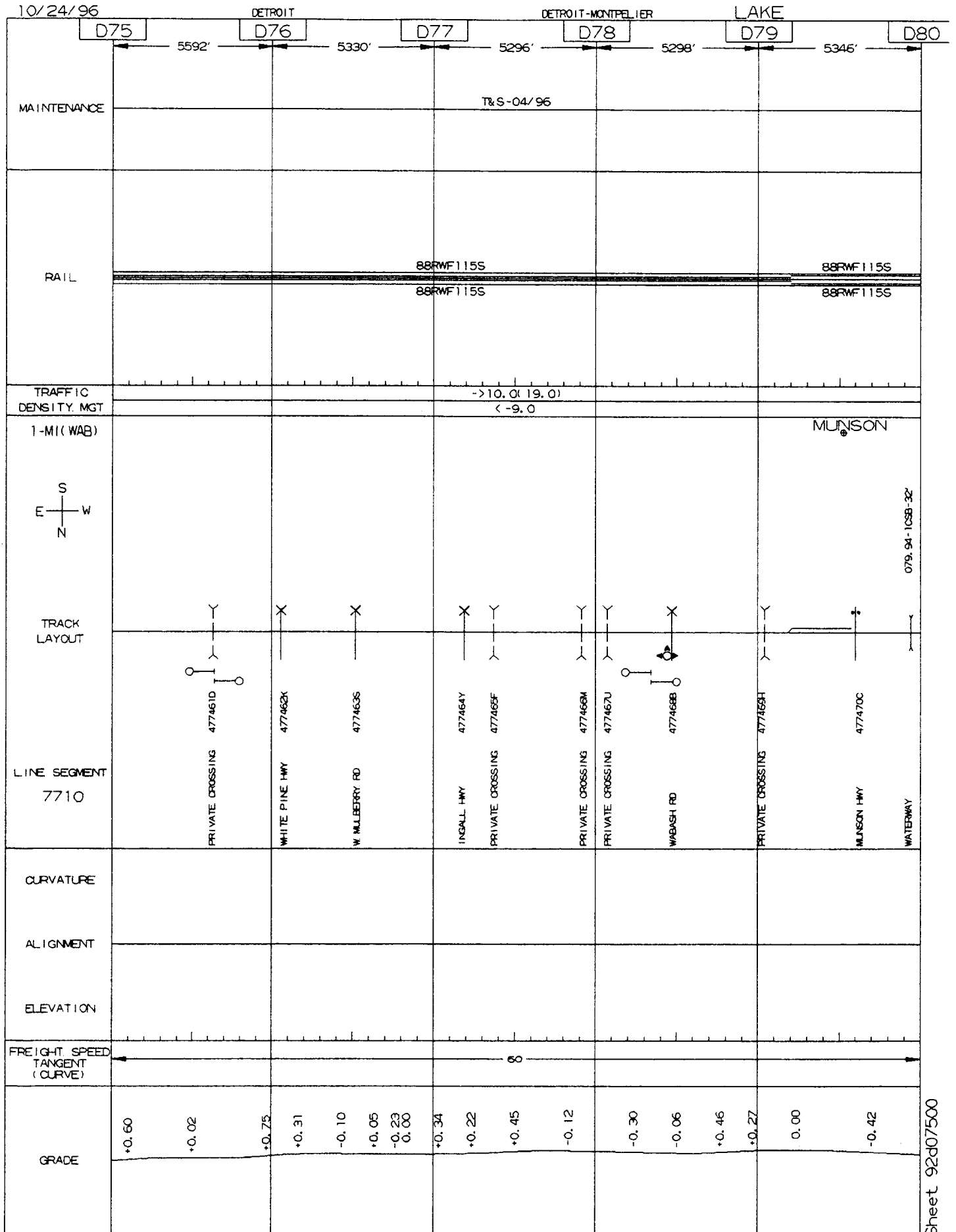
DETROIT

DETROIT-MONTPELIER

LAKE







Sheet 92d08000

10/24/96

DETROIT

DETROIT-MONTPELIER

LAKE

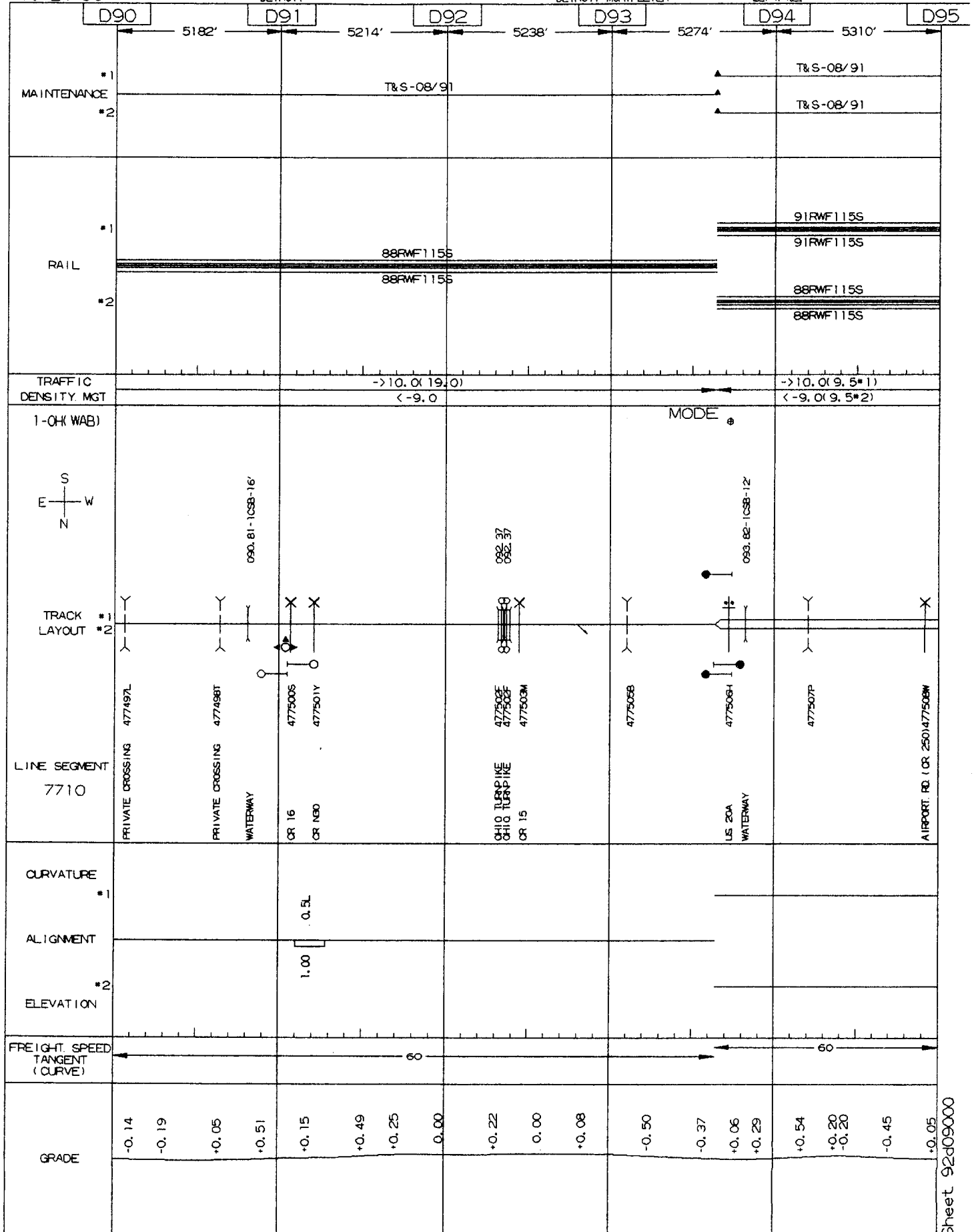
	D85	D86	D87	D88	D89	D90
	5318'	5342'	5210'	5392'	5374'	
MAINTENANCE		T&S-04/96			T&S-08/91	
RAIL			88RWF115S	88RWF115S		
TRAFFIC DENSITY MGT			->10.0 19.0			
1-OK (WAB)	ALVORDTON				(KUNKLE)	
TRACK LAYOUT						
LINE SEGMENT	7710					
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			80			
GRADE	+0.14 0.00 -0.37	0.00 +0.18 +0.64 +0.42	0.00 -0.22 0.00 +0.37	-0.03 +0.12 -0.26 +0.16 +0.52	+0.83 +0.68 +0.16 -0.05 +0.09 -0.14	

10/24/96

DETROIT

DETROIT-MONTPELIER

LAKE

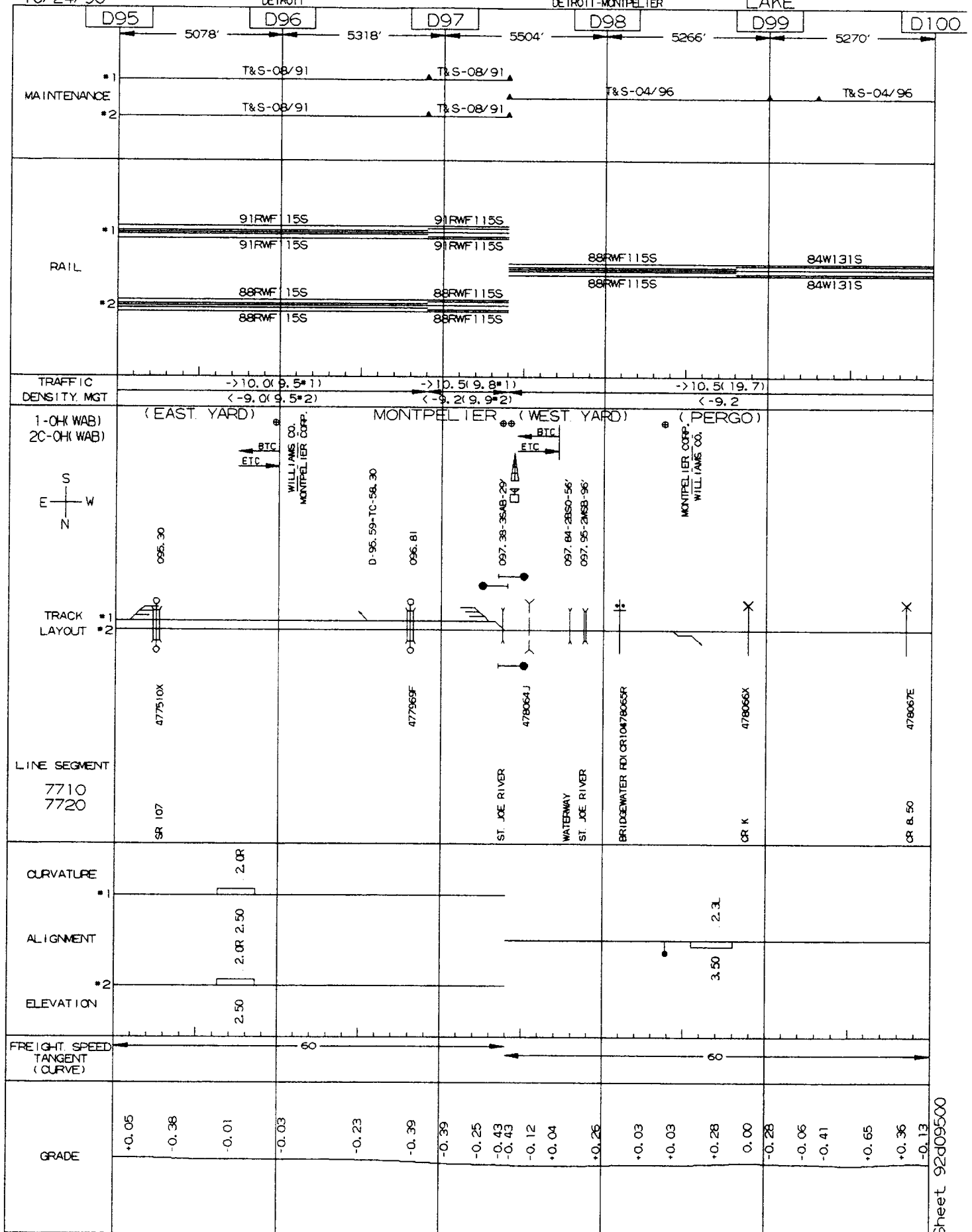


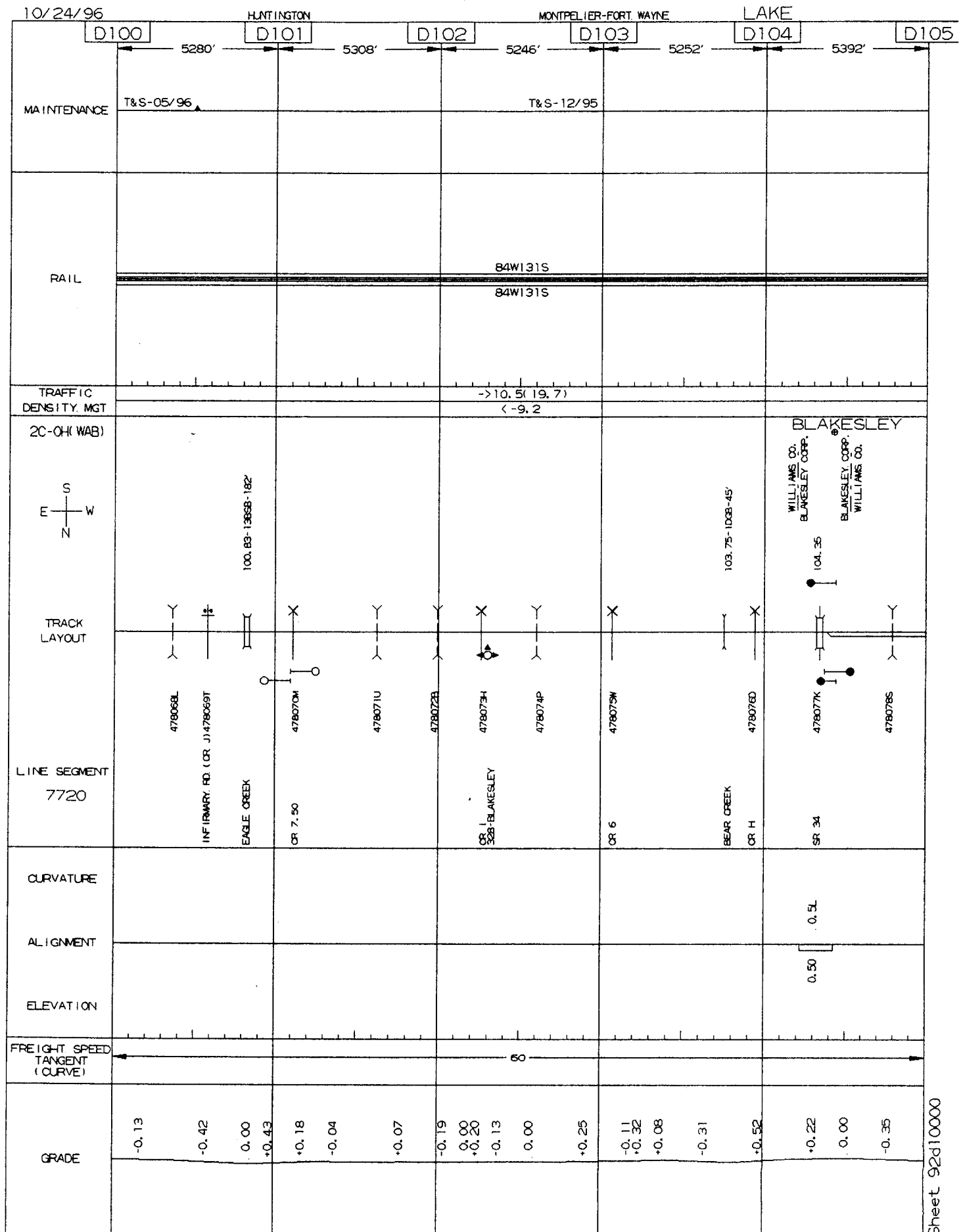
10/24/96

DETROIT

DETROIT-MONTPELIER

LAKE



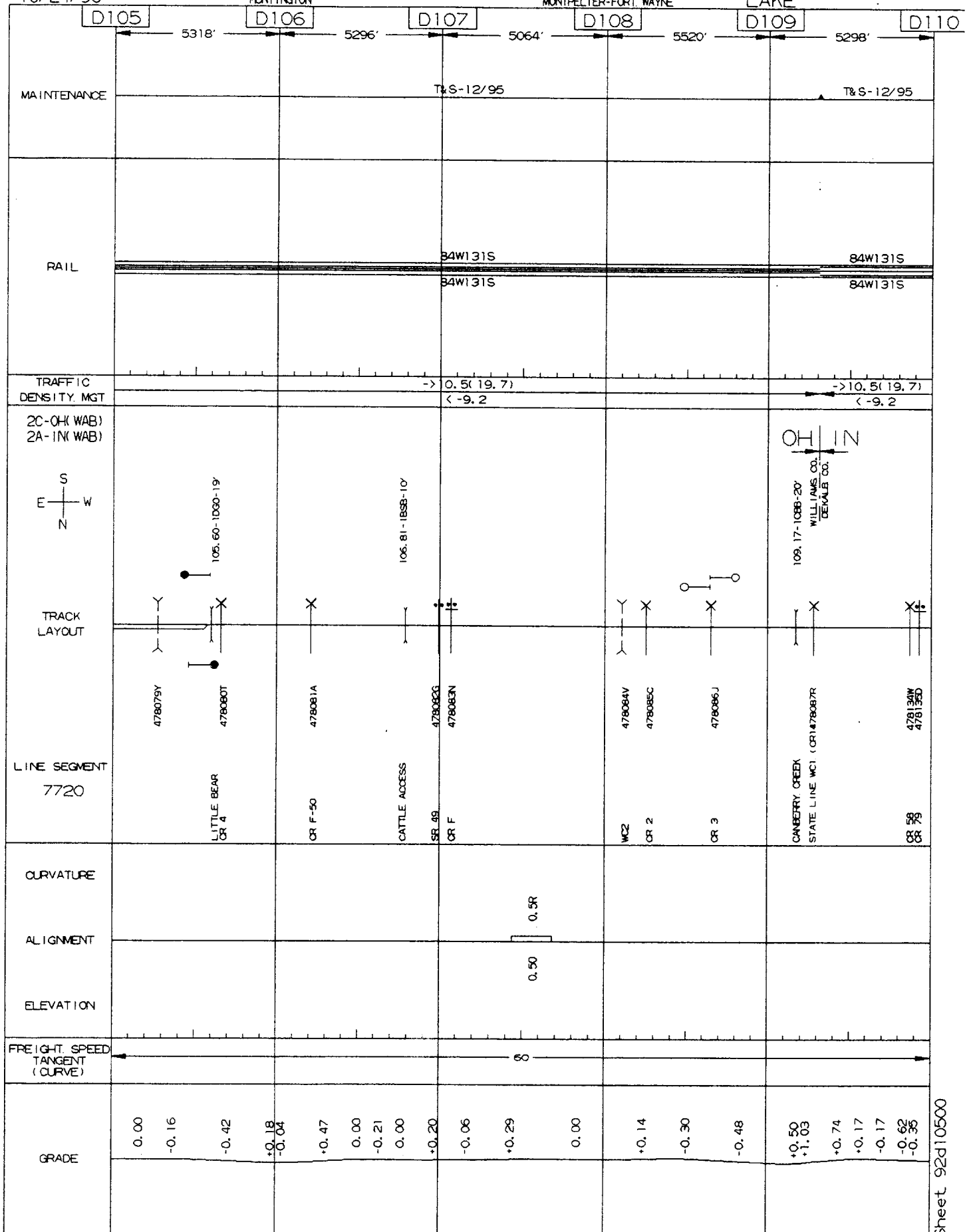


10/24/96

HUNTINGTON

MONTPELIER-FORT WAYNE

LAKE

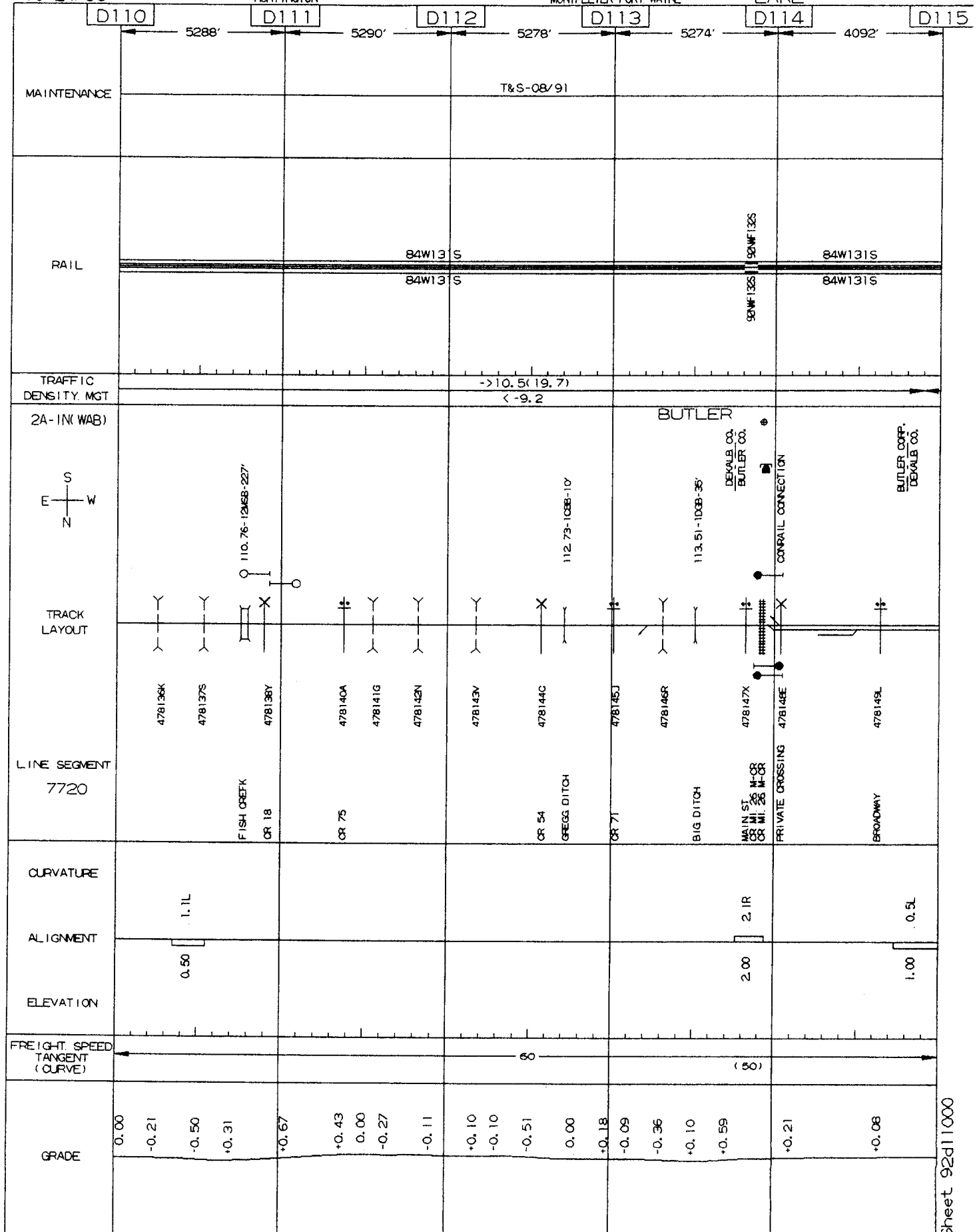


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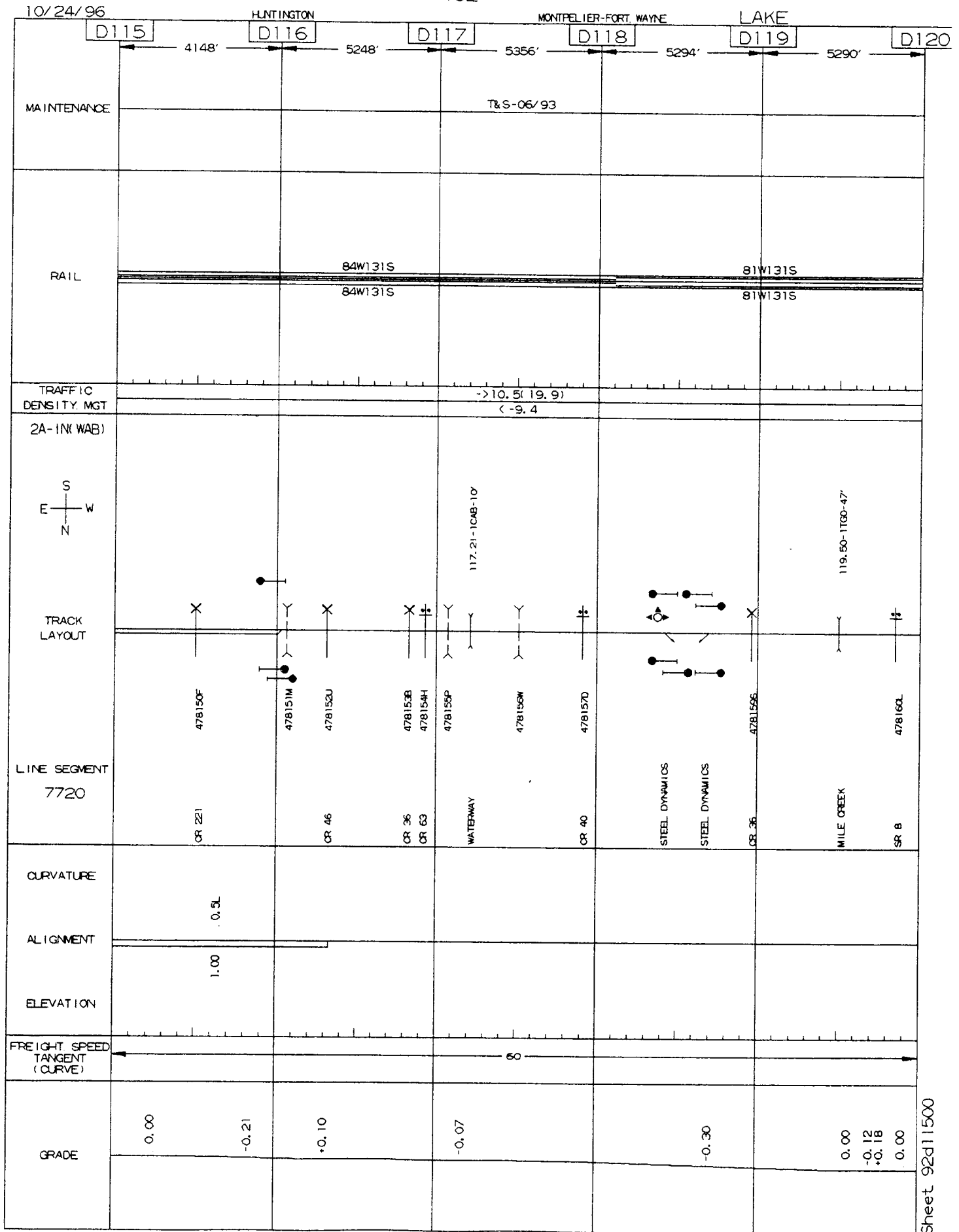
HUNTINGTON

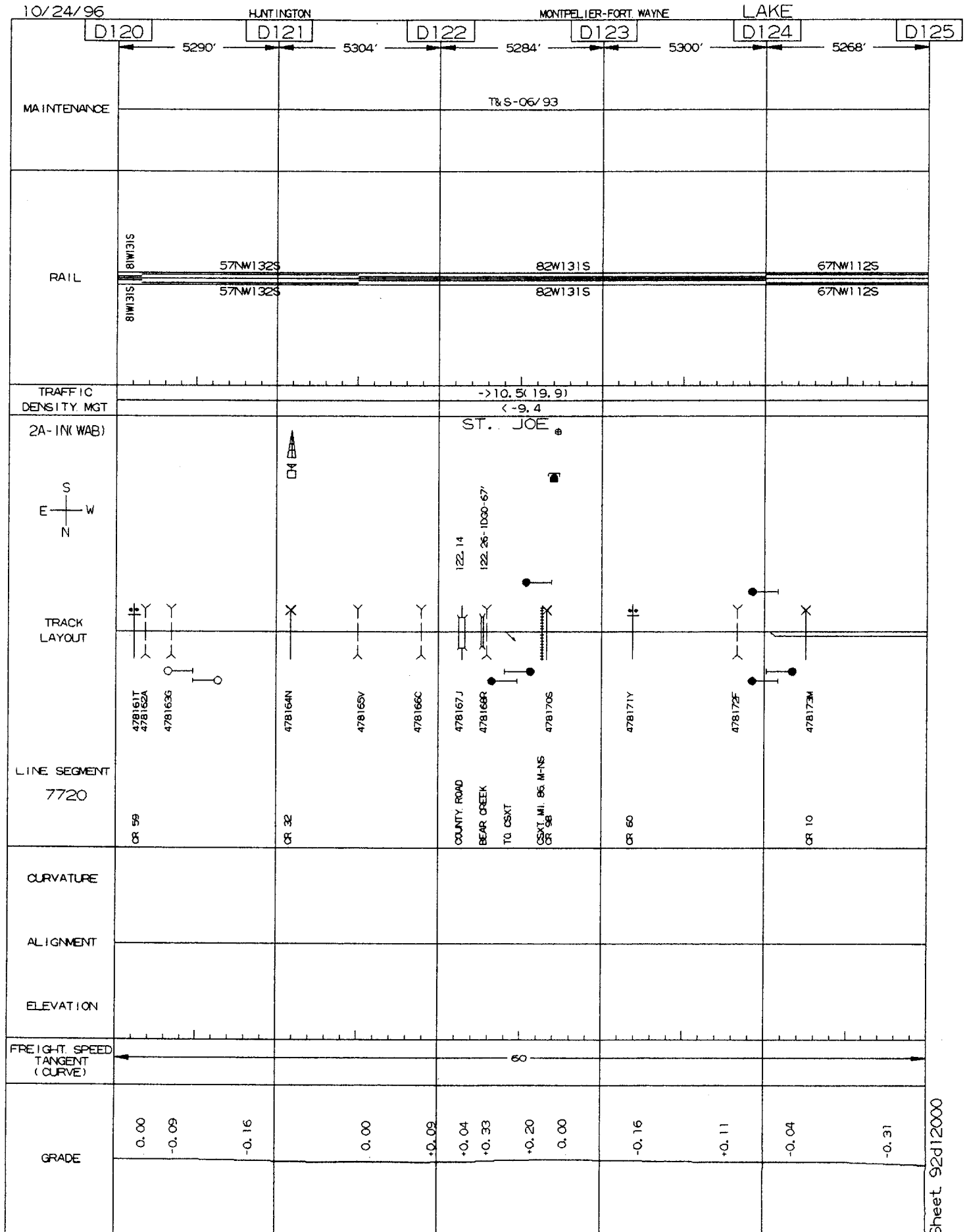
MONTPELIER-FORT WAYNE

LAKE

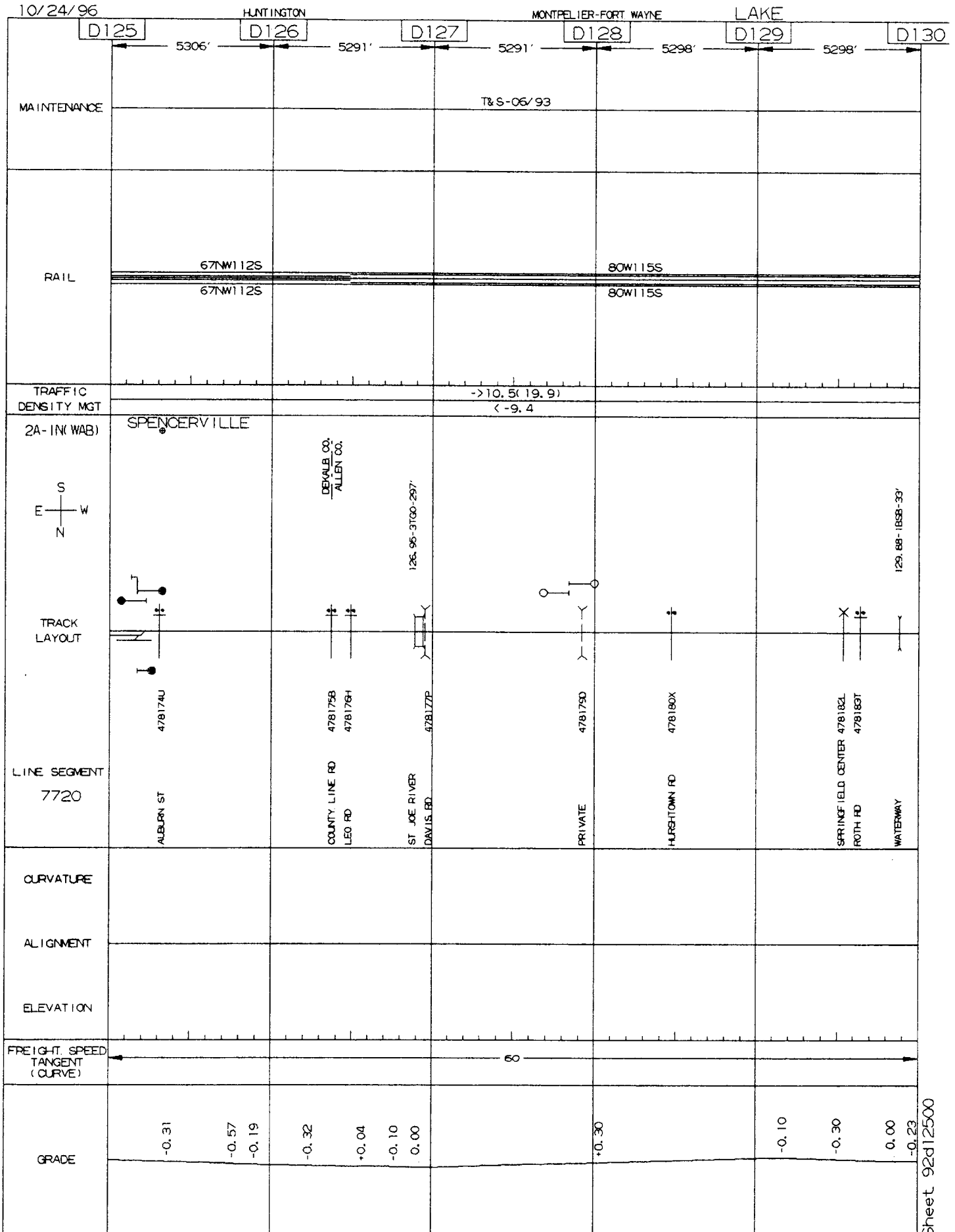








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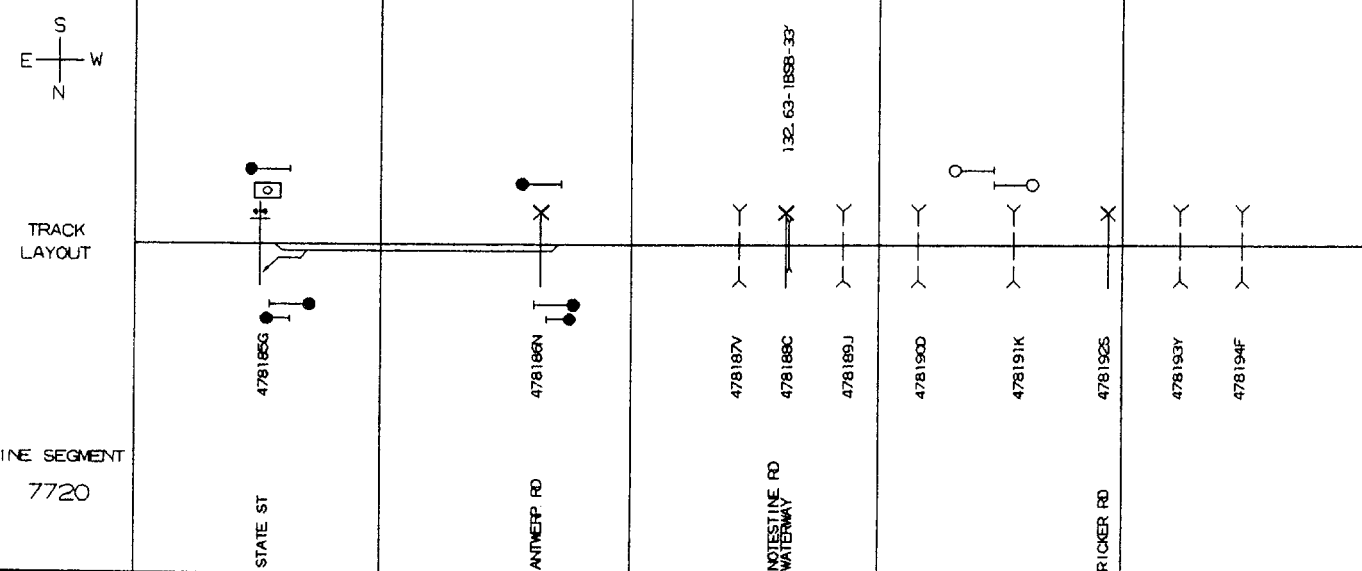


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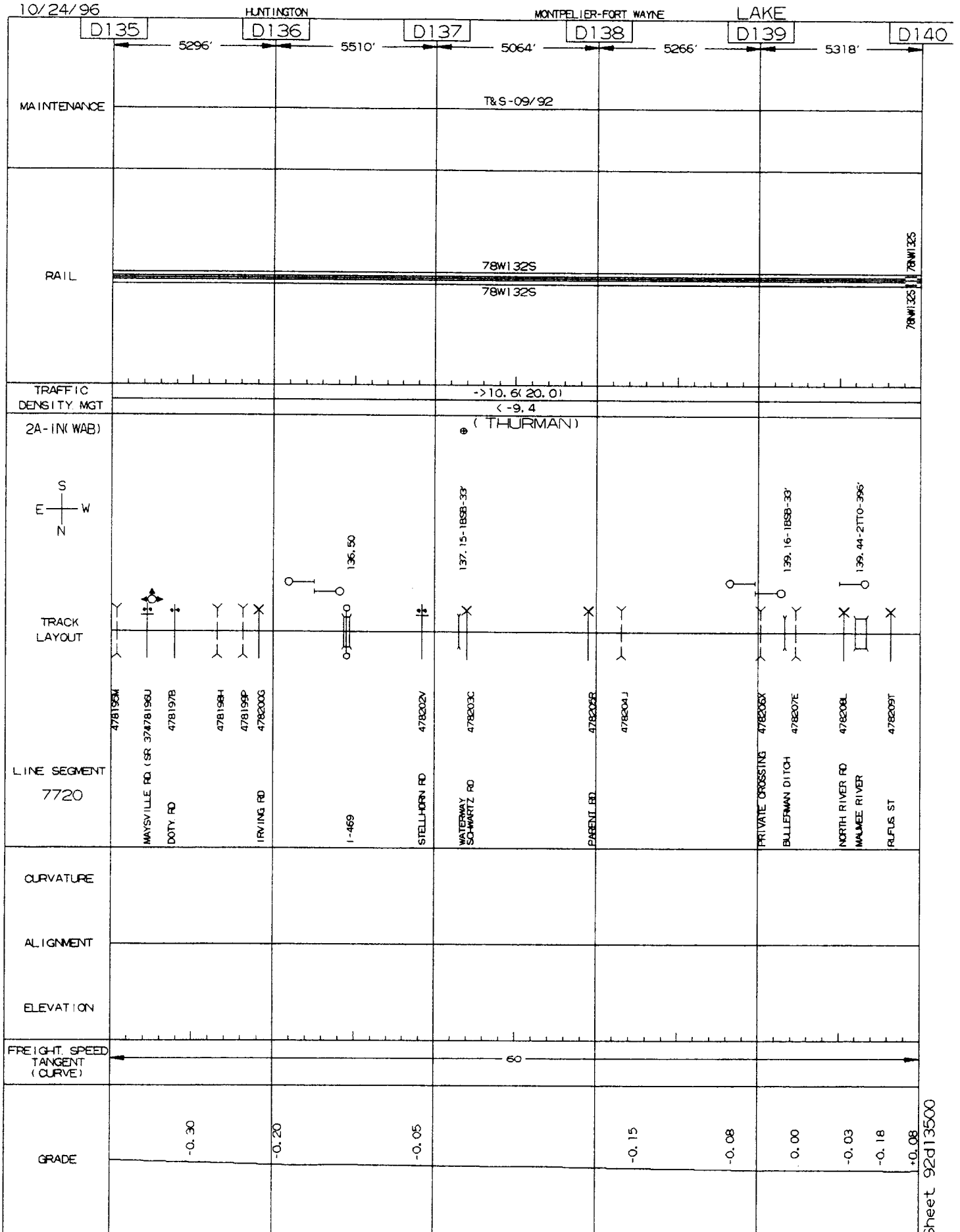
HUNTINGTON

MONTPELIER-FORT WAYNE

LAKE

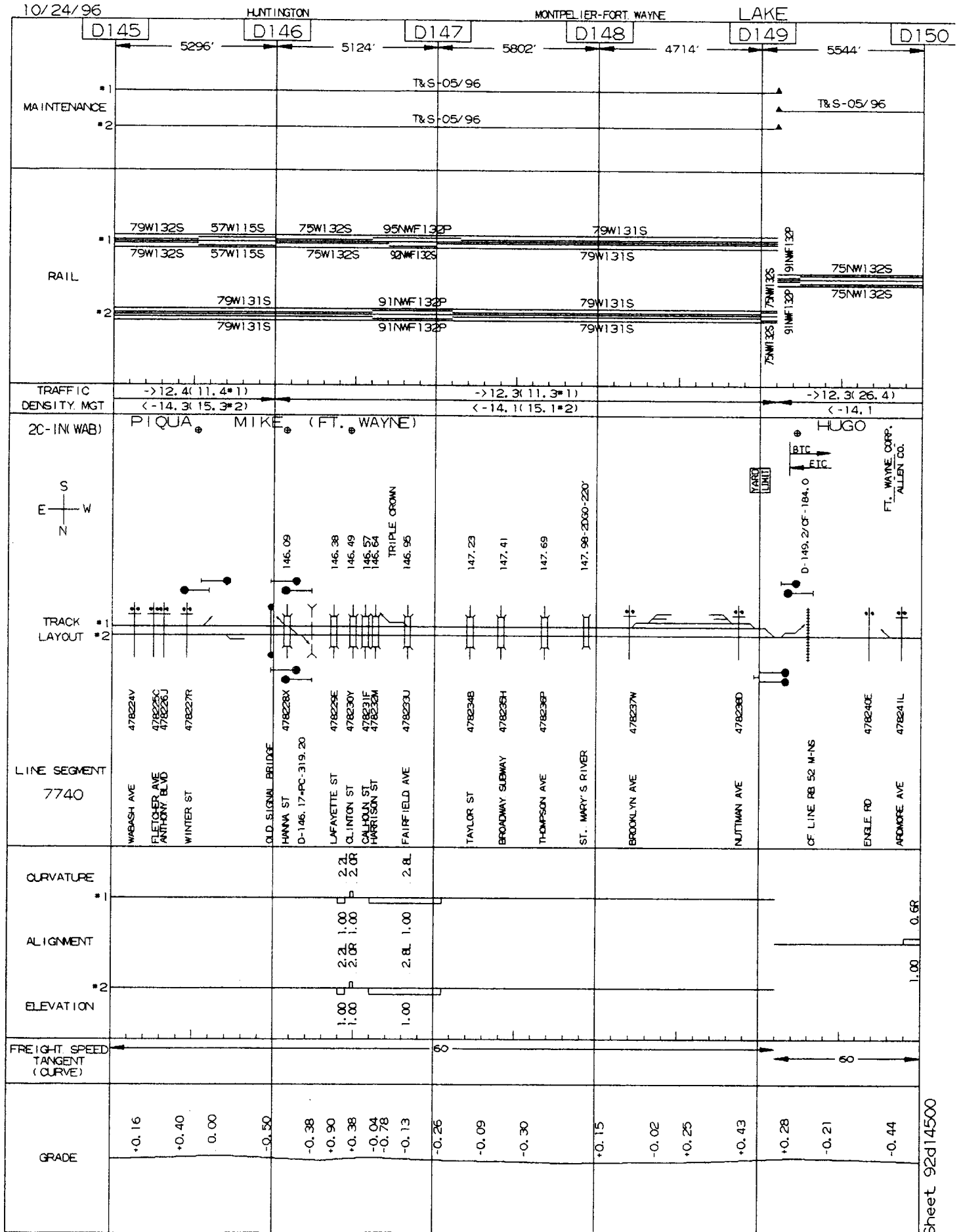
	D130	D131	D132	D133	D134	D135
	5264'	5314'	5288'	5272'	5300'	
MAINTENANCE	T&S-09/92					
RAIL	80W115S 80W115S			78W132S 78W132S		
TRAFFIC DENSITY MGT	->10.5(19.9) <-9.4			->10.6(20.0) <-9.4		
2A-IN(WAB)	GRABILL					
TRACK LAYOUT						
LINE SEGMENT	7720					
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	60					
GRADE	0.00 +0.06 0.00 +0.10	0.00	-0.31	0.00 +0.23	-0.17	-0.30

10/24/96



Sheet 92d14000

10/24/96



10/24/96

HUNTINGTON

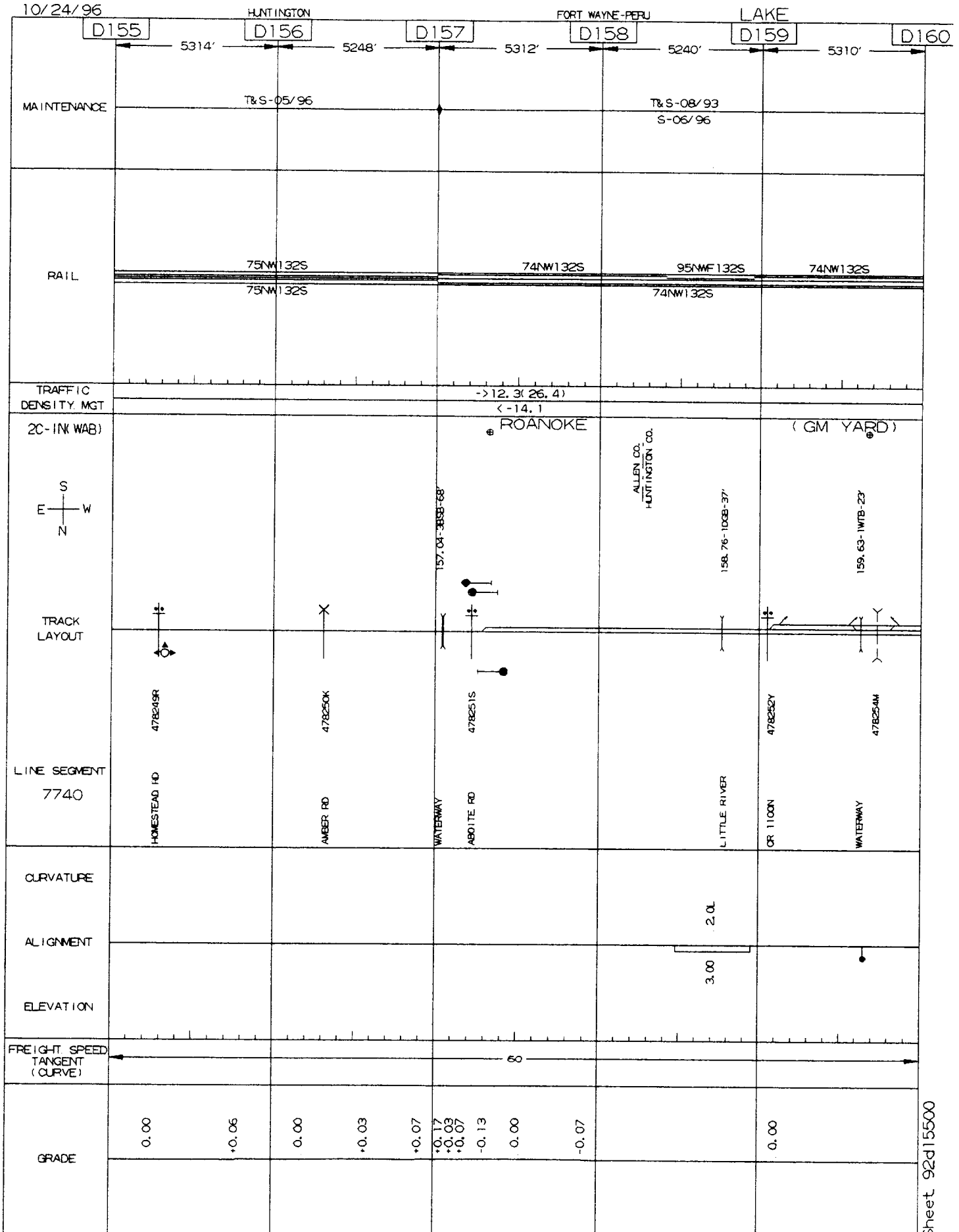
FORT WAYNE-PERU

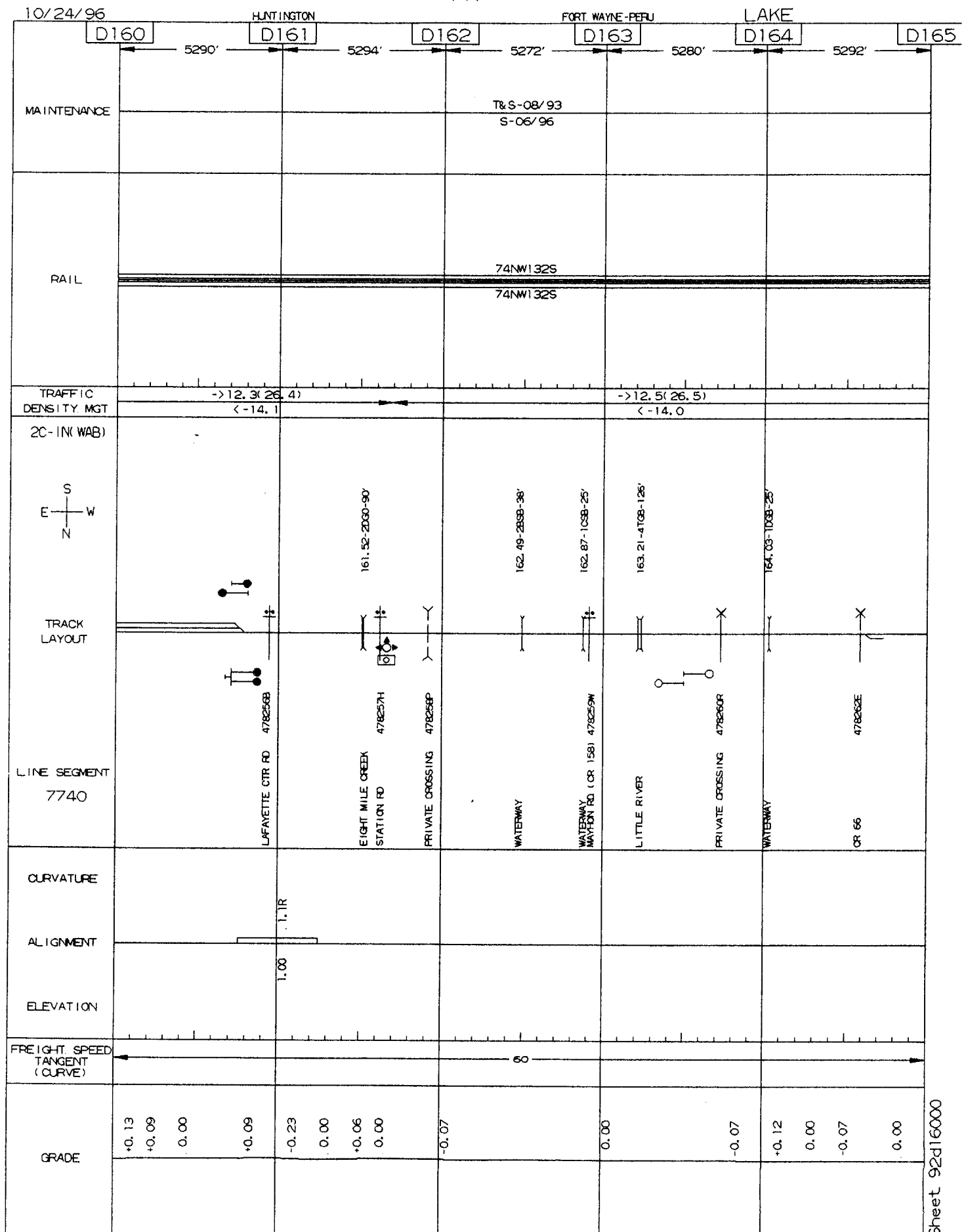
LAKE

	D150	D151	D152	D153	D154	D155
	5256'	5331'	5291'	5286'	5302'	
MAINTENANCE			T&S-05/96			
RAIL			75NW132S			
			75NW132S			
TRAFFIC DENSITY MGT			-> 12.3( 26.4)			
			< -14.1			
2C-1N(WAB)						
<div> <div>S</div> <div>E</div> <div>N</div> <div>W</div> </div>						
TRACK LAYOUT						
LINE SEGMENT 7740						
		SMITH RD	WATERWAY		ELLISON RD	
		478243A			478247C 478247C 478248J	
			151.30-1WTB-28'		153.50 153.50	
					1-69 1-69	
CURVATURE						
ALIGNMENT	0.6R					
ELEVATION	1.00					
FREIGHT. SPEED TANGENT (CURVE)			60			
GRADE	-0.44 -0.27	+0.03 -0.03 +0.20 -0.12	0.00 -0.12	0.00 -0.07 0.00 +0.04 -0.06 0.00 -0.08 0.00		

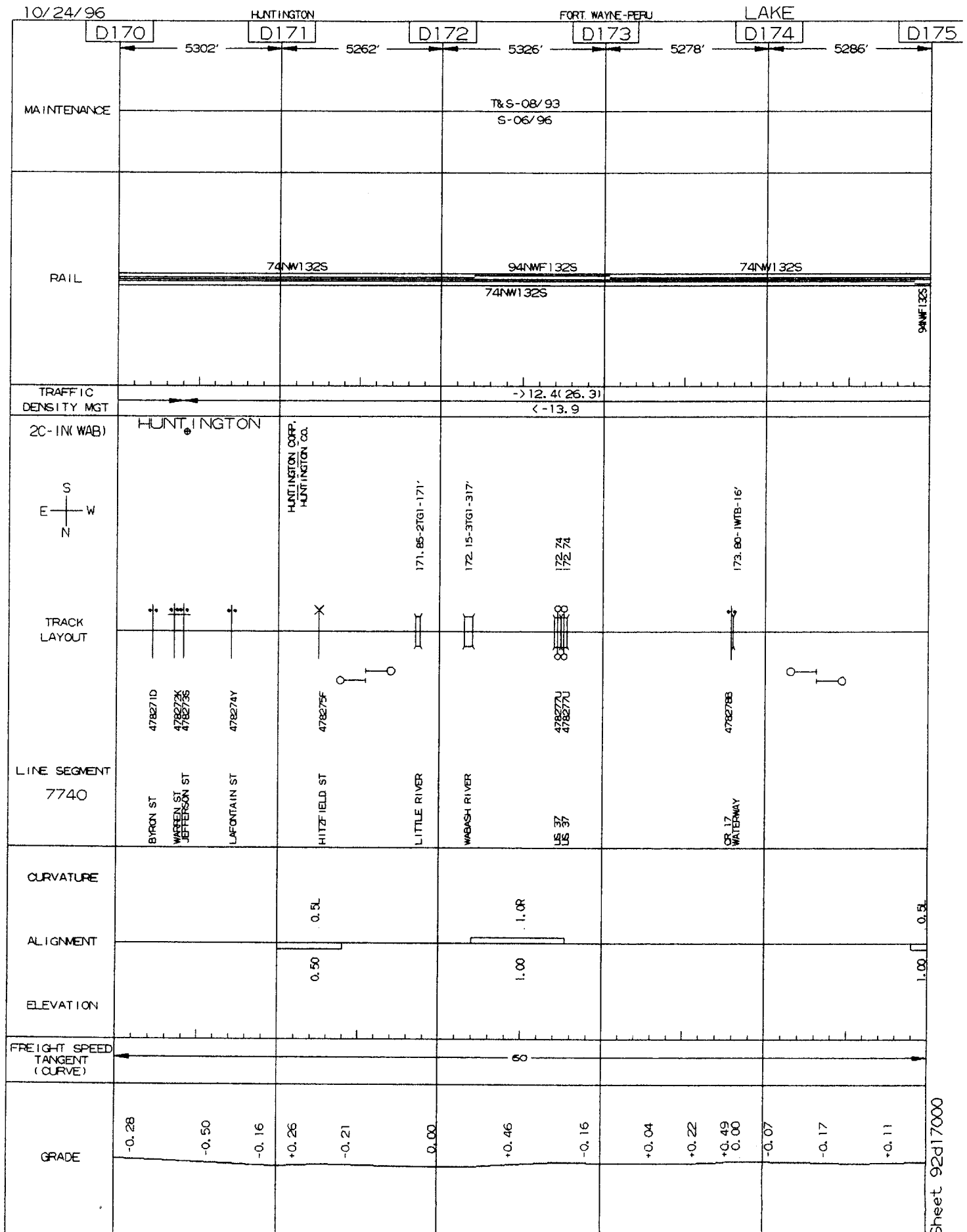


10/24/96





Sheet 92d16500

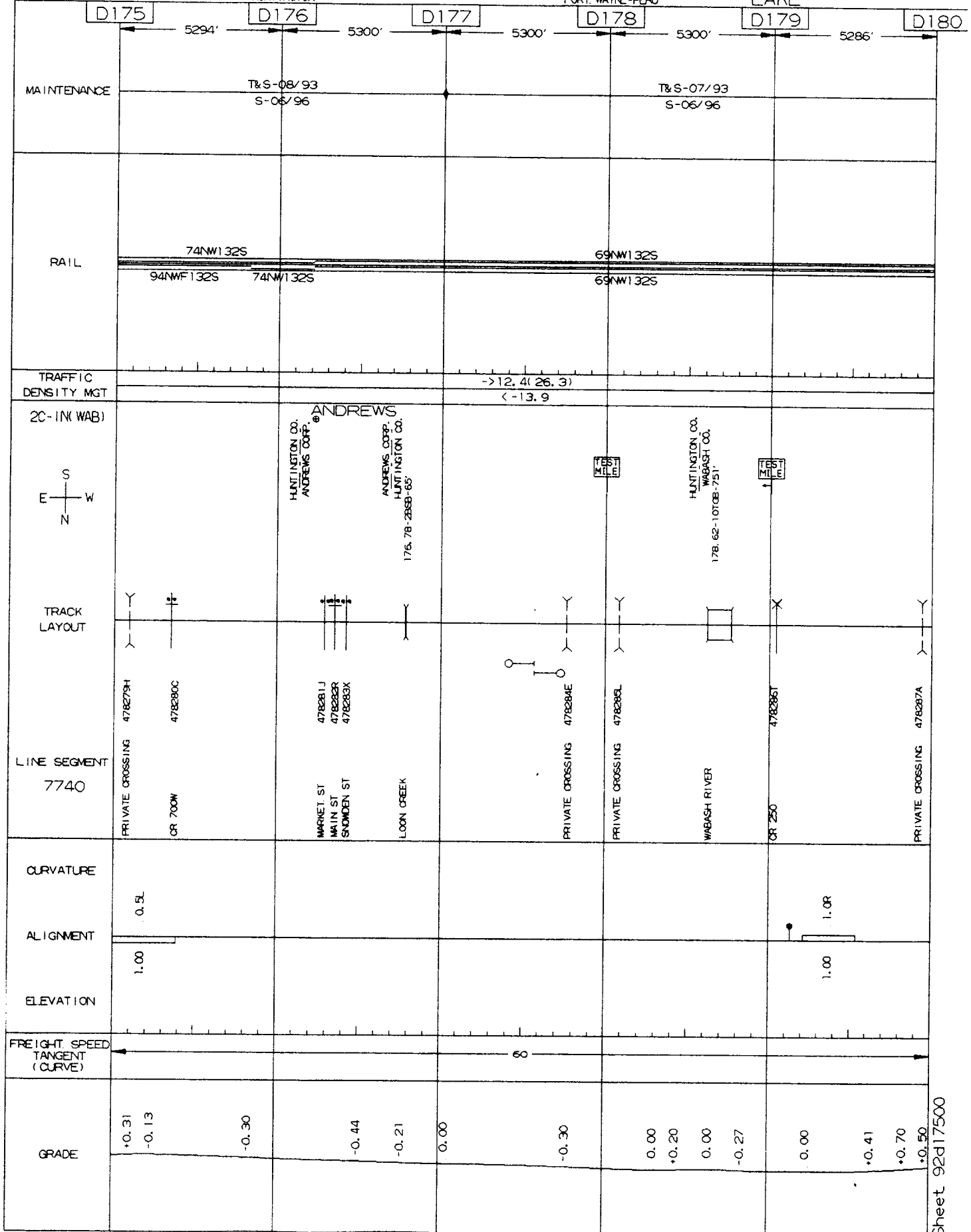


10/24/96

HUNTINGTON

FORT WAYNE-PERU

LAKE

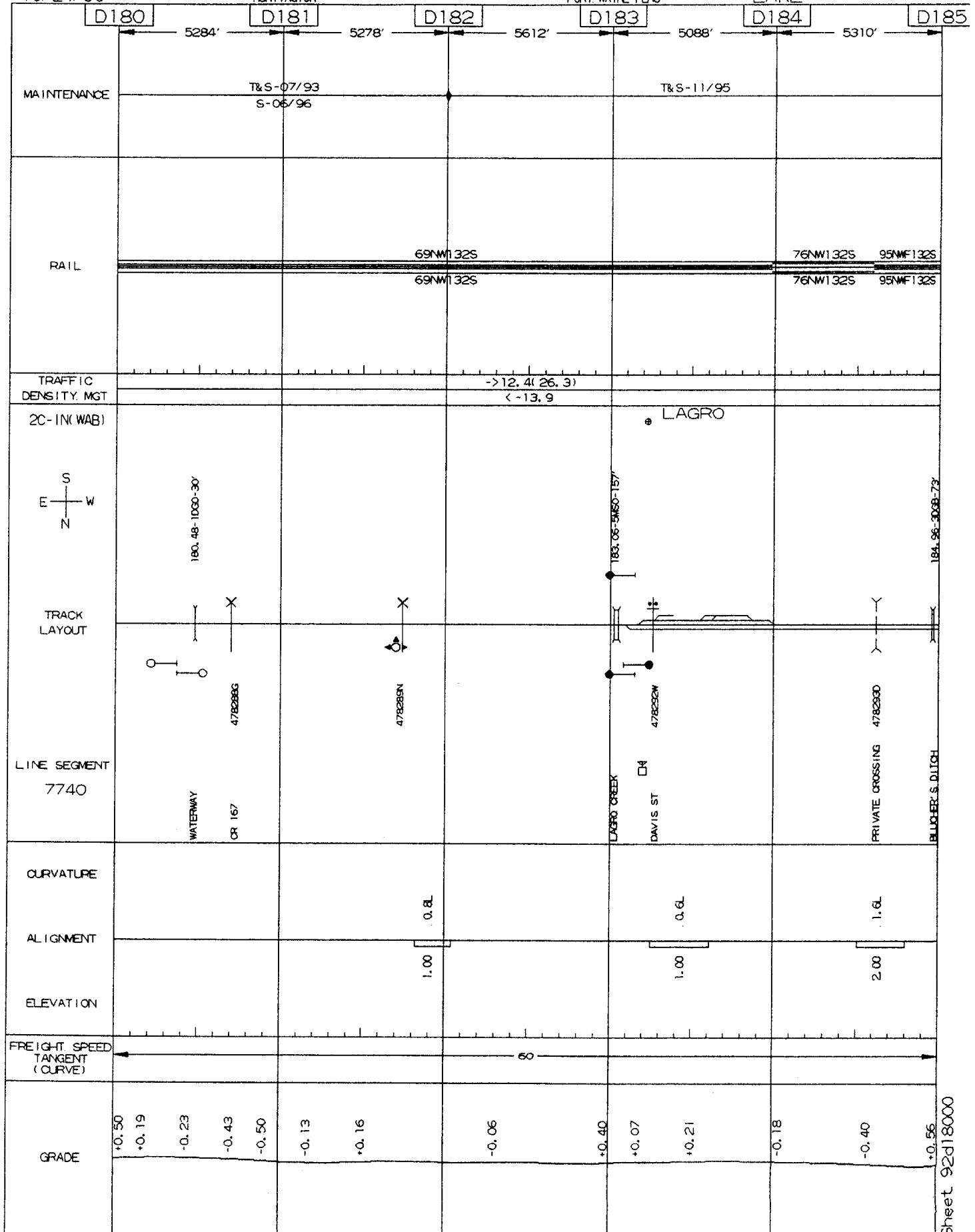


10/24/96

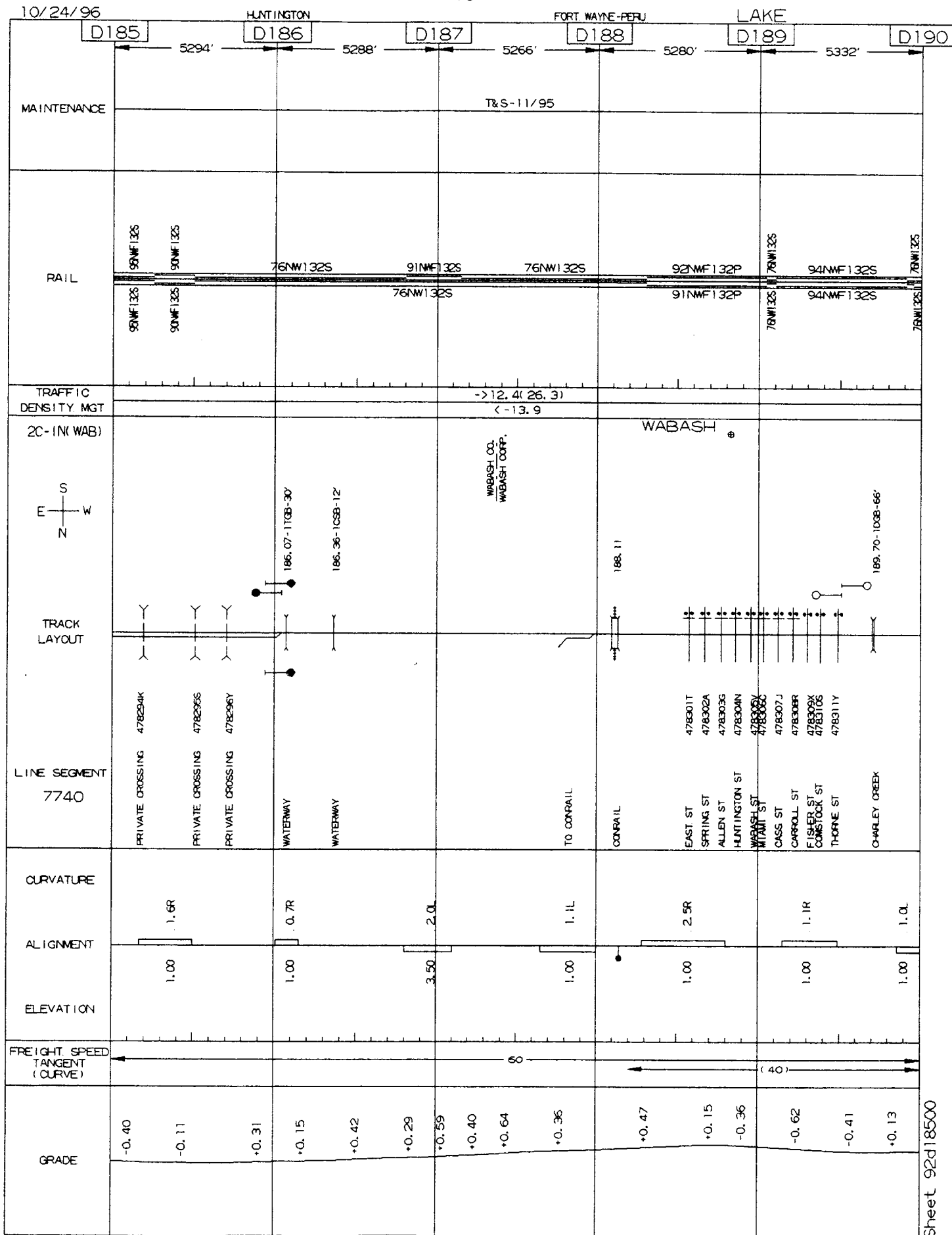
HUNTINGTON

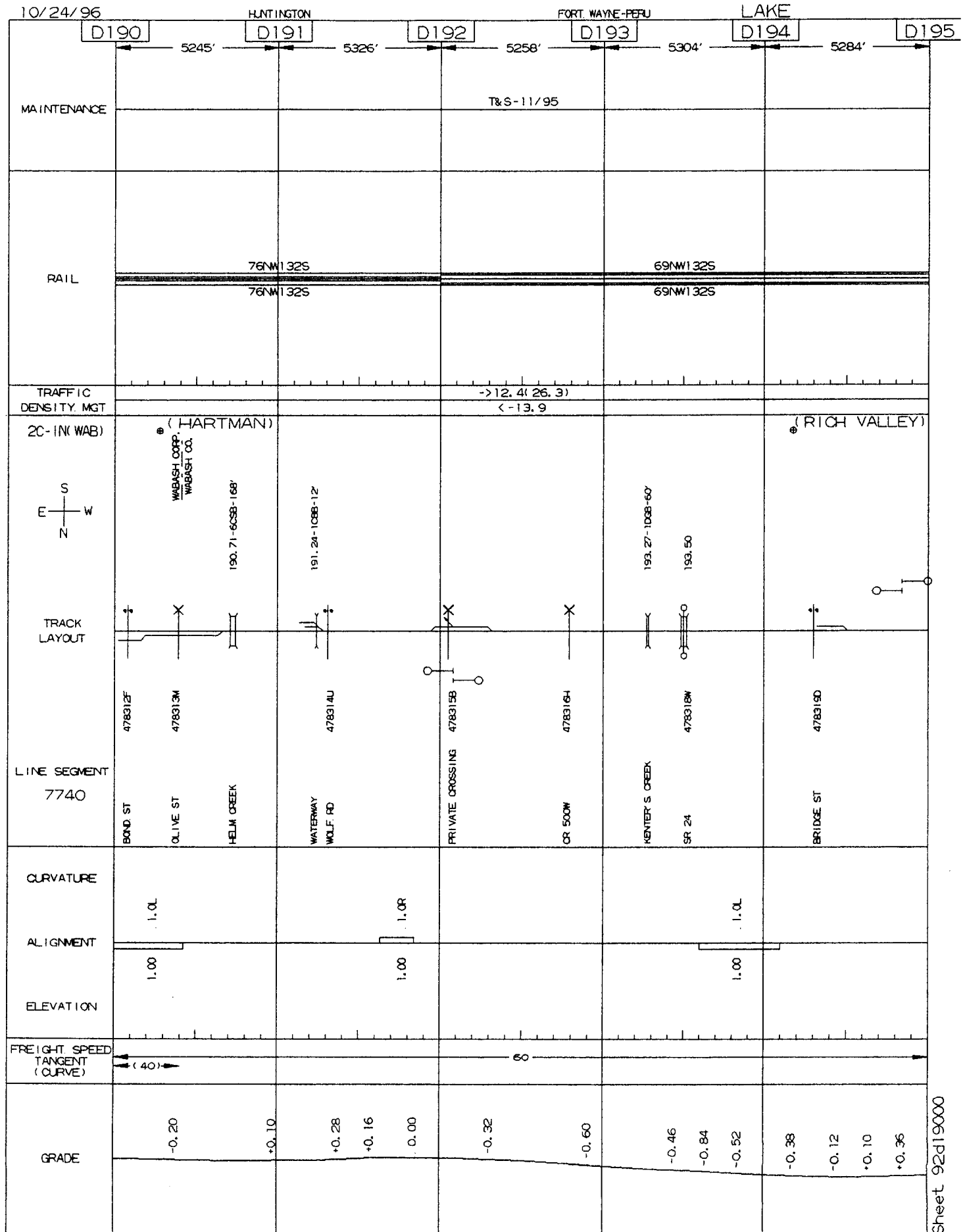
FORT WAYNE-PERU

LAKE



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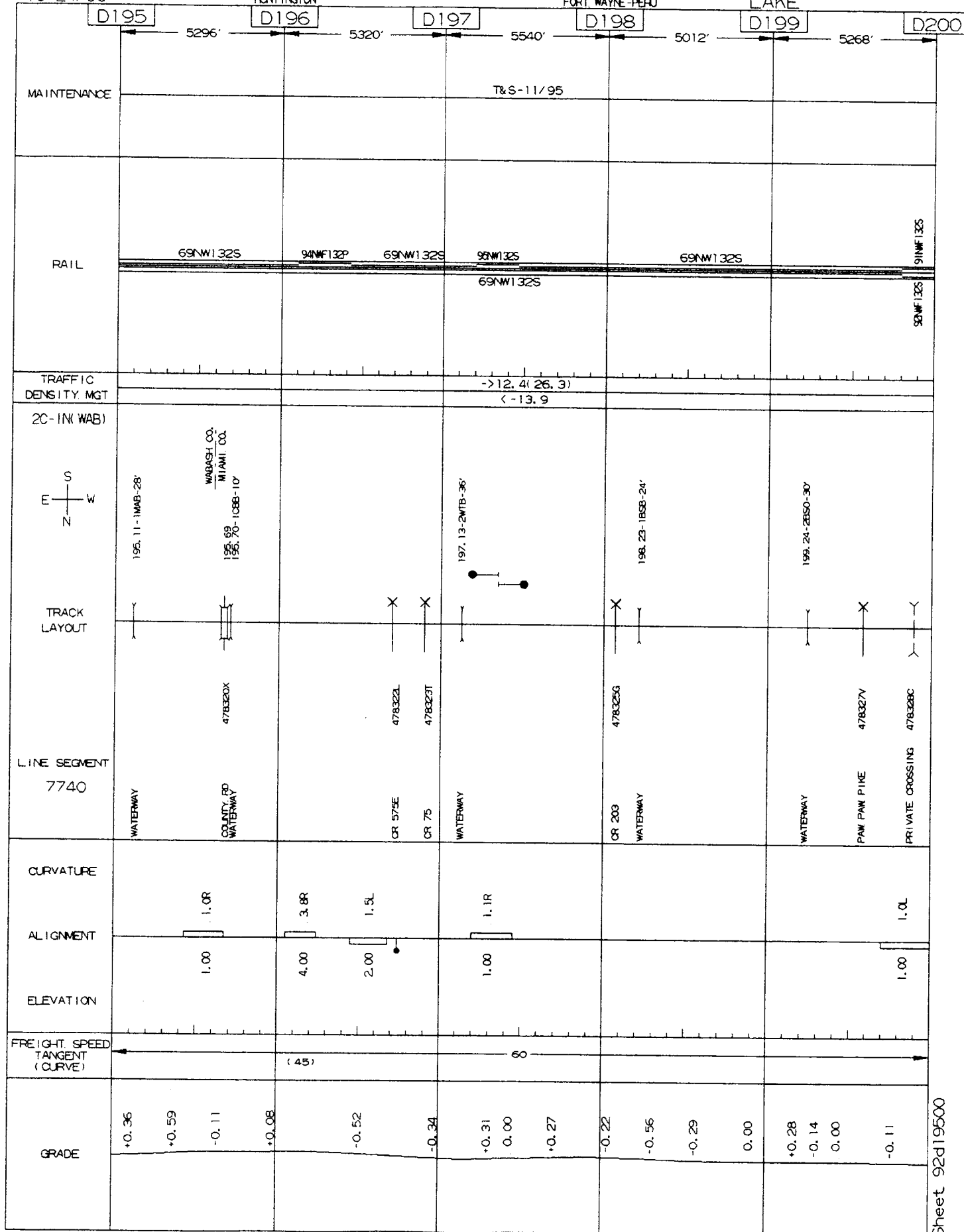


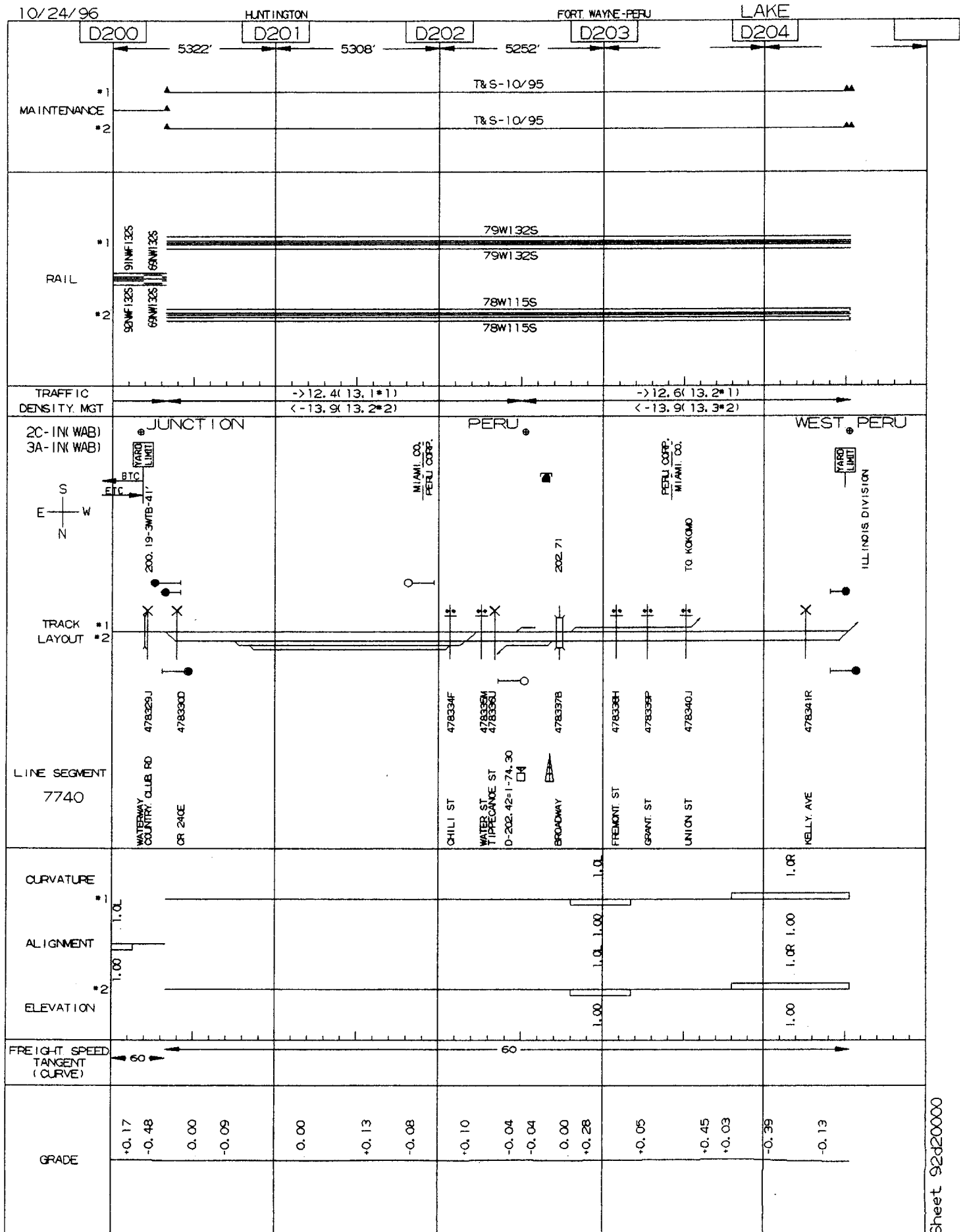
10/24/96

HUNTINGTON

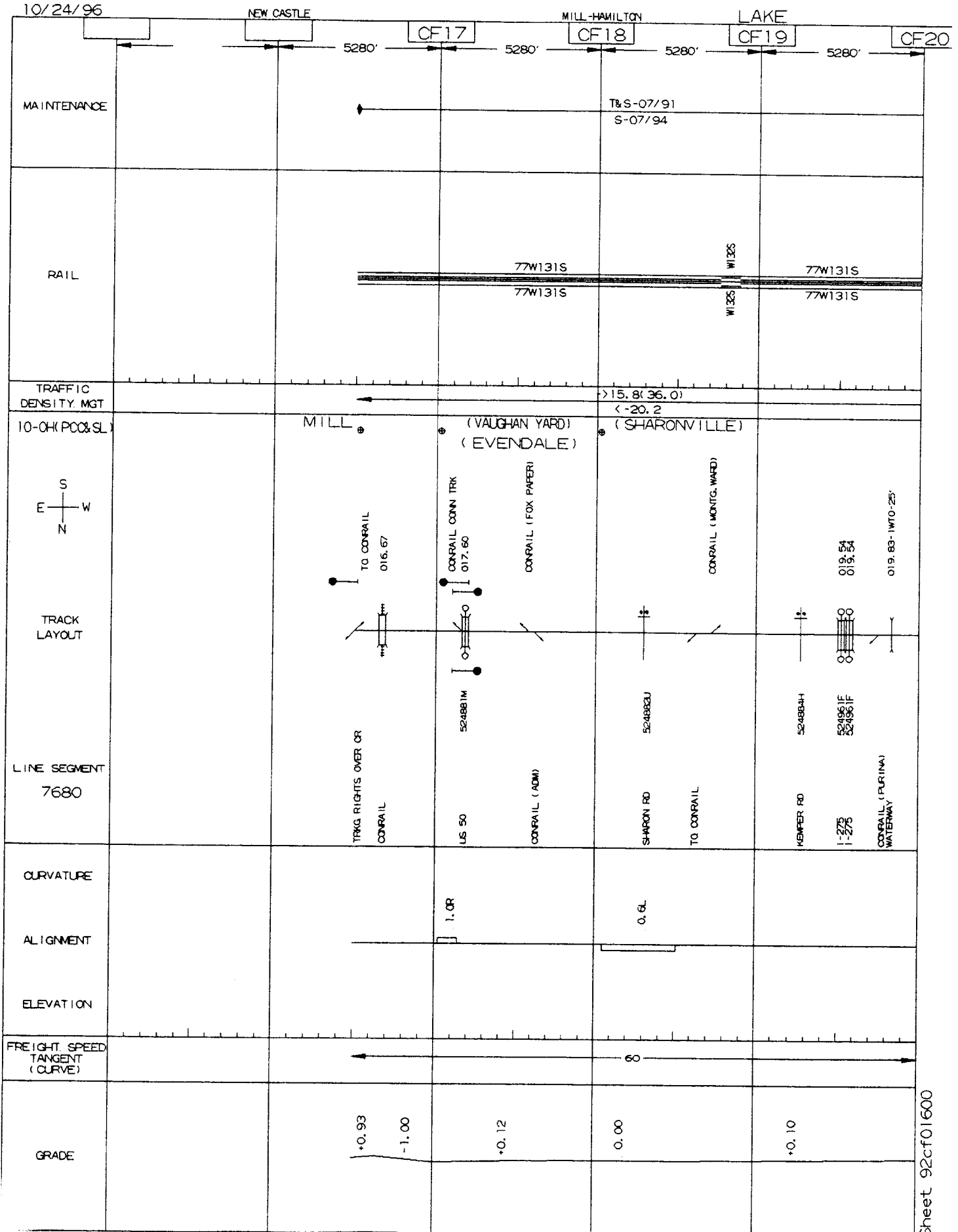
FORT WAYNE-PERU

LAKE





10/24/96

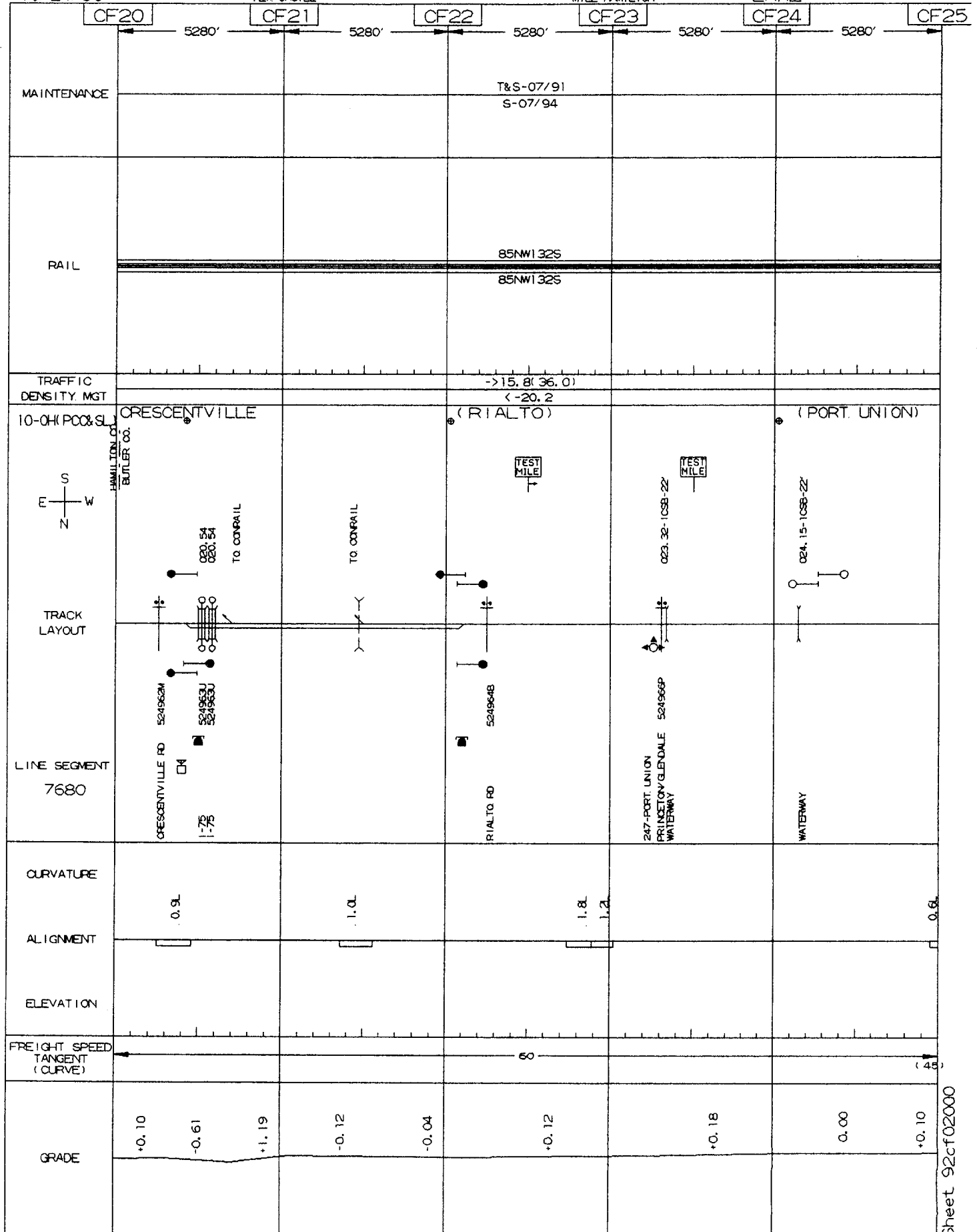


10/24/96

NEW CASTLE

MILL-HAMILTON

LAKE

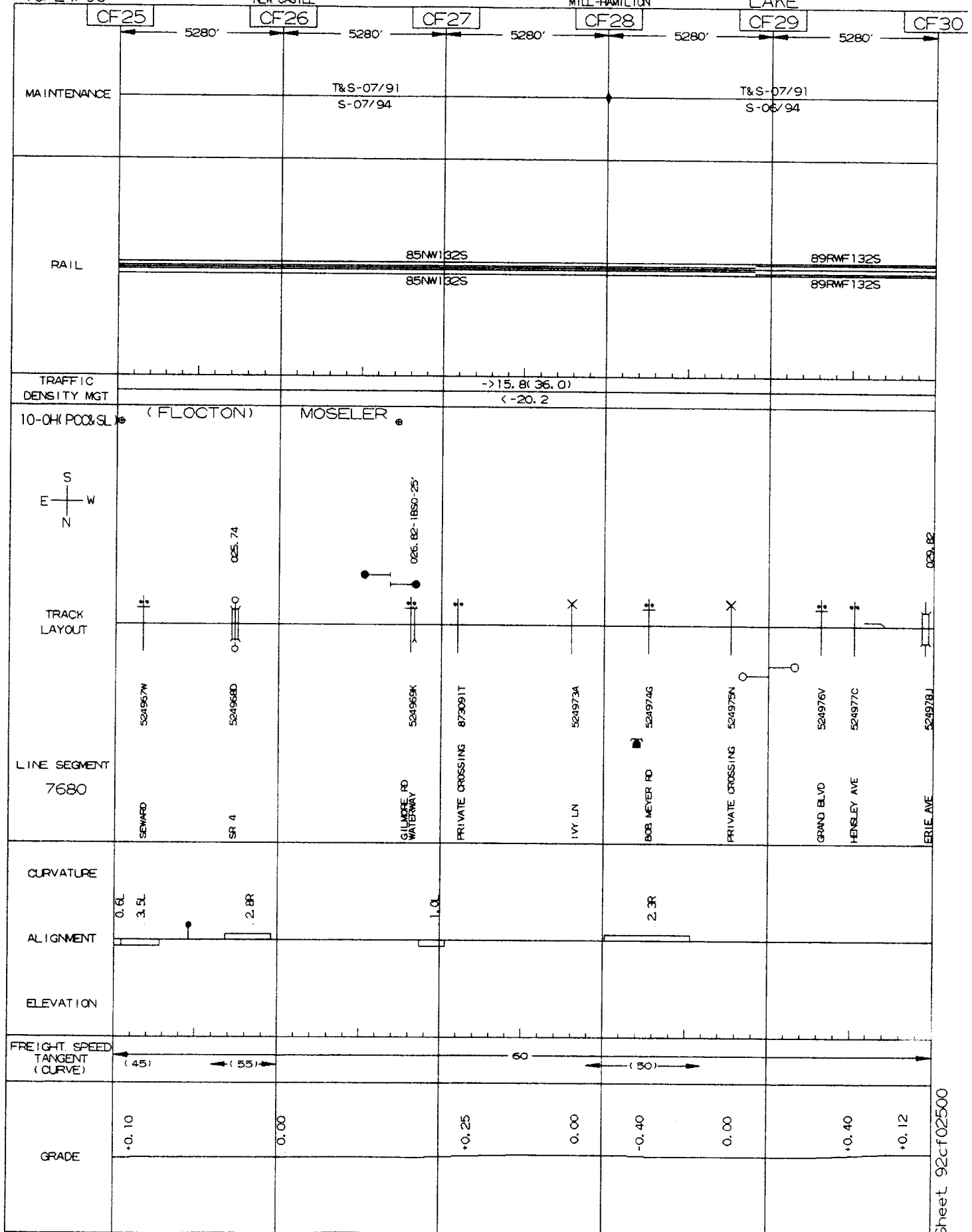


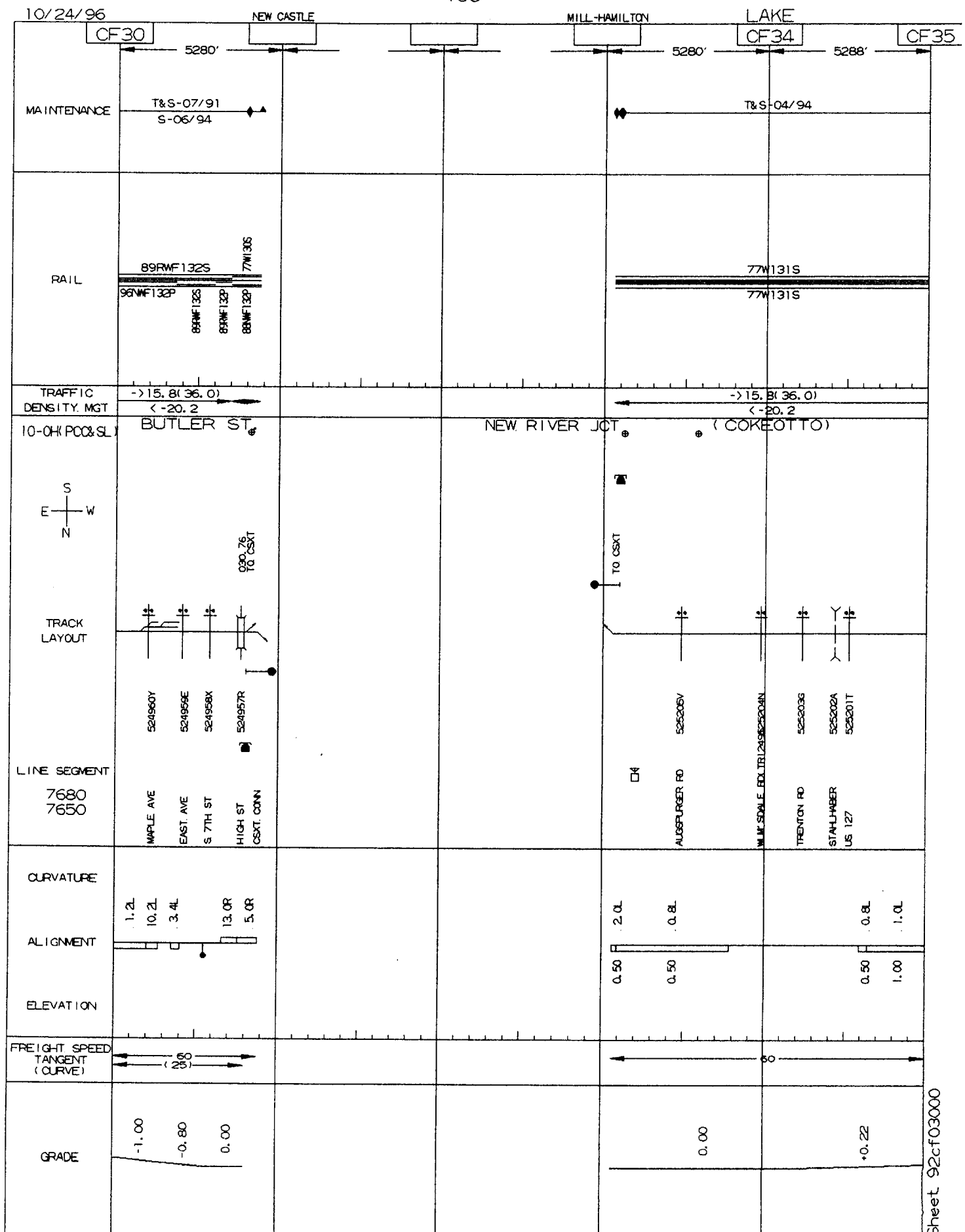
10/24/96

NEW CASTLE

MILL-HAMILTON

LAKE



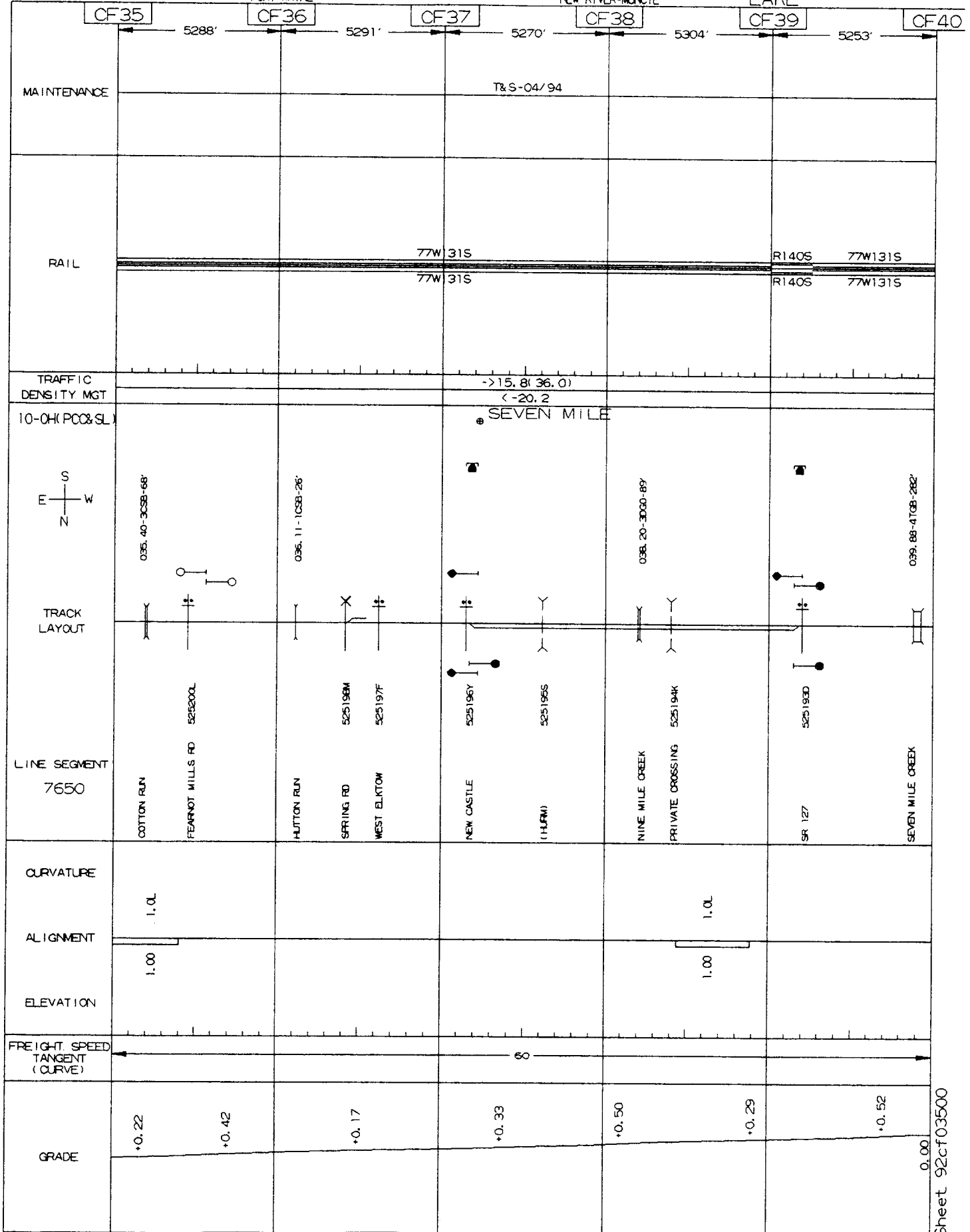


10/24/96

FORT WAYNE

NEW RIVER-MUNCIE

LAKE

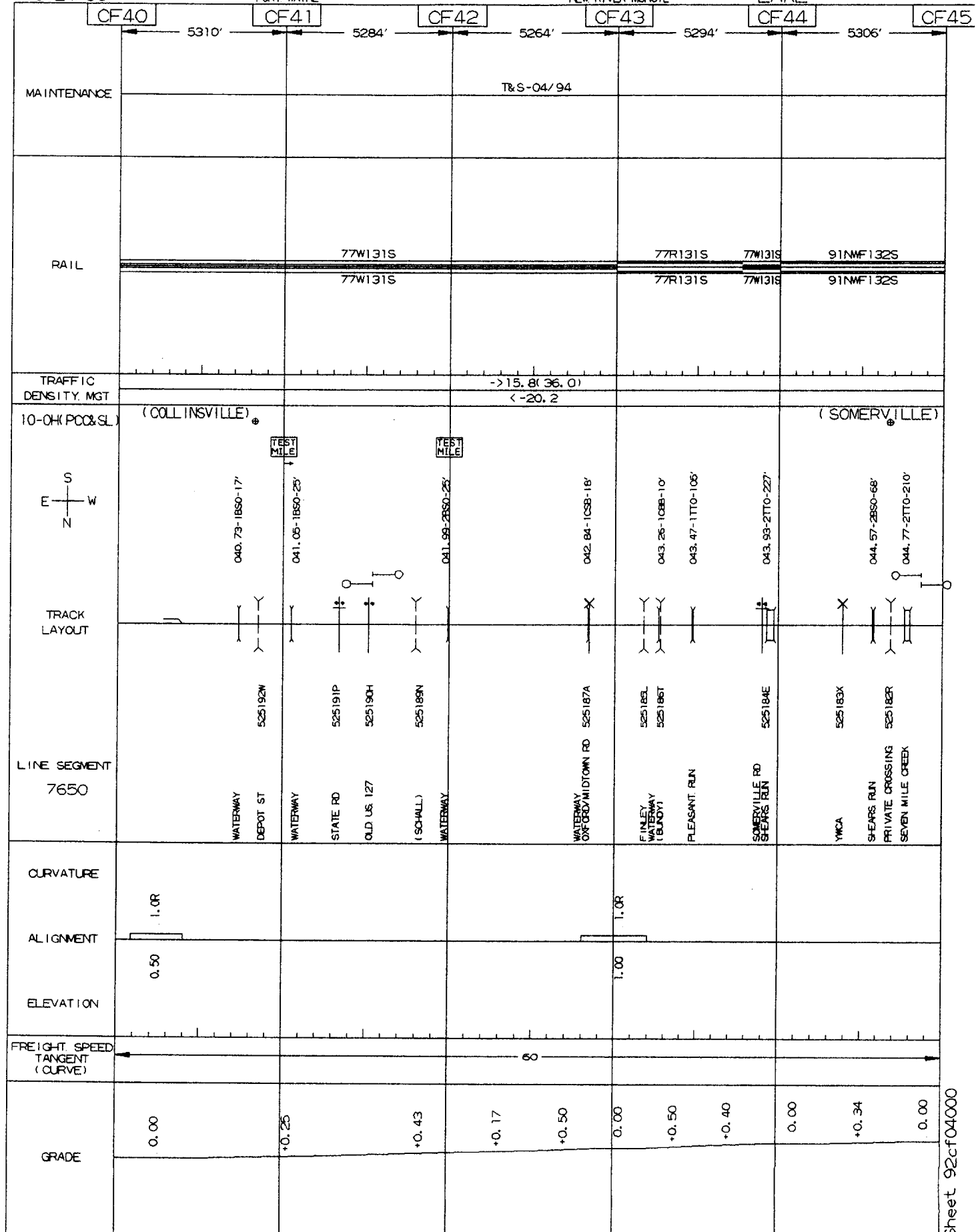


10/24/96

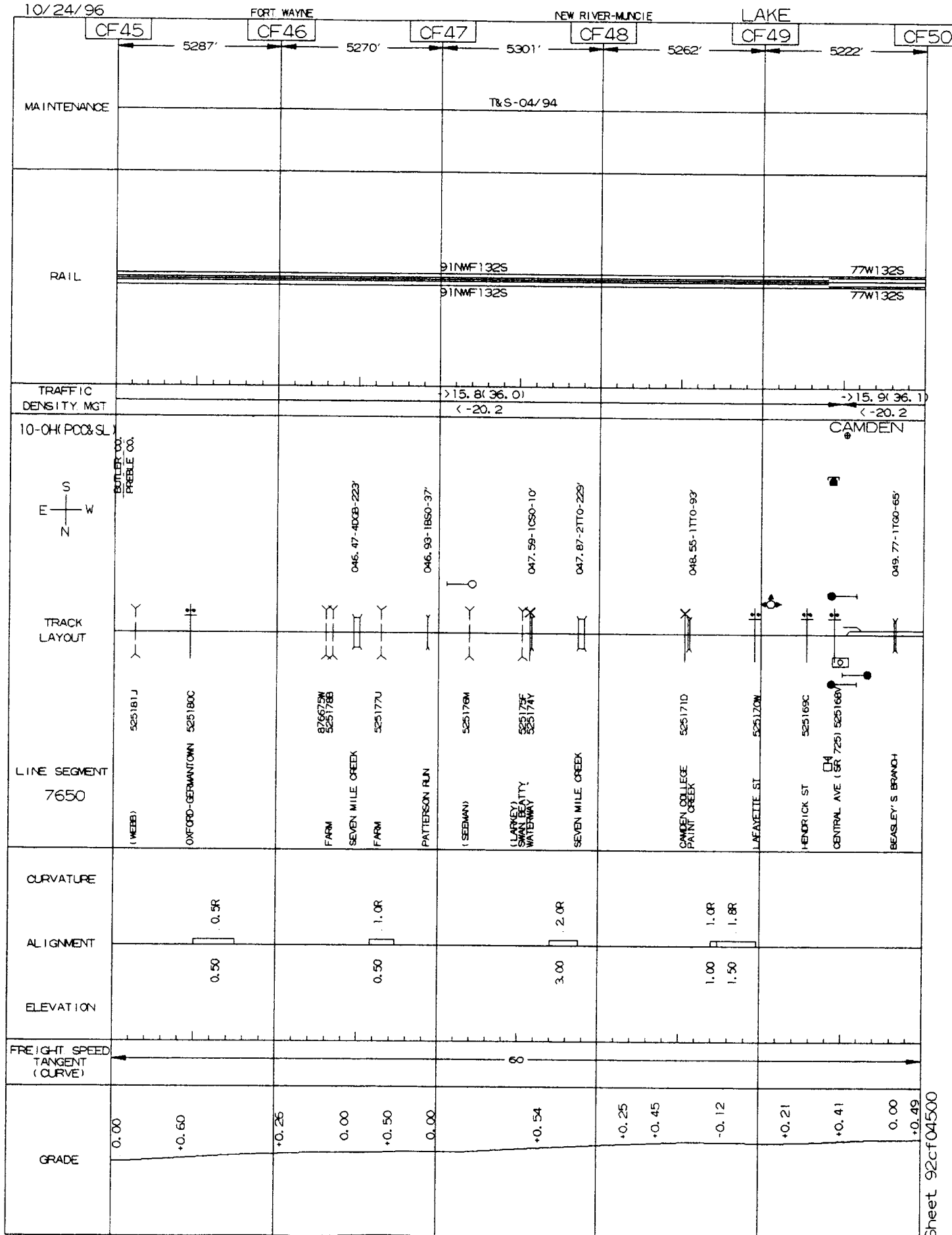
FORT WAYNE

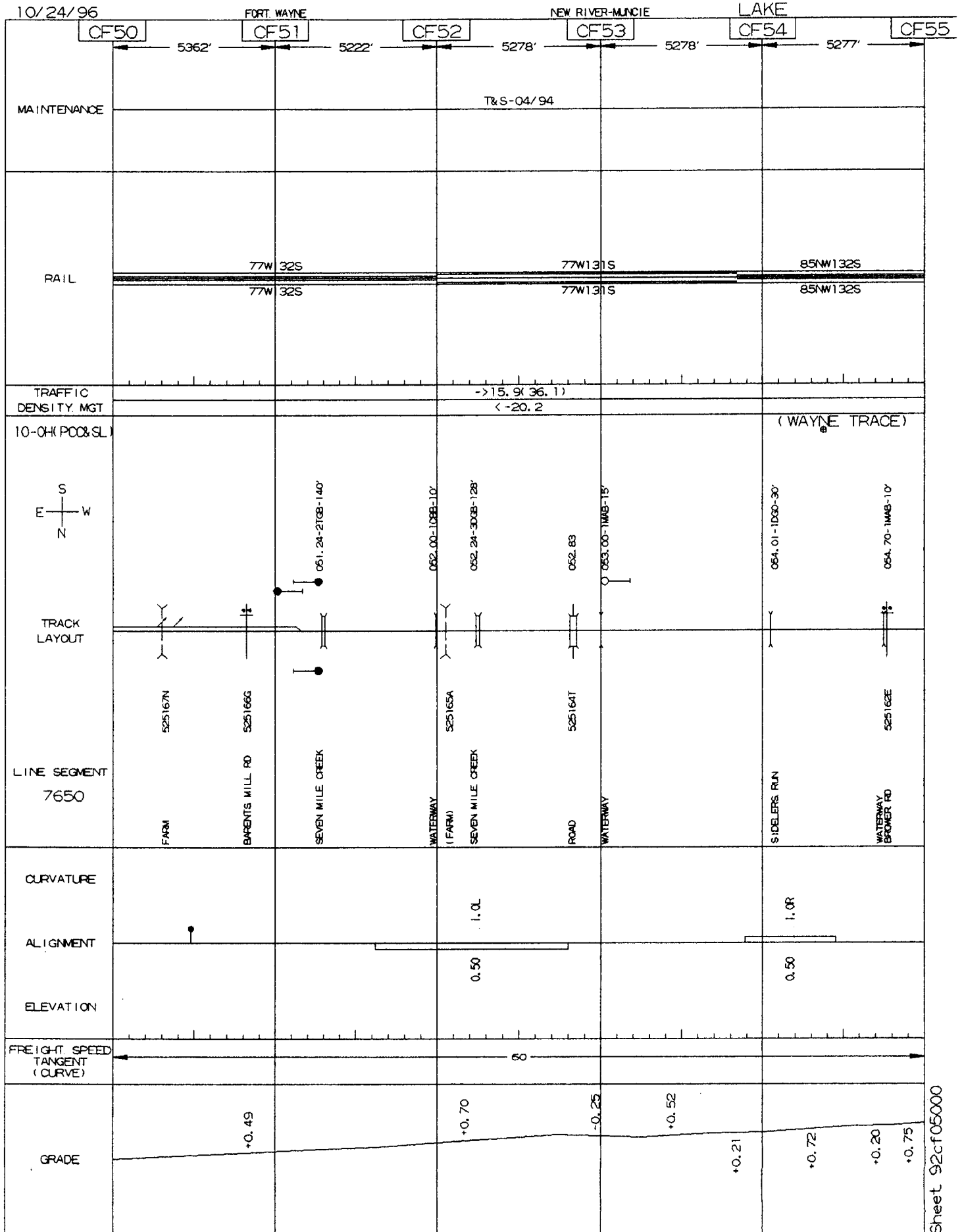
NEW RIVER-MUNCIE

LAKE







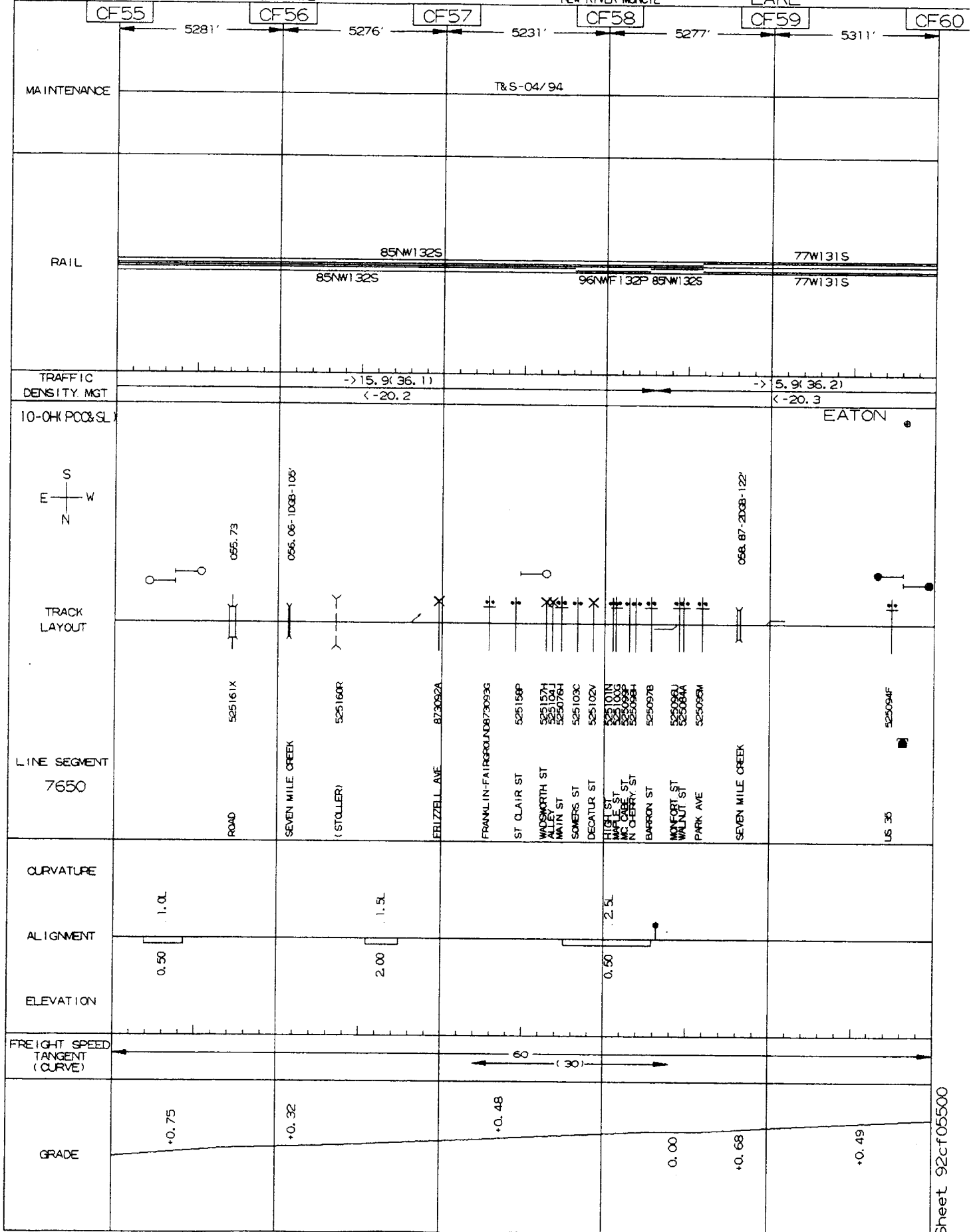


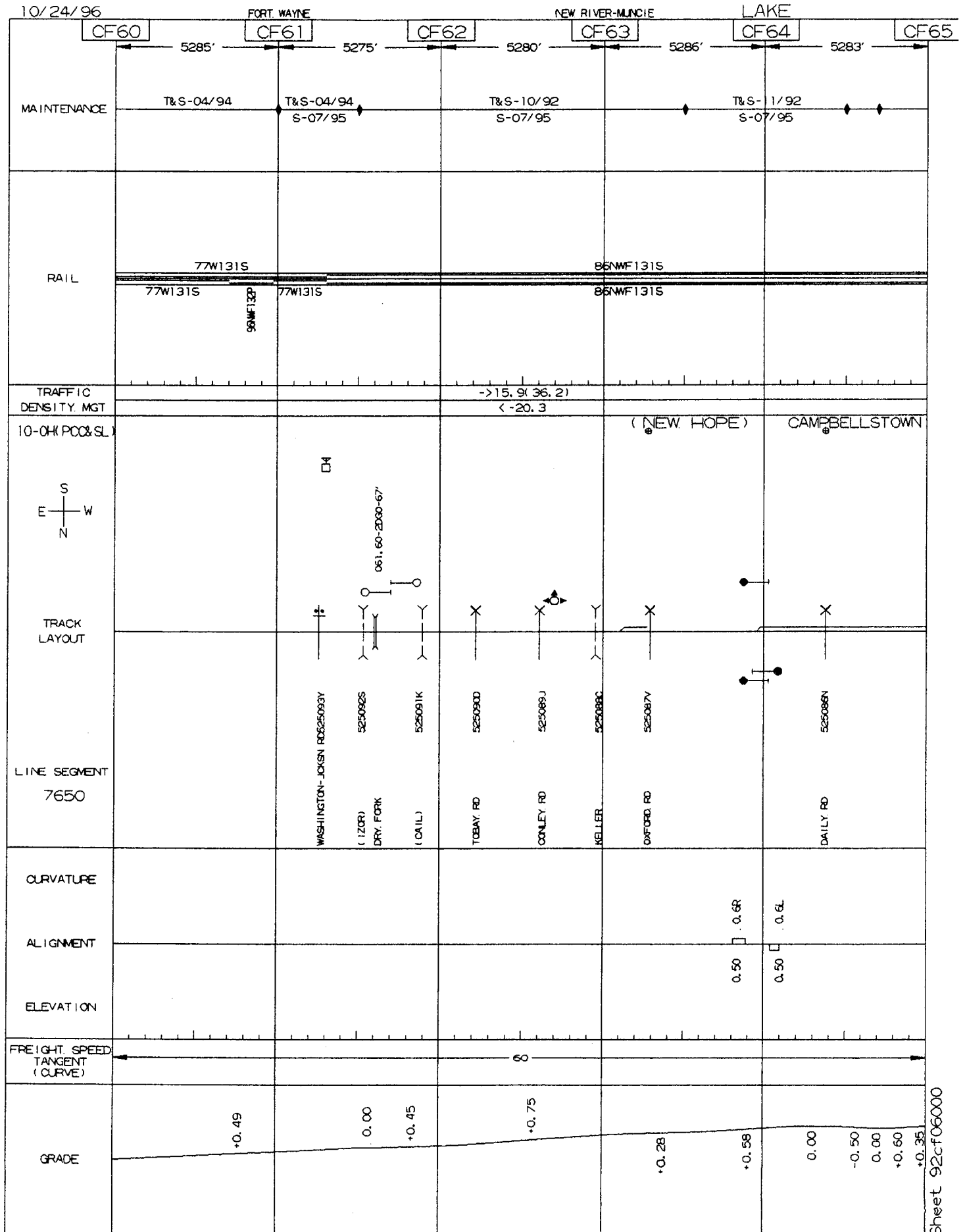
10/24/96

FORT WAYNE

NEW RIVER-MUNCIE

LAKE



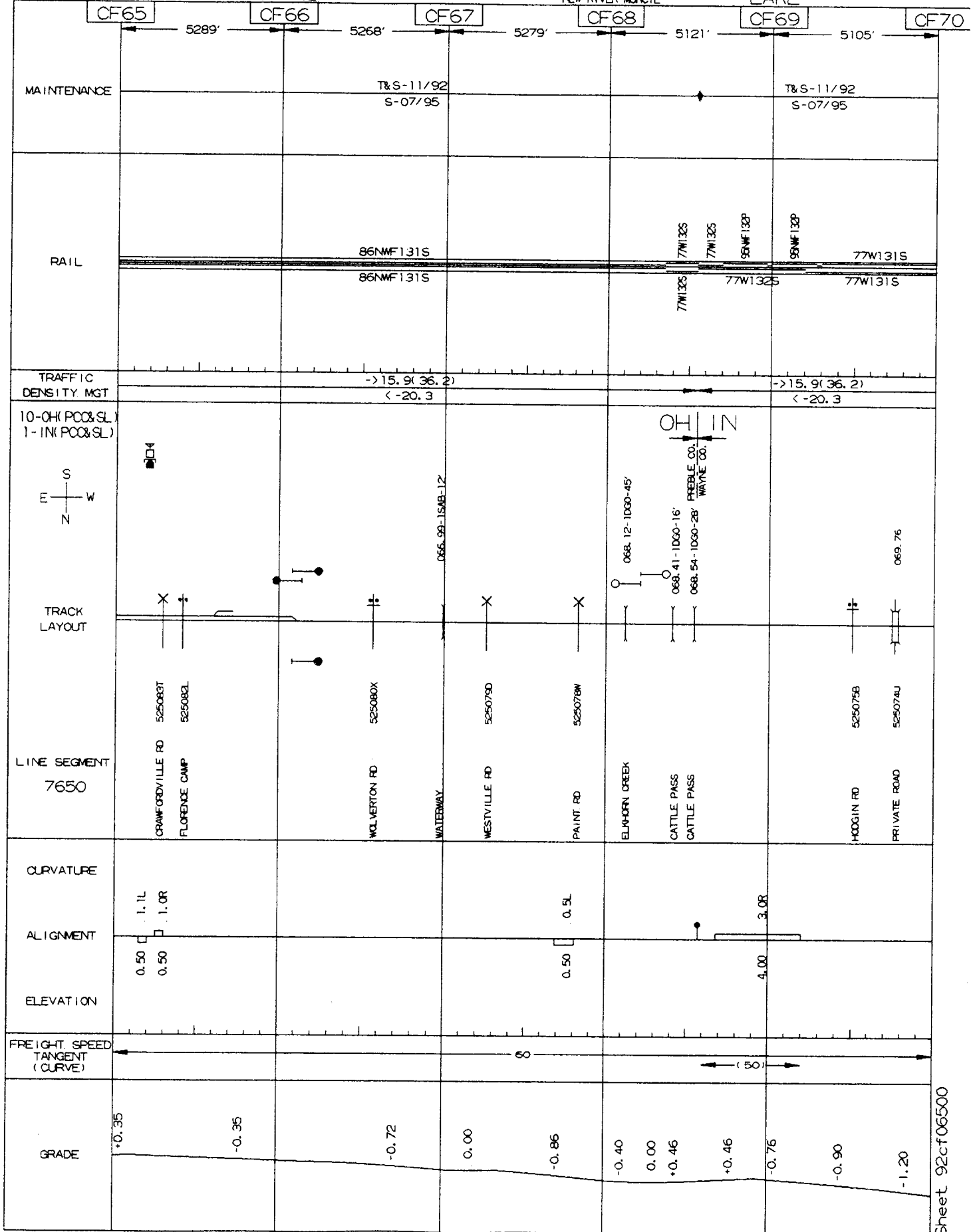


10/24/96

FORT WAYNE

NEW RIVER-MUNCIE

LAKE

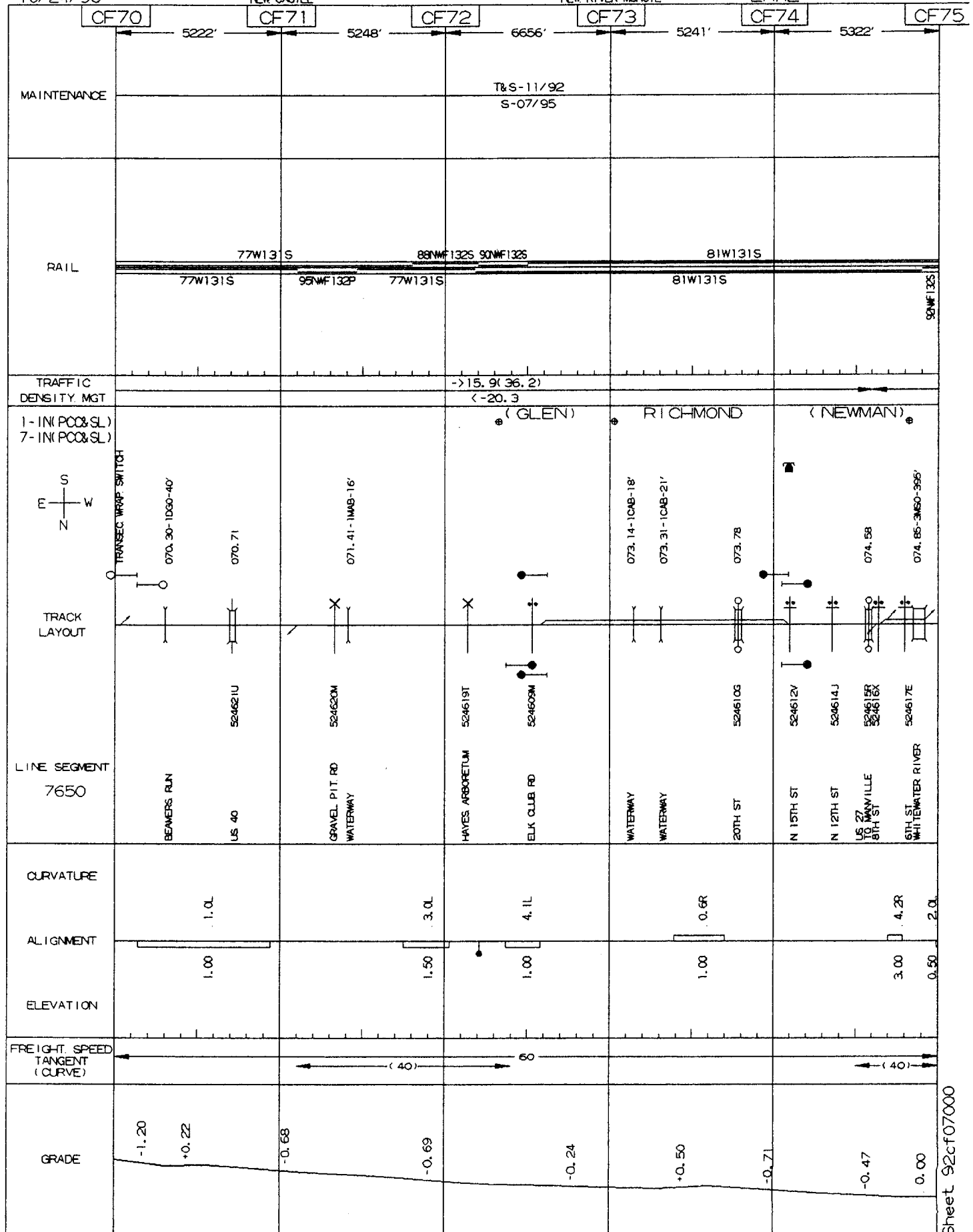


10/24/96

NEW CASTLE

NEW RIVER-MUNCIE

LAKE

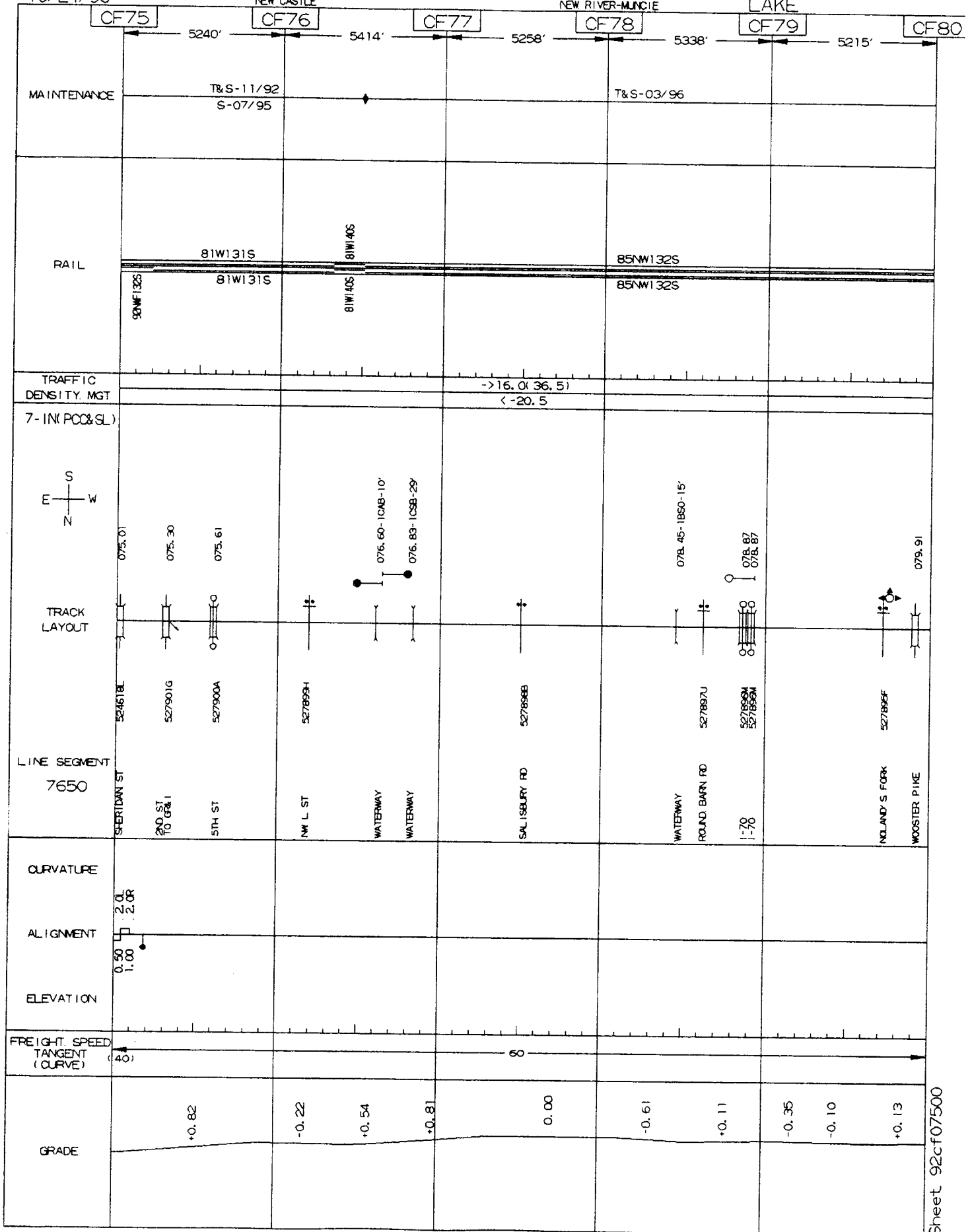


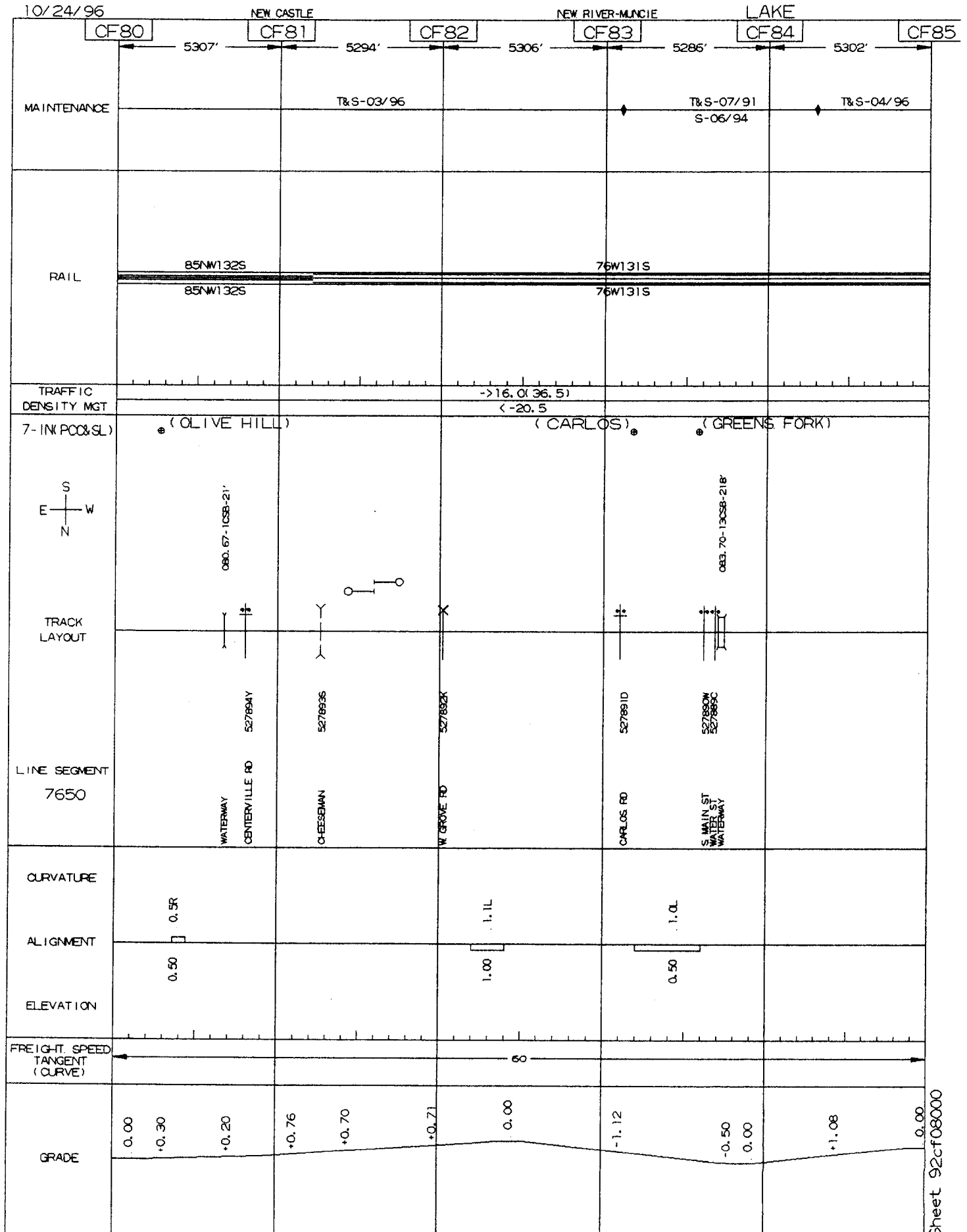
10/24/96

NEW CASTLE

NEW RIVER-MUNCIE

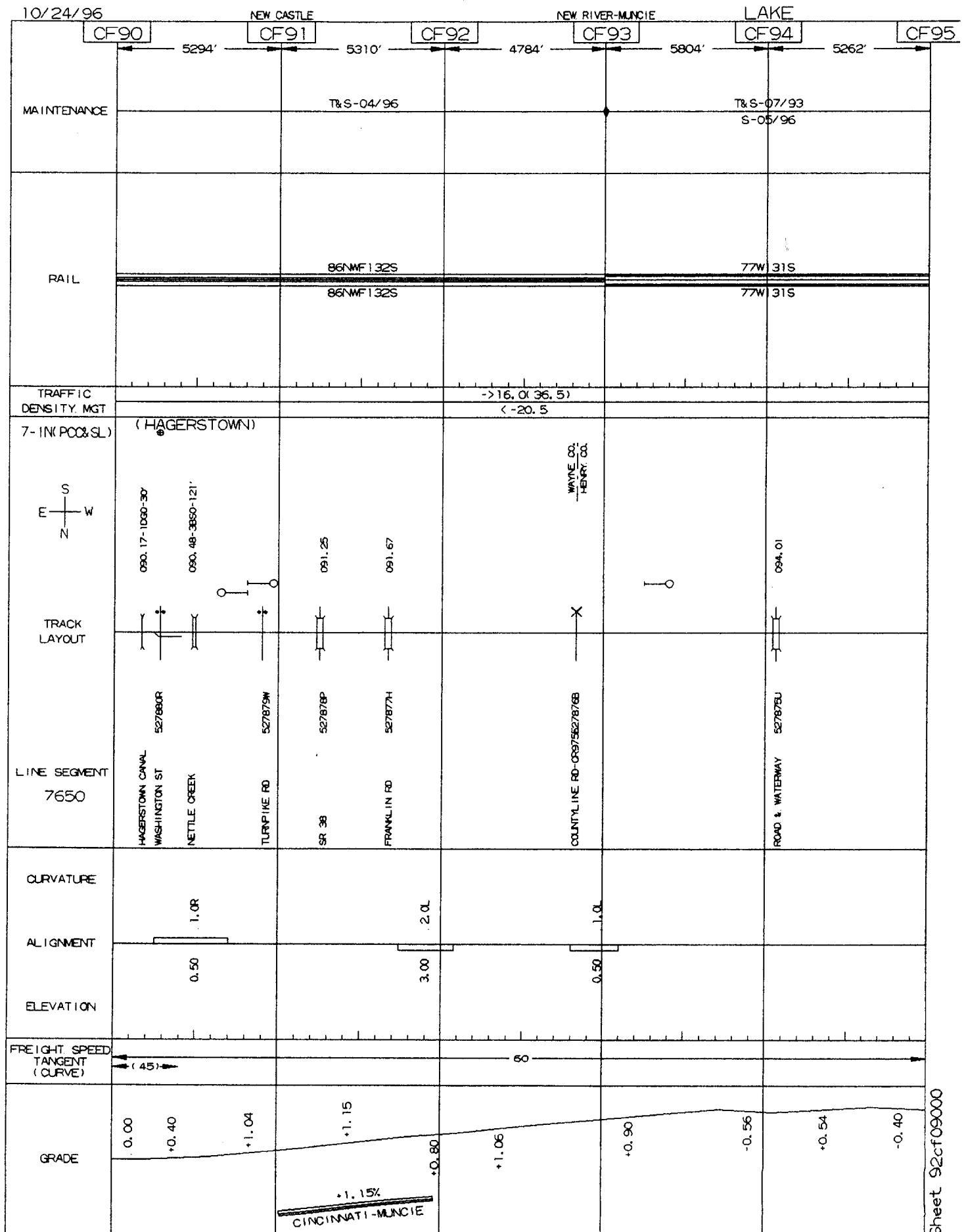
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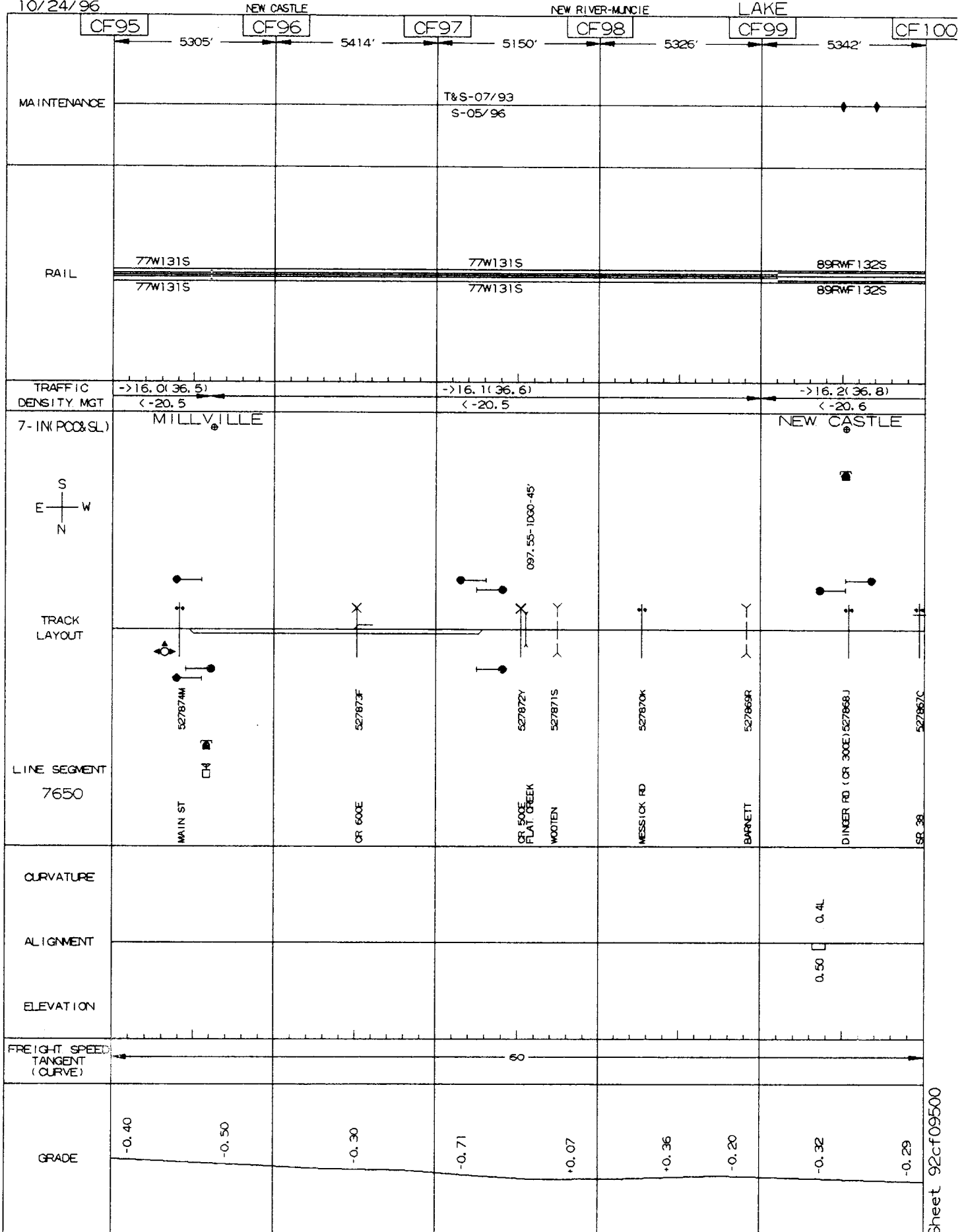


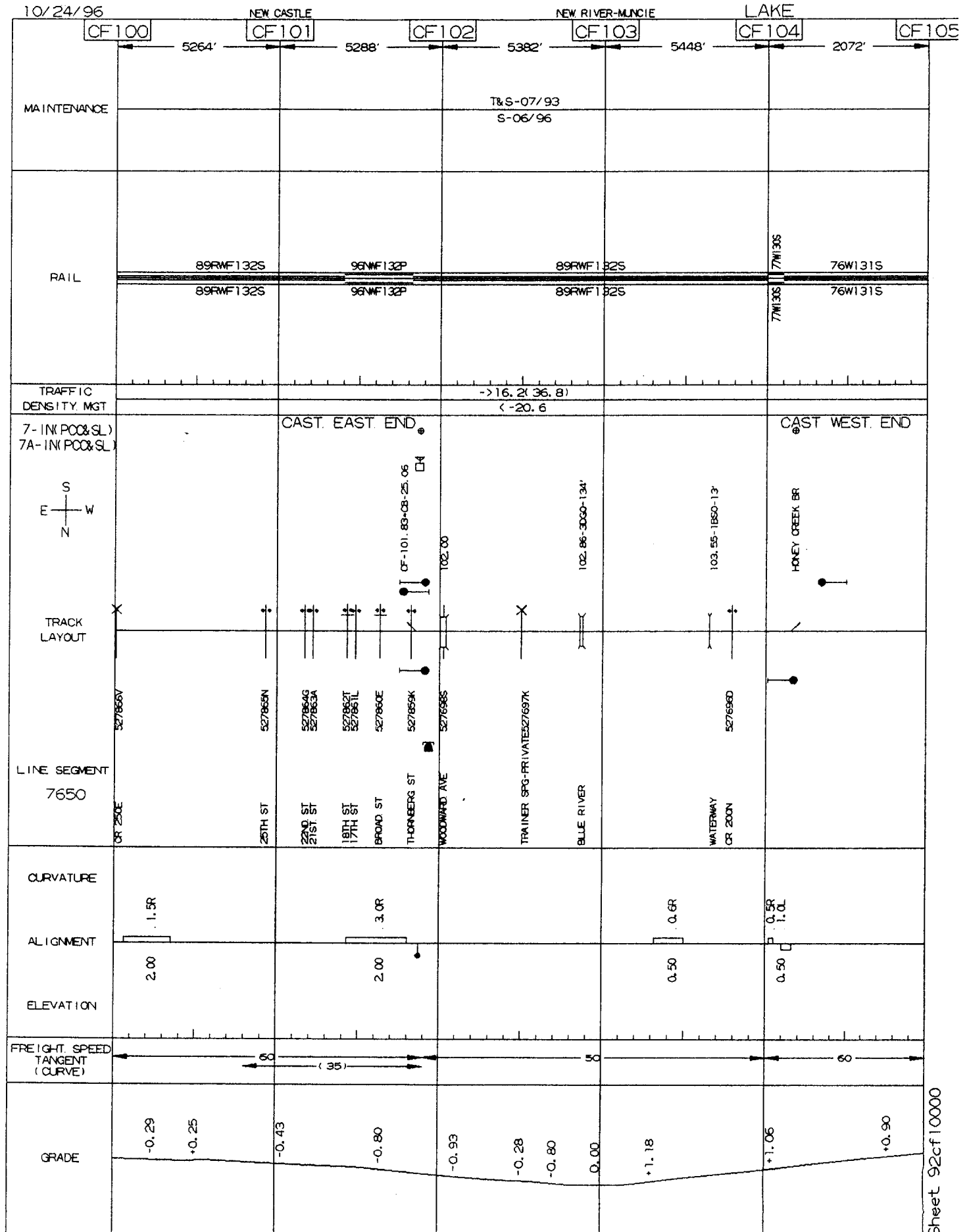


Sheet 92cf08500



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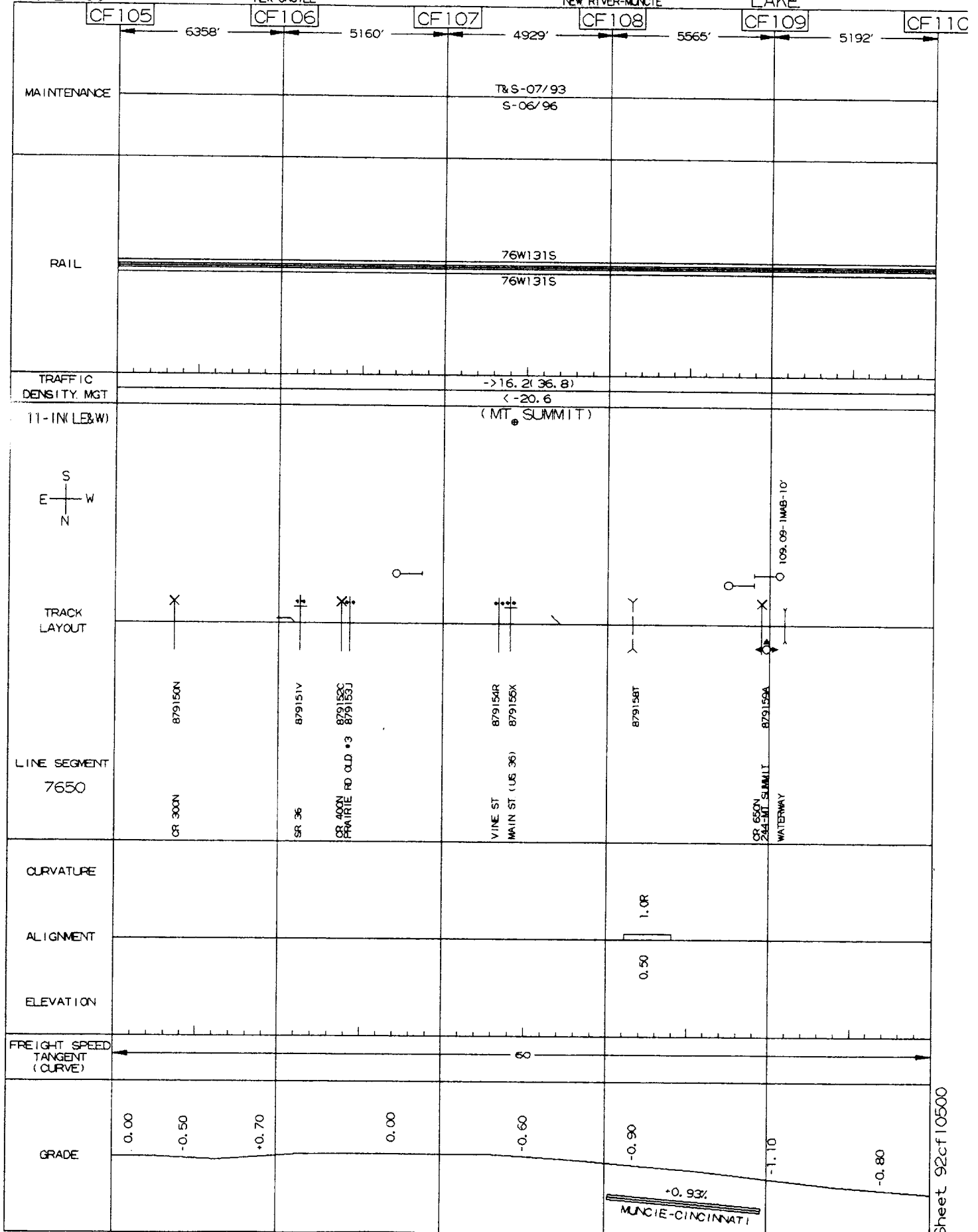


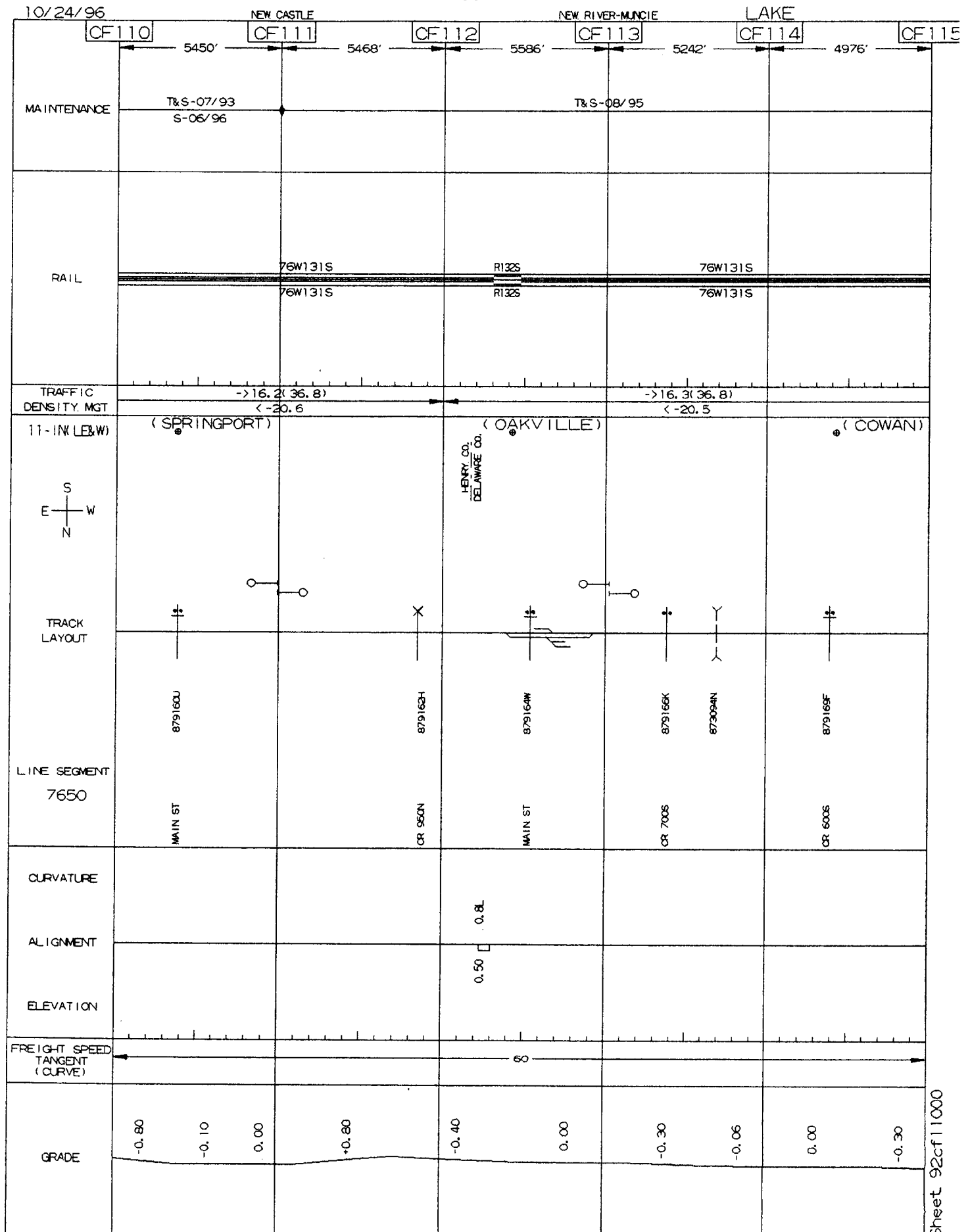
10/24/96

NEW CASTLE

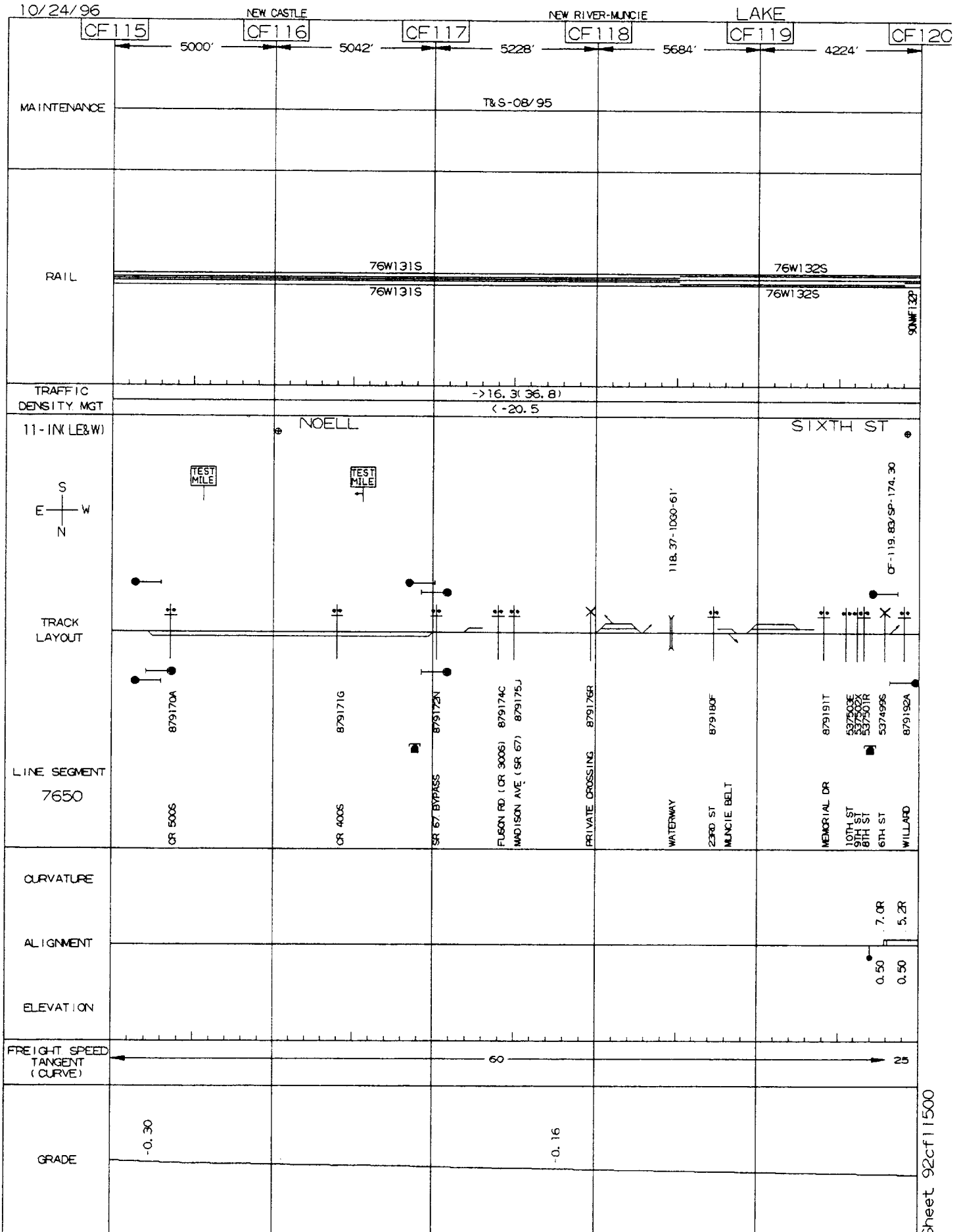
NEW RIVER-MUNCIE

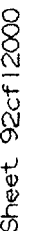
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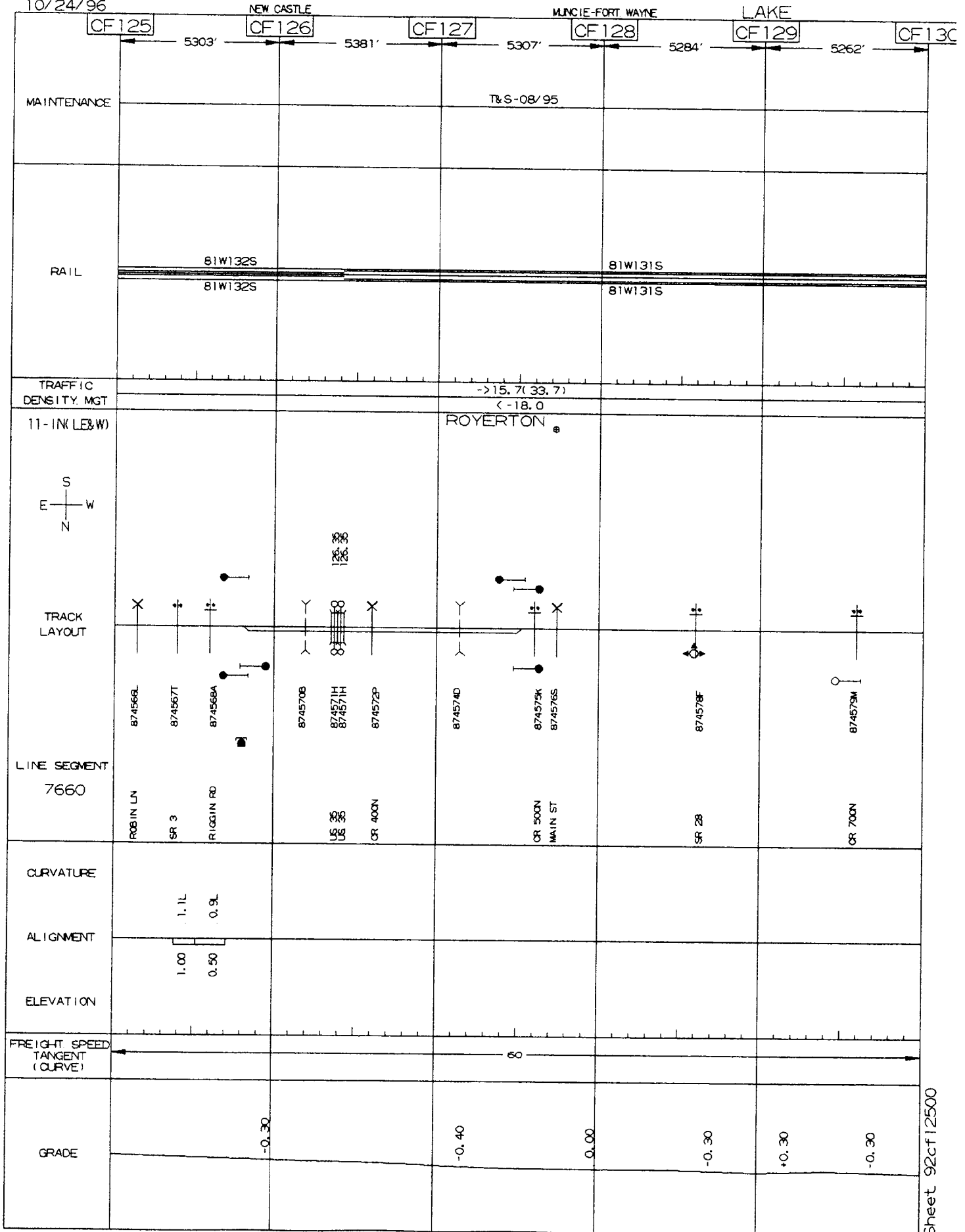
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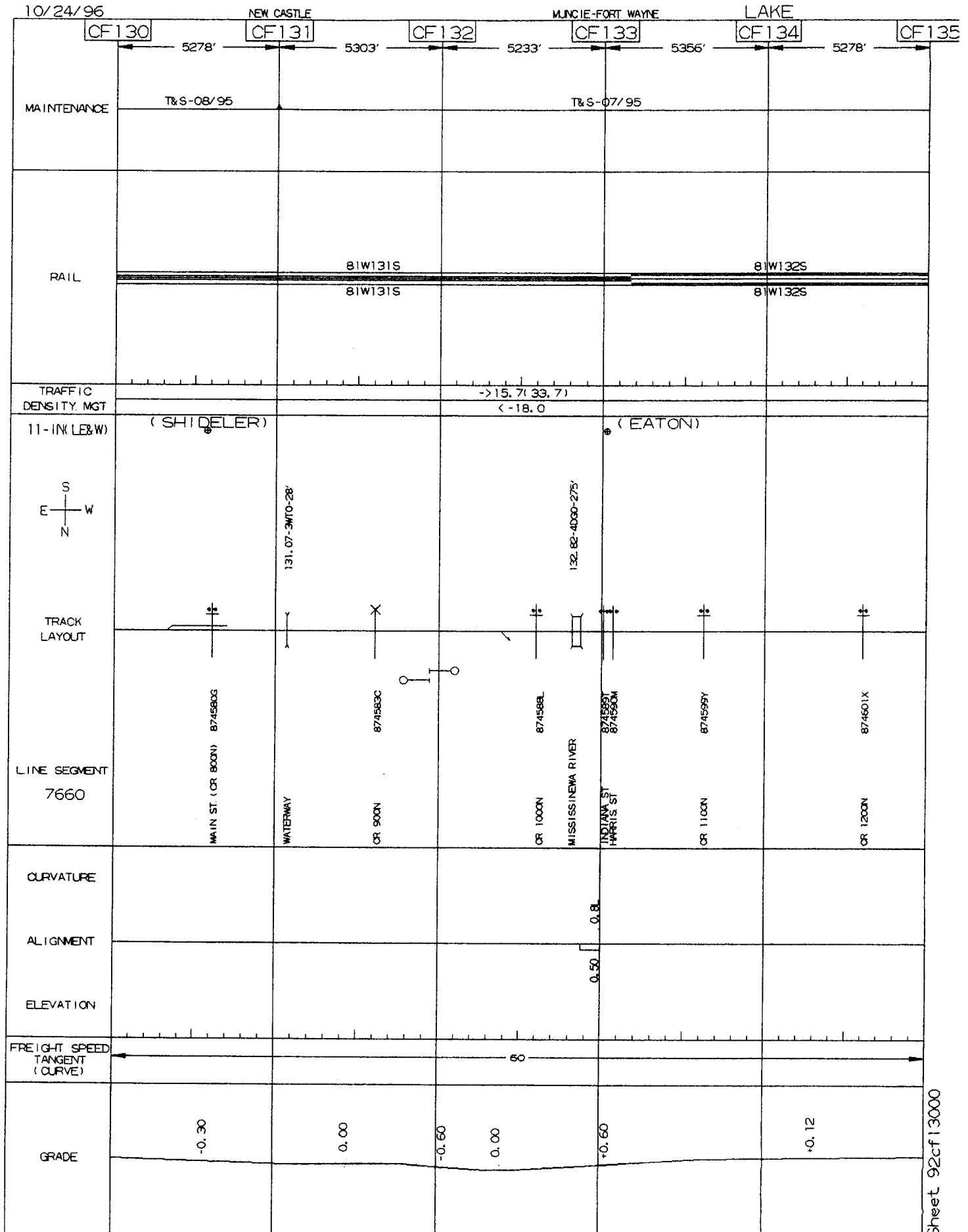






10/24/96





10/24/96

		NEW CASTLE		MUNCIE-FORT WAYNE		LAKE	
		CF 135	CF 136	CF 137	CF 138	CF 139	CF 140
		5306'	5294'	5286'	5293'	5306'	
MAINTENANCE				T&S-07/95			
RAIL				81W132S			
				81W132S			
TRAFFIC DENSITY MGT				-> 15.7 (33.7) < -18.0			
11-IN (LE&W)							
TRACK LAYOUT							
LINE SEGMENT							
CURVATURE							
ALIGNMENT							
ELEVATION							
FREIGHT. SPEED TANGENT (CURVE)				60			
GRADE		+0.12 -0.60	-0.17	0.00	-0.40	+0.40	-0.60
		+0.60% FT. WAYNE-MUNCIE					

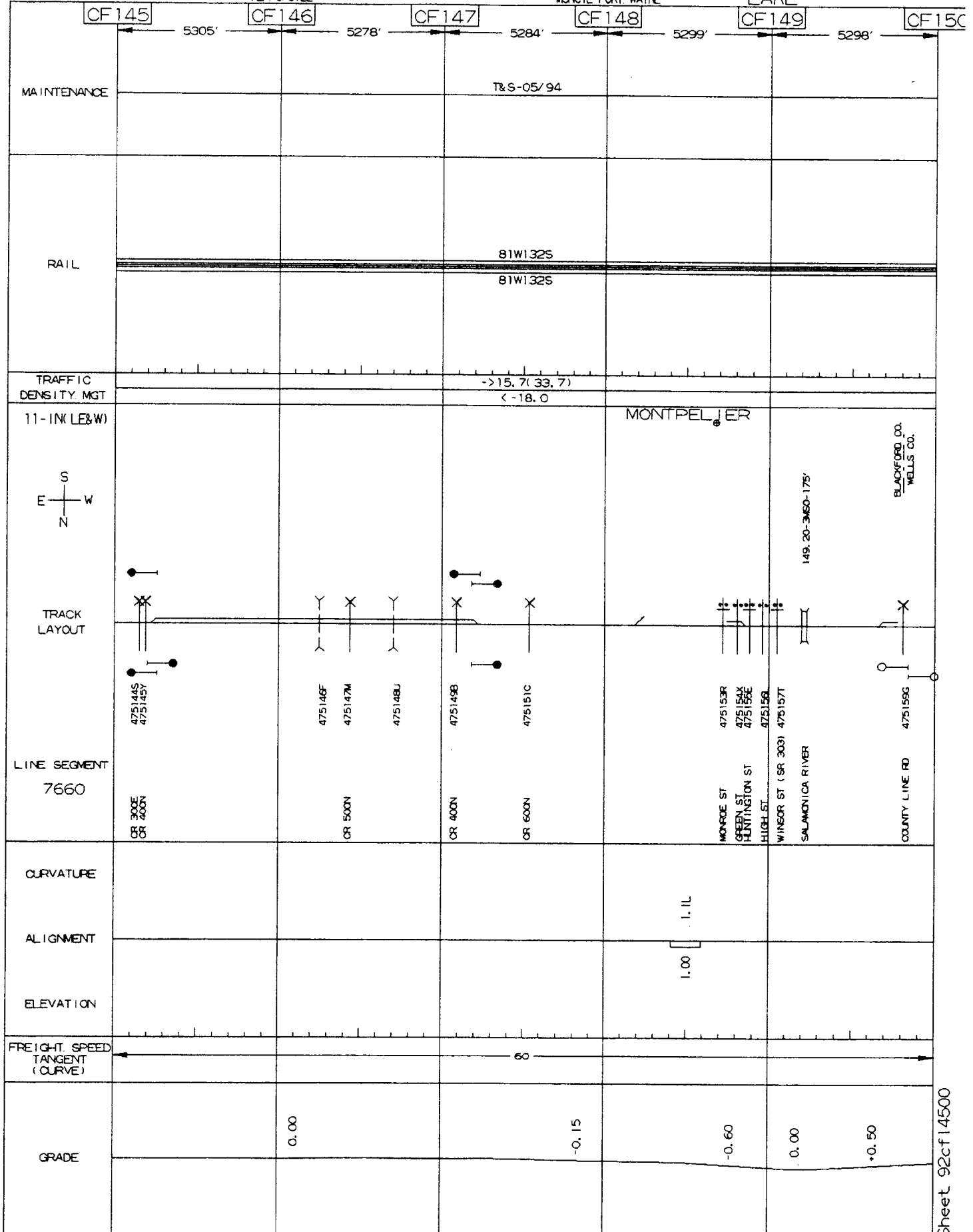
10/24/96		NEW CASTLE		MUNCIE-FORT. WAYNE		LAKE					
CF140		CF141		CF142		CF143		CF144		CF145	
5276'		5290'		5295'		5287'		5291'			
MAINTENANCE		T&S-07/95				T&S-05/94					
RAIL				81W132S							
				81W132S							
TRAFFIC DENSITY MGT				->15.7(33.7)							
11-IN(LE&W)		(HARTFORD CITY)		<-18.0							
<div><div>S</div><div>E</div><div>N</div><div>W</div></div>		<div><div>140.17</div><div>140.37-1MAB-19'</div><div>CH</div></div>									
TRACK LAYOUT		<div><div><div><div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div>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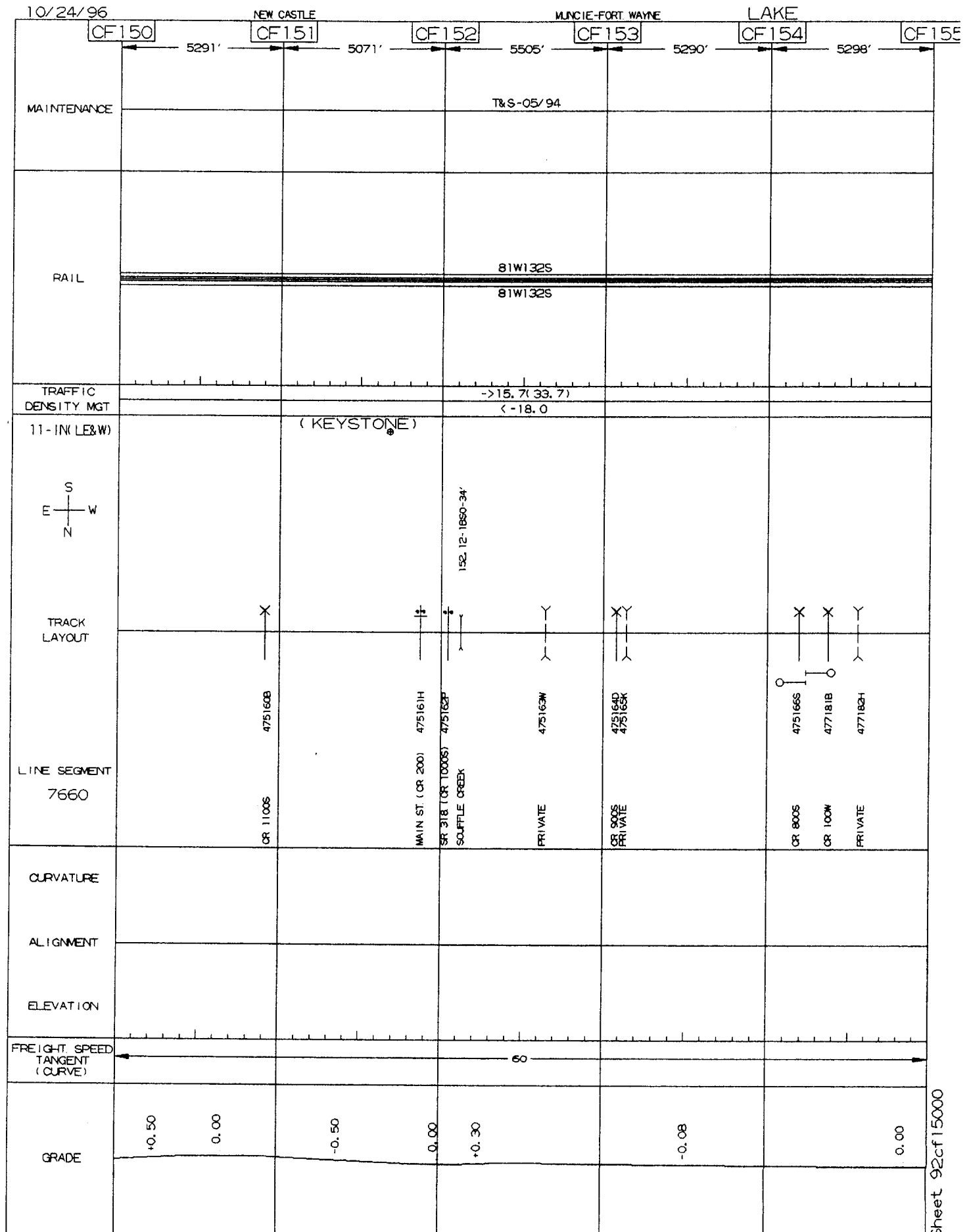
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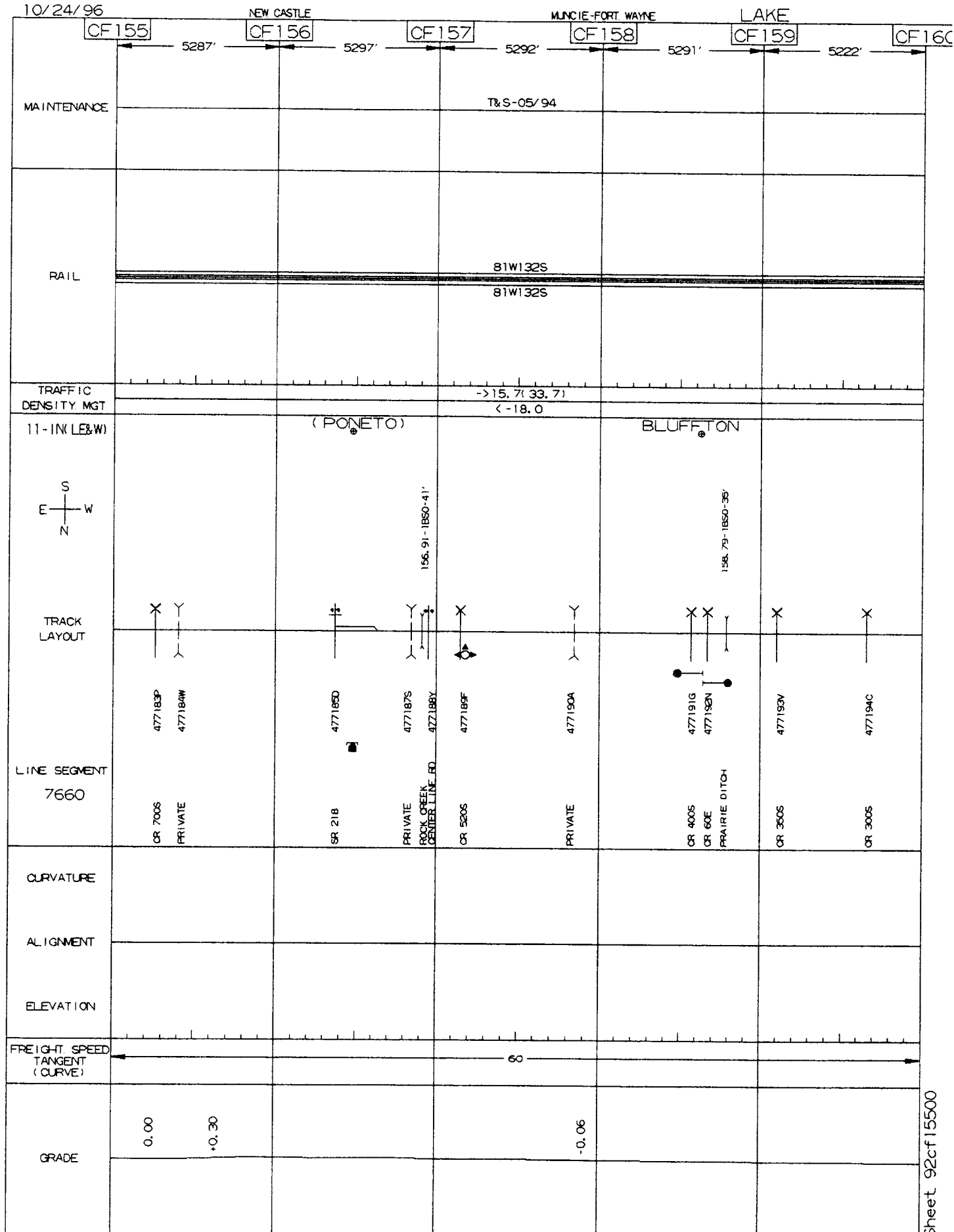
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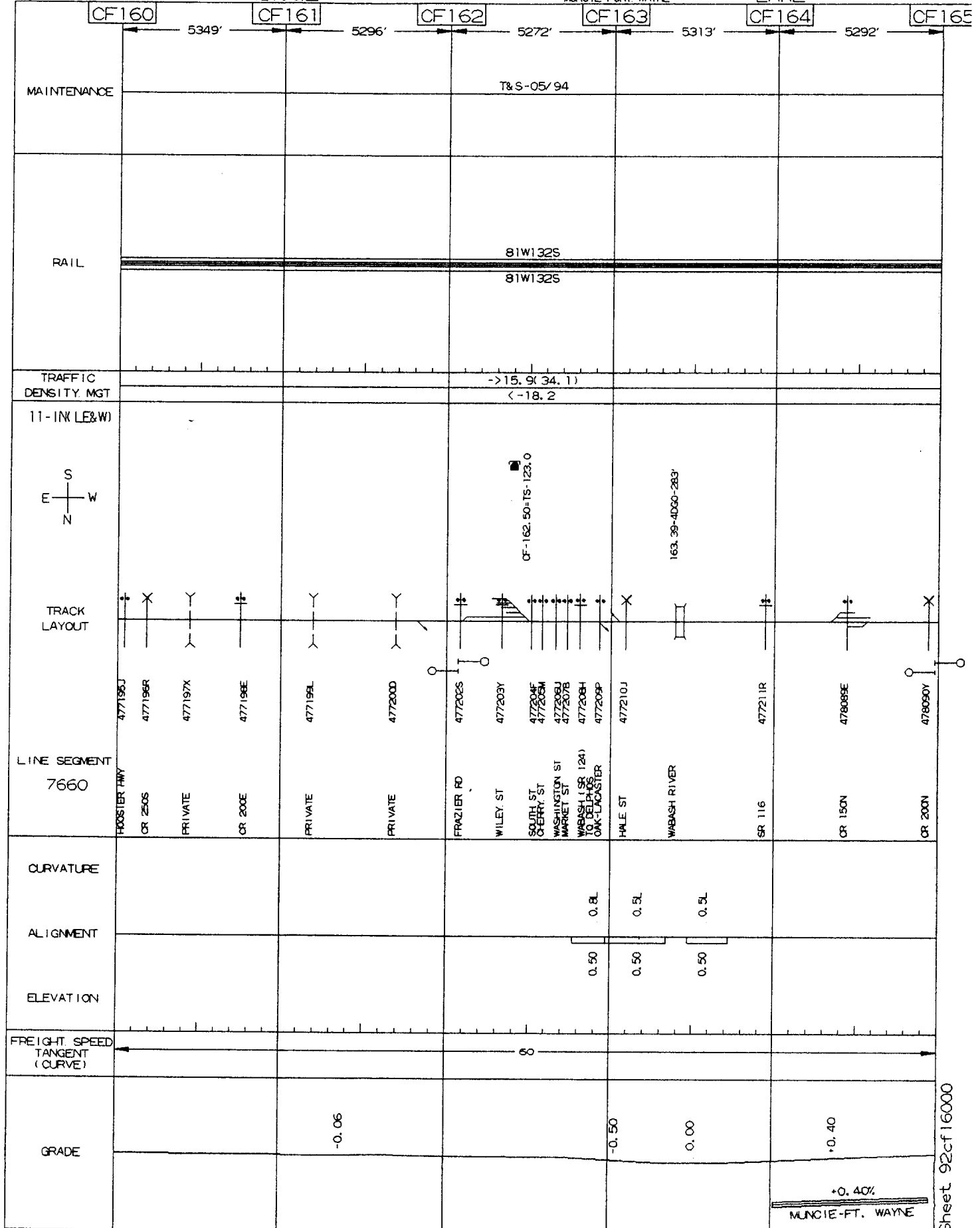


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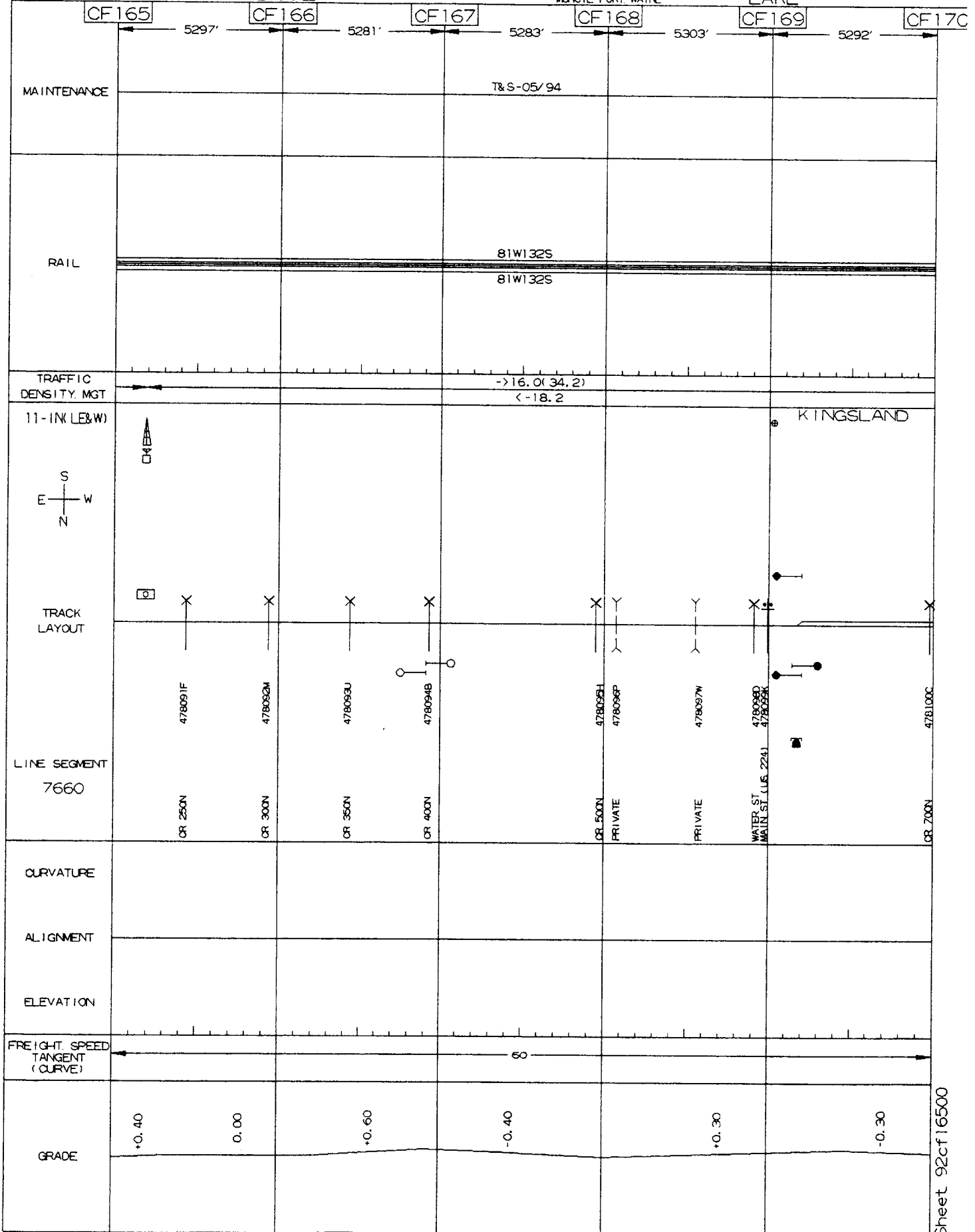


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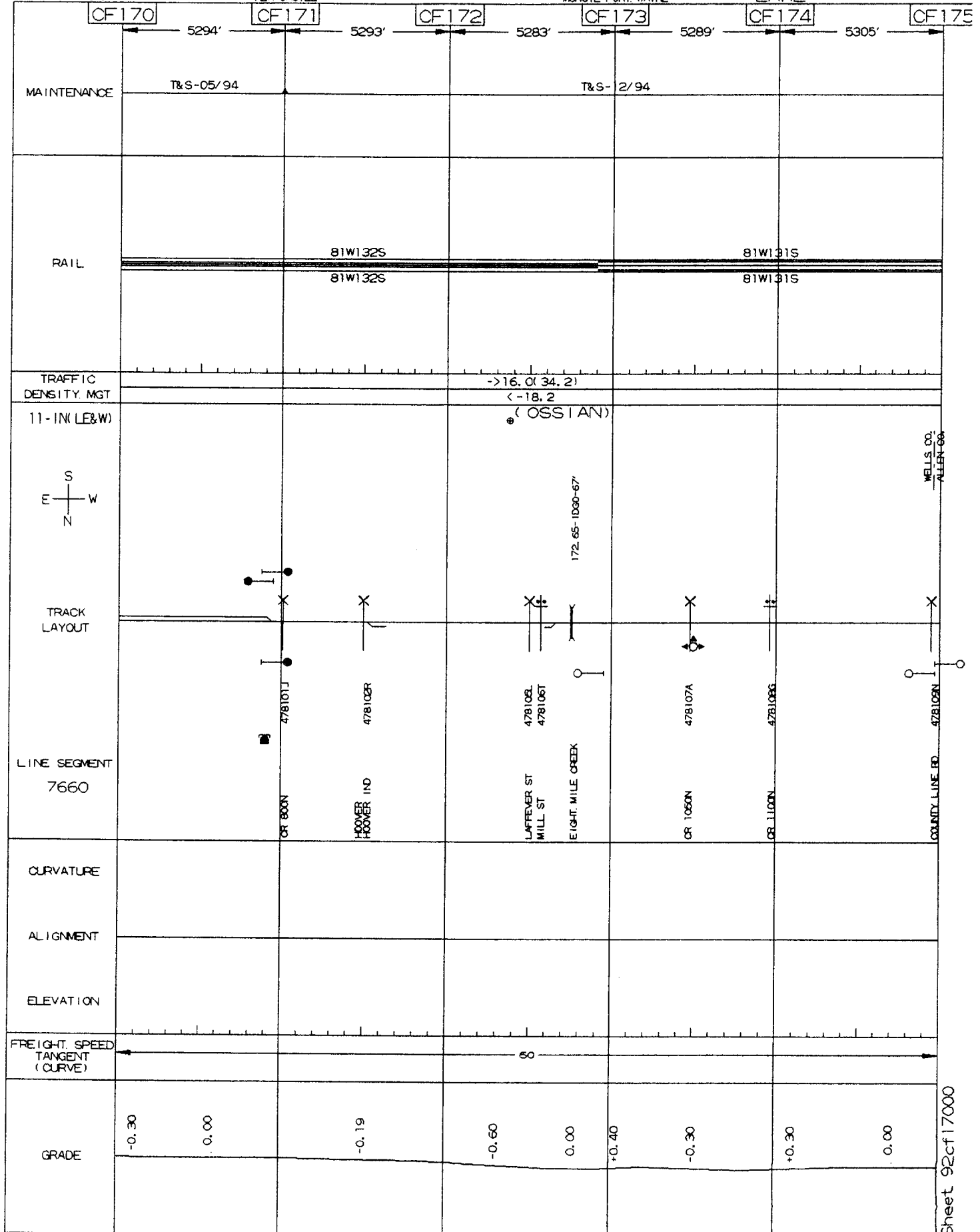


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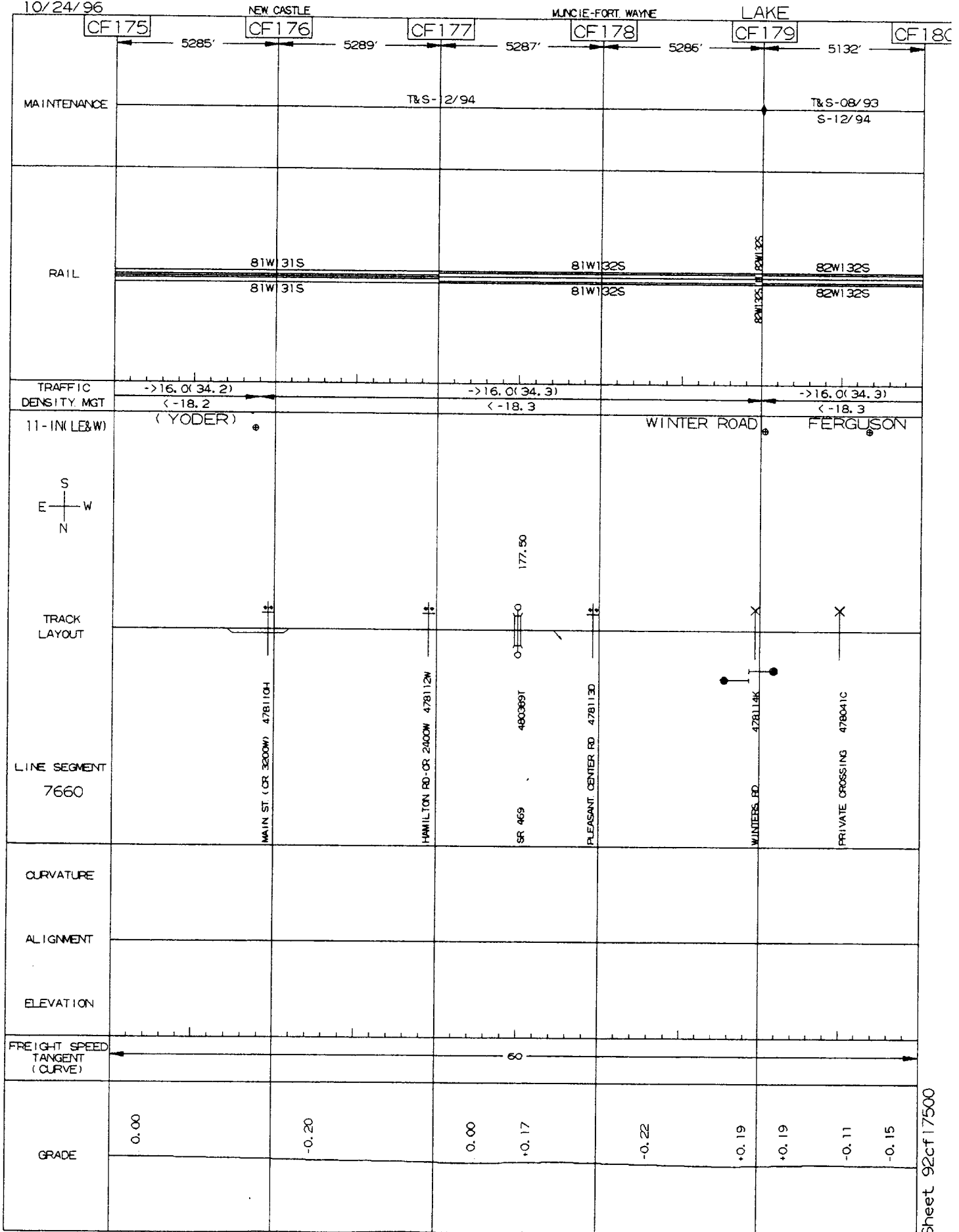
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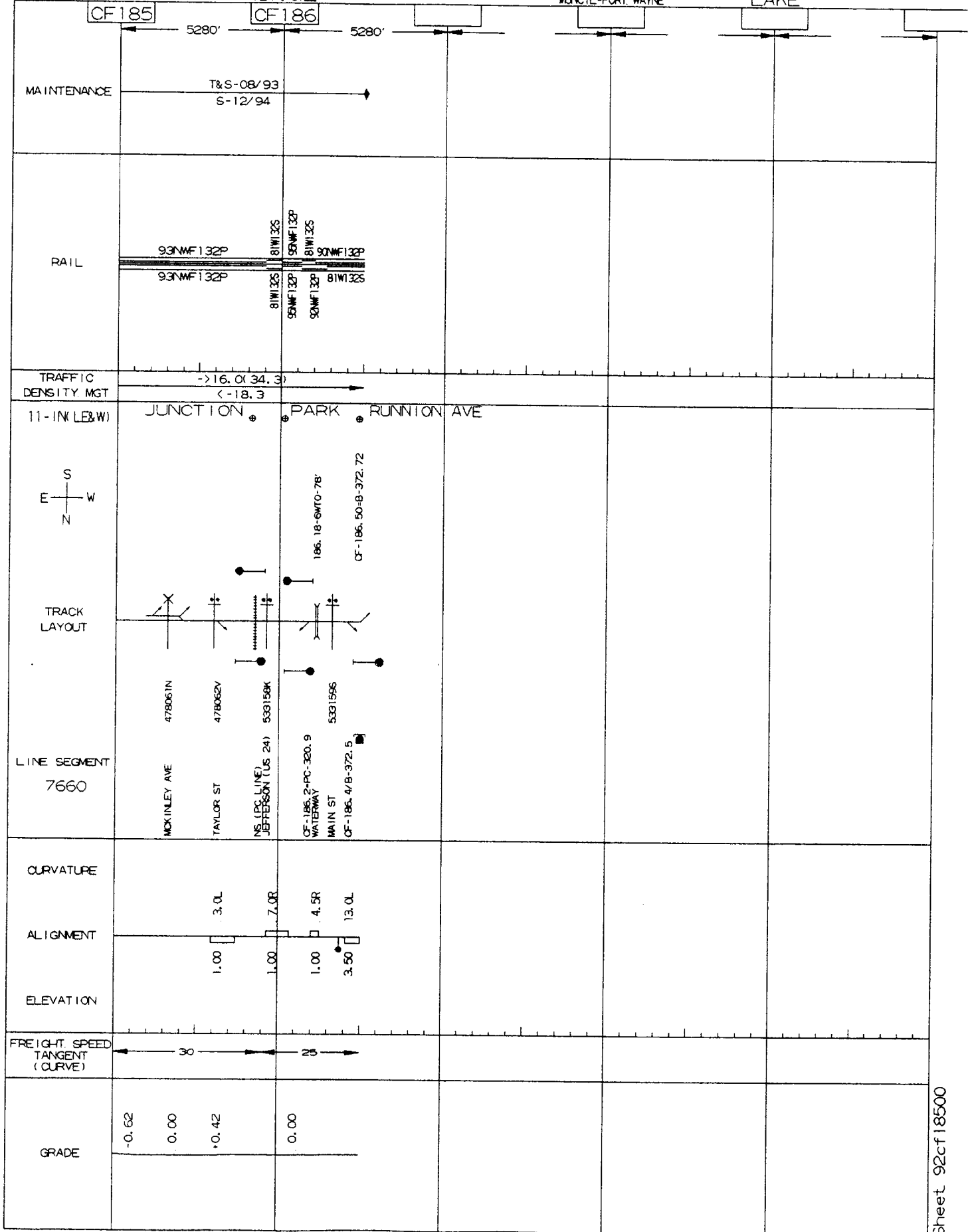
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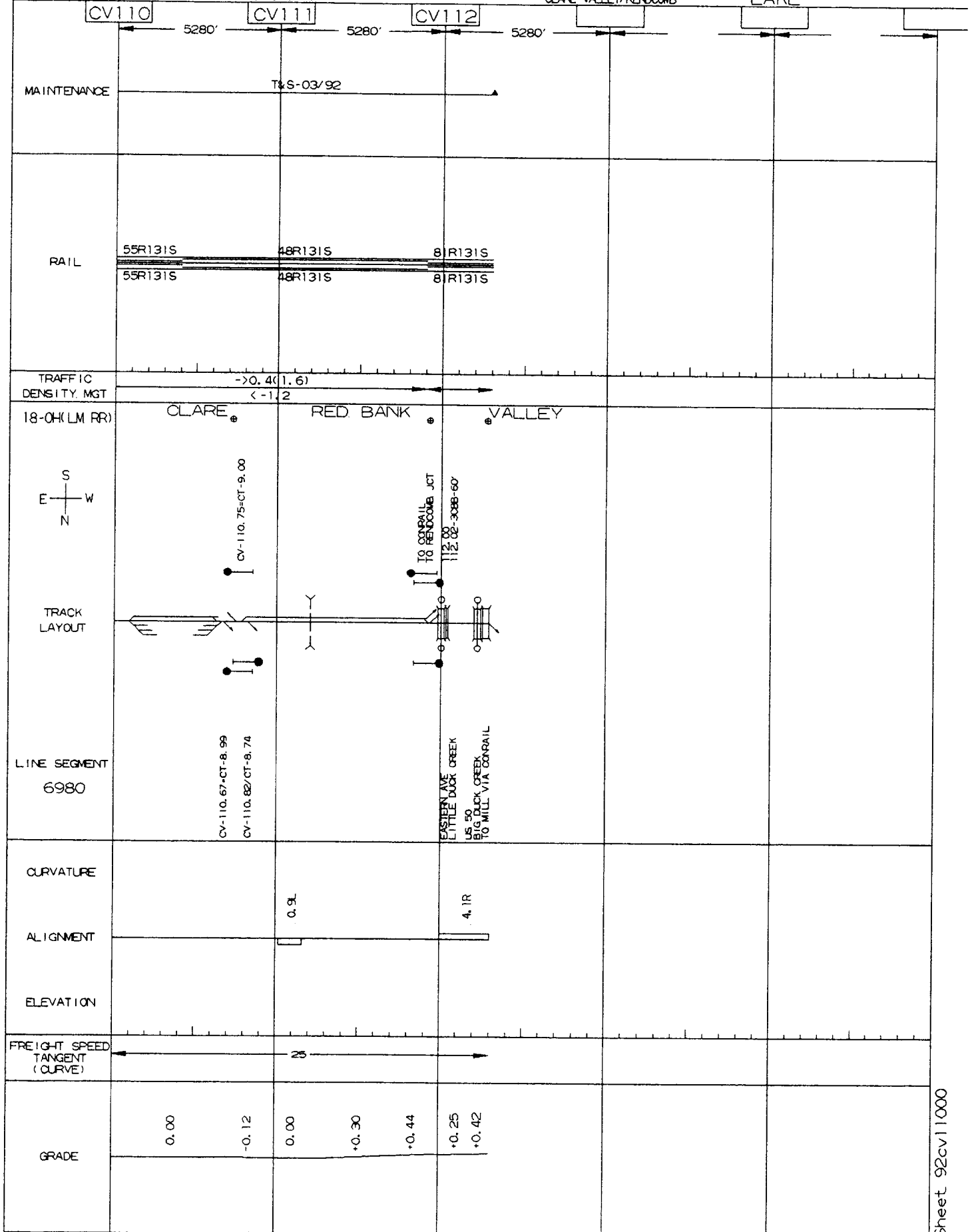
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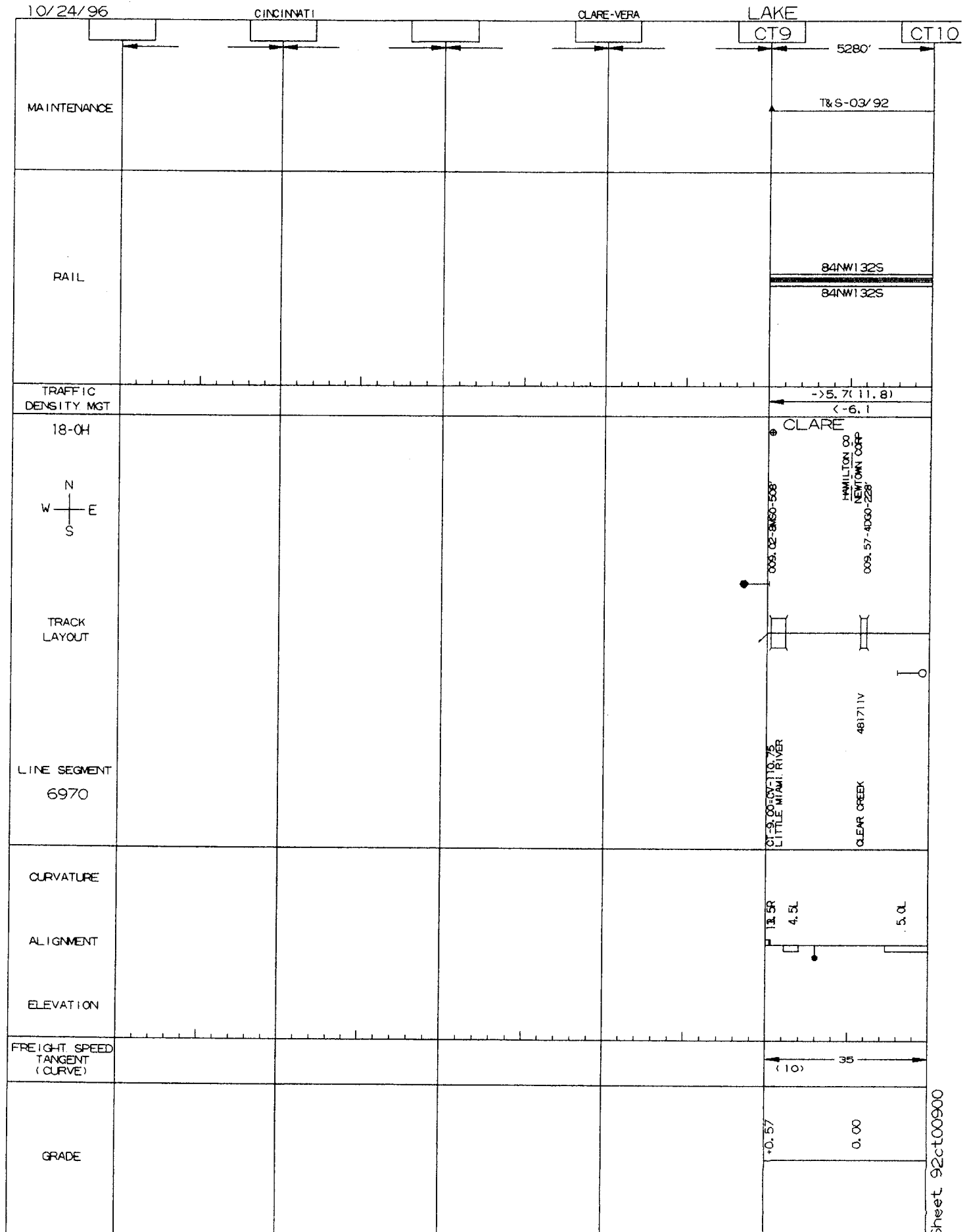
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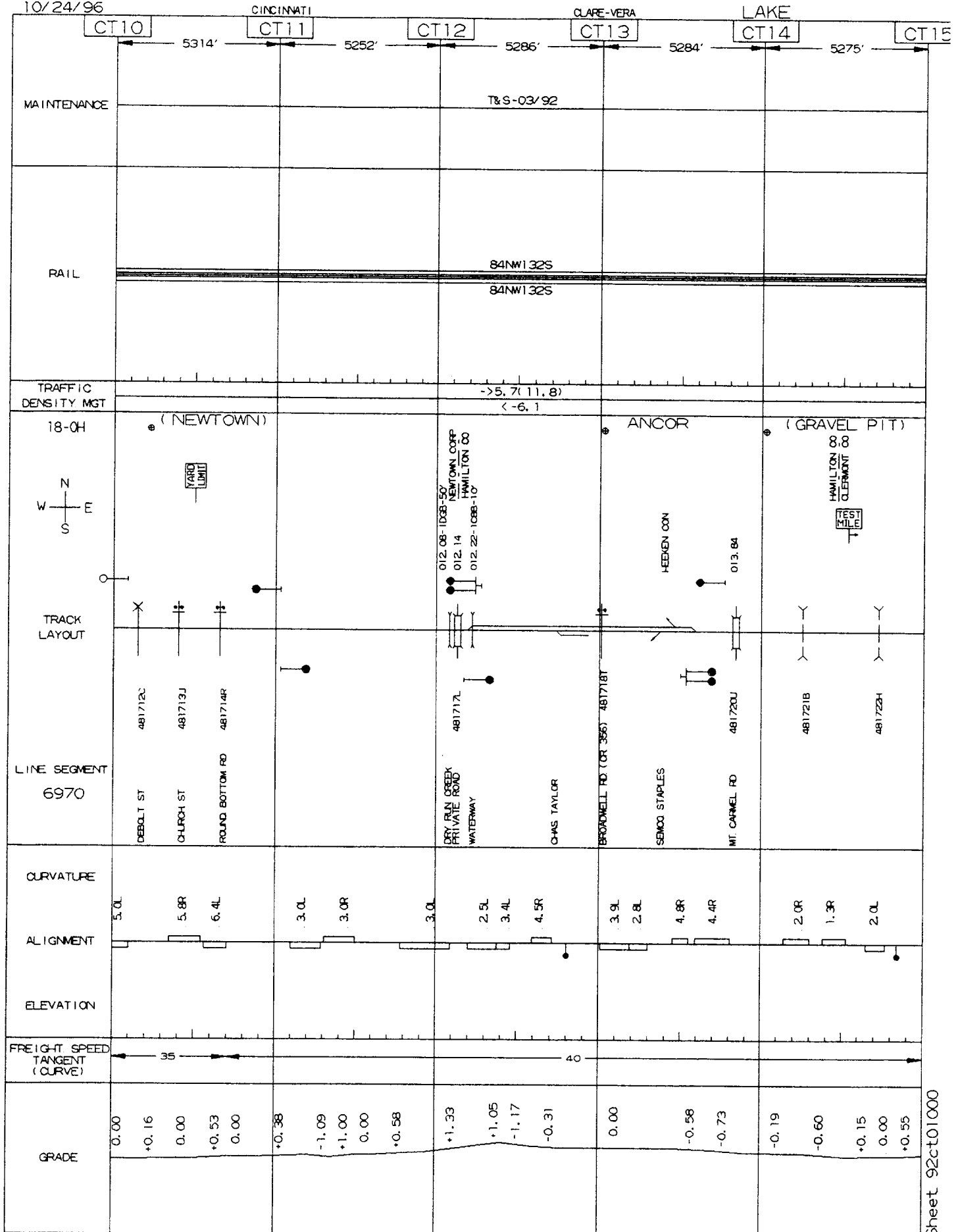
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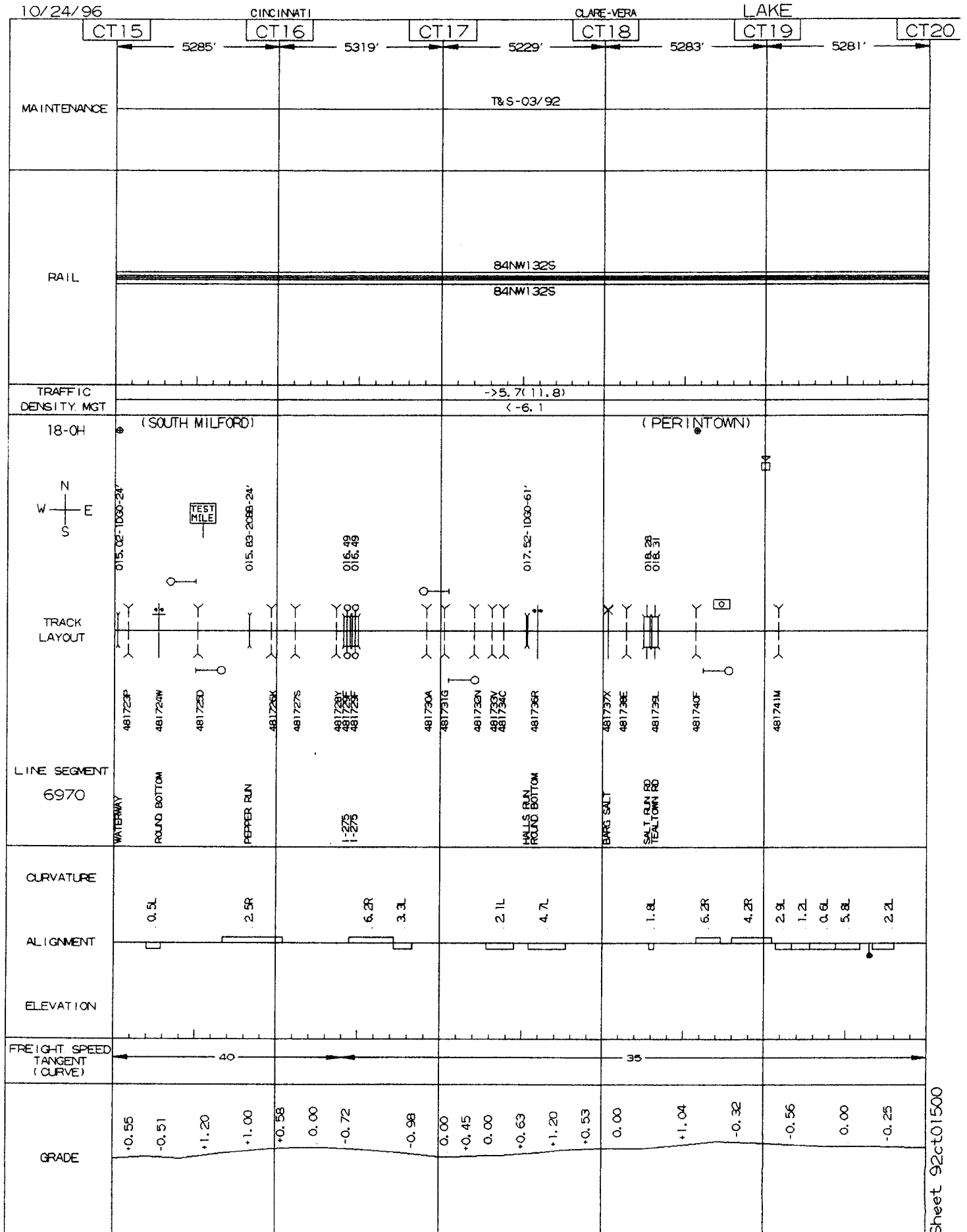






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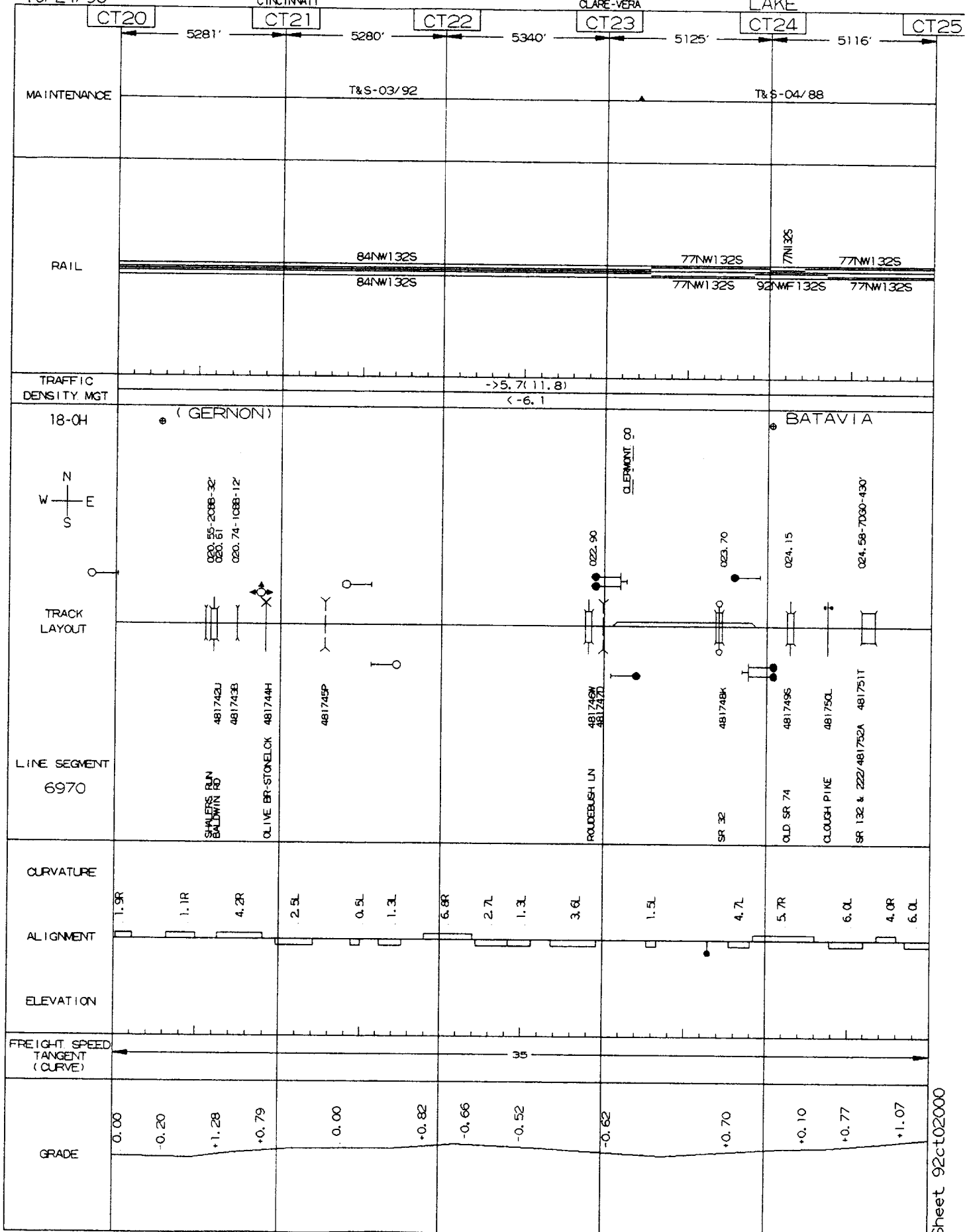


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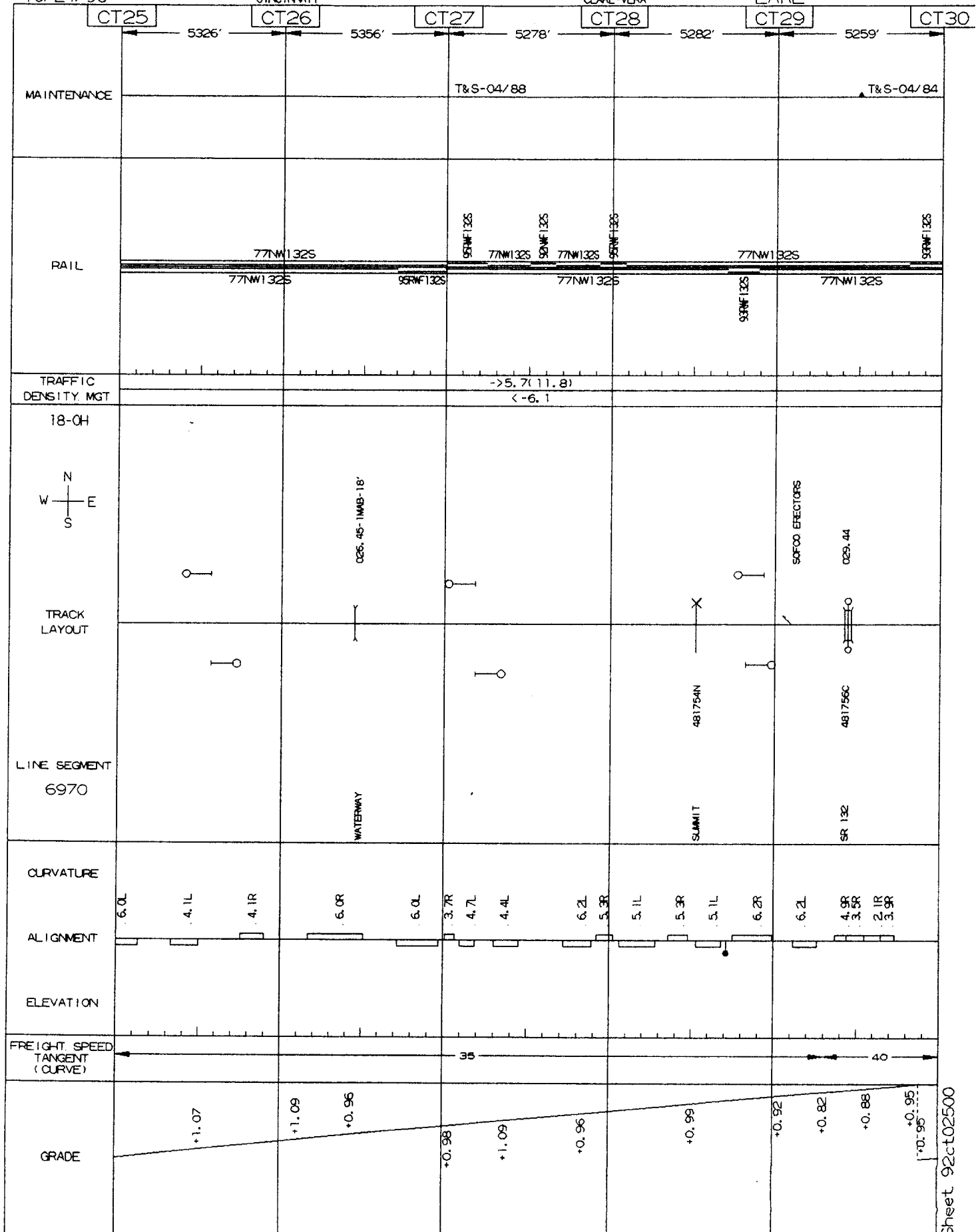


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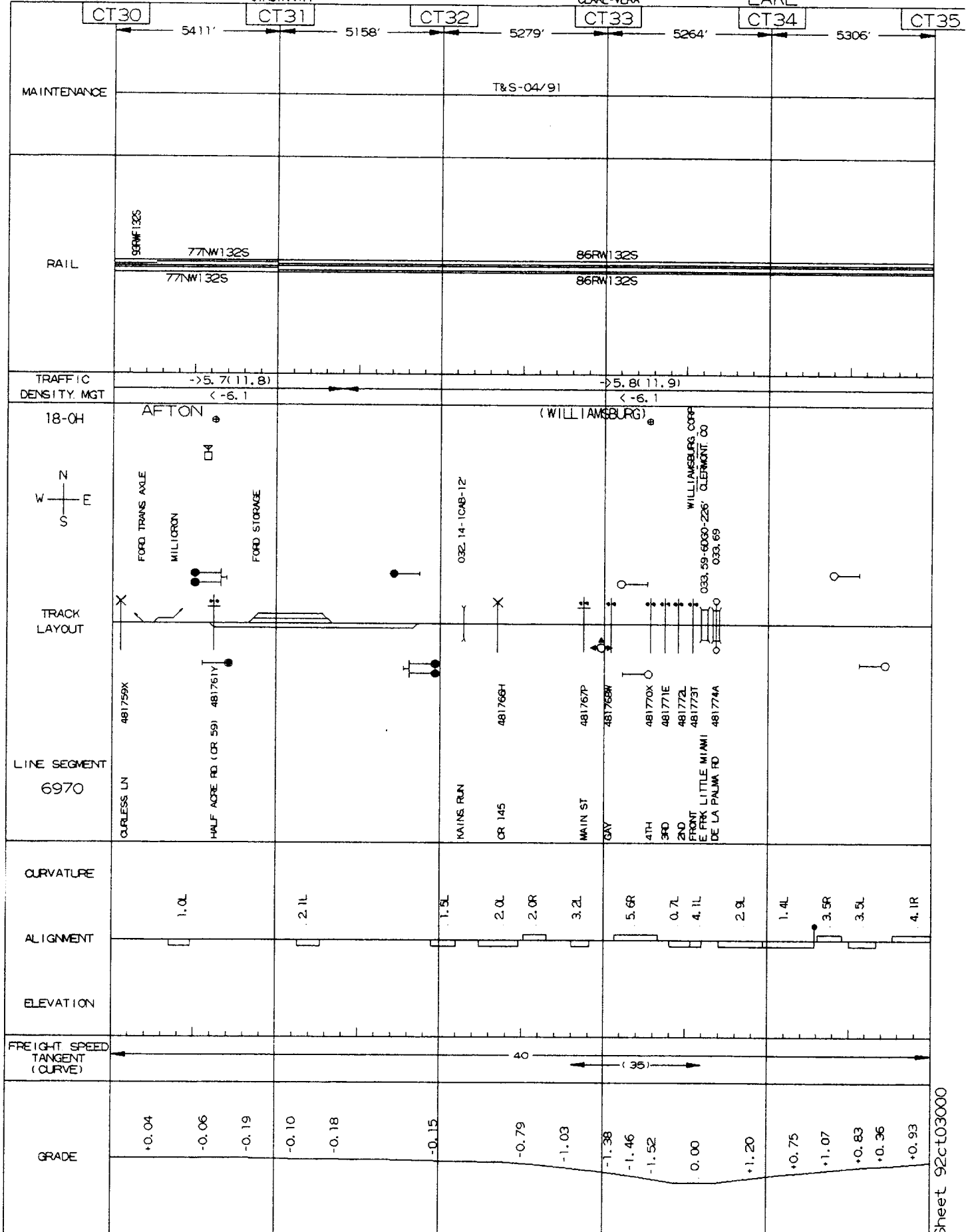


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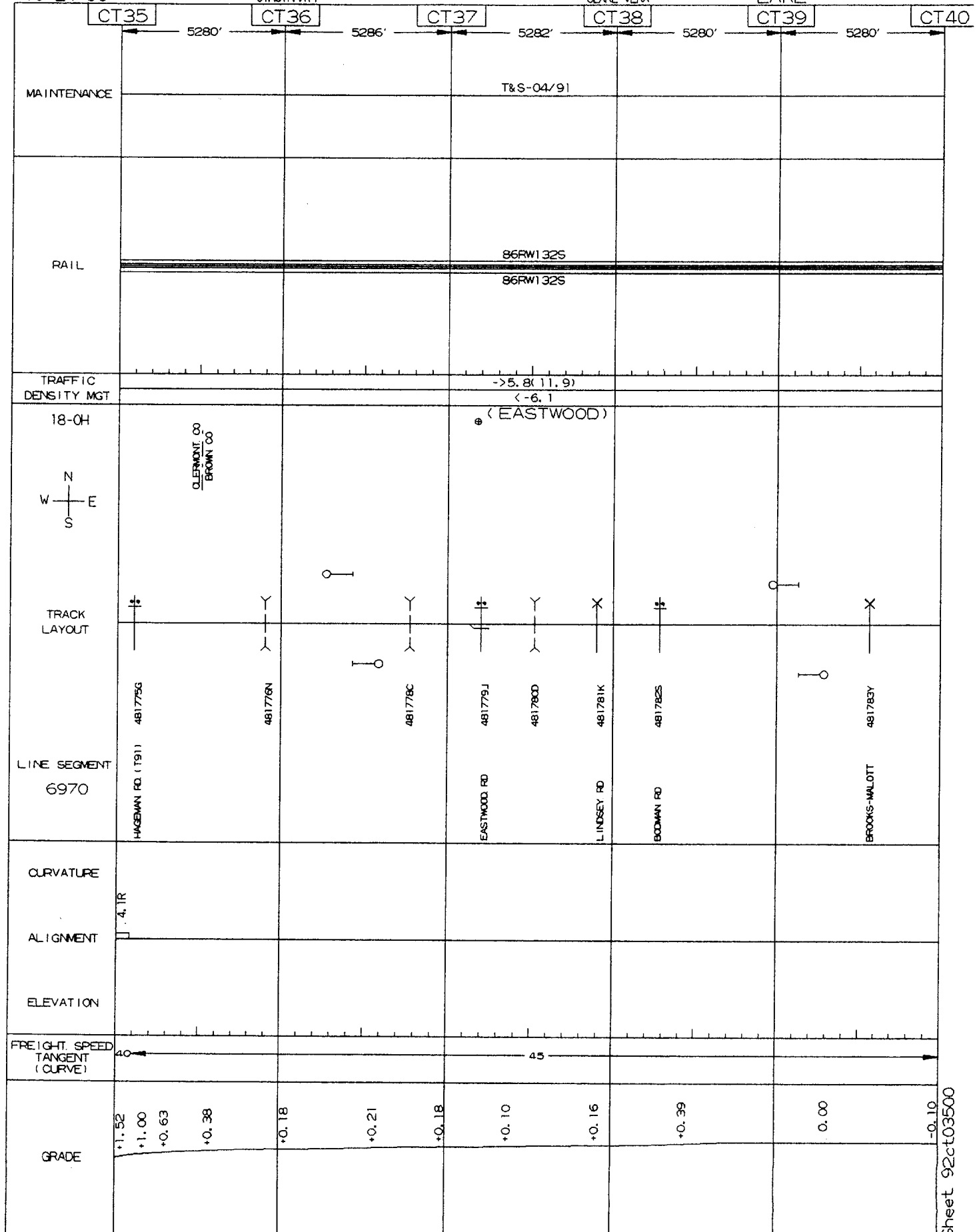


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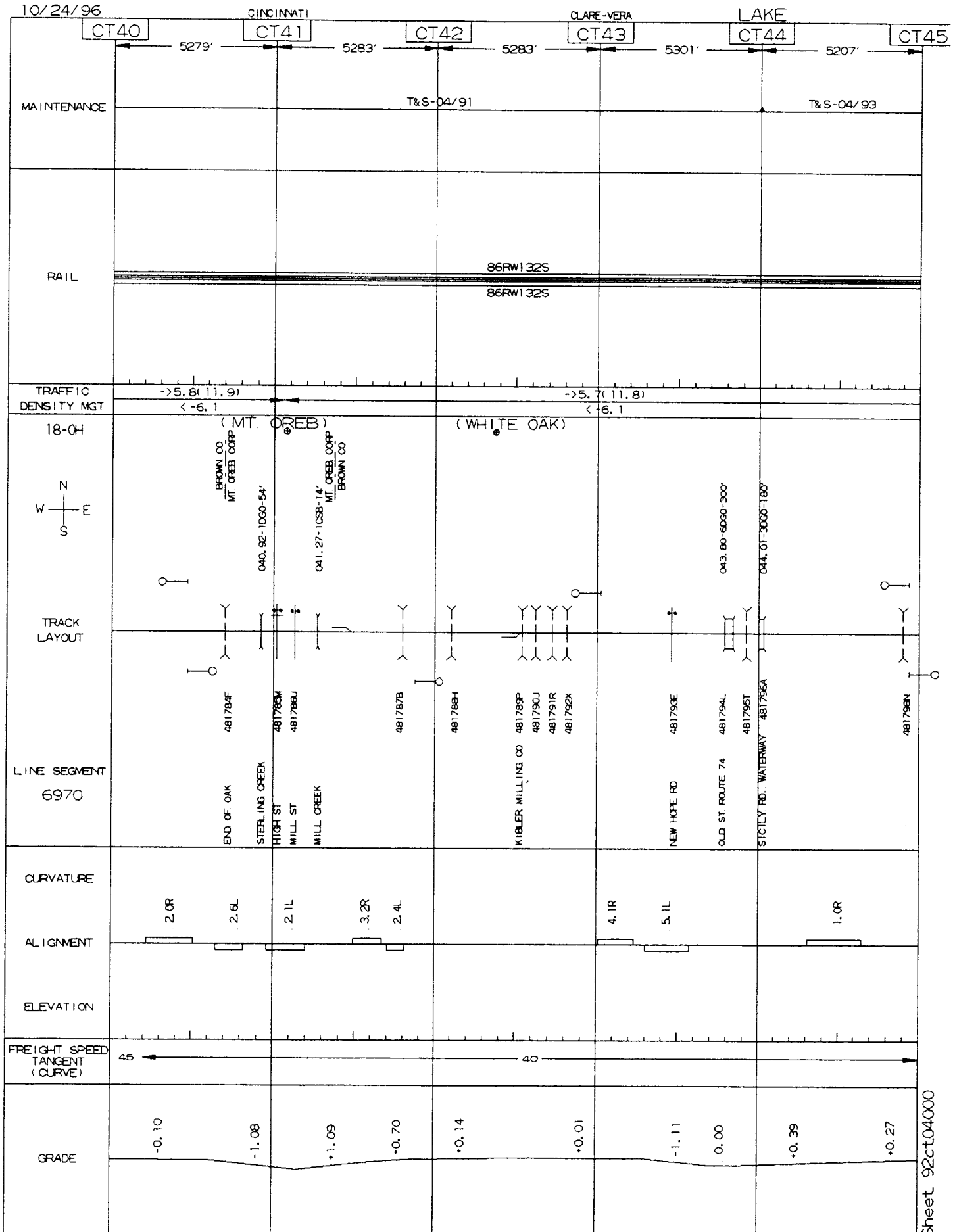
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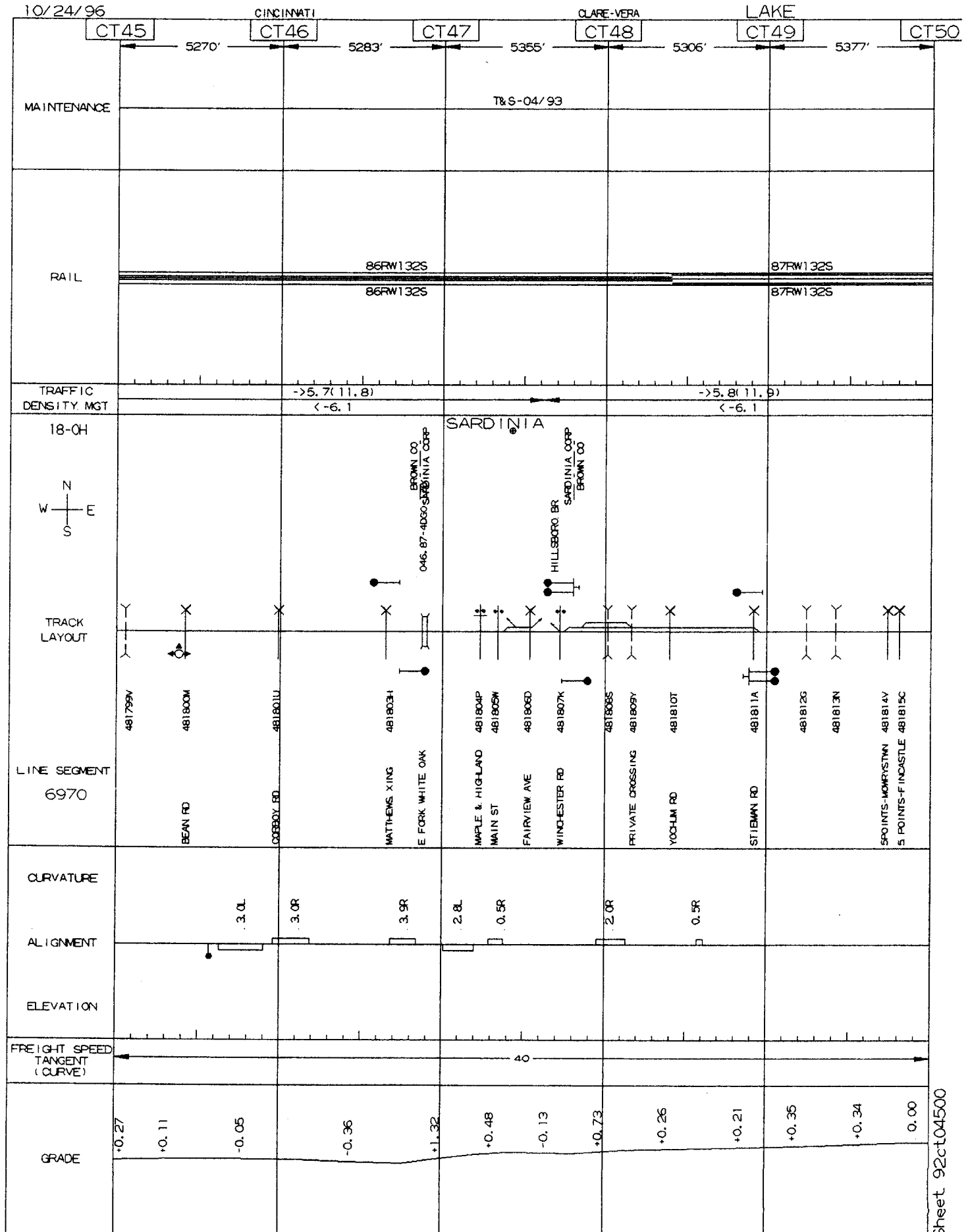
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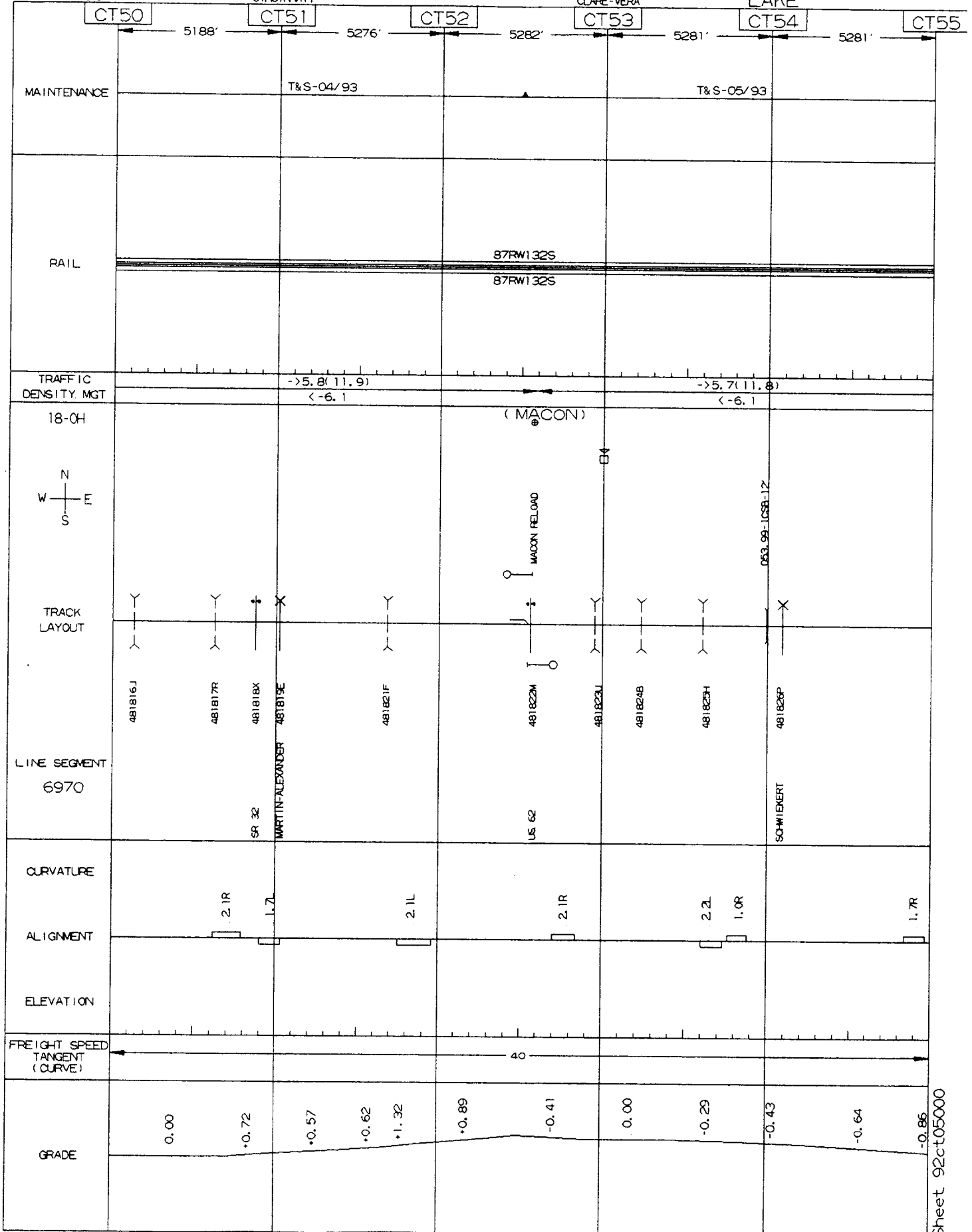


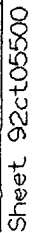
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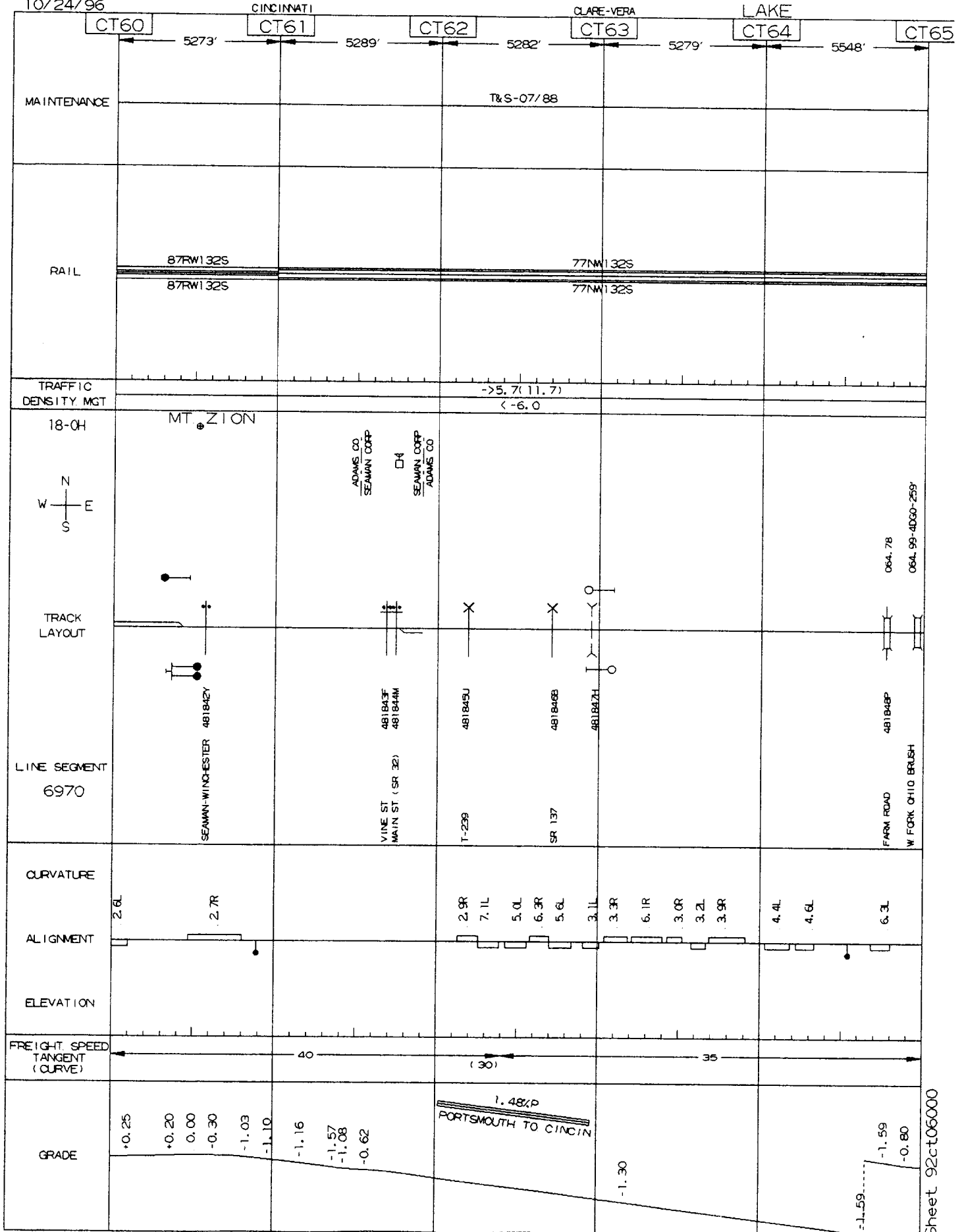
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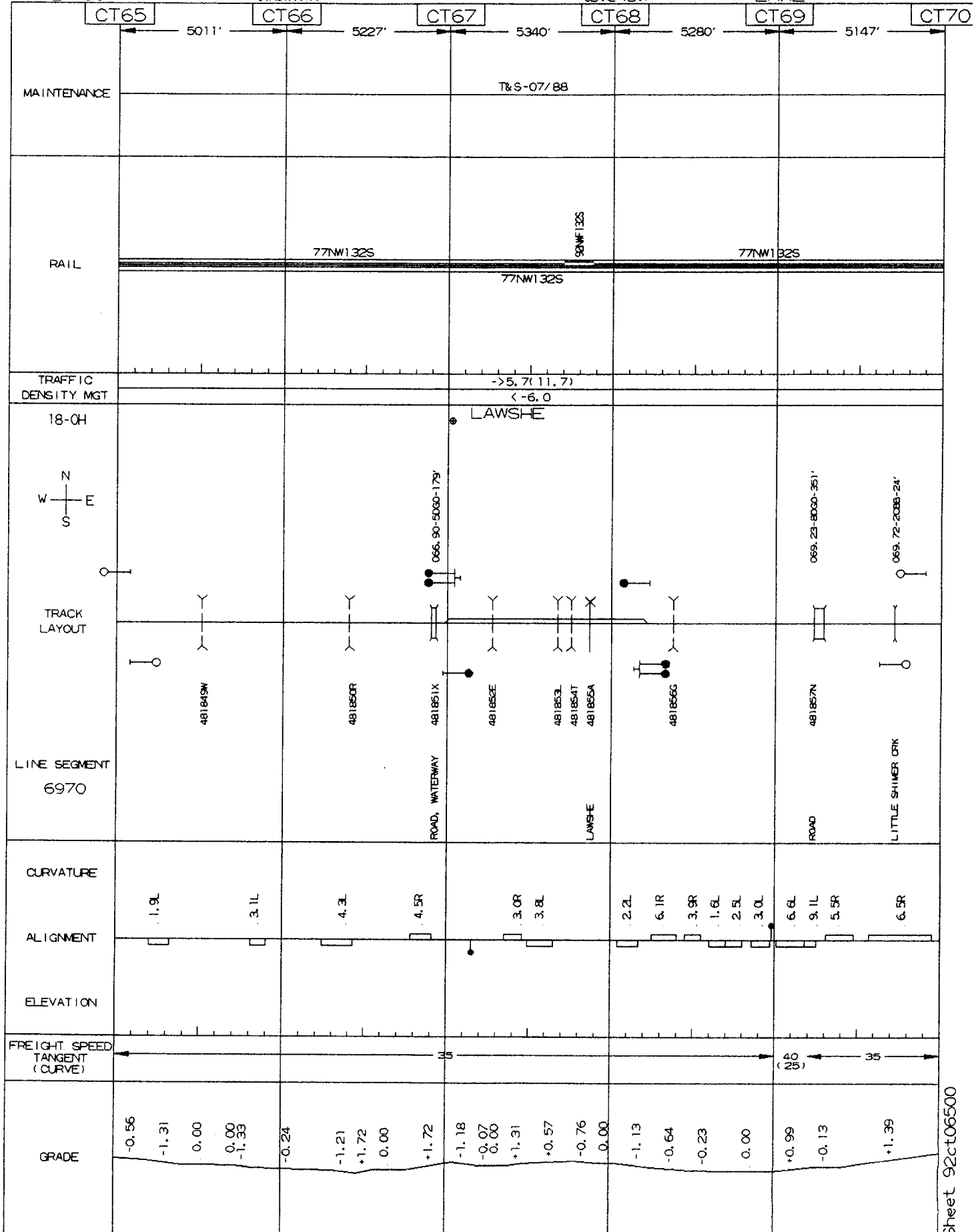


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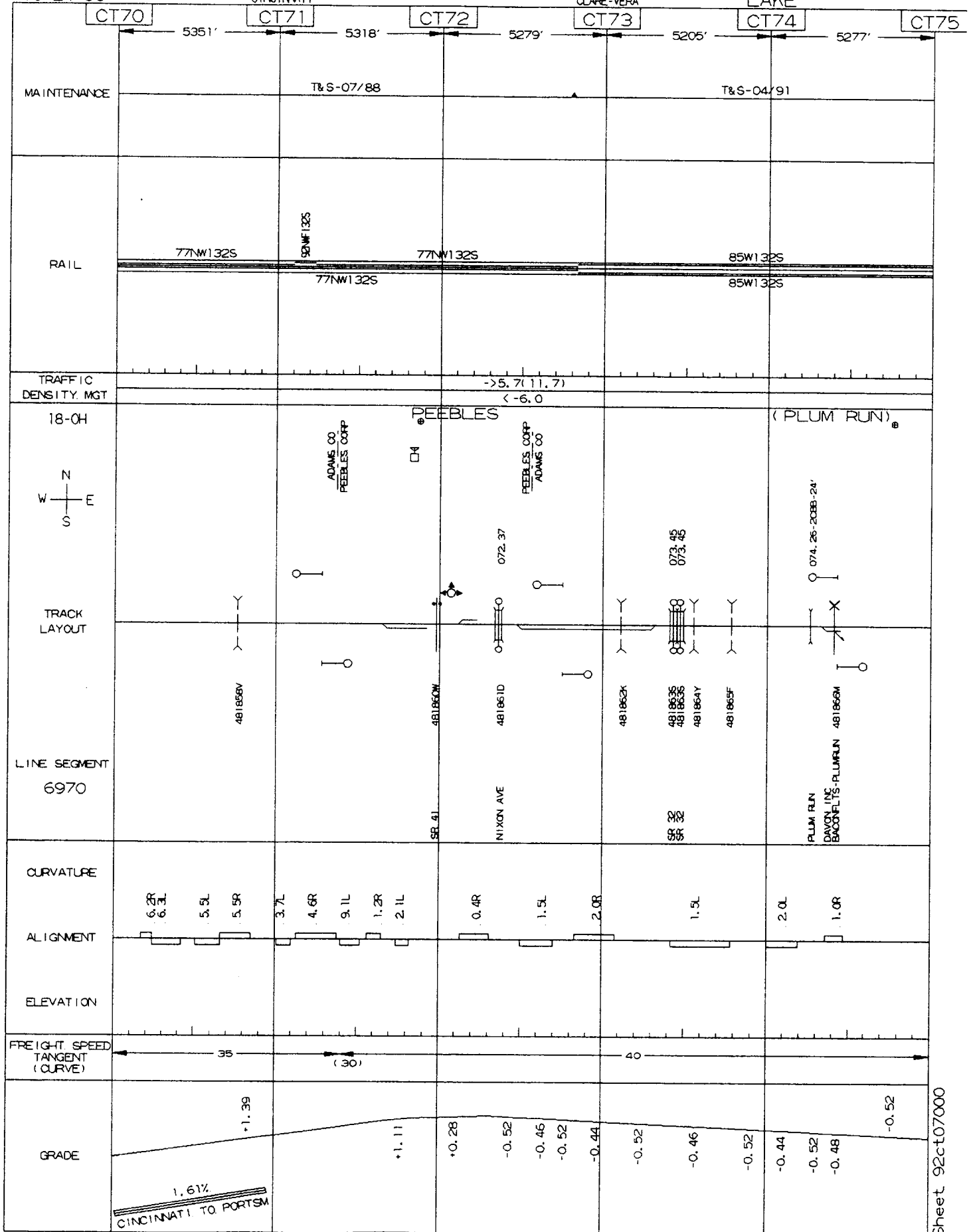


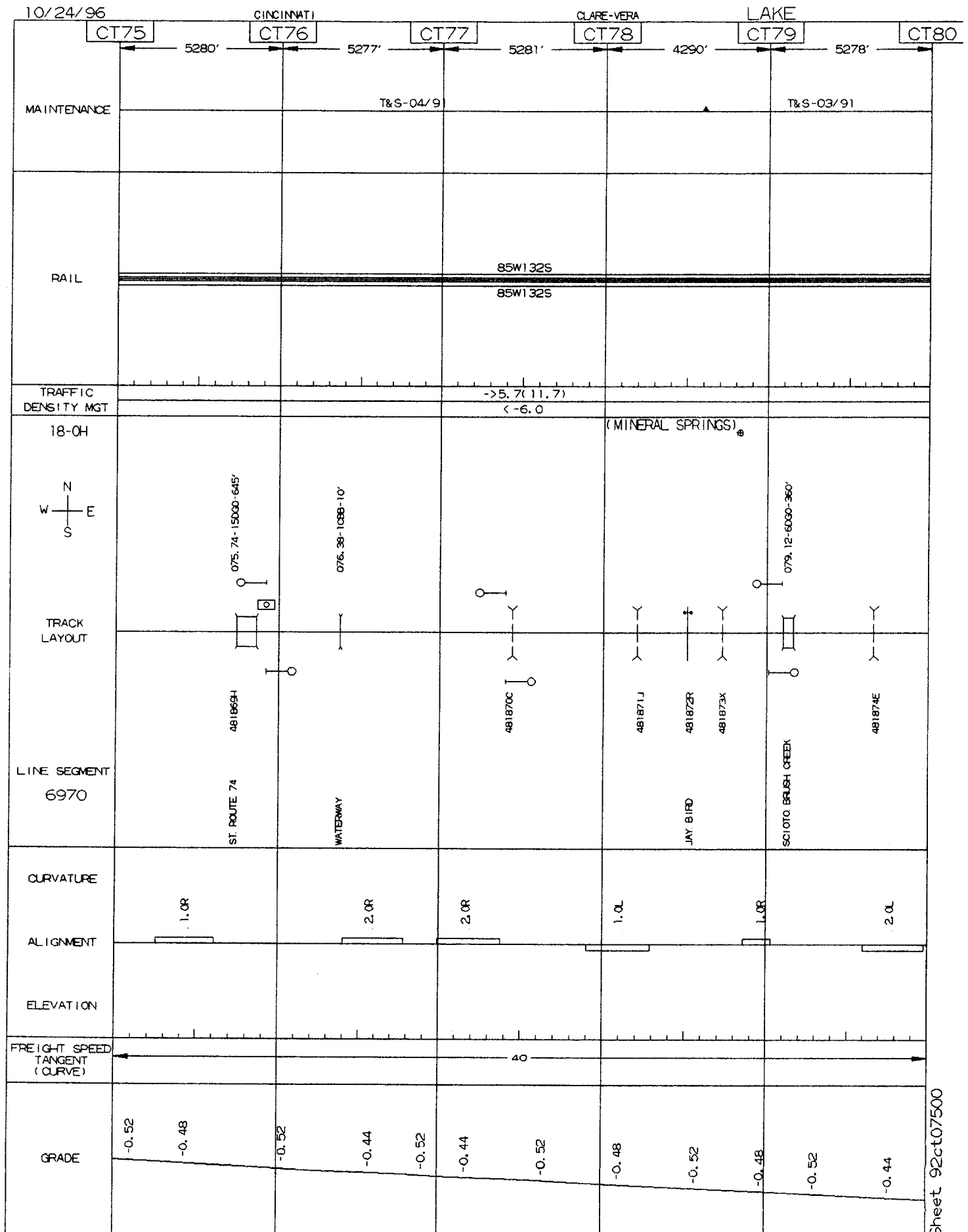
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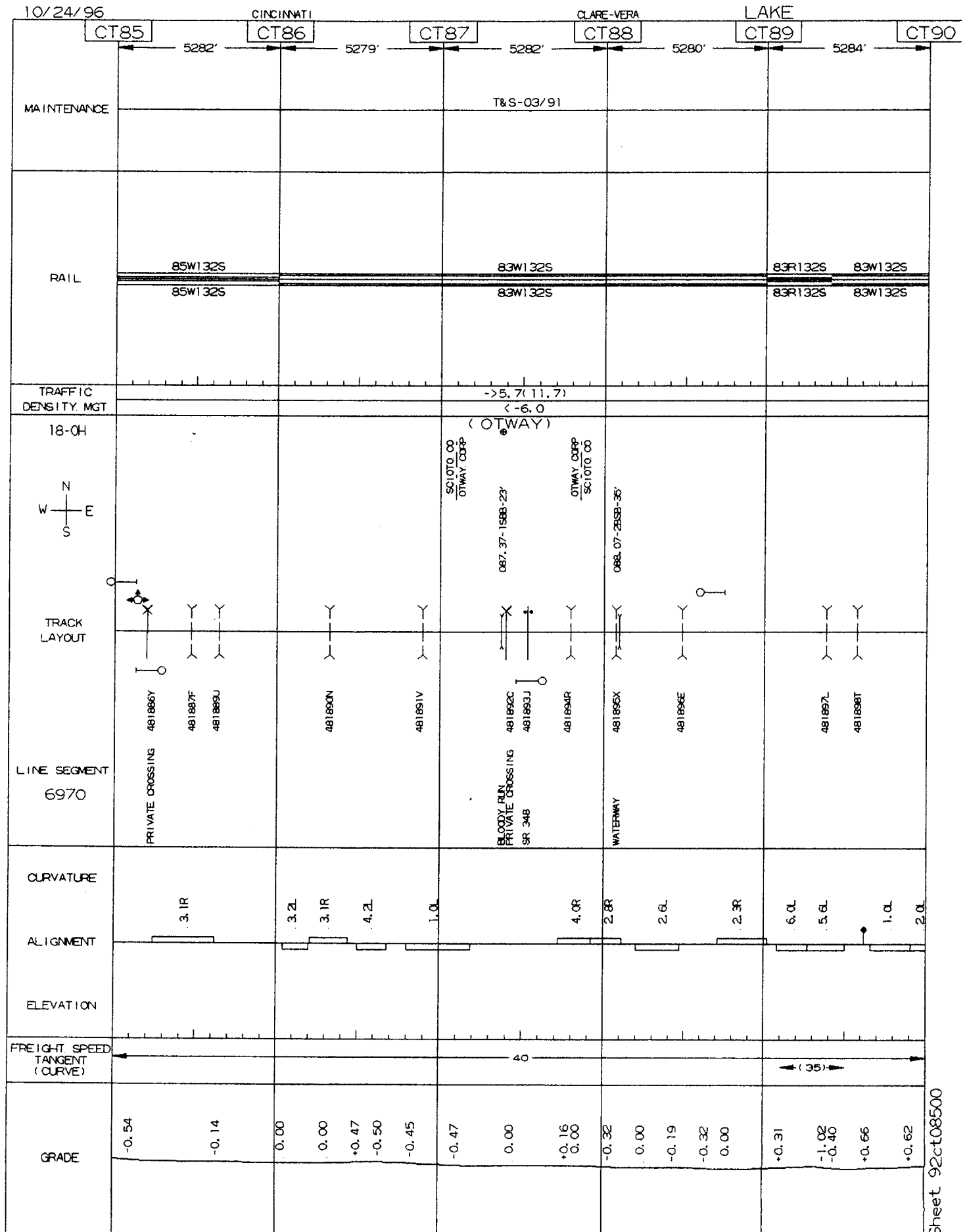
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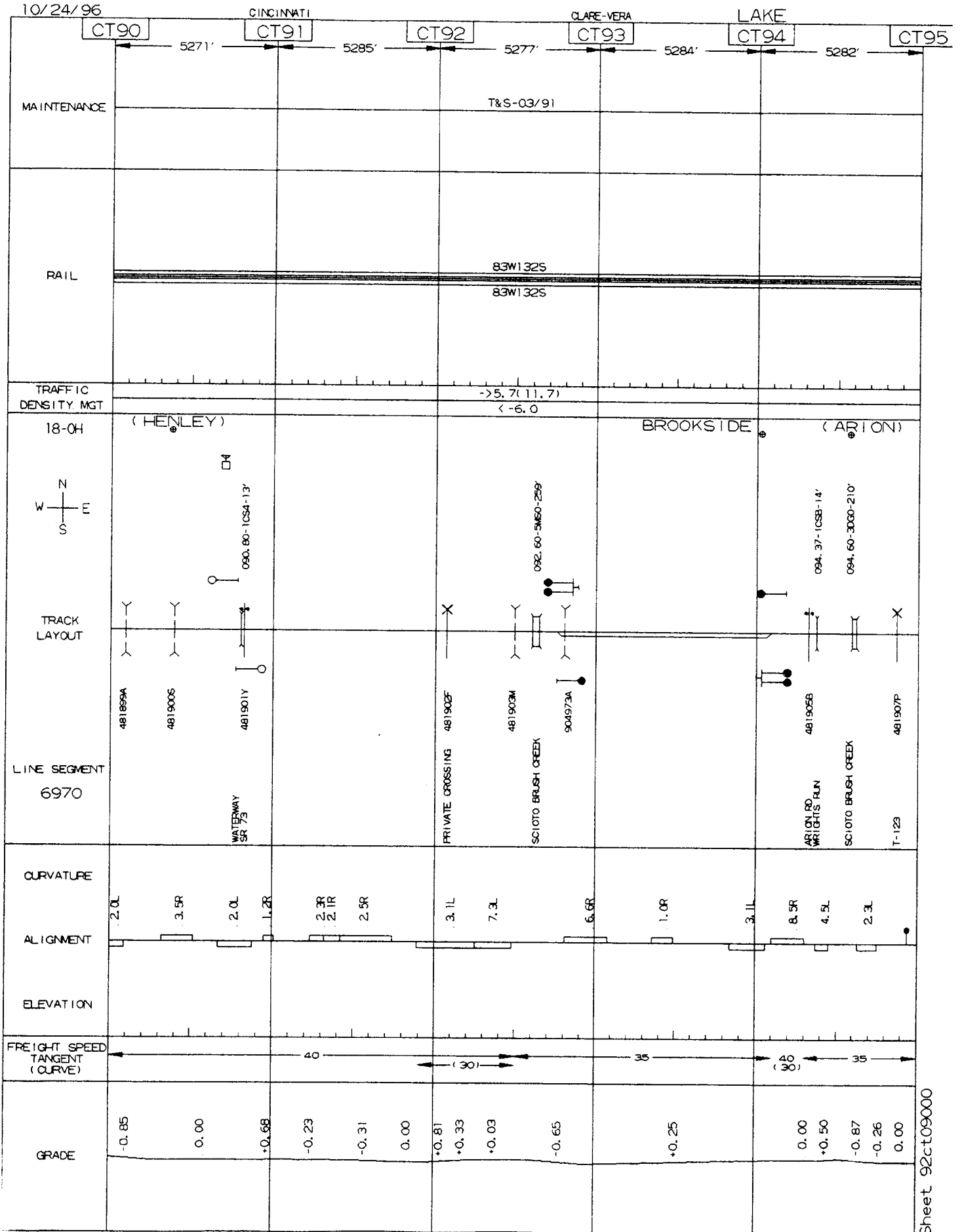


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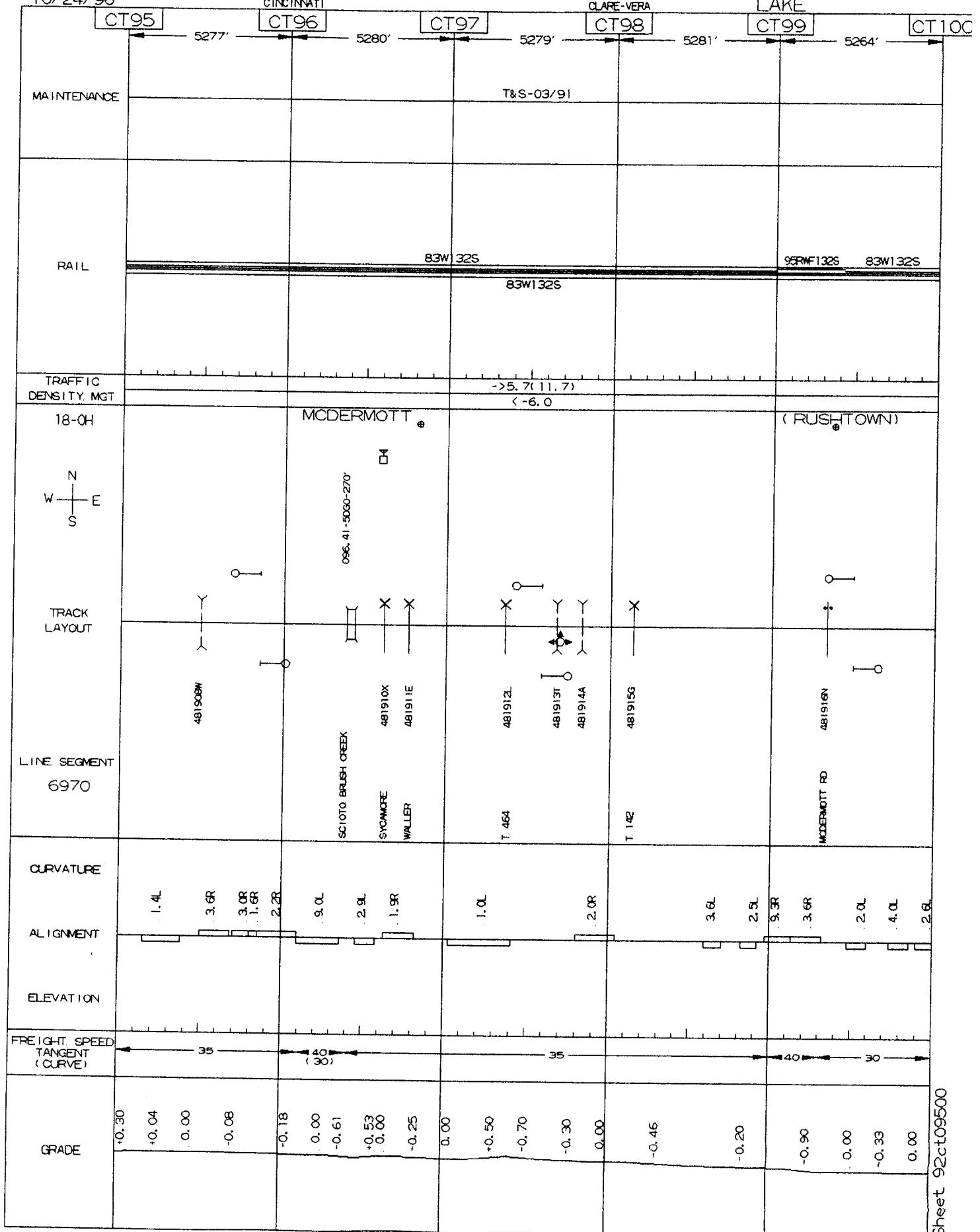


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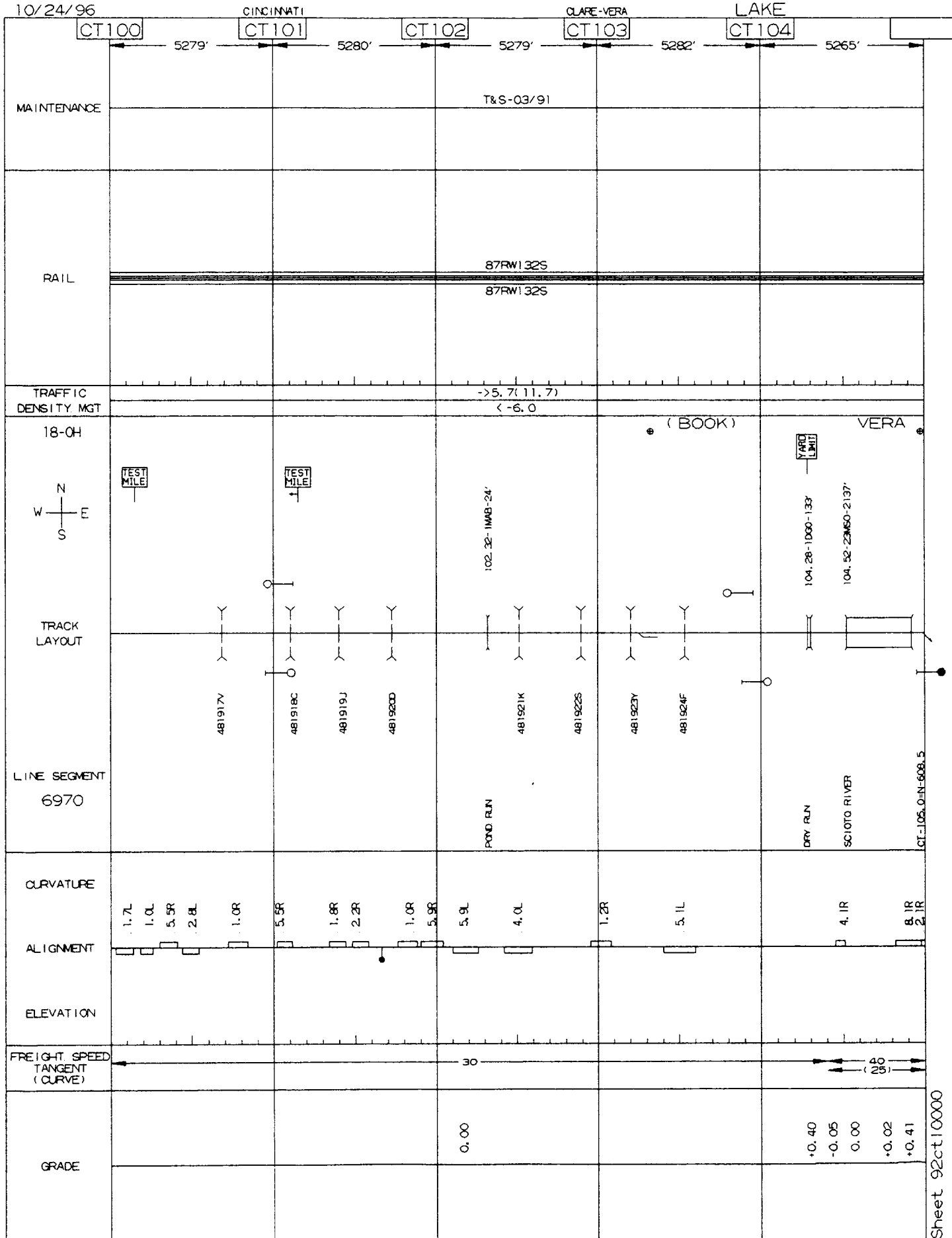
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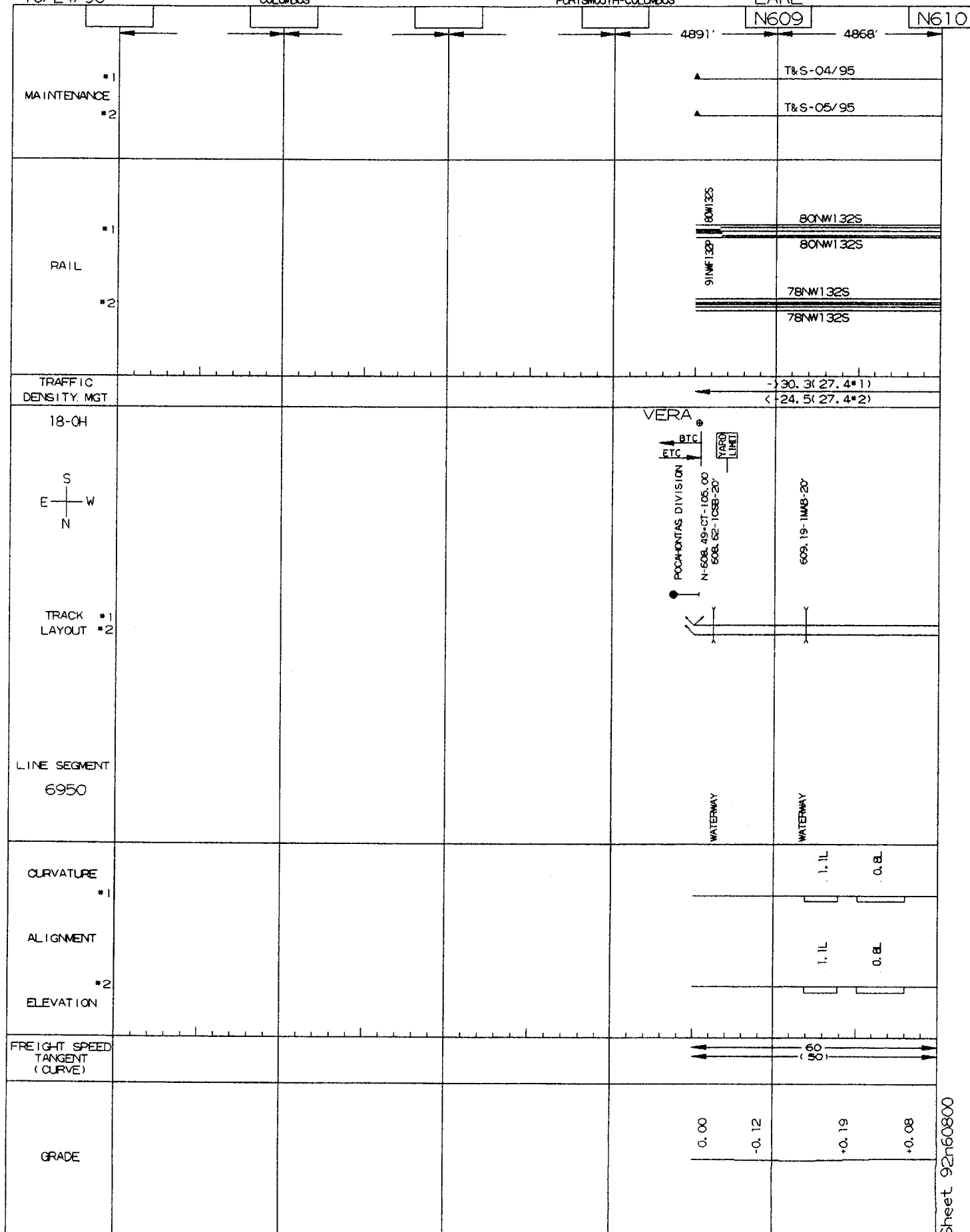
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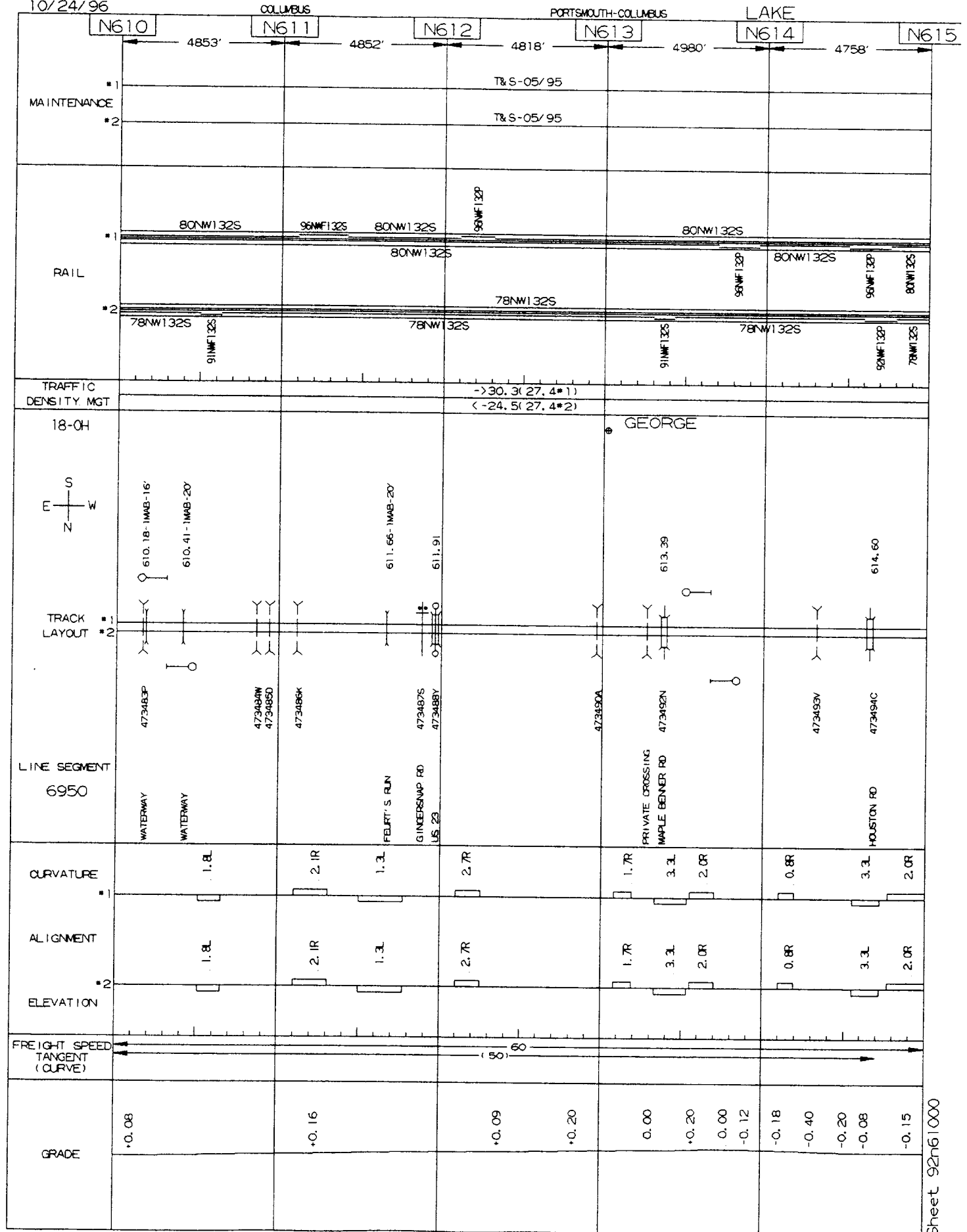
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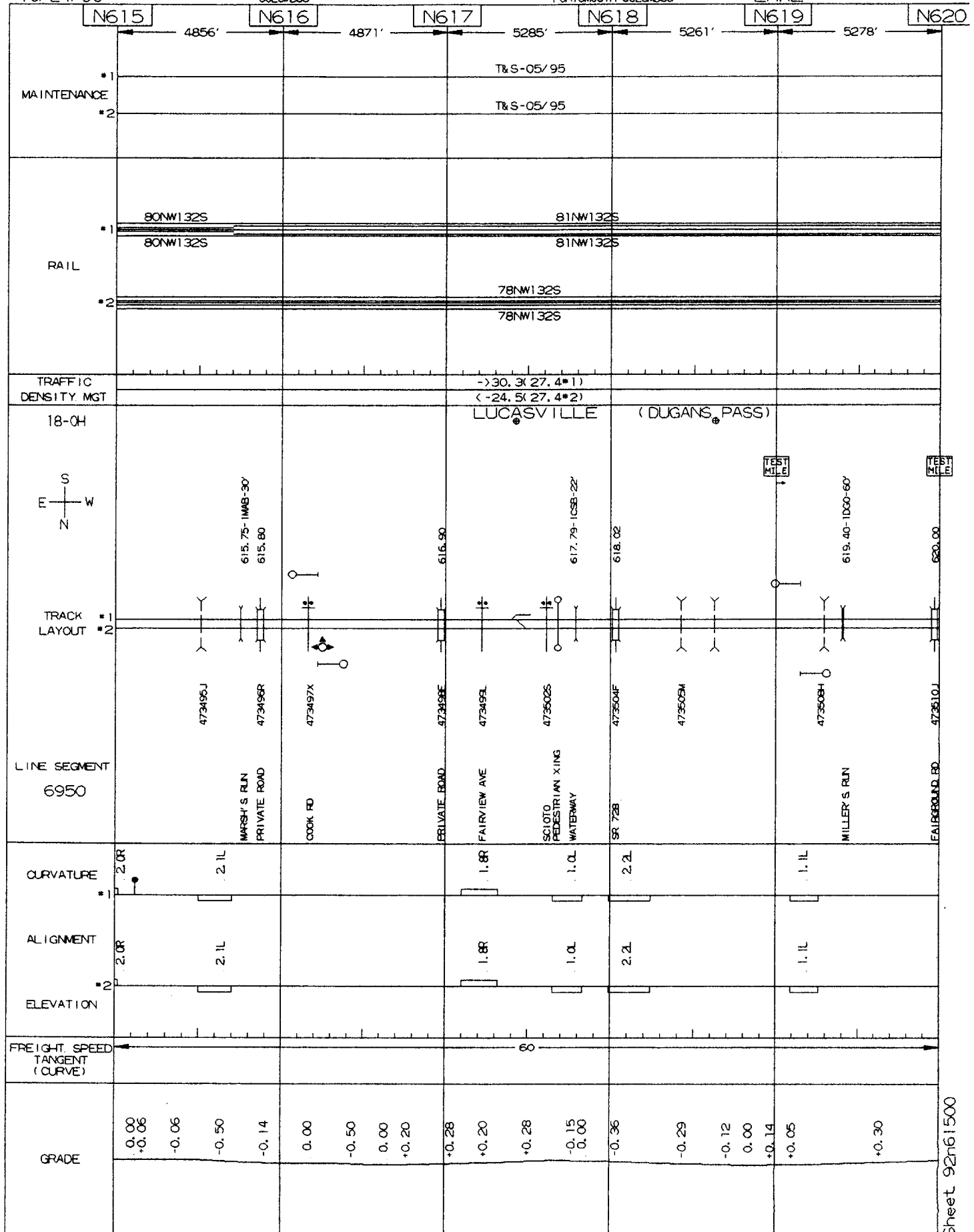


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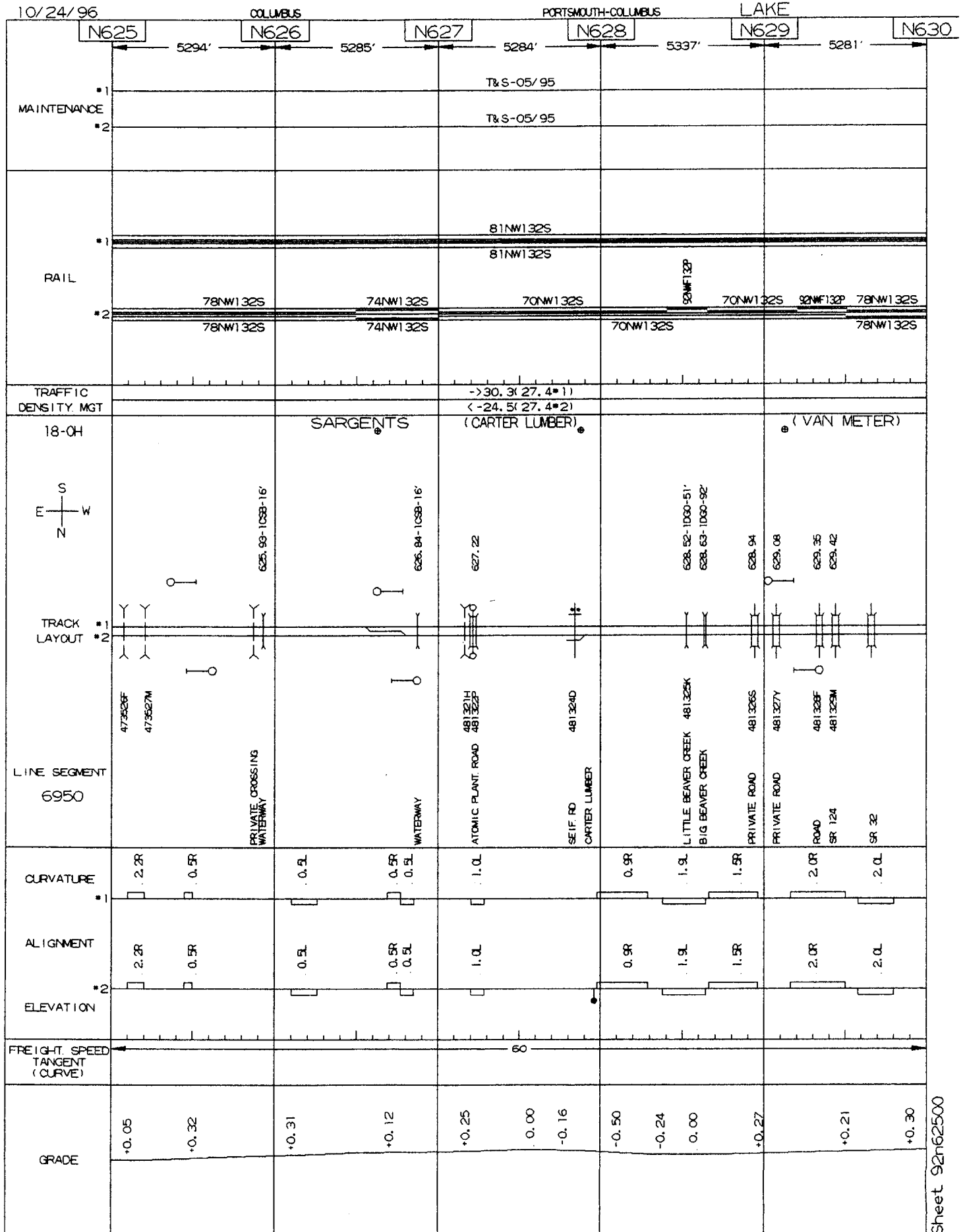
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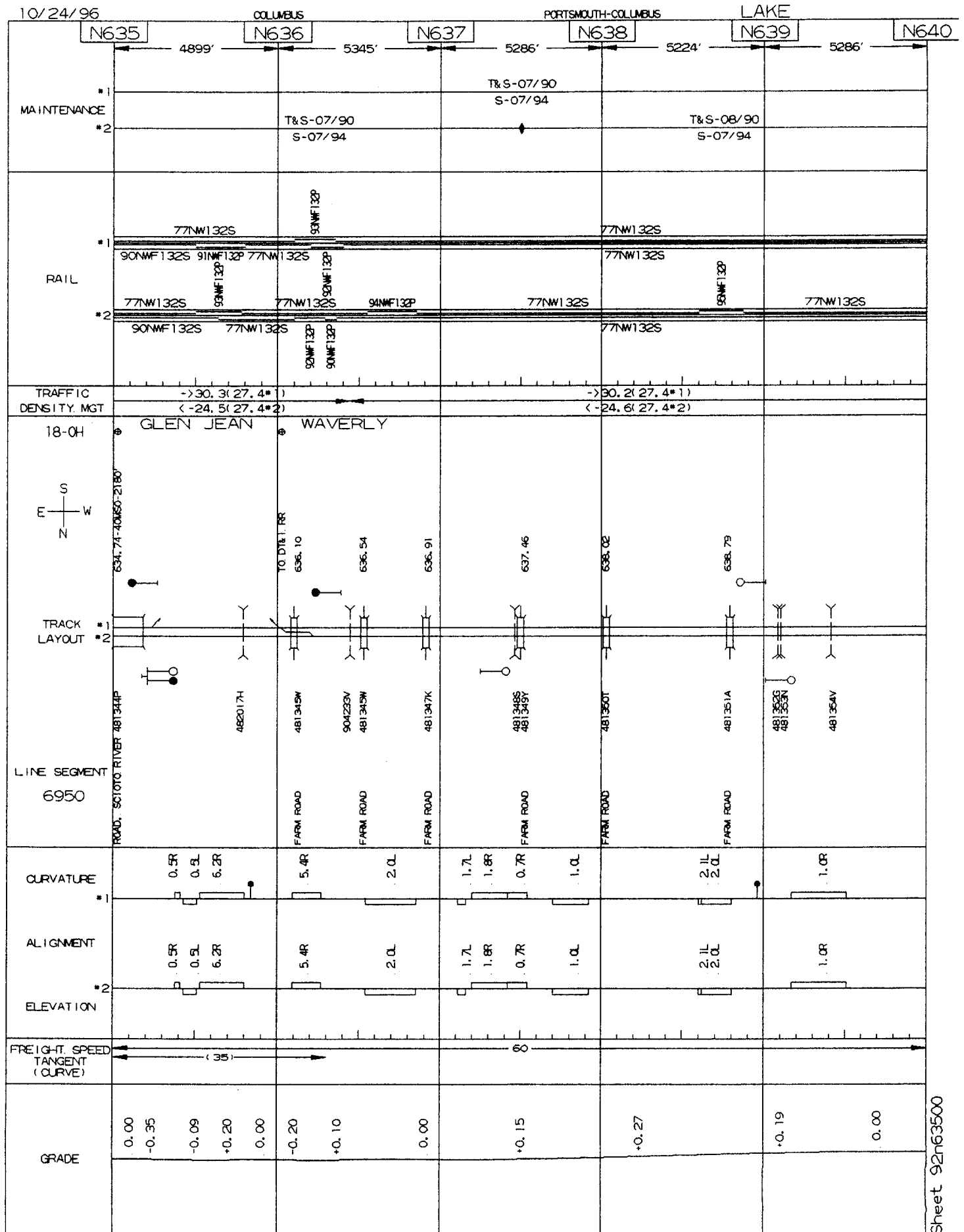


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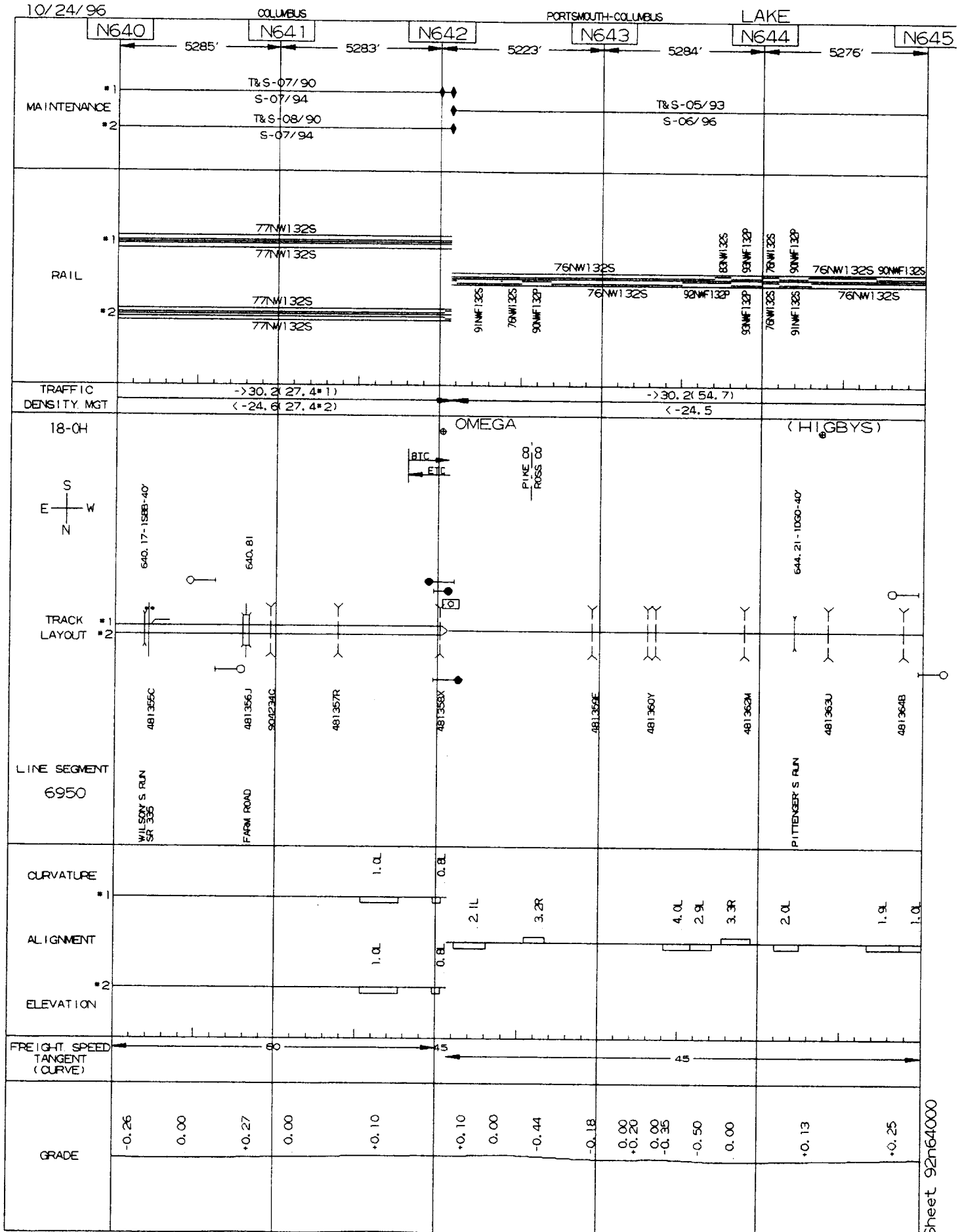


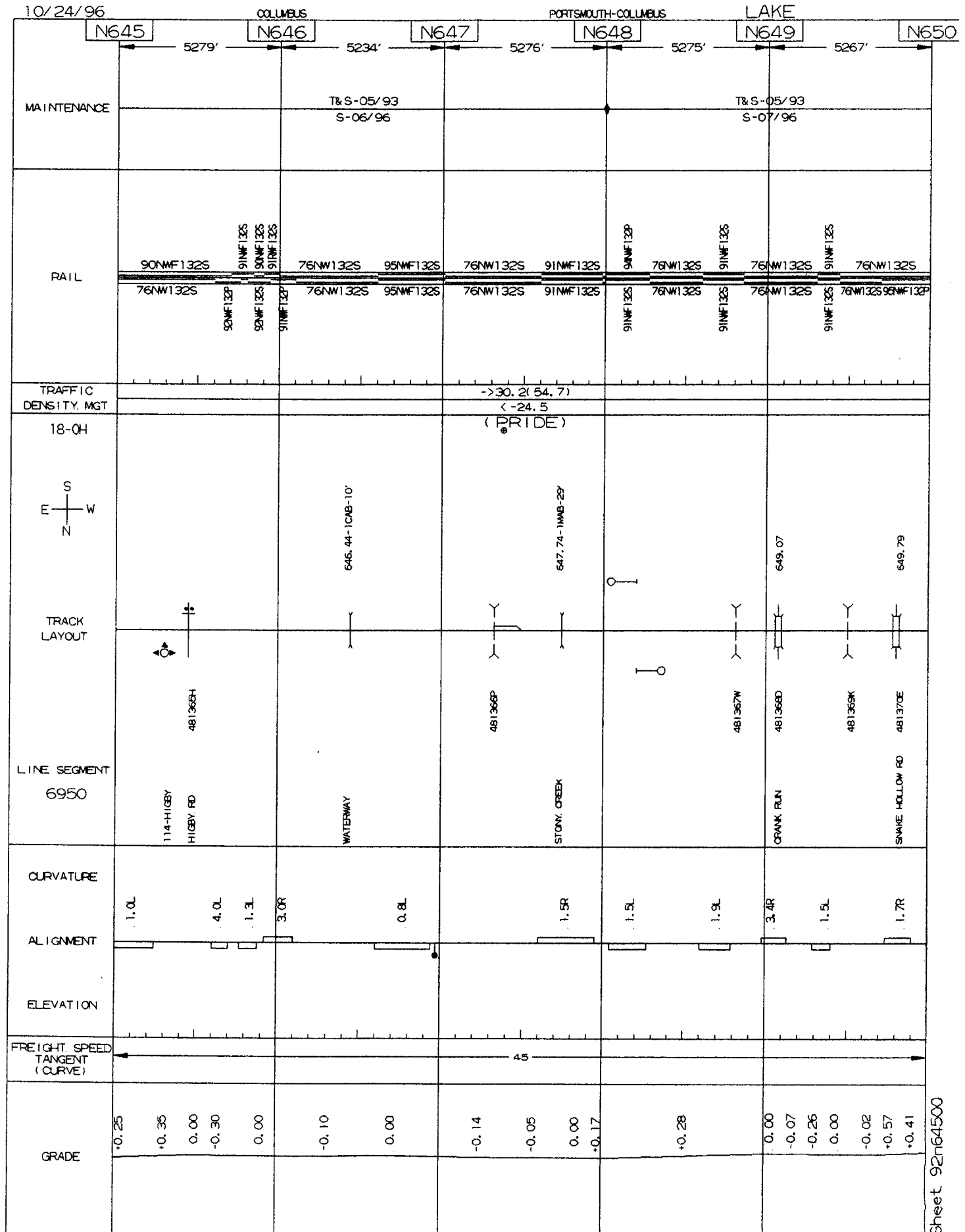


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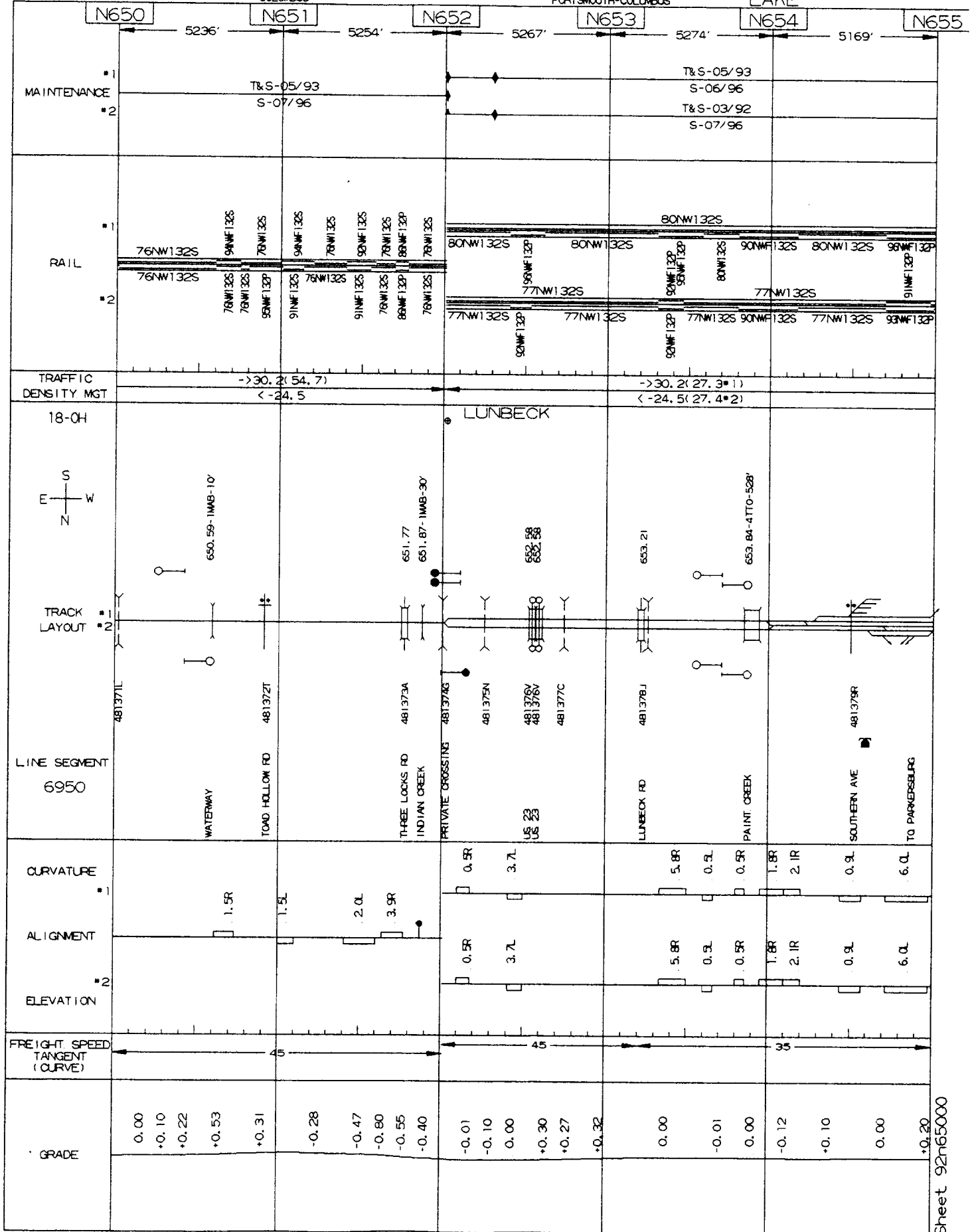


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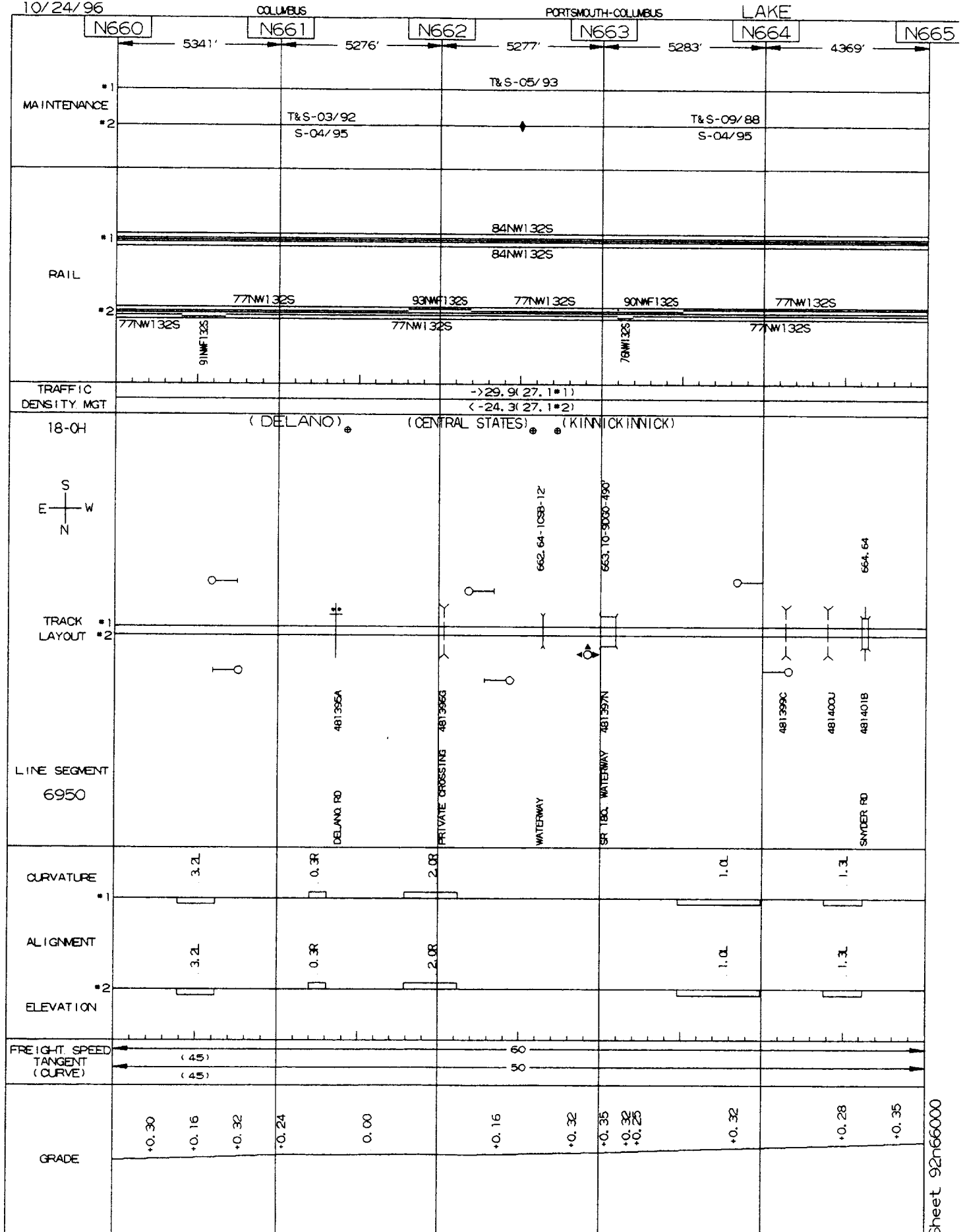
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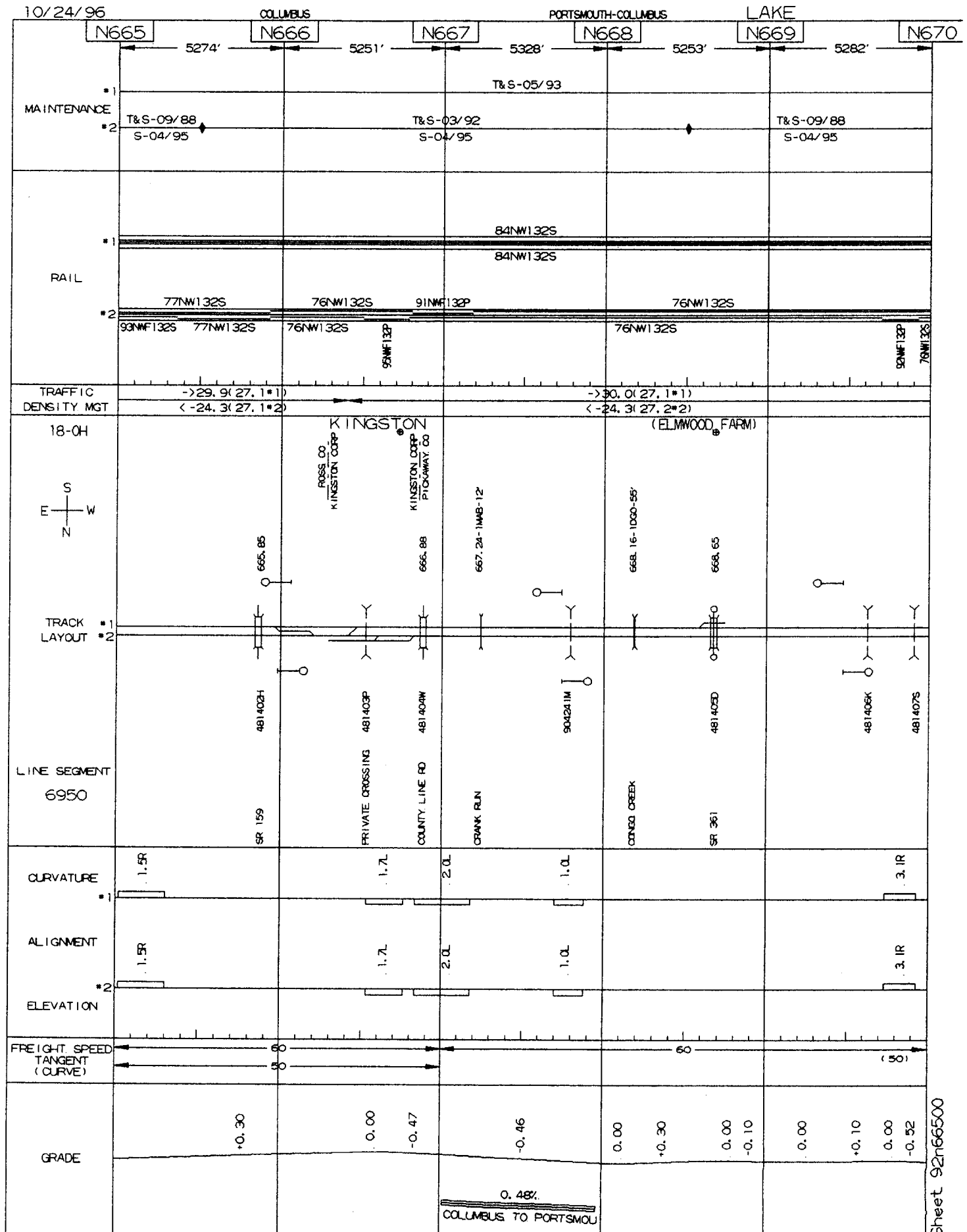
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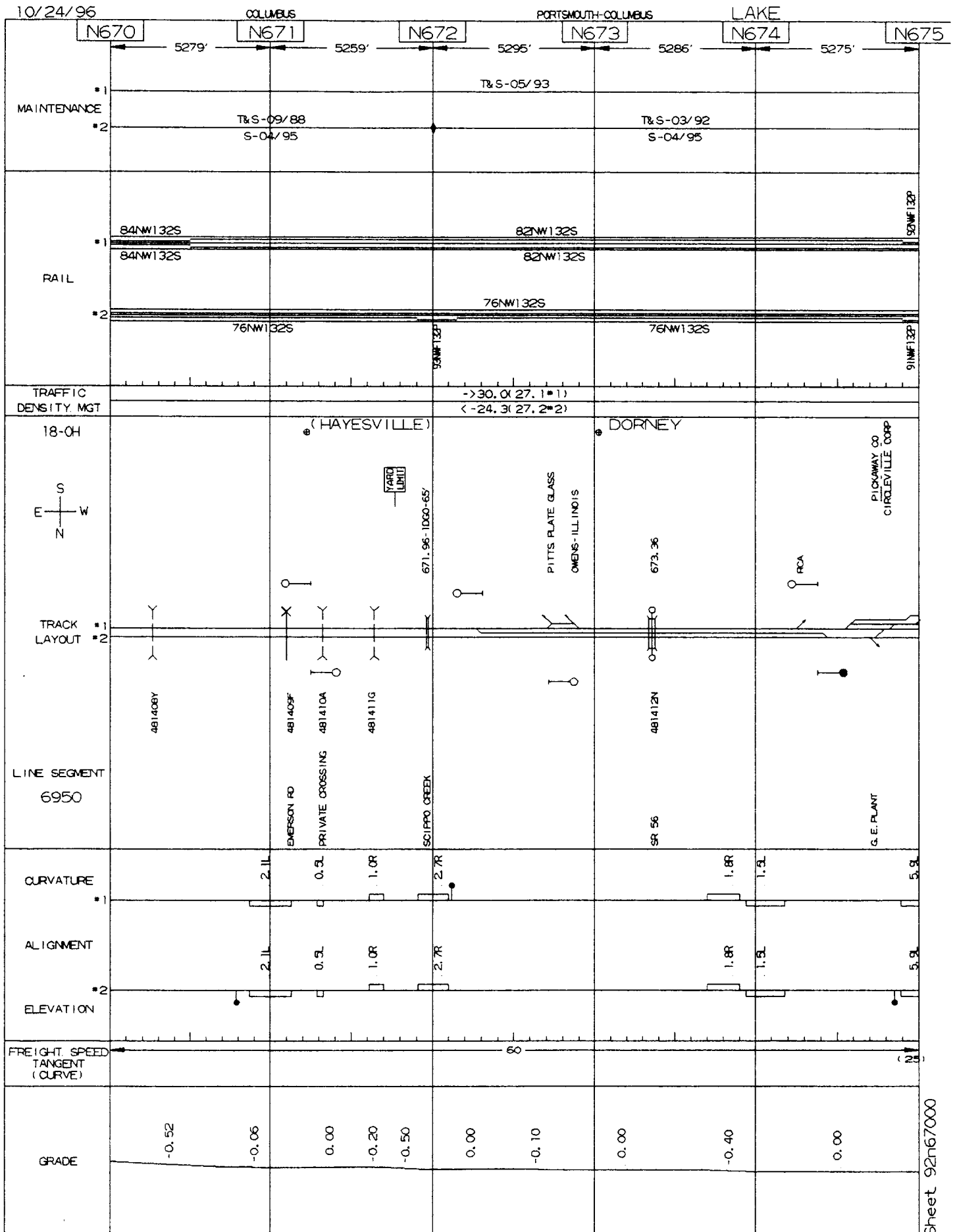
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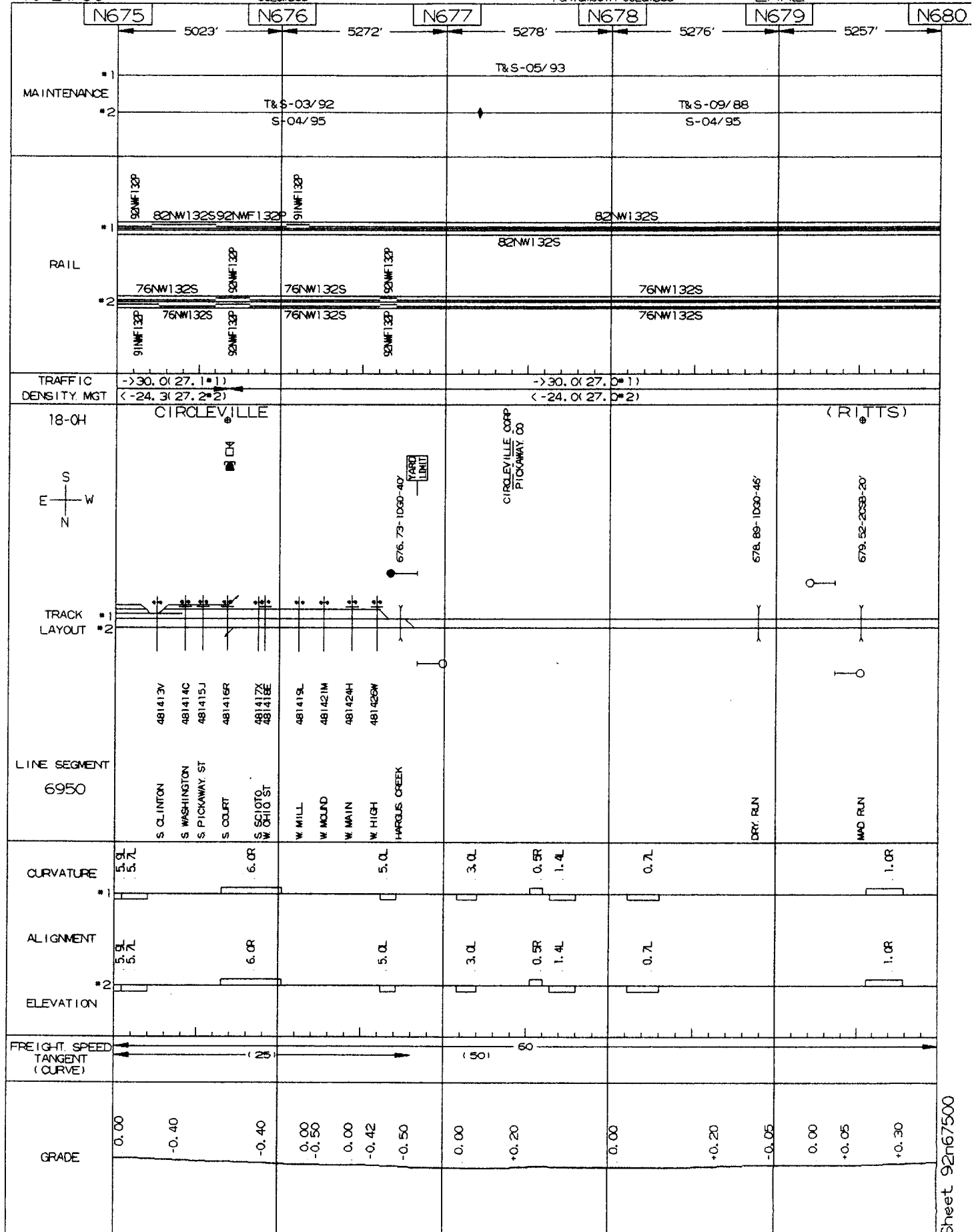


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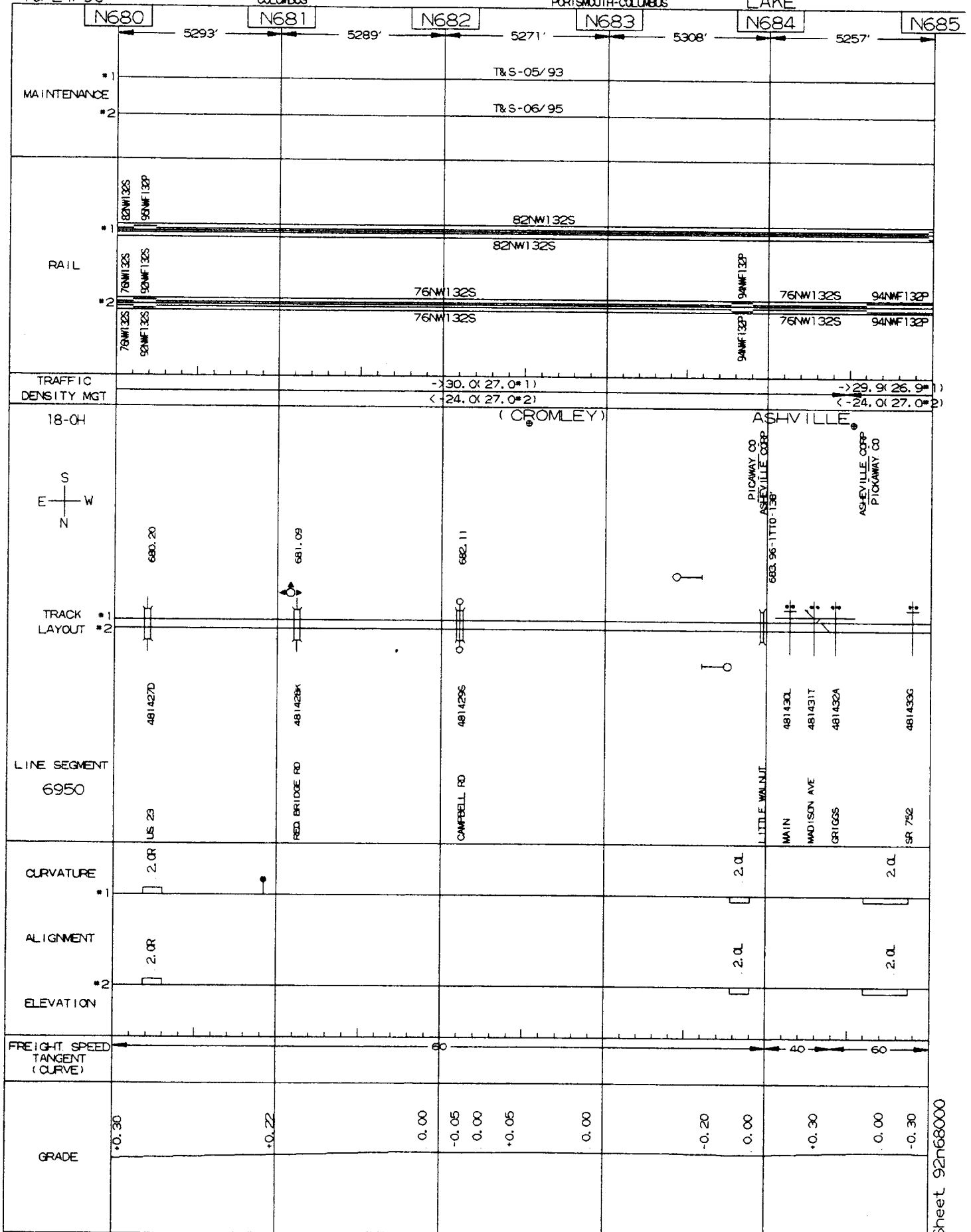


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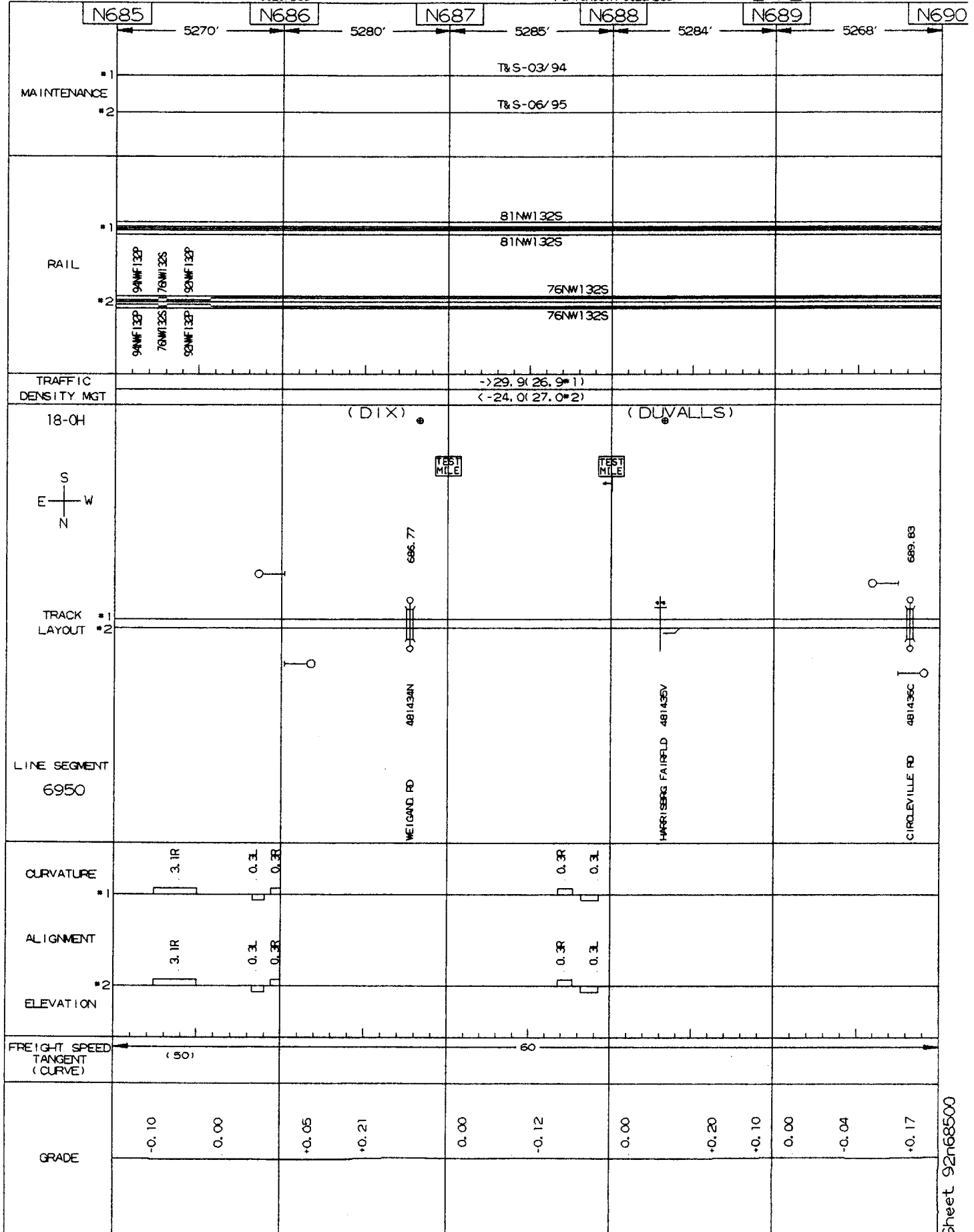


10/24/96

COLUMBUS

PORTSMOUTH-COLUMBUS

LAKE

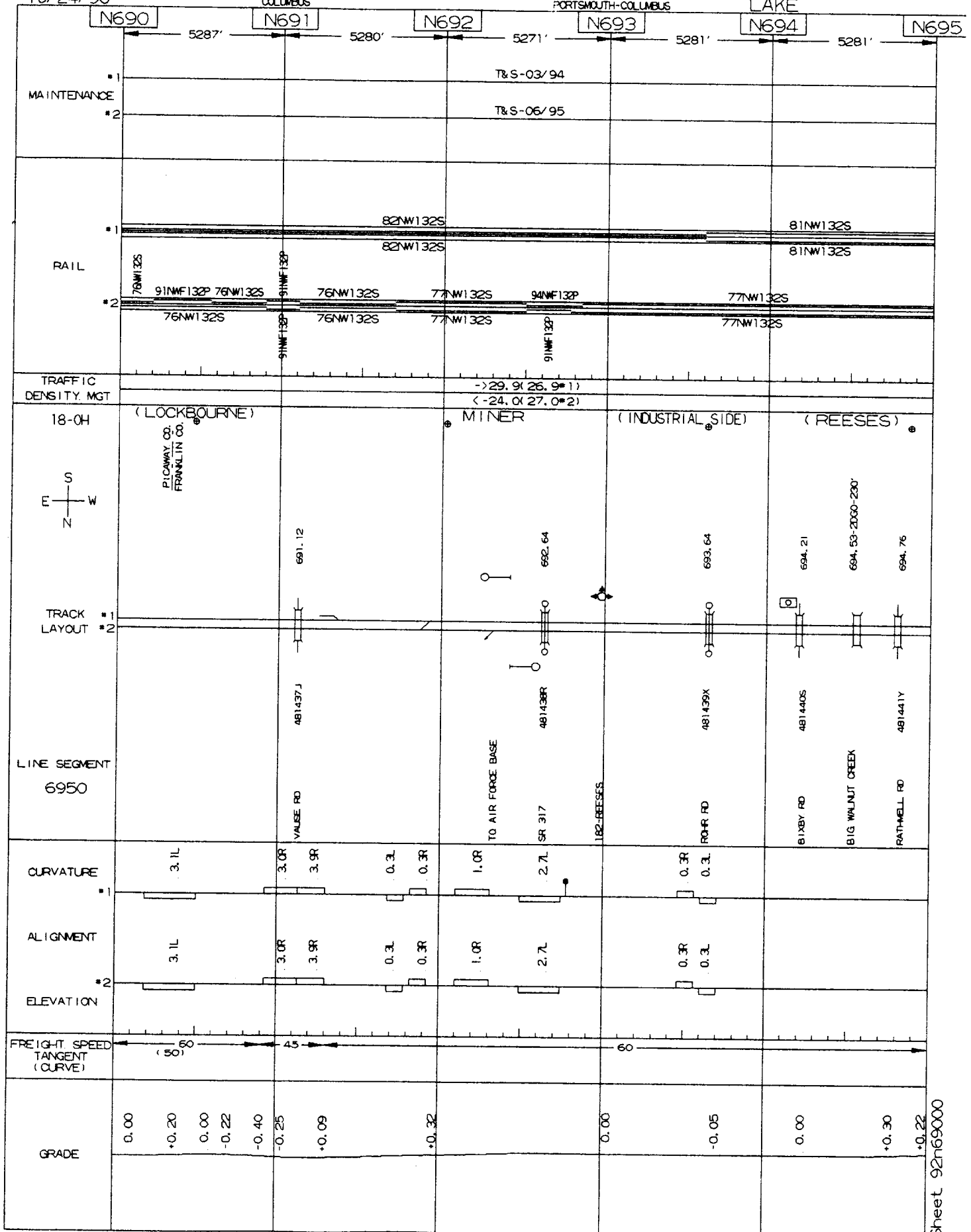


10/24/96

COLUMBUS

PORTSMOUTH-COLUMBUS

LAKE

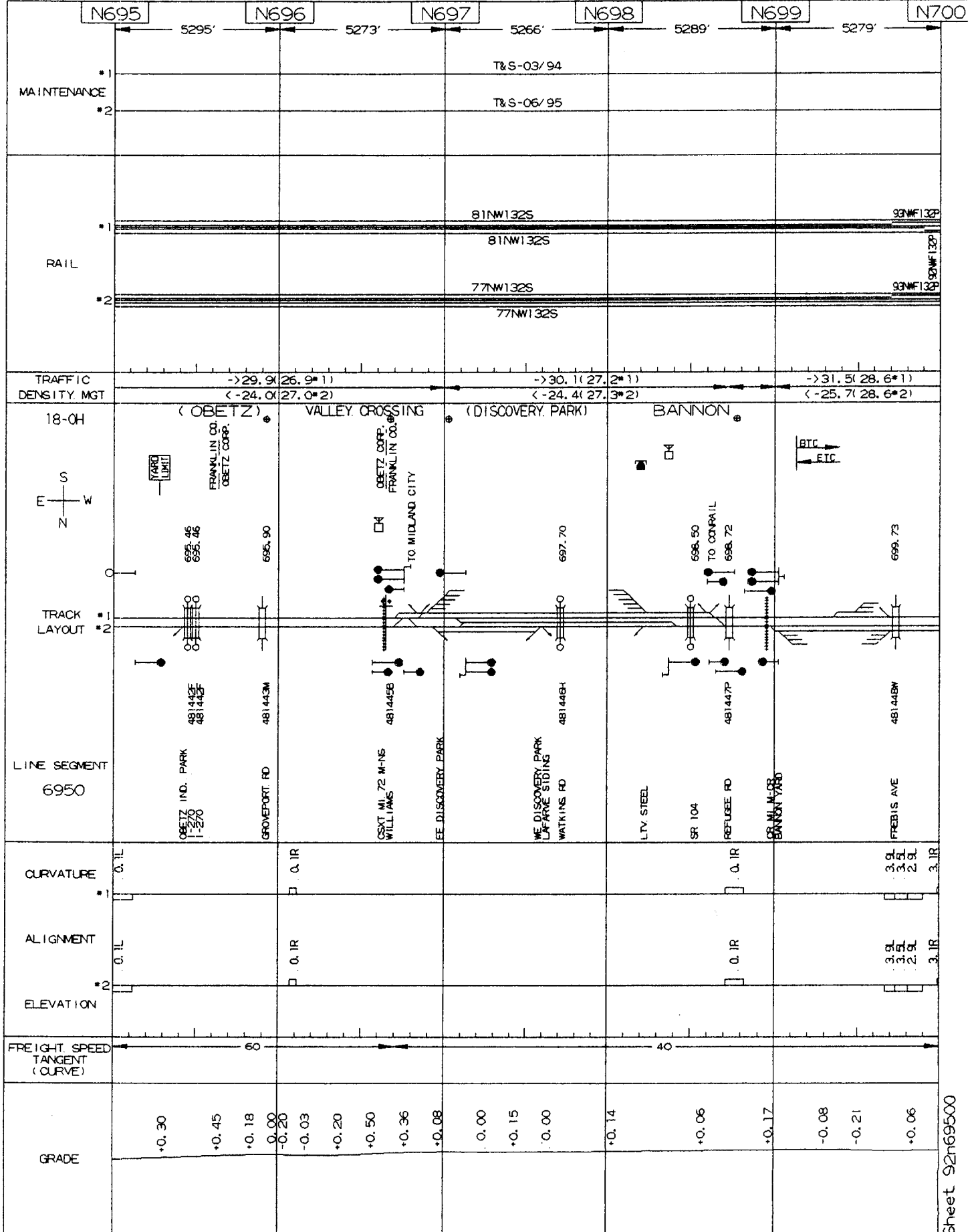


10/24/96

COLUMBUS

PORTSMOUTH-COLUMBUS

LAKE

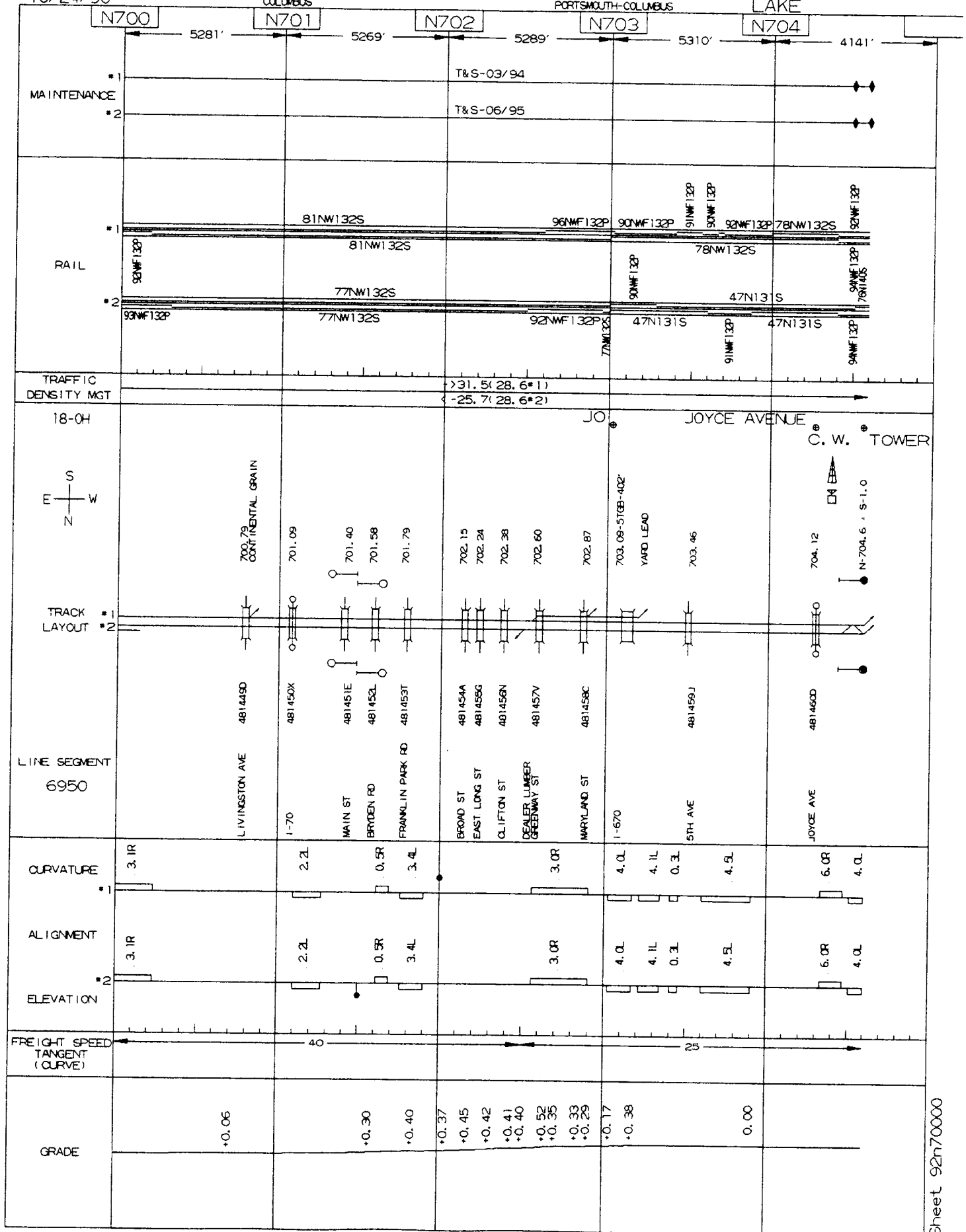


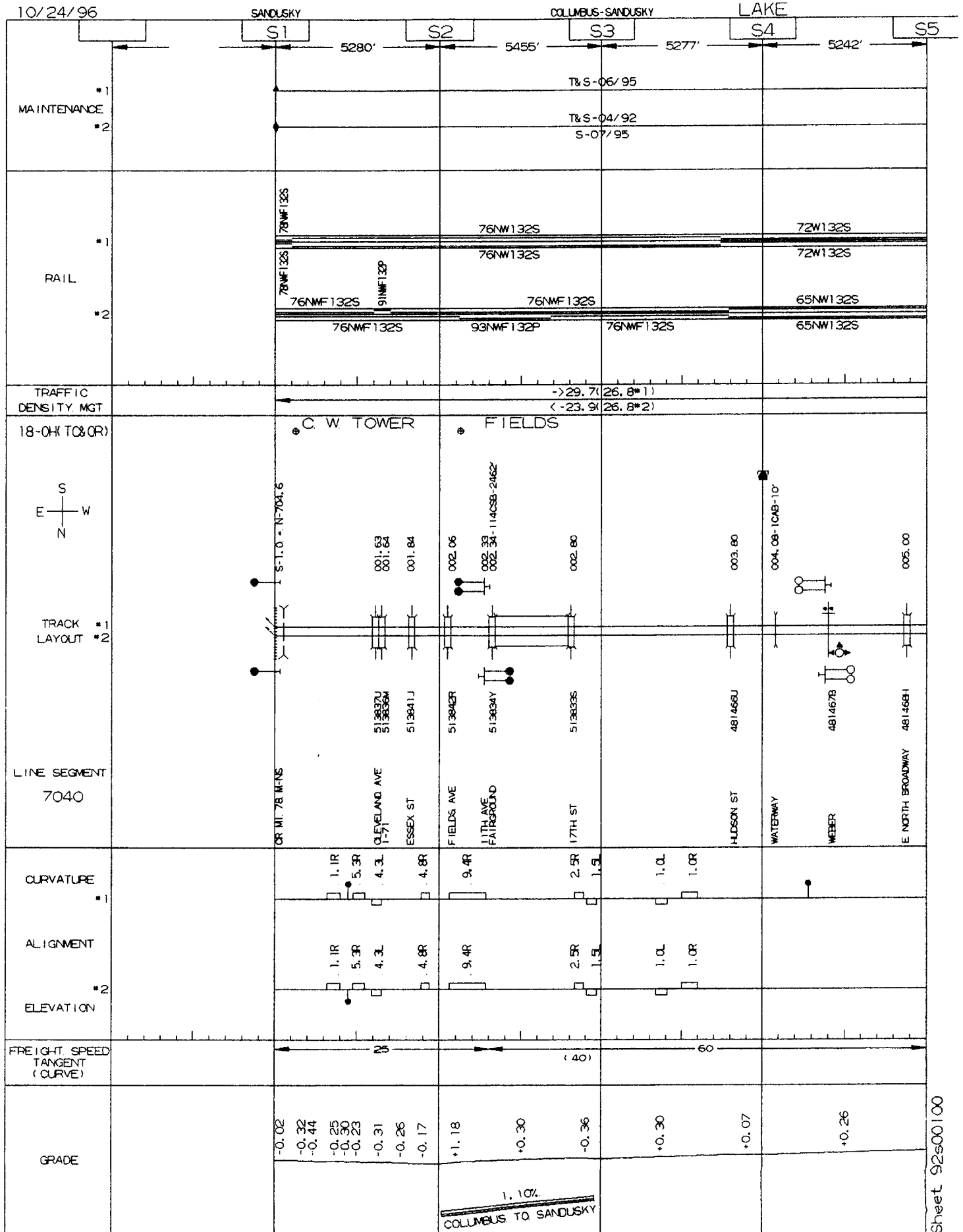
10/24/96

COLUMBUS

PORTSMOUTH-COLUMBUS

LAKE





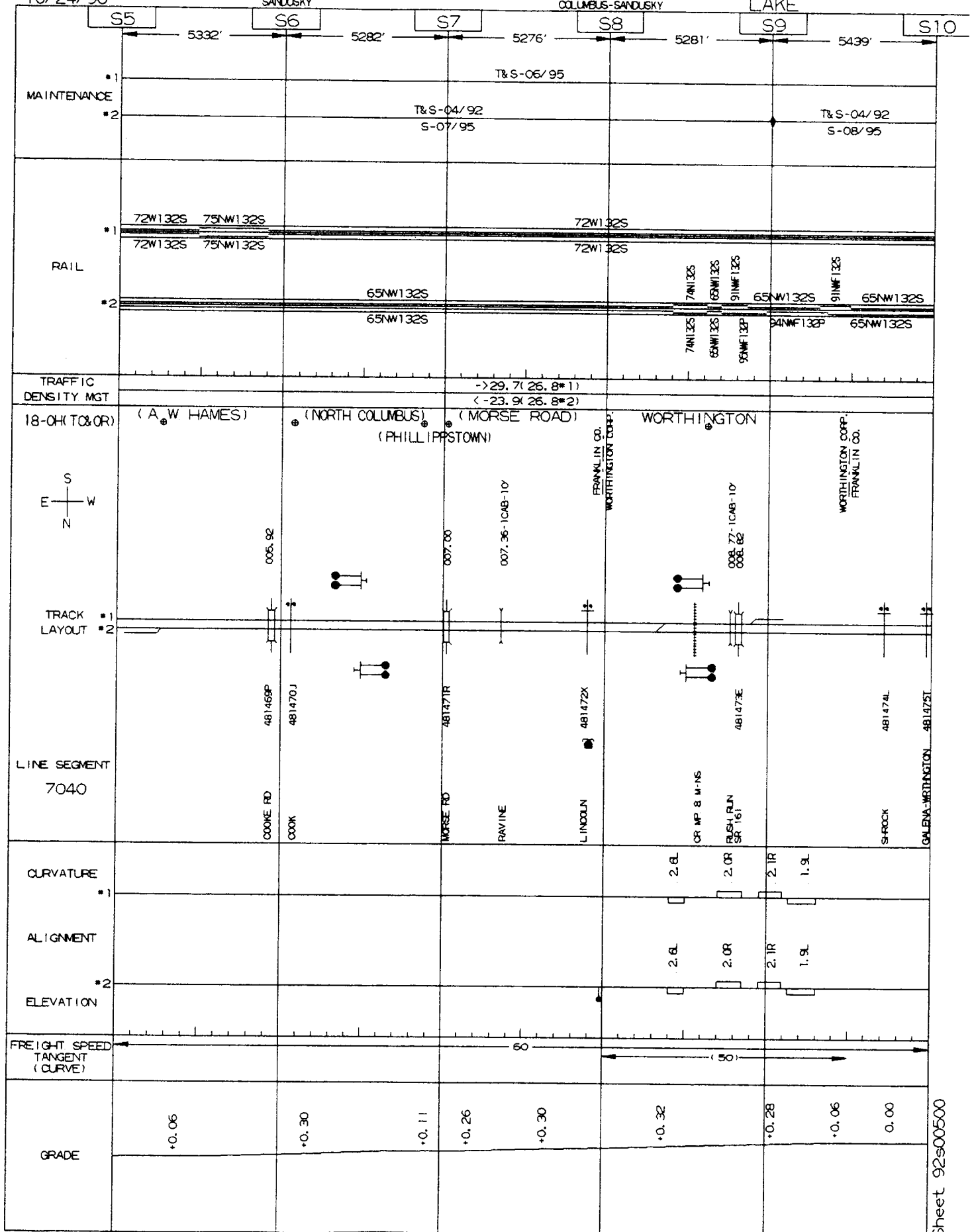


10/24/96

SANDUSKY

COLUMBUS-SANDUSKY

LAKE

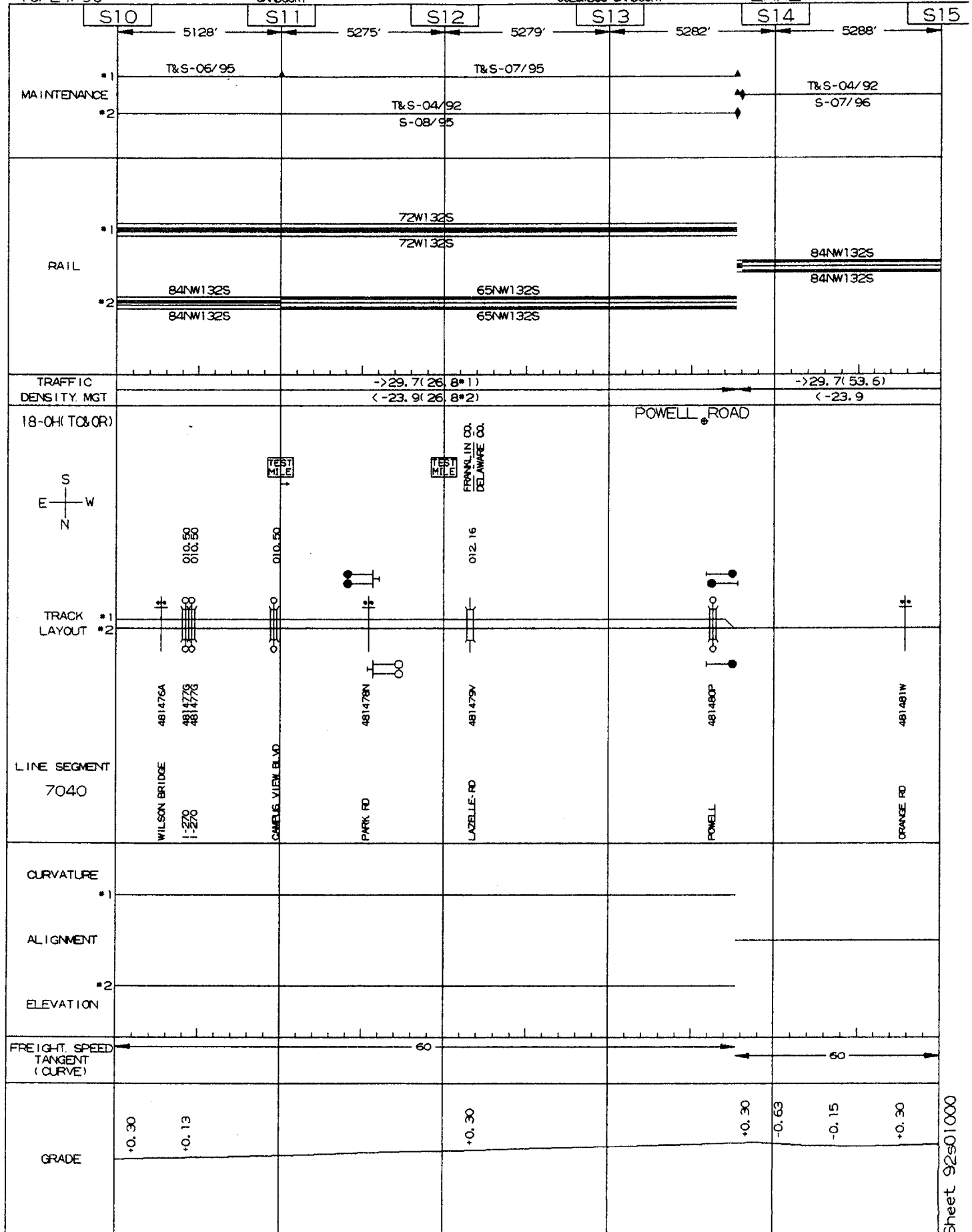


10/24/96

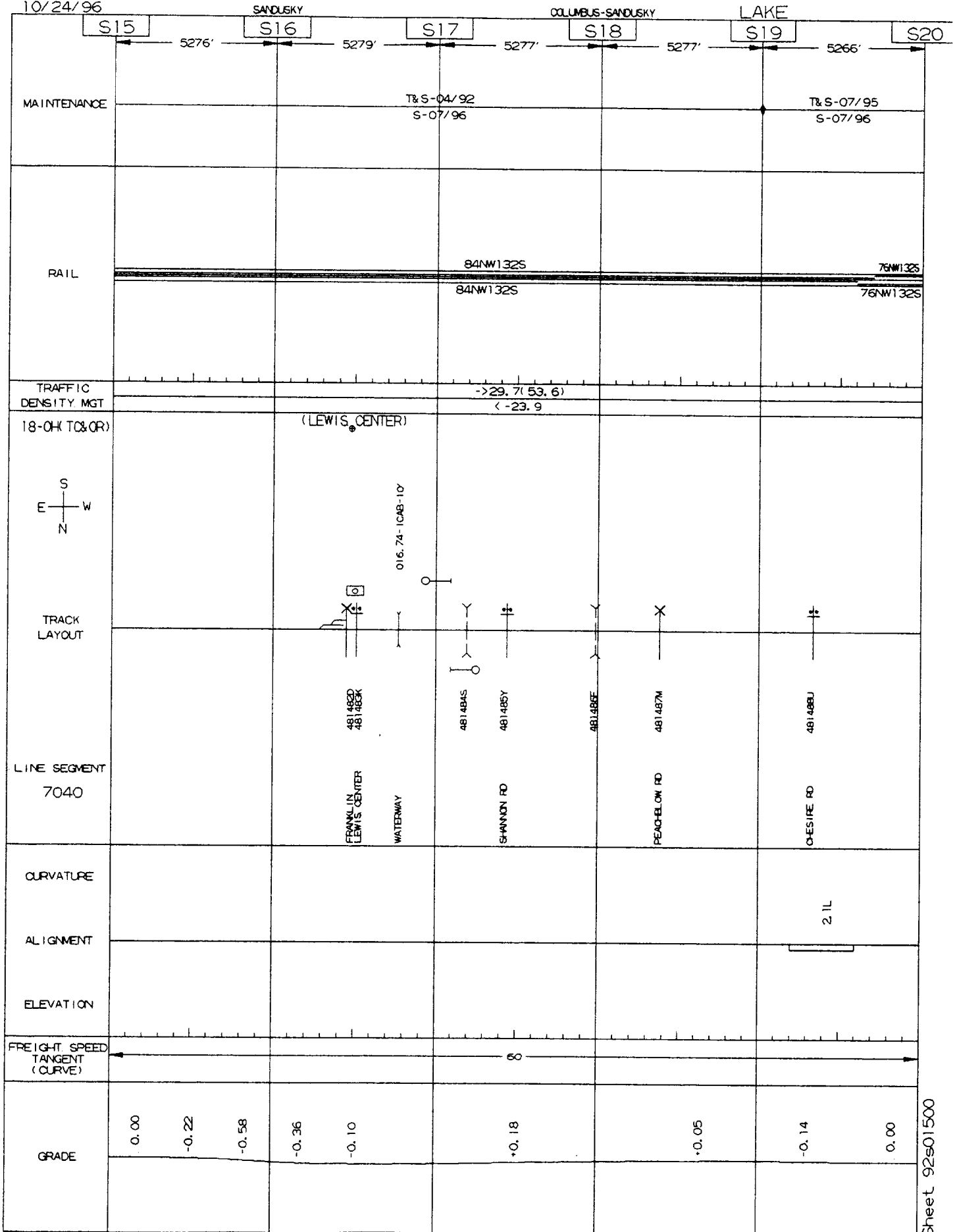
SANDUSKY

COLUMBUS-SANDUSKY

LAKE



10/24/96

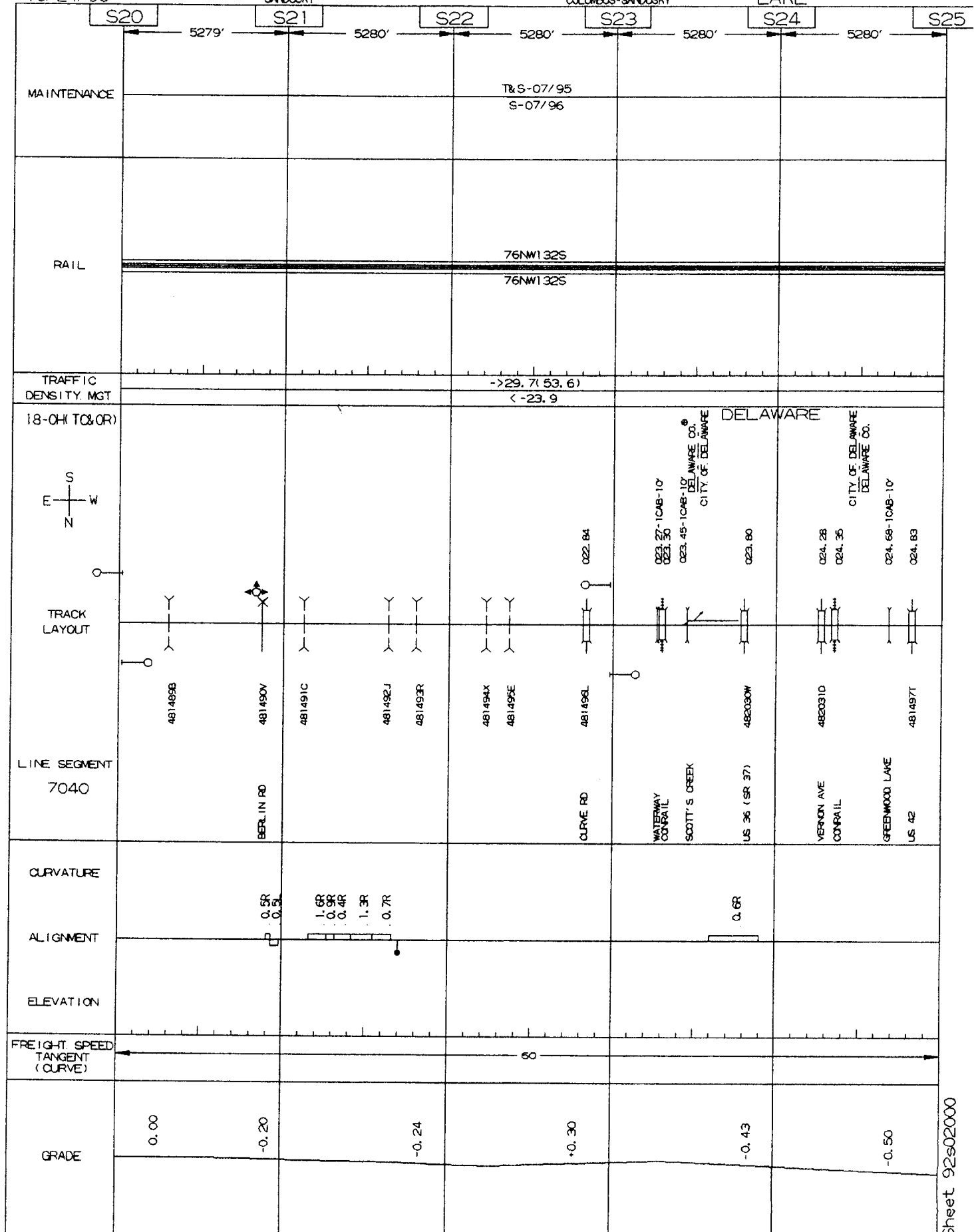


10/24/96

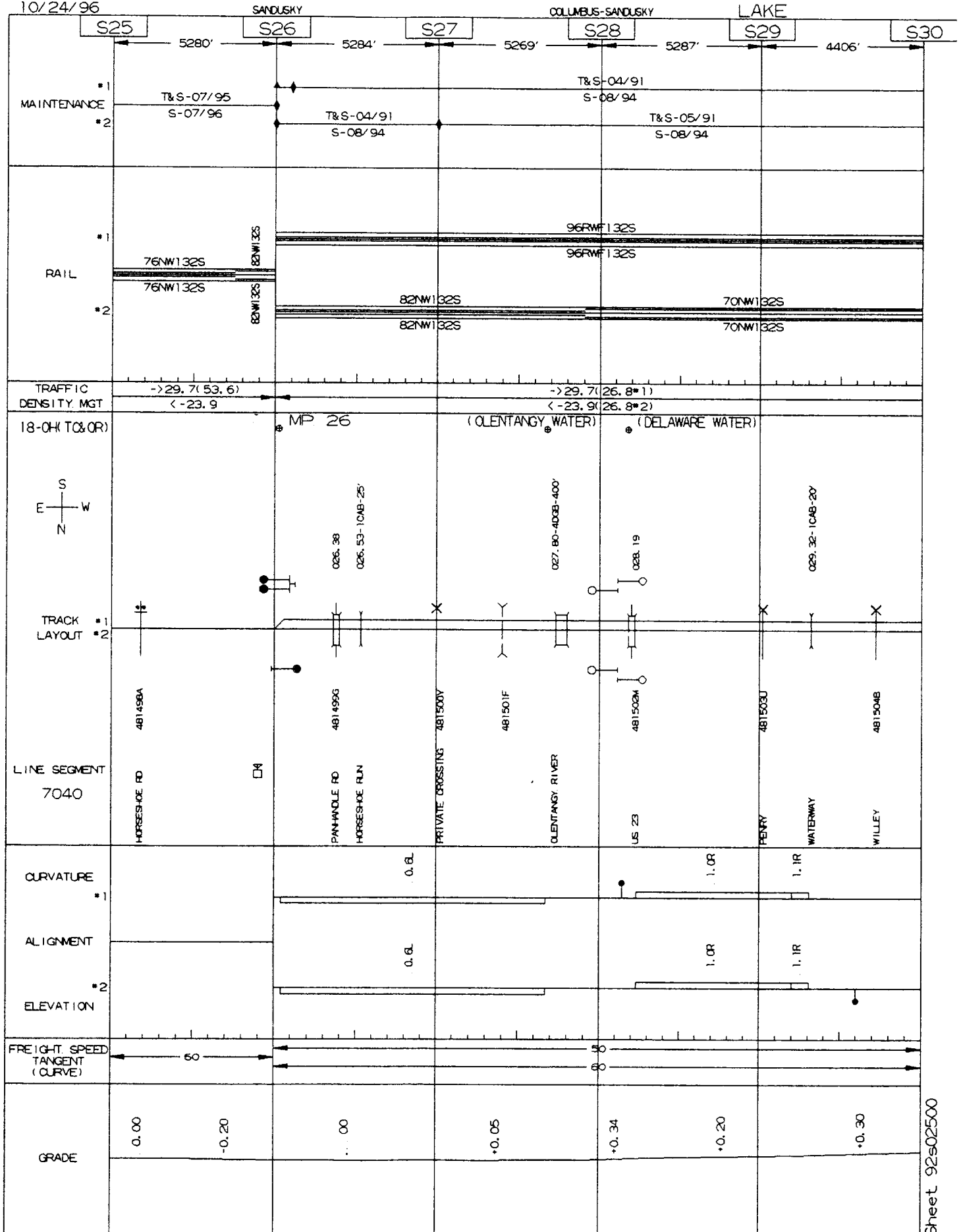
SANDUSKY

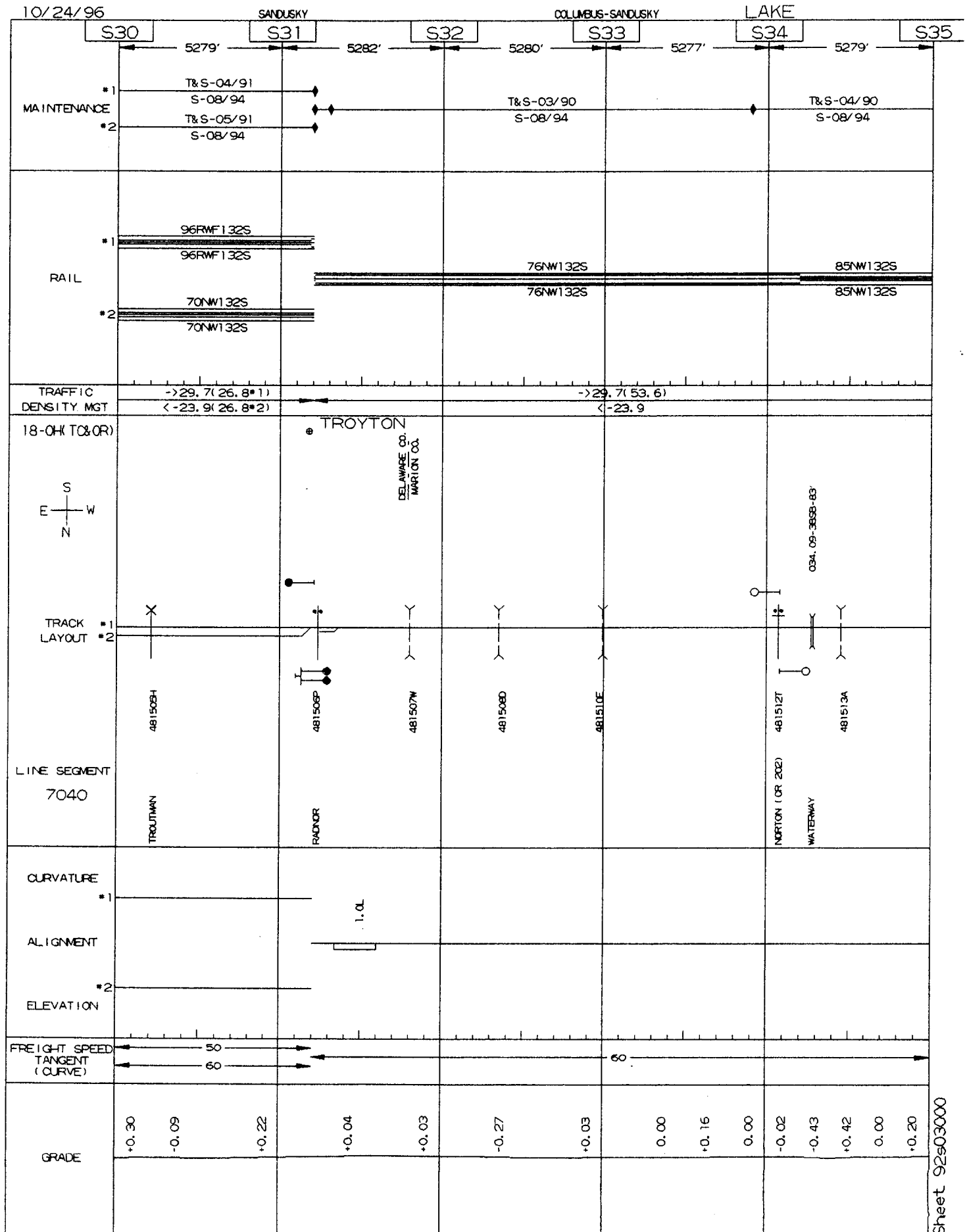
COLUMBUS-SANDUSKY

LAKE



10/24/96





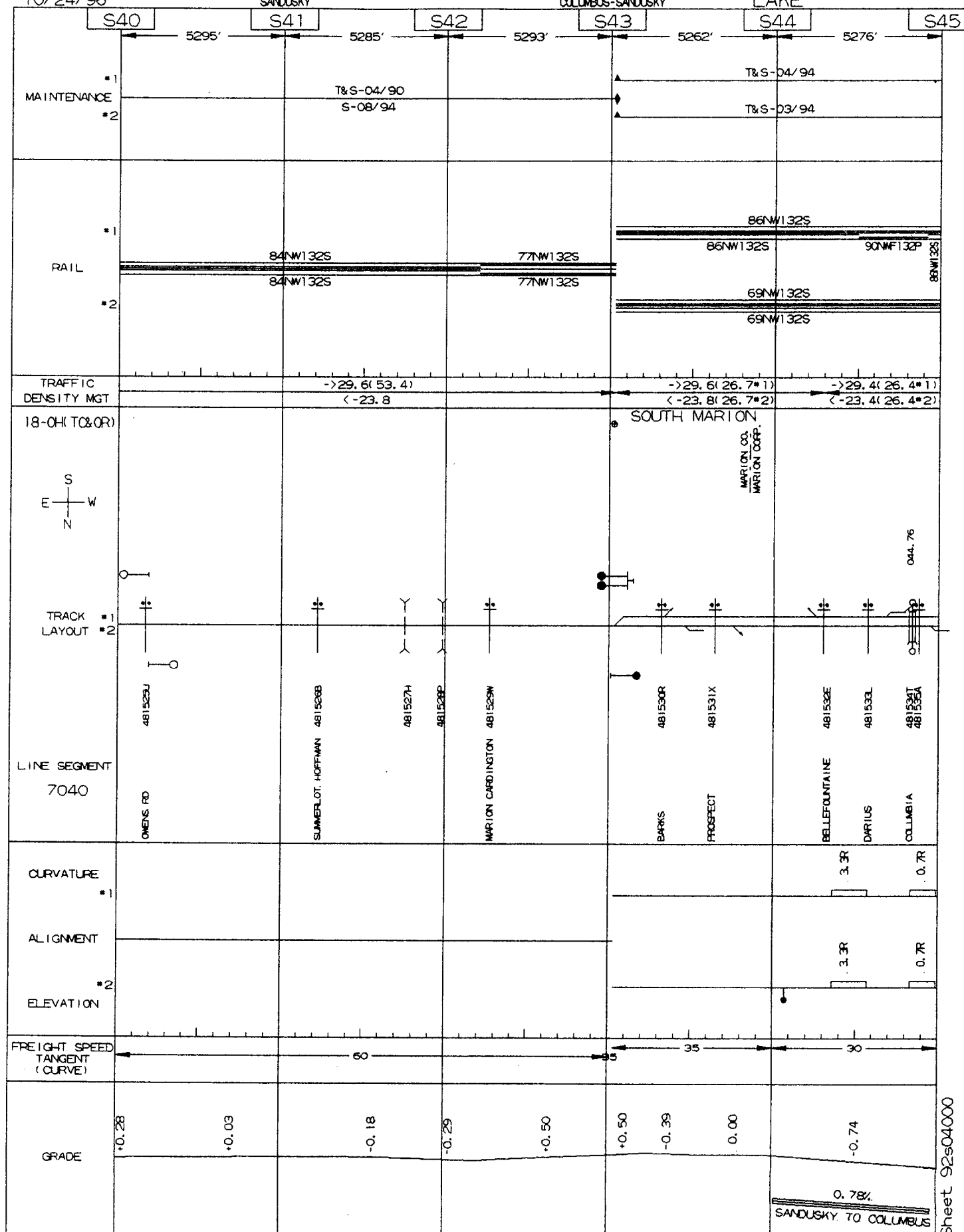
Sheet 92s03500

10/24/96

SANDUSKY

COLUMBUS-SANDUSKY

LAKE



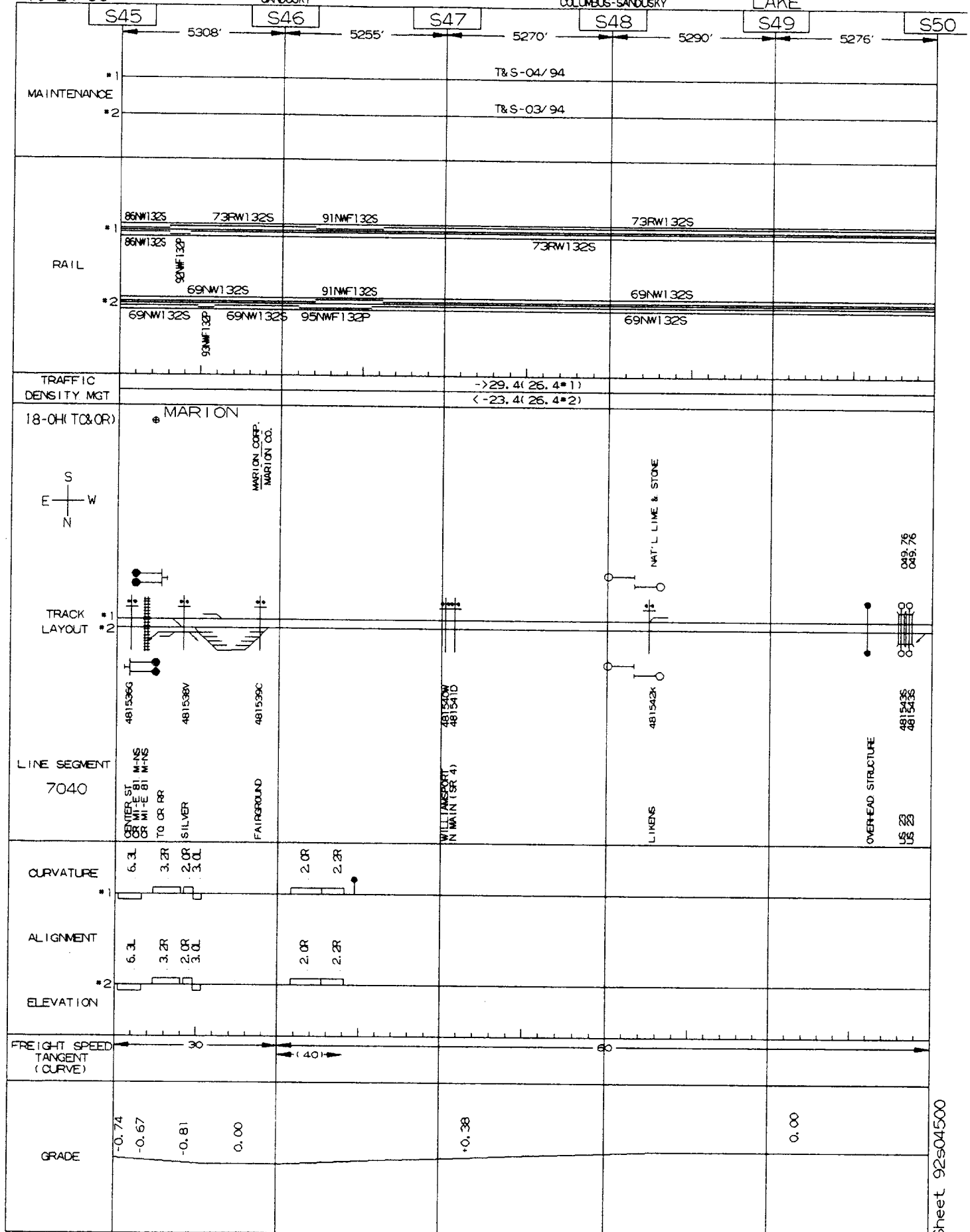


10/24/96

SANDUSKY

COLUMBUS-SANDUSKY

LAKE



Sheet 92s05000

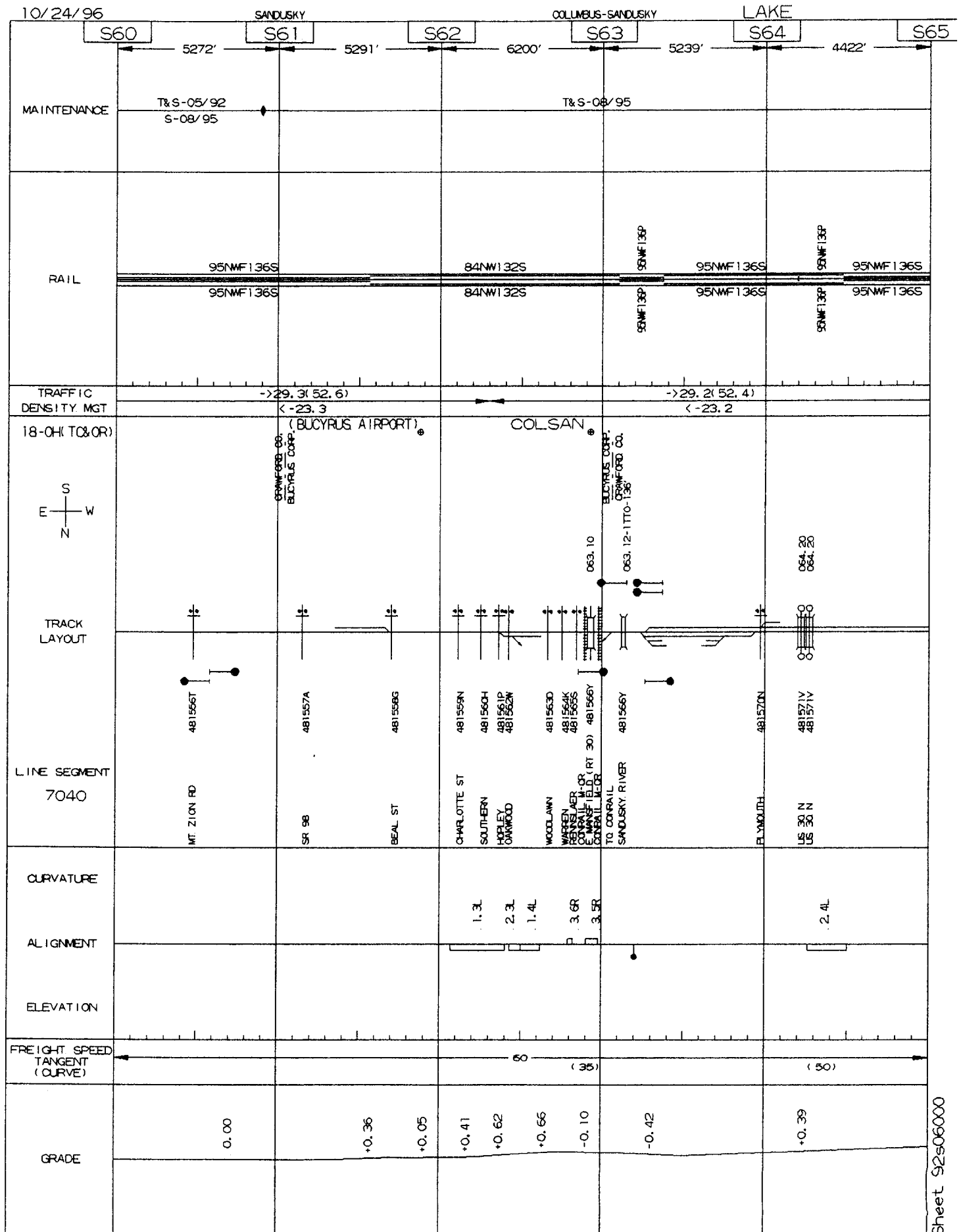
10/24/96

SANDUSKY

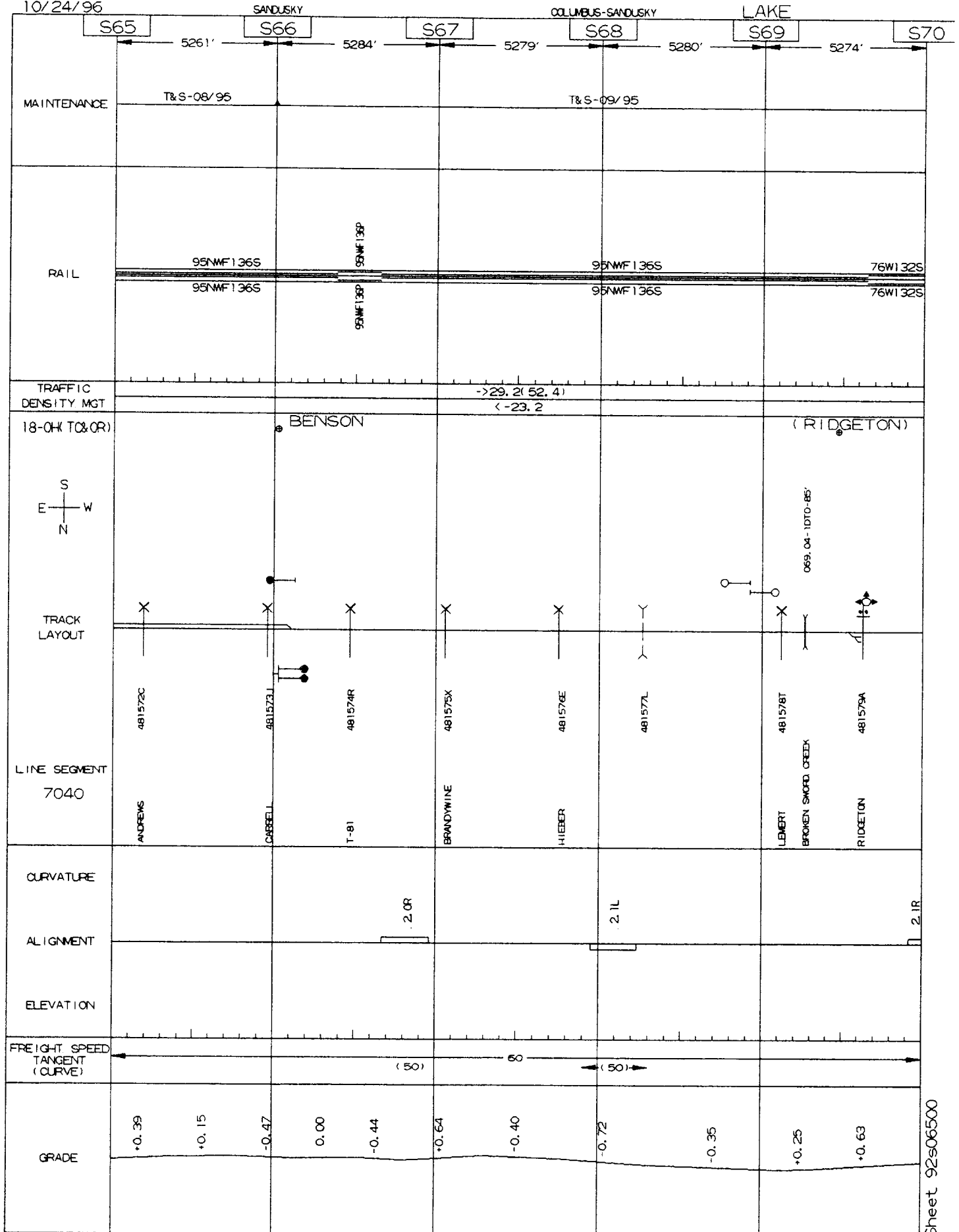
COLUMBUS-SANDUSKY

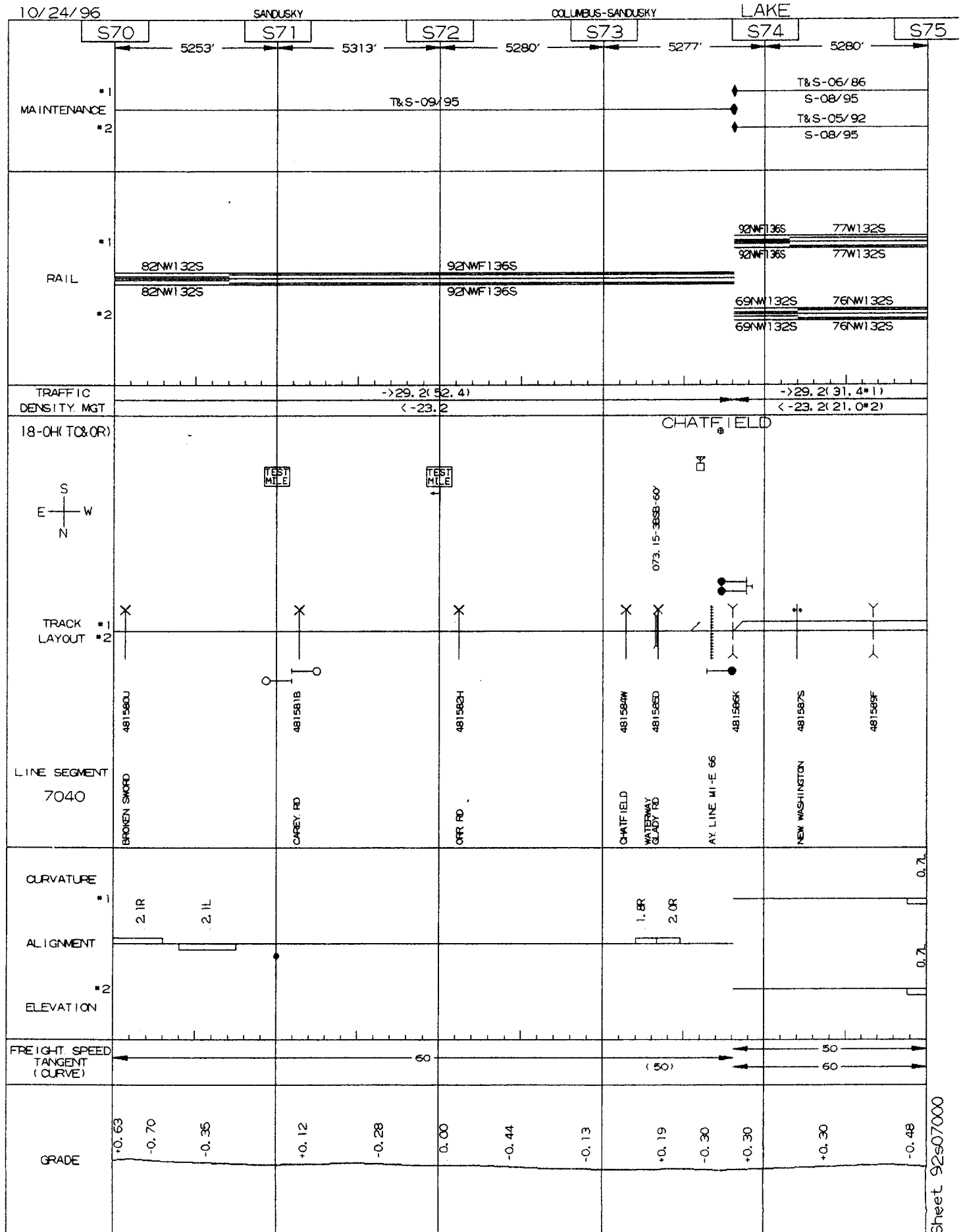
LAKE

	S55	S56	S57	S58	S59	S60
MAINTENANCE	5280'	5279'	5282'	5274'	5277'	
			T&S-05/92 S-08/95			
RAIL			95NMF1365 95NMF1365			
TRAFFIC DENSITY MGT			->29.3 (52.6) <-23.3			
18-041 T&OR	(MONNETTE)					
	S E—W N	055.77-2038-24'			059.12-2858-62'	
TRACK LAYOUT	481551J 481552R MONNETTE DALLAS TWP 115 WATERWAY	481553X DALLAS TWP 96	481554E CALDWELL			
LINE SEGMENT 7040						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			60			
GRADE	+0.04 -0.28	+0.44 0.00	+0.19	-0.18	+0.19 0.00	

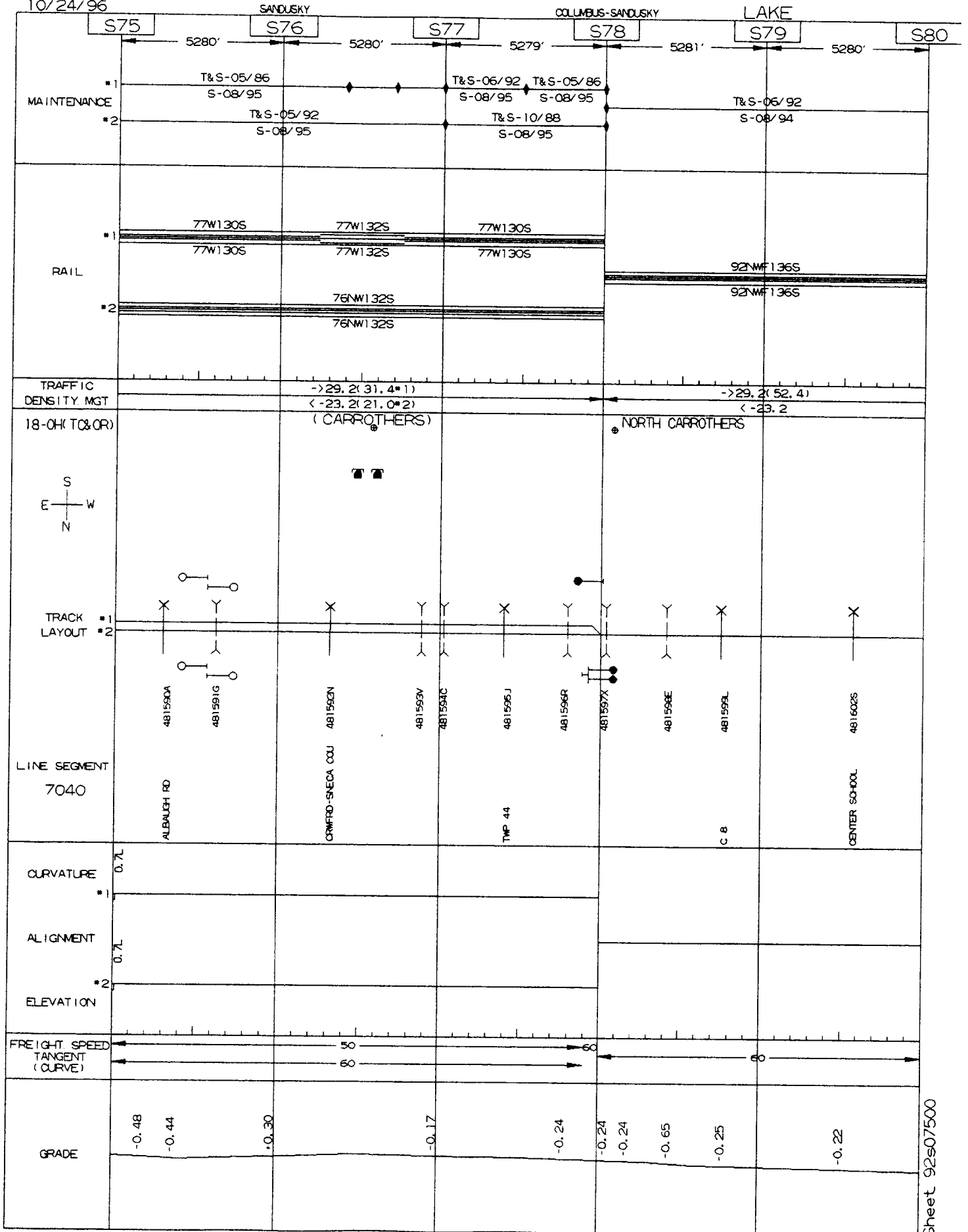


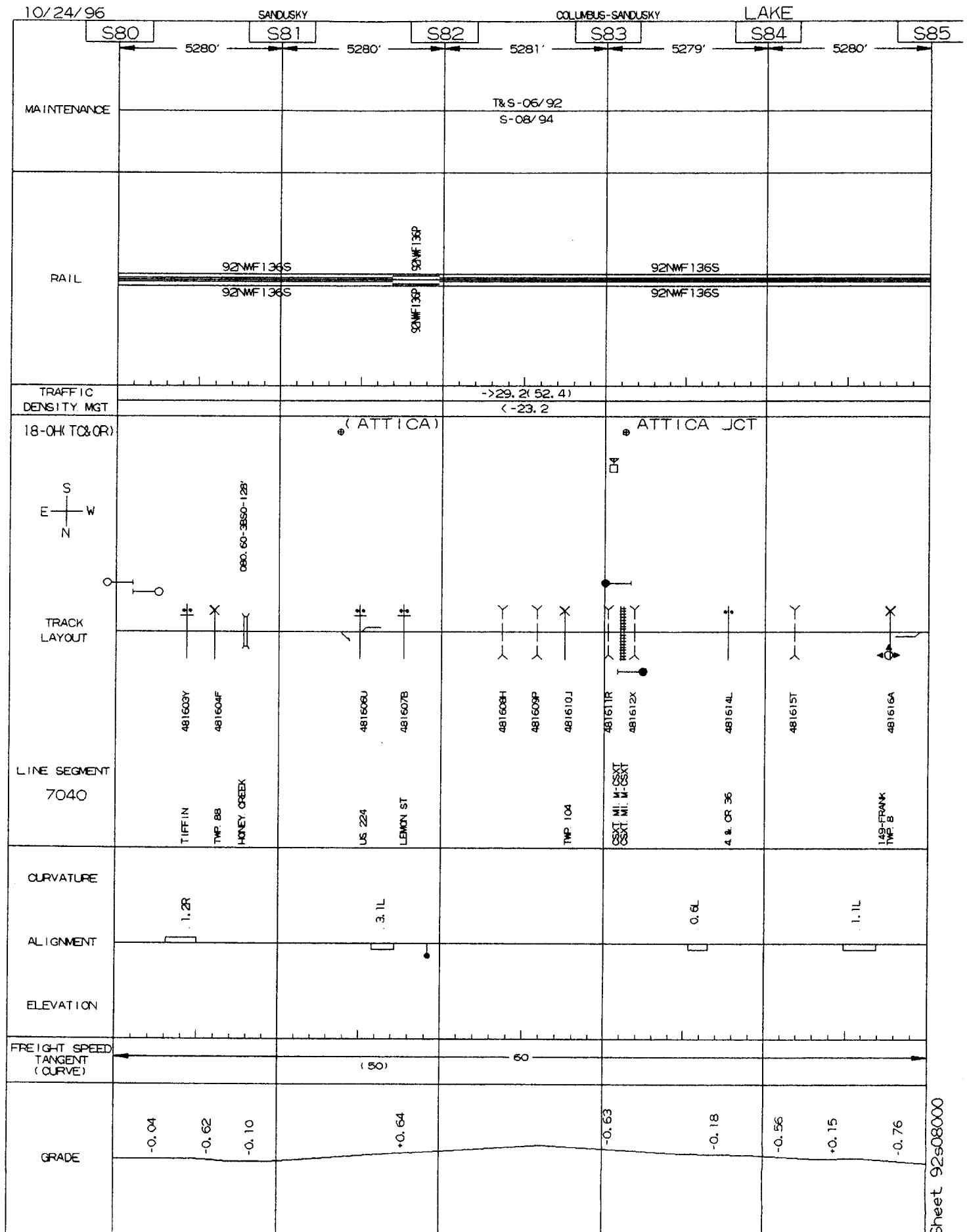
10/24/96





10/24/96





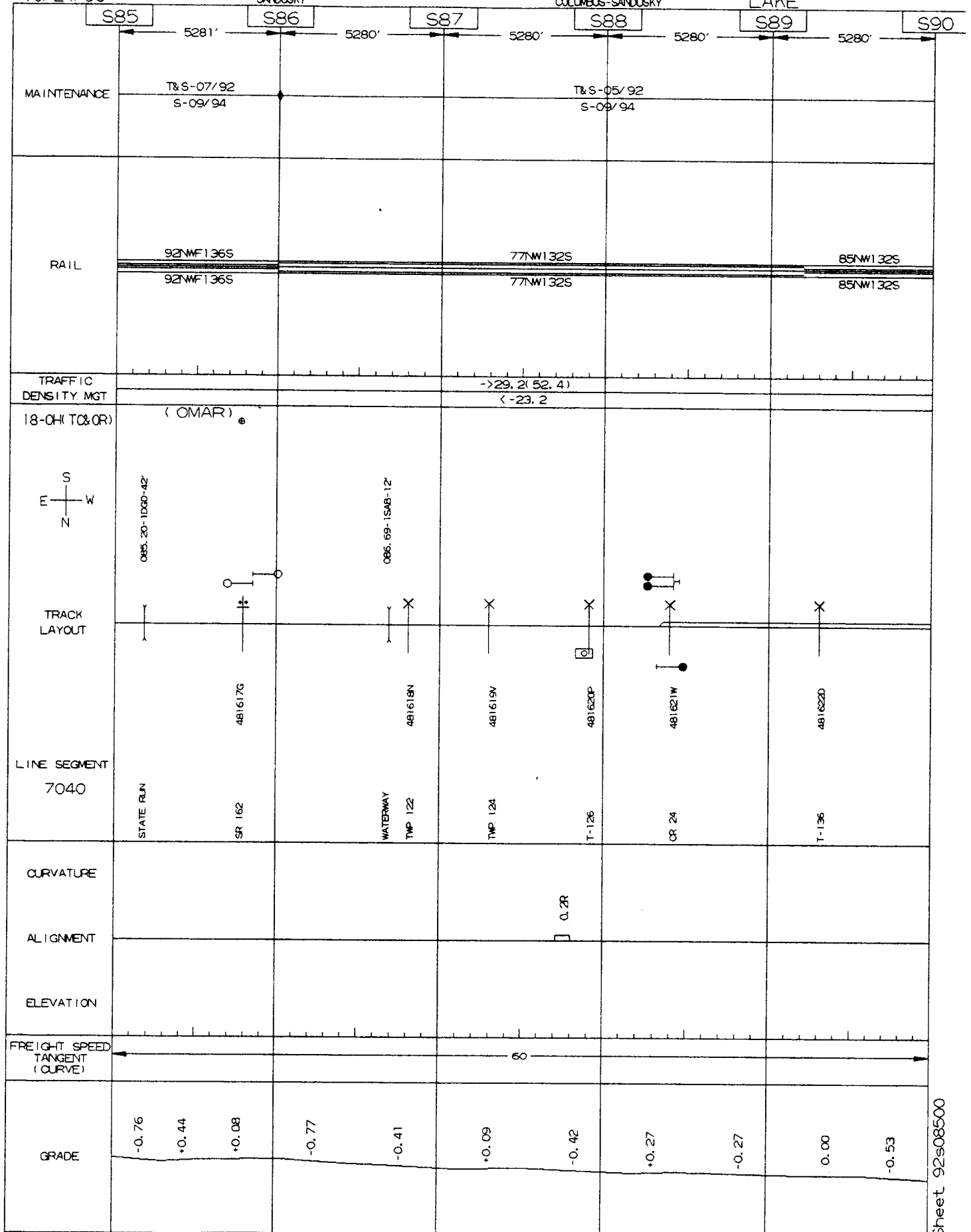


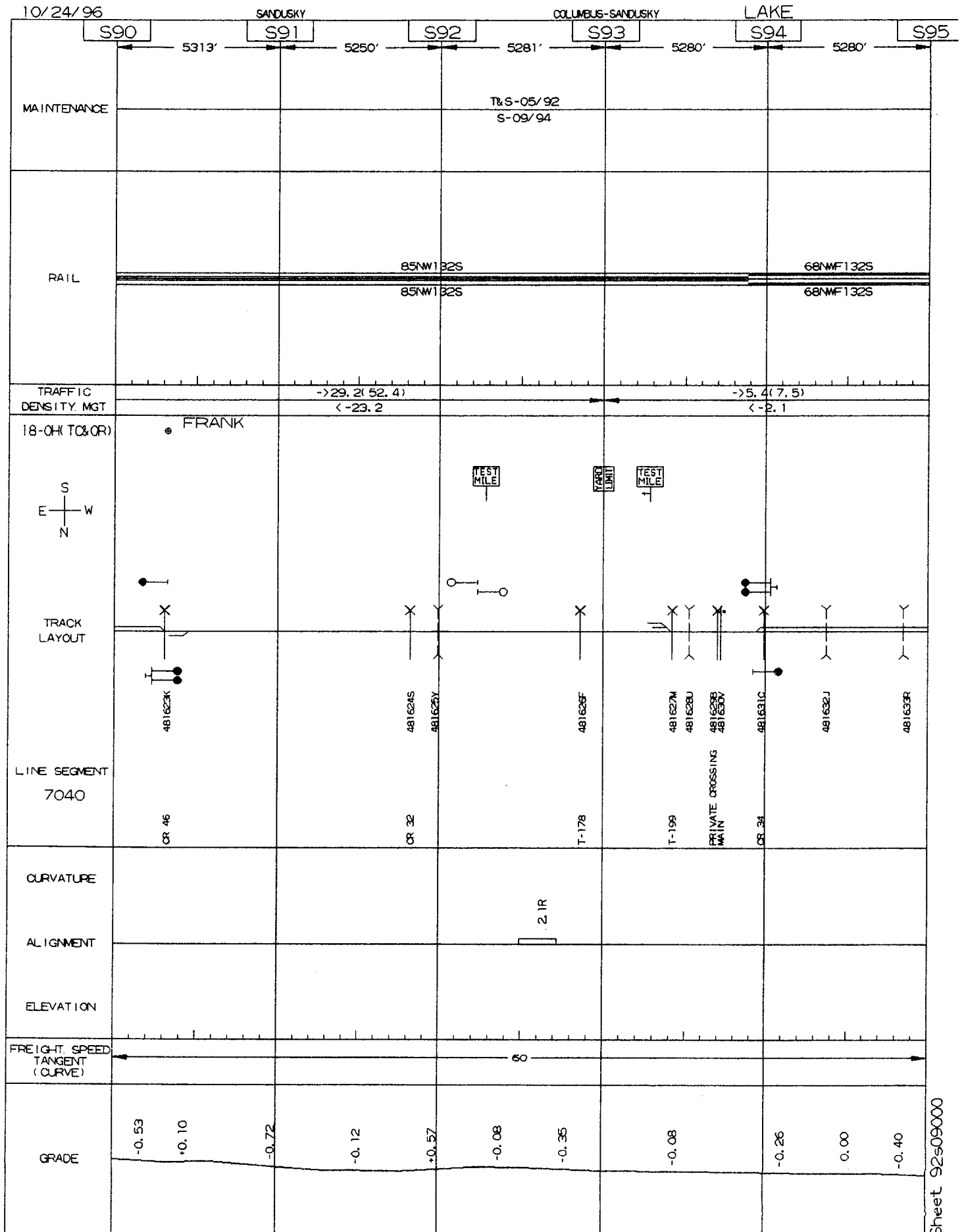
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SANDUSKY

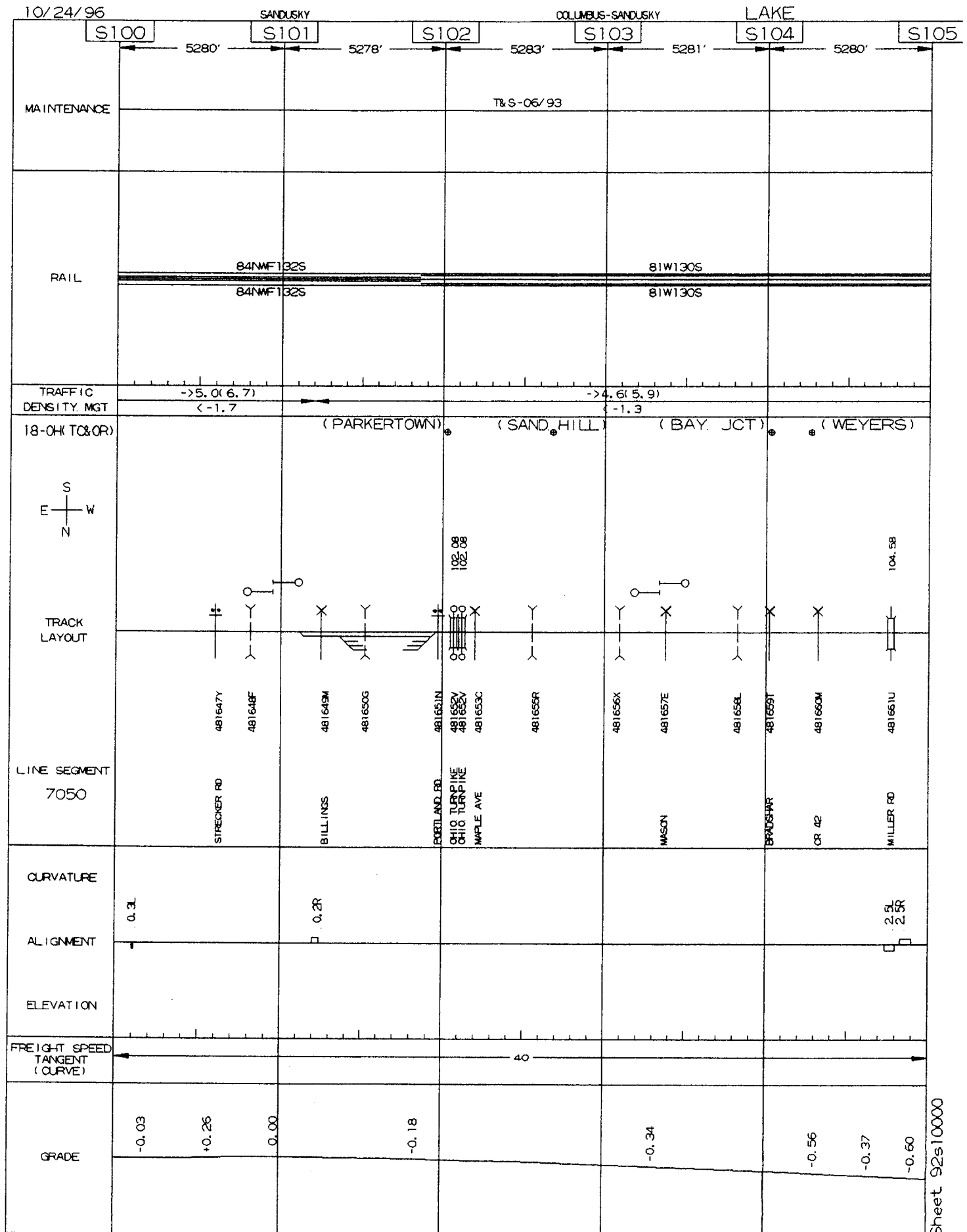
COLUMBUS-SANDUSKY

LAKE





Sheet 92s09500

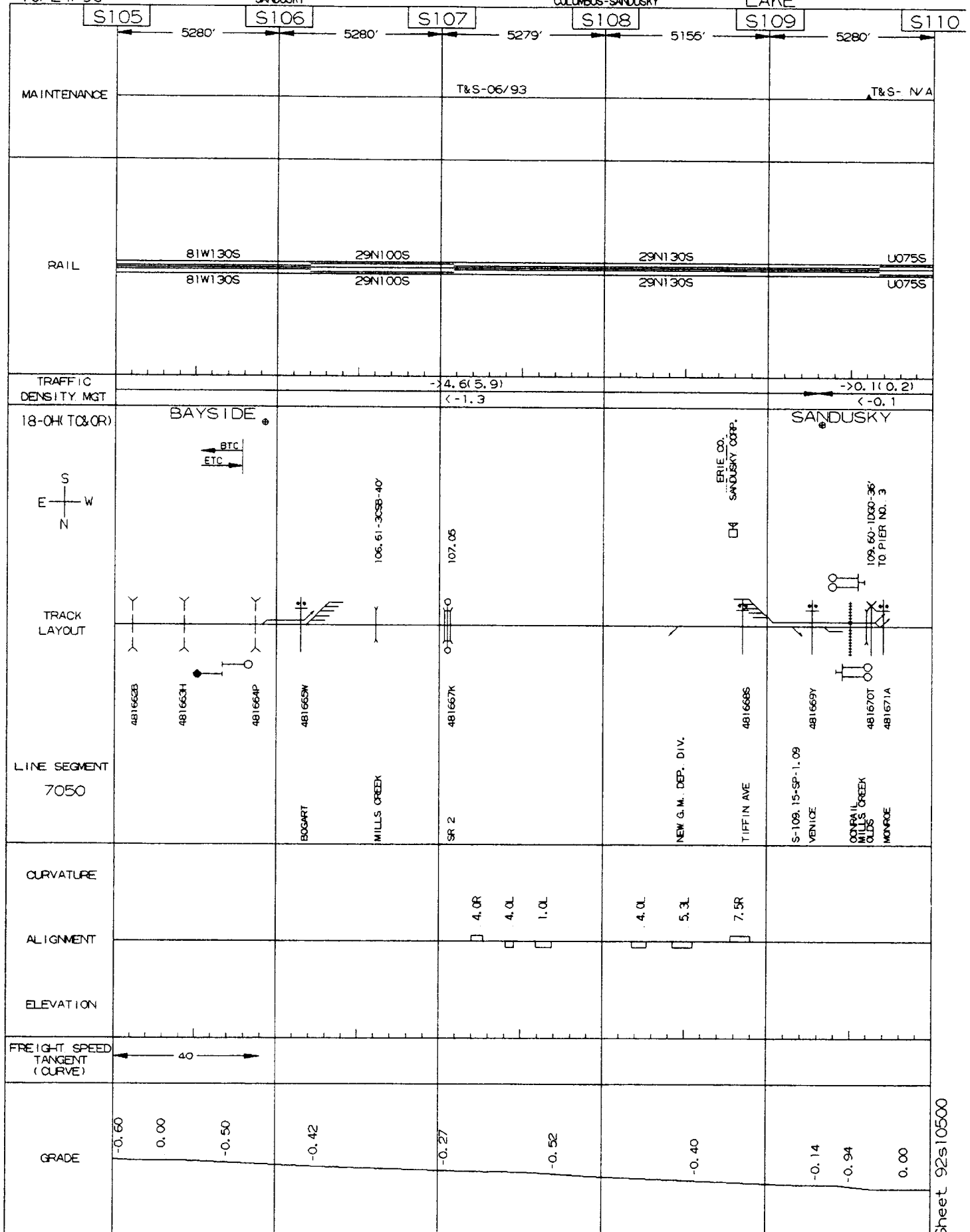


10/24/96

SANDUSKY

COLUMBUS-SANDUSKY

LAKE



10/24/96

SANDUSKY

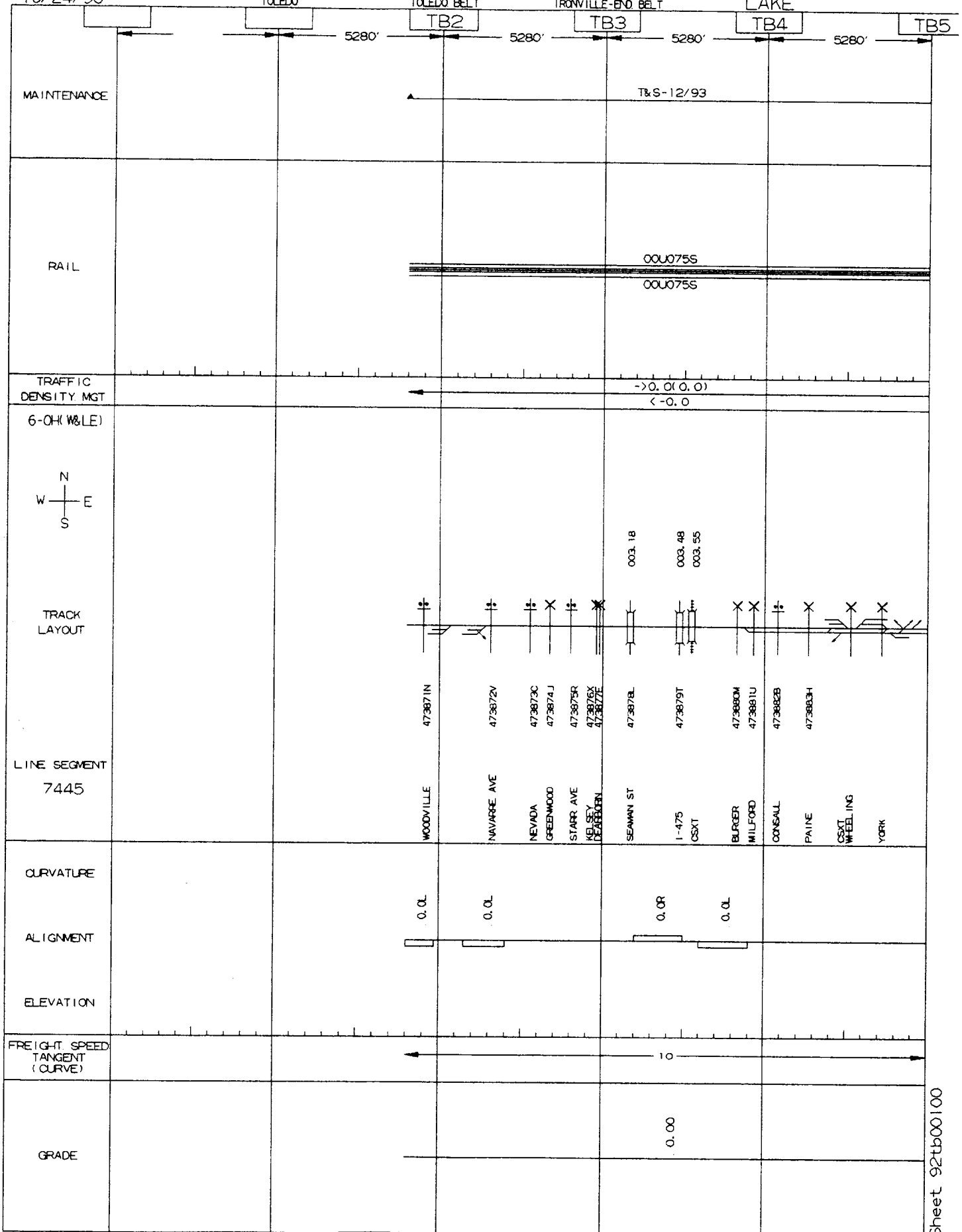
COLUMBUS-SANDUSKY

LAKE

	S110	S111				
	5280'	5175'				
MAINTENANCE	T&S- N/A	T&S-12/01				
RAIL	U075S U075S	00U075S 00U075S				
TRAFFIC DENSITY MGT	->0.1 (0.2) <-0.1					
18-OH (T&OR)						
<div data-bbox="194 871 284 961"> <p>S E — W N</p> </div>						
TRACK LAYOUT						
LINE SEGMENT 7050						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)						
GRADE	0.0					

250

10/24/96



10/24/96

TOLEDO

TOLEDO BELT

IRONVILLE-END BELT

LAKE

TB5

5280'

MAINTENANCE

RAIL

00U075S  
00U075S

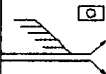
TRAFFIC  
DENSITY. MGT

IRONVILLE

N  
W — E  
S

TB-5, 37=OS-3, 69

TRACK  
LAYOUT



LINE SEGMENT  
7445

TB-5, 37=T-5, 37

CURVATURE

ALIGNMENT

ELEVATION

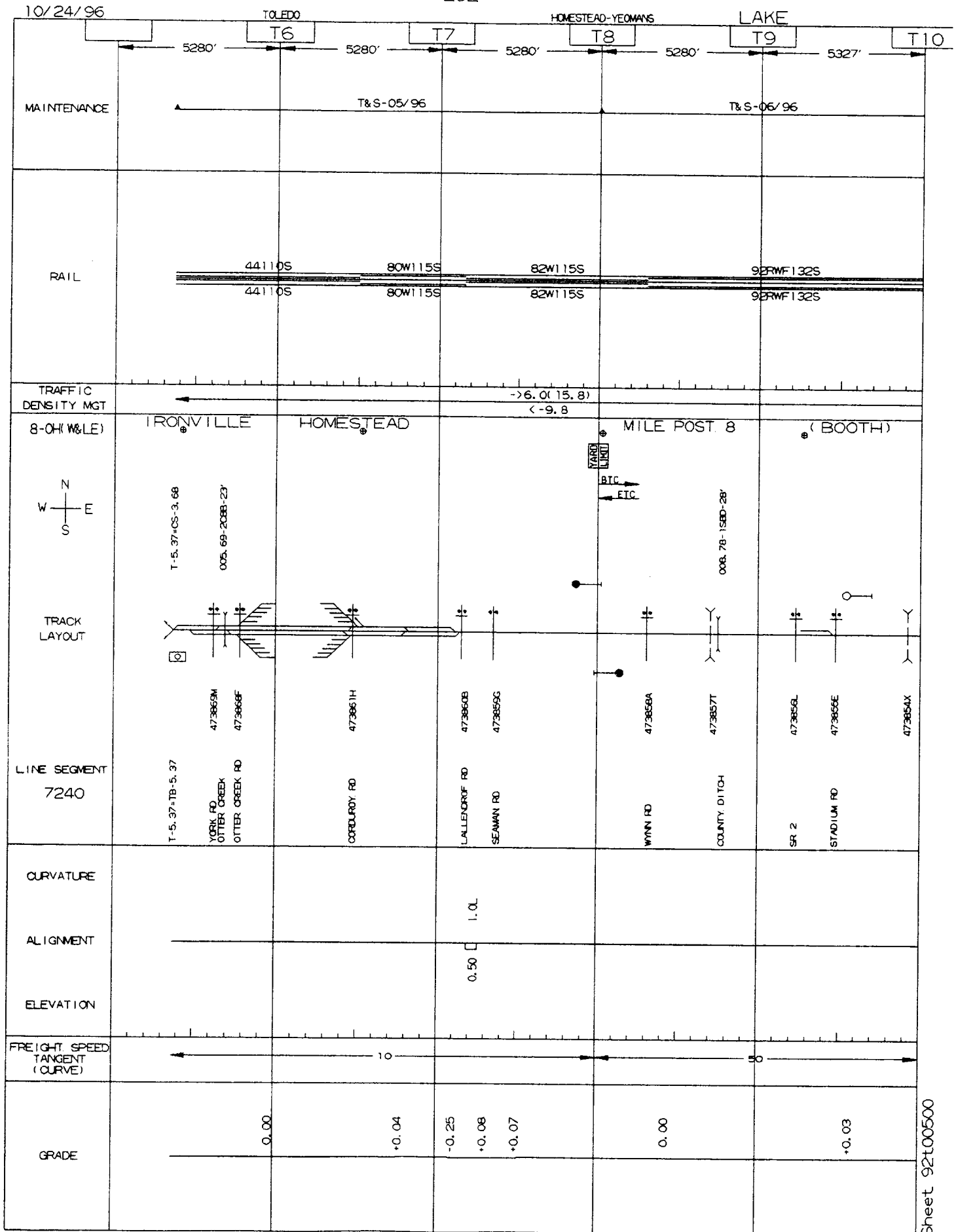
FREIGHT SPEED  
TANGENT  
(CURVE)

10

GRADE

0.00



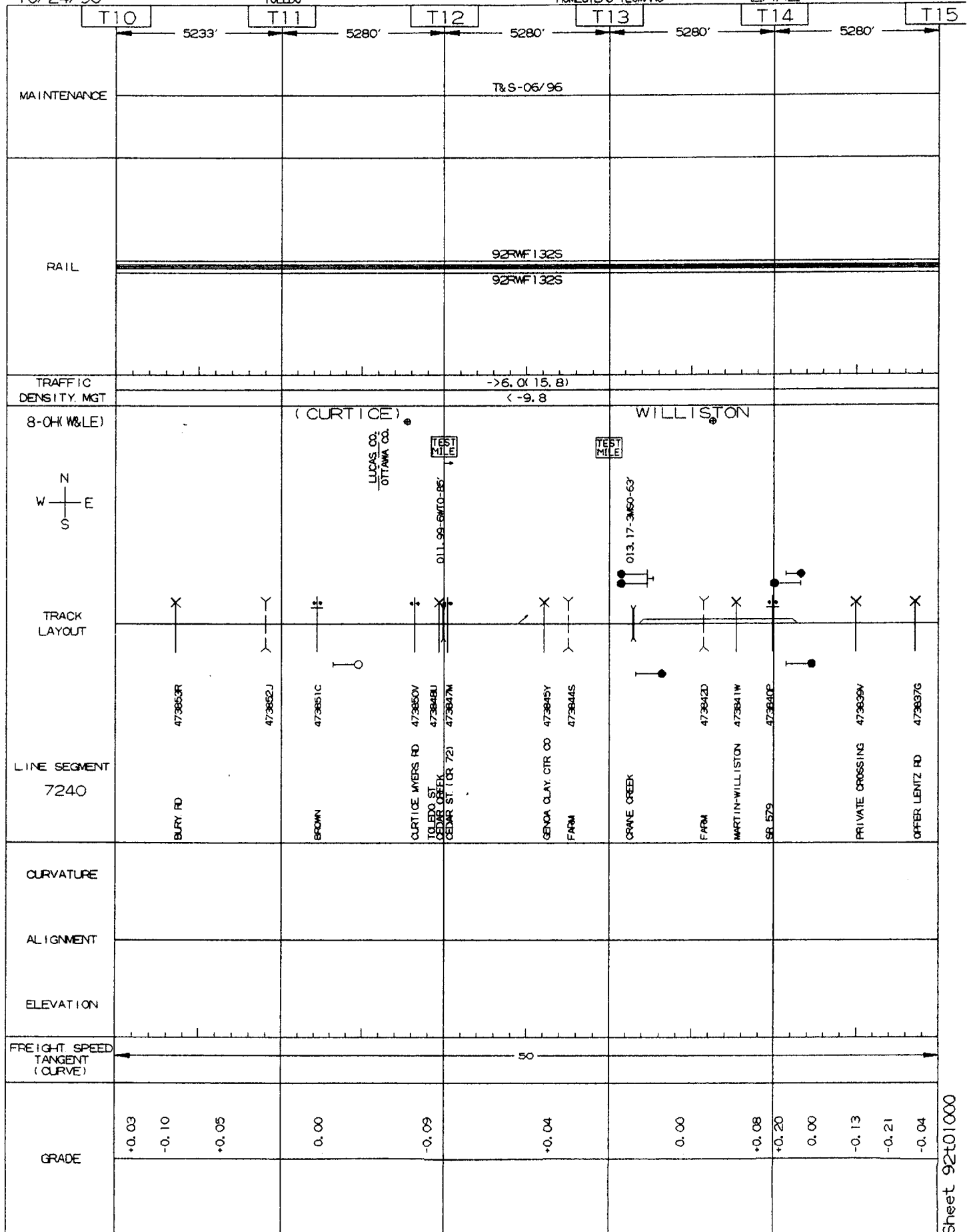


10/24/96

TOLEDO

HOMESTEAD-YEOMANS

LAKE

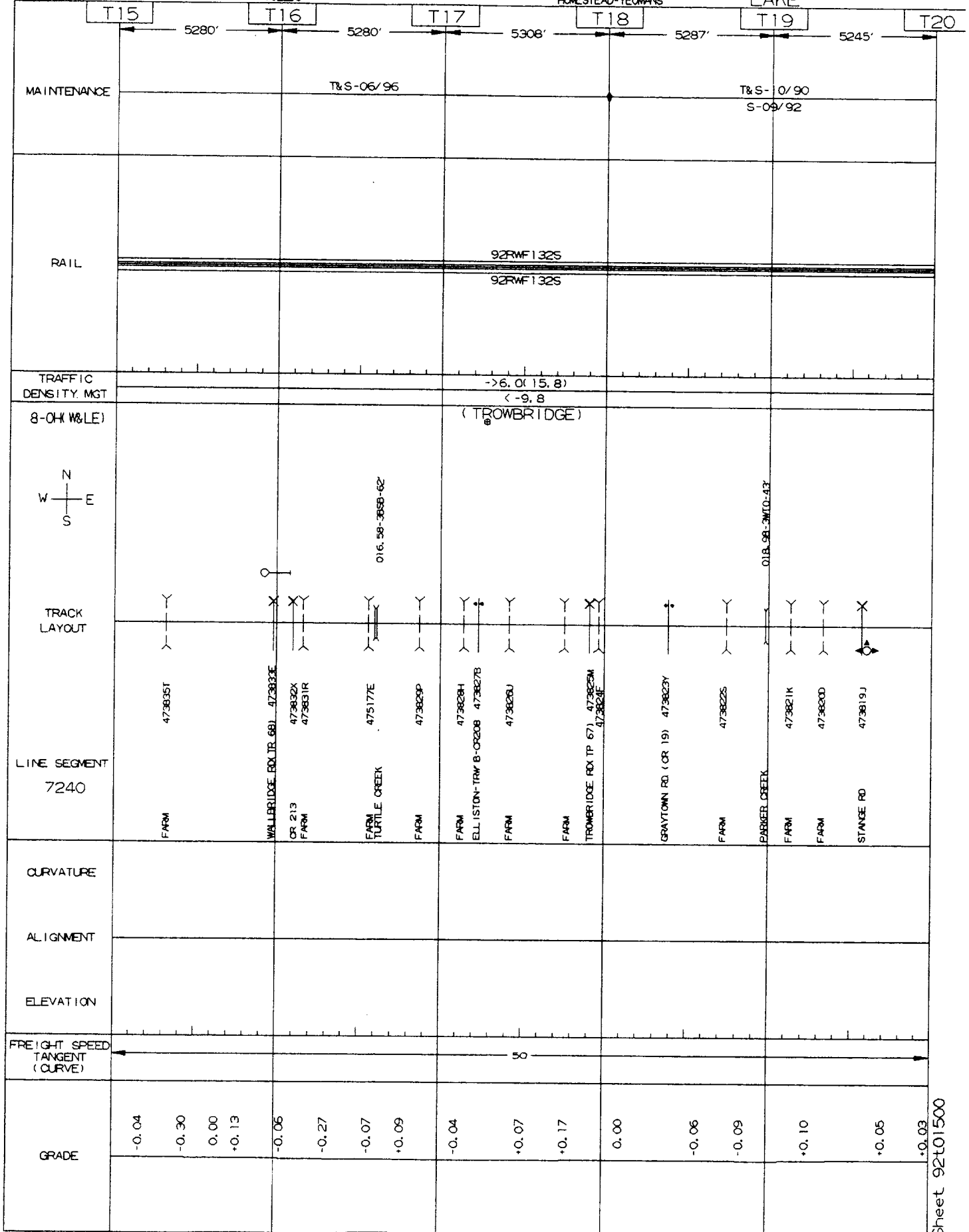


10/24/96

TOLEDO

HOMESTEAD-YEOMANS

LAKE



10/24/96

TOLEDO

HOMESTEAD-YEOMANS

LAKE

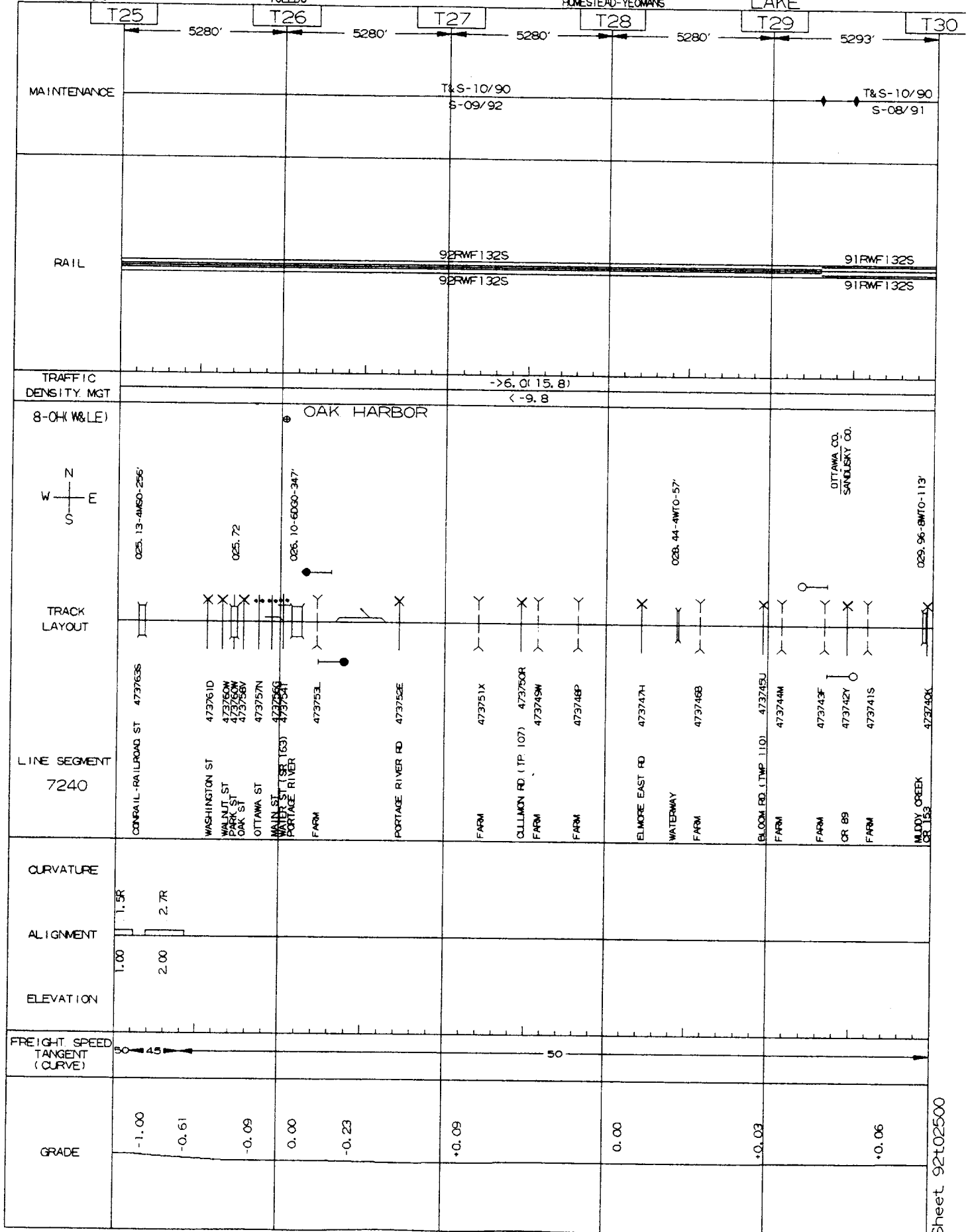
	T20	T21	T22	T23	T24	T25
	5280'	5280'	5280'	5280'	5280'	5280'
MAINTENANCE			T&S-10/90 S-09/92			
RAIL			92RWF132S 92RWF132S			
TRAFFIC DENSITY MGT			->6.0 (15.8) <-9.8			
8-OH (W&LE)	(TOUSSAINT)	(LIMESTONE)				
W N E S						
TRACK LAYOUT						
LINE SEGMENT 7240	473779N FARM SR 590 473778S	473776T TOUSSAINT RD 473777L FARM ROCKY RIDGE RD (OR 75) 473774E TOUSSAINT CREEK 473773X FARM LICKERT HARDER-TR 22 473772R	473771J FARM	473769H BENTON-CARROLL (OR 23) 473767U FARM 473765U FARM SALEM-CARROLL (OR 98) 473765M TOUSSAINT-PORT. (TP: 92) 473765F FARM		
CURVATURE						
ALIGNMENT						
ELEVATION						2.50 2.00 1.00
FREIGHT SPEED TANGENT (CURVE)			50			
GRADE	+0.03 +0.15 -0.43	-0.06	+0.13 0.00	+0.06 -0.05	0.00	+0.61

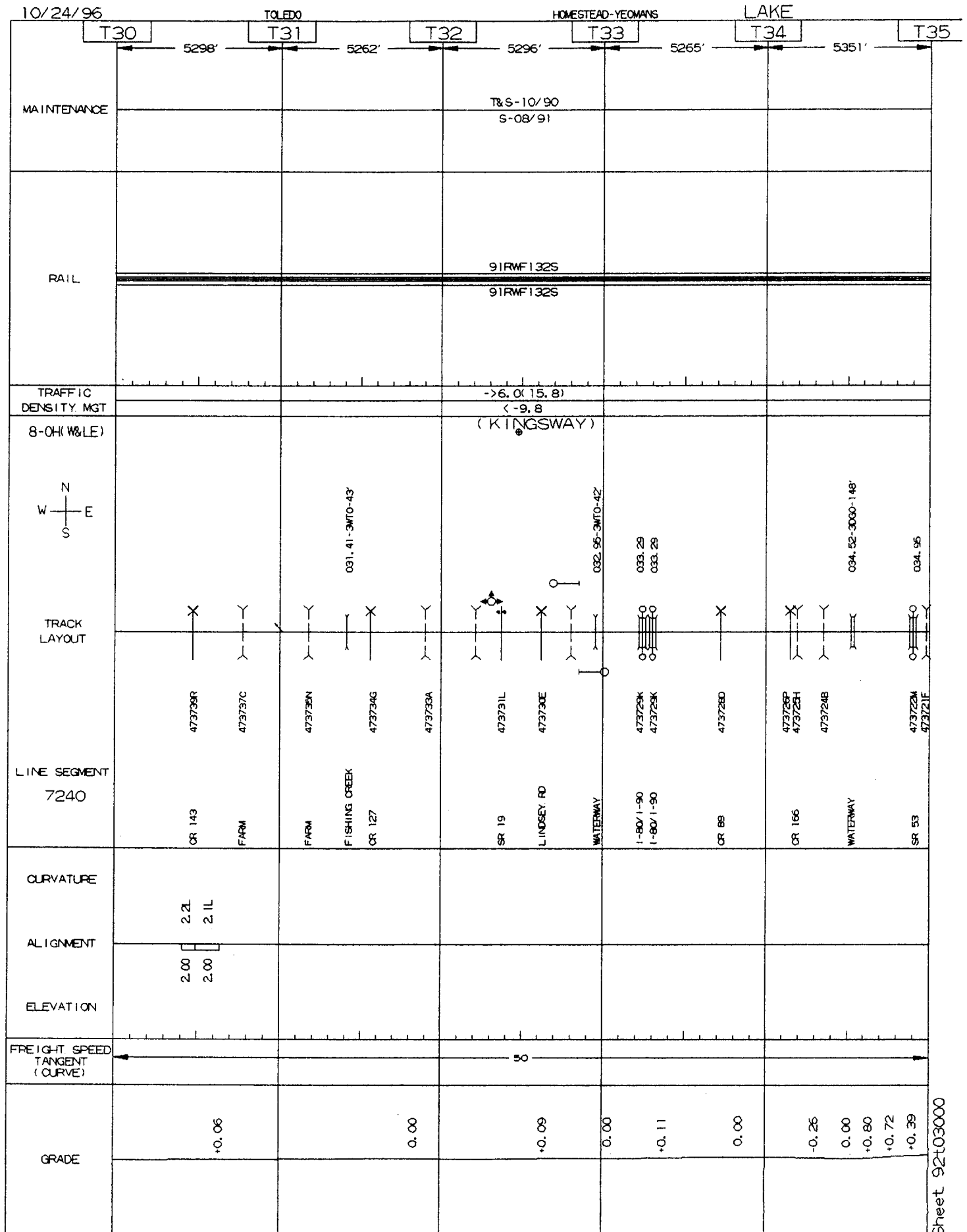
10/24/96

TOLEDO

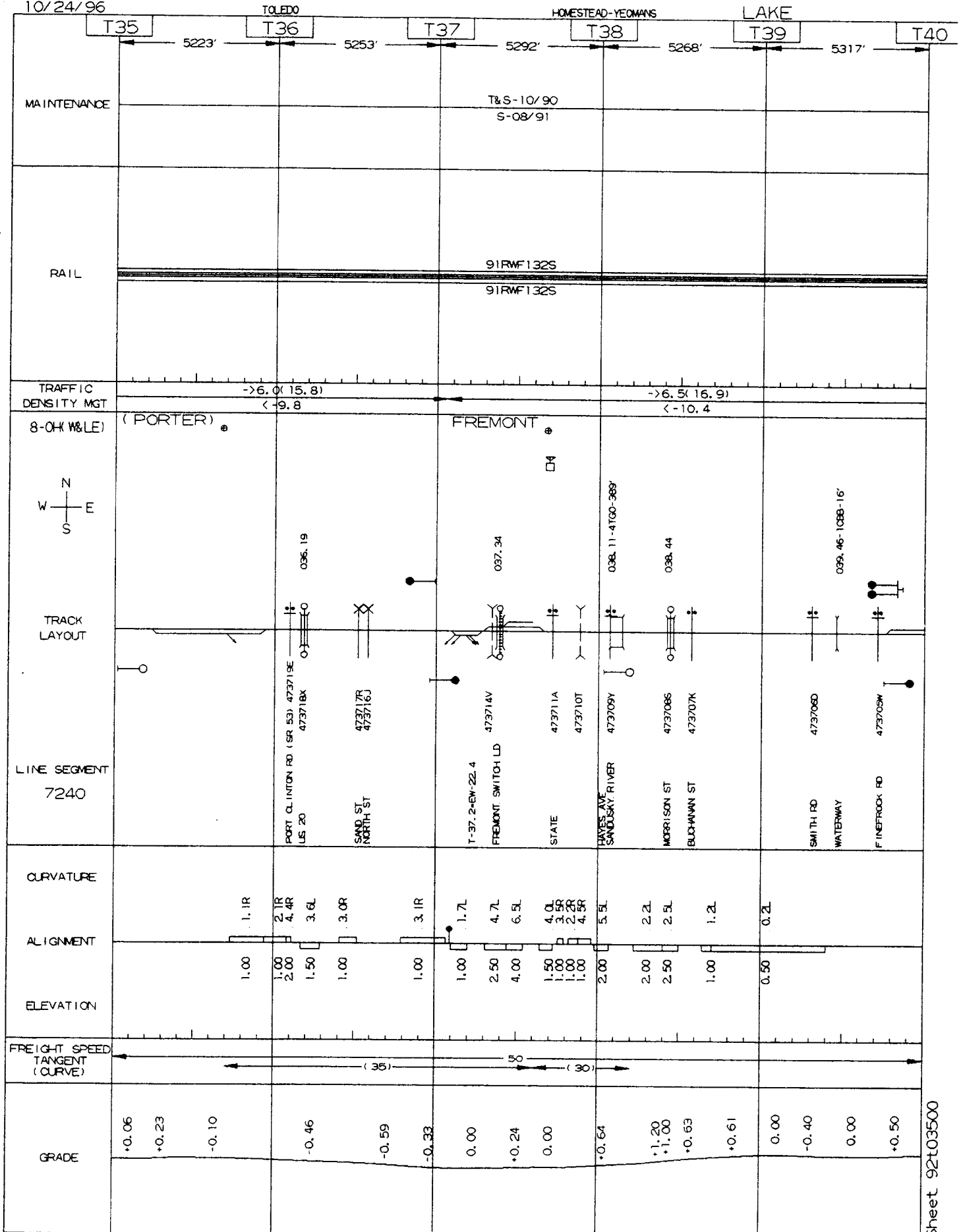
HOMESTEAD-YEOMANS

LAKE





10/24/96

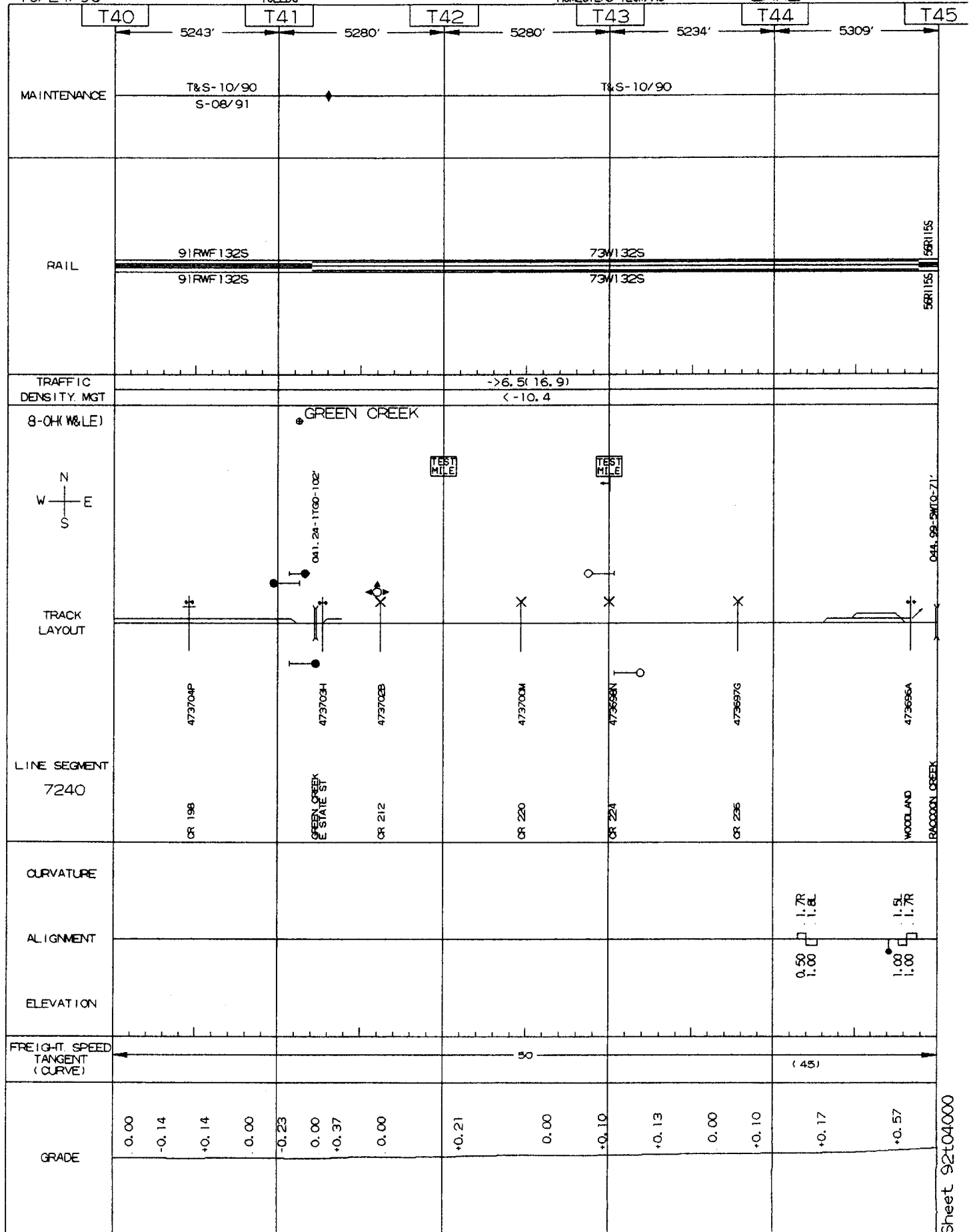


10/24/96

TOLEDO

HOMESTEAD-YEOMANS

LAKE



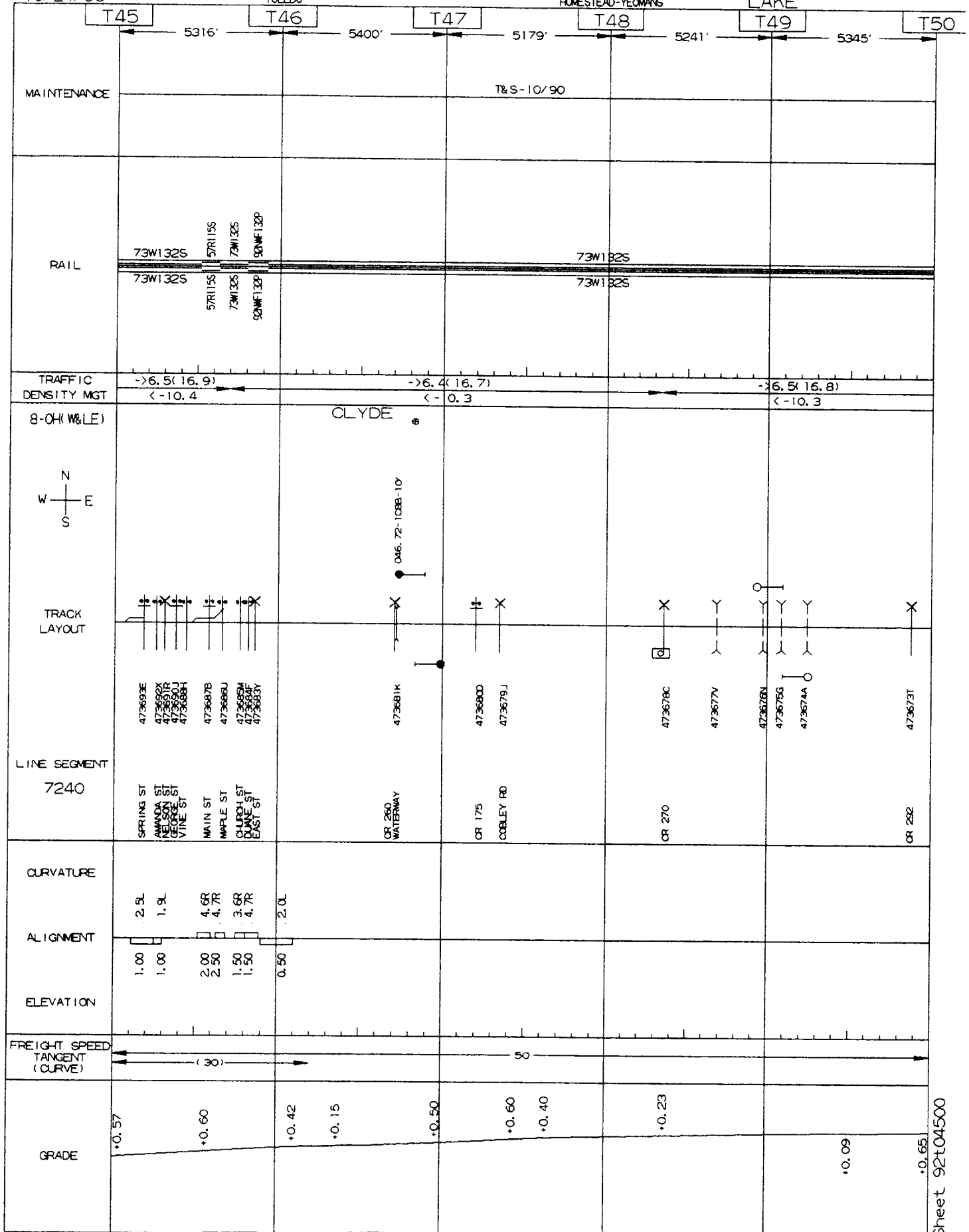


10/24/96

TOLEDO

HOMESTEAD-YEOMANS

LAKE



Sheet 92t05000

10/24/96

LIMA

ARCADIA-LIMA

LAKE

SP49

SP50

MAINTENANCE

5280' 5274'

T&S-09/89

RAIL

74W115S

74W115S

TRAFFIC  
DENSITY MGT

>0.6 (1.7)  
<-1.1

9-0H (NY&SL)

DA (ARCADIA)

S  
E—W  
N

RTC  
ETC

SP-48.39-B-285.88

TRACK  
LAYOUT

476778S

476779Y

476780T

476781A

LINE SEGMENT  
7770

JOSLYN ST

MAIN ST

CR 254

CR 216

CURVATURE

ALIGNMENT

ELEVATION

2.5R

1.0R

1.1L

FREIGHT SPEED  
TANGENT  
(CURVE)

35 49

GRADE

-0.40

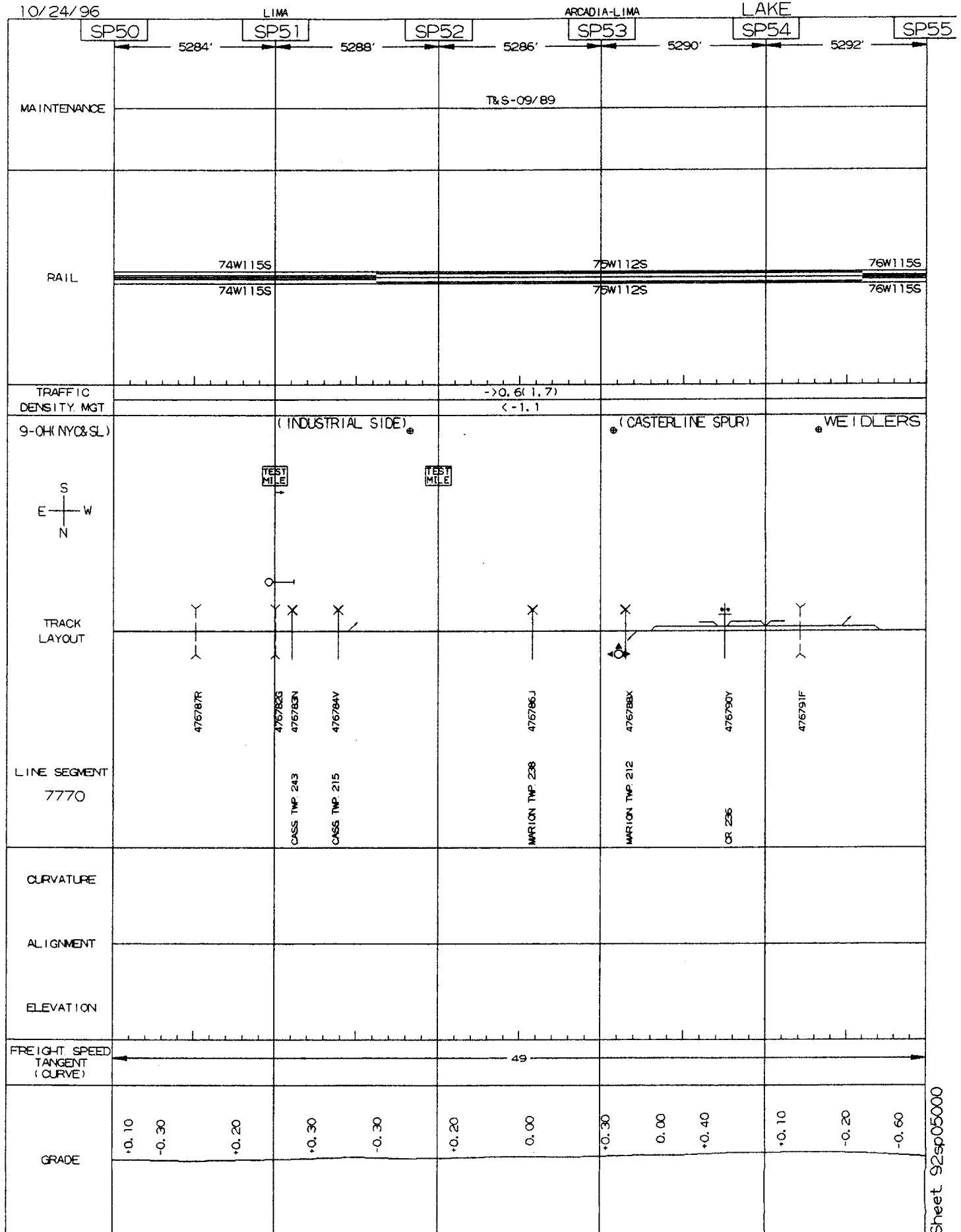
+0.50

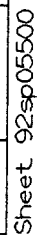
-0.30

+0.50

+0.10

10/24/96



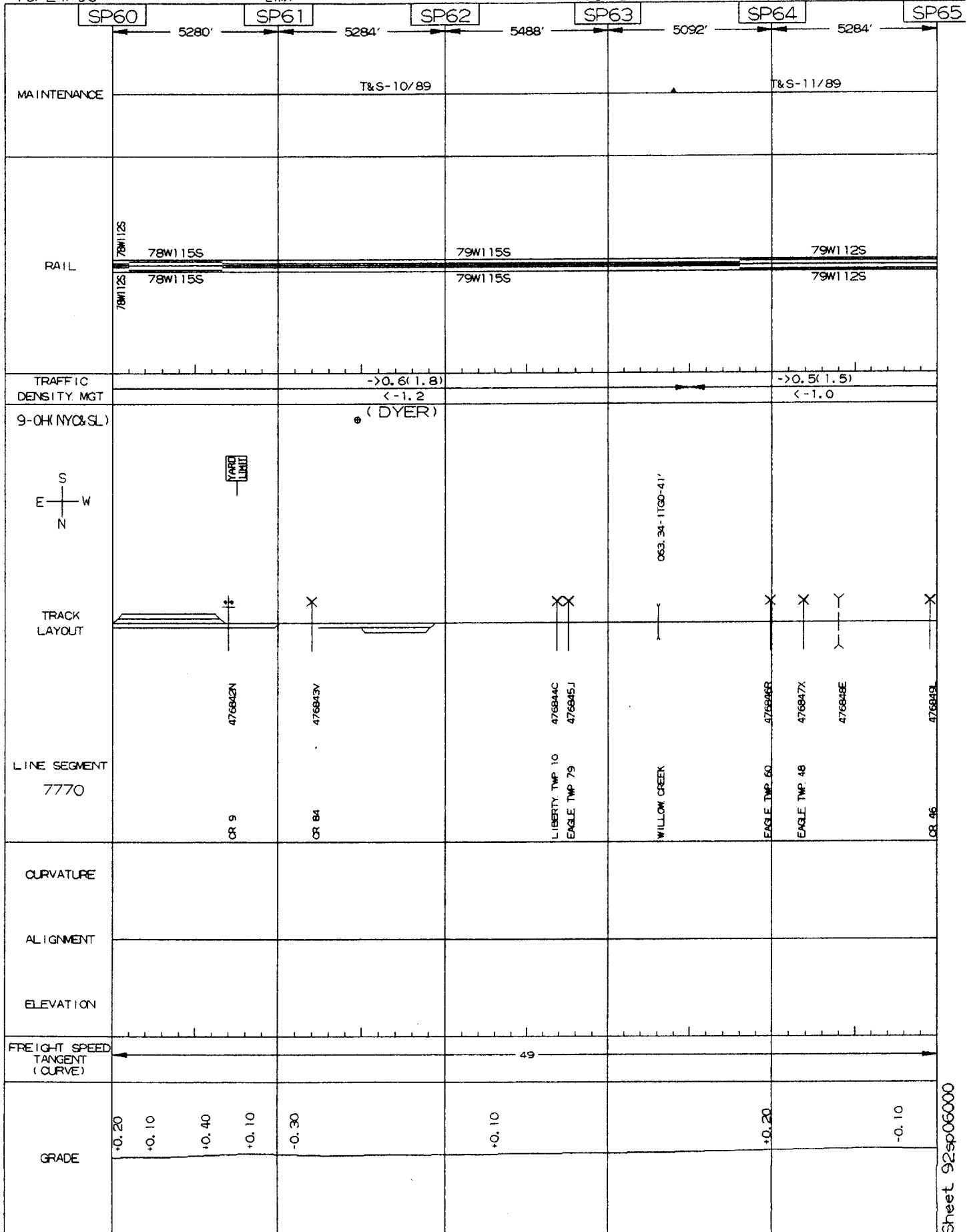


10/24/96

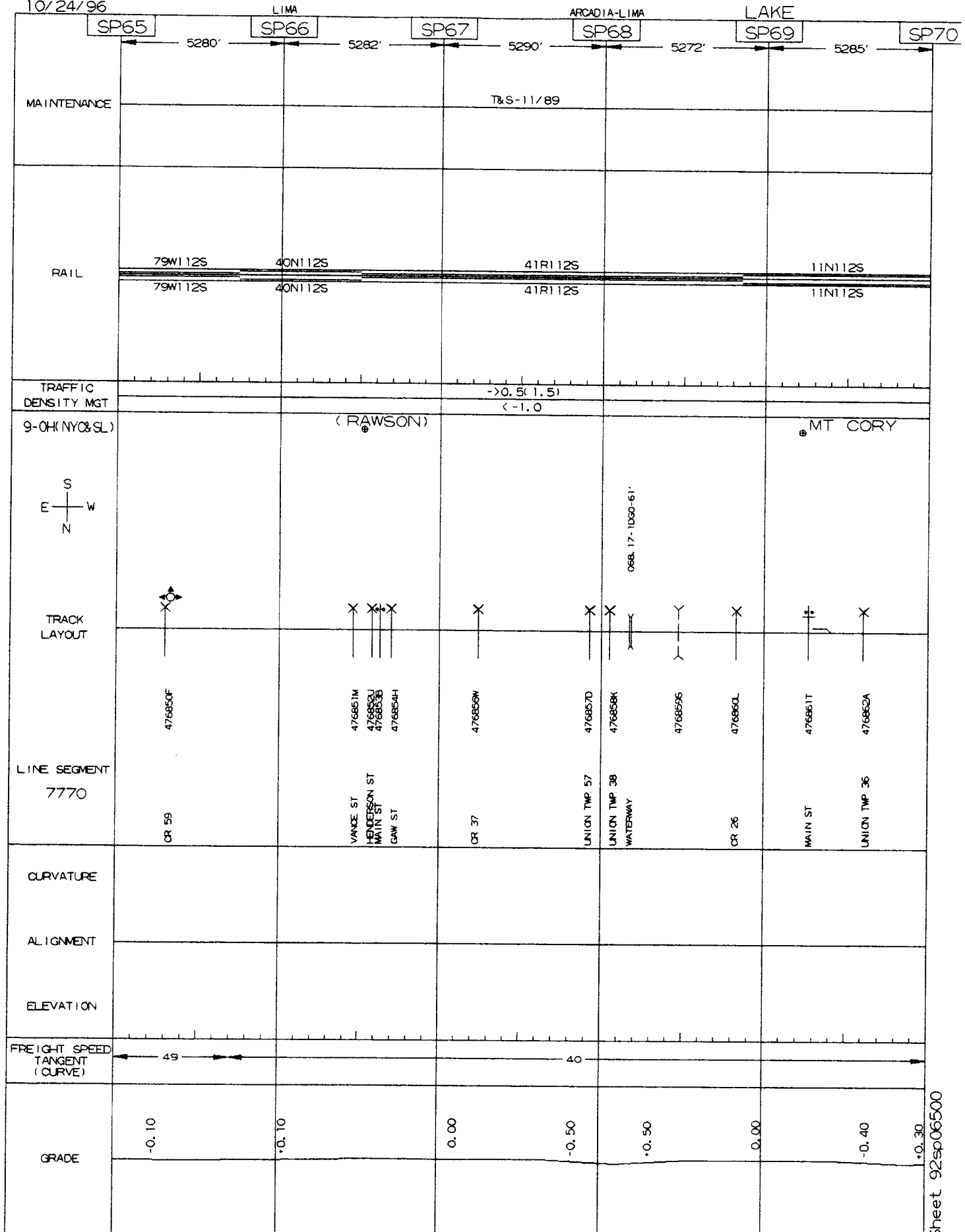
LIMA

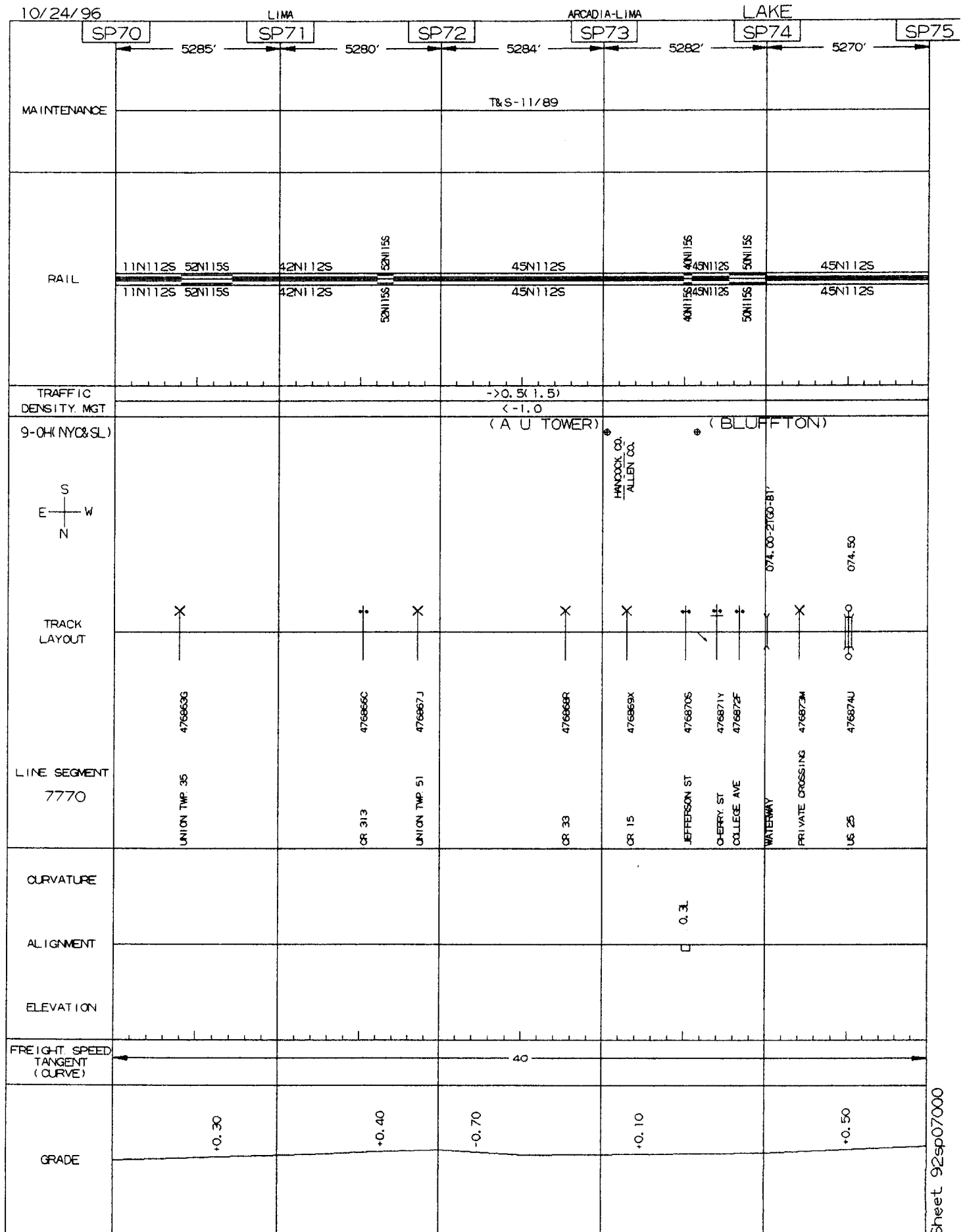
ARCADIA-LIMA

LAKE



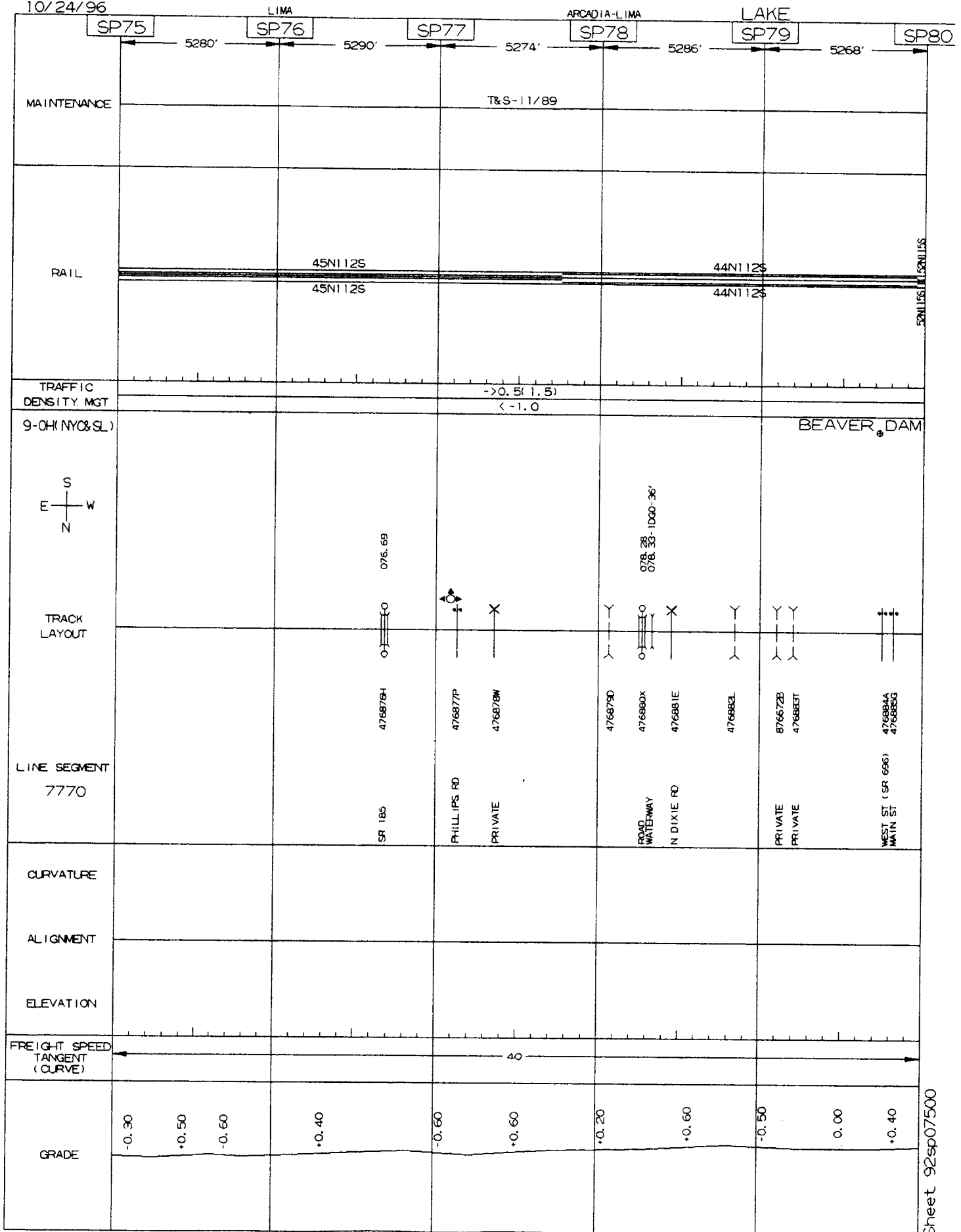
10/24/96

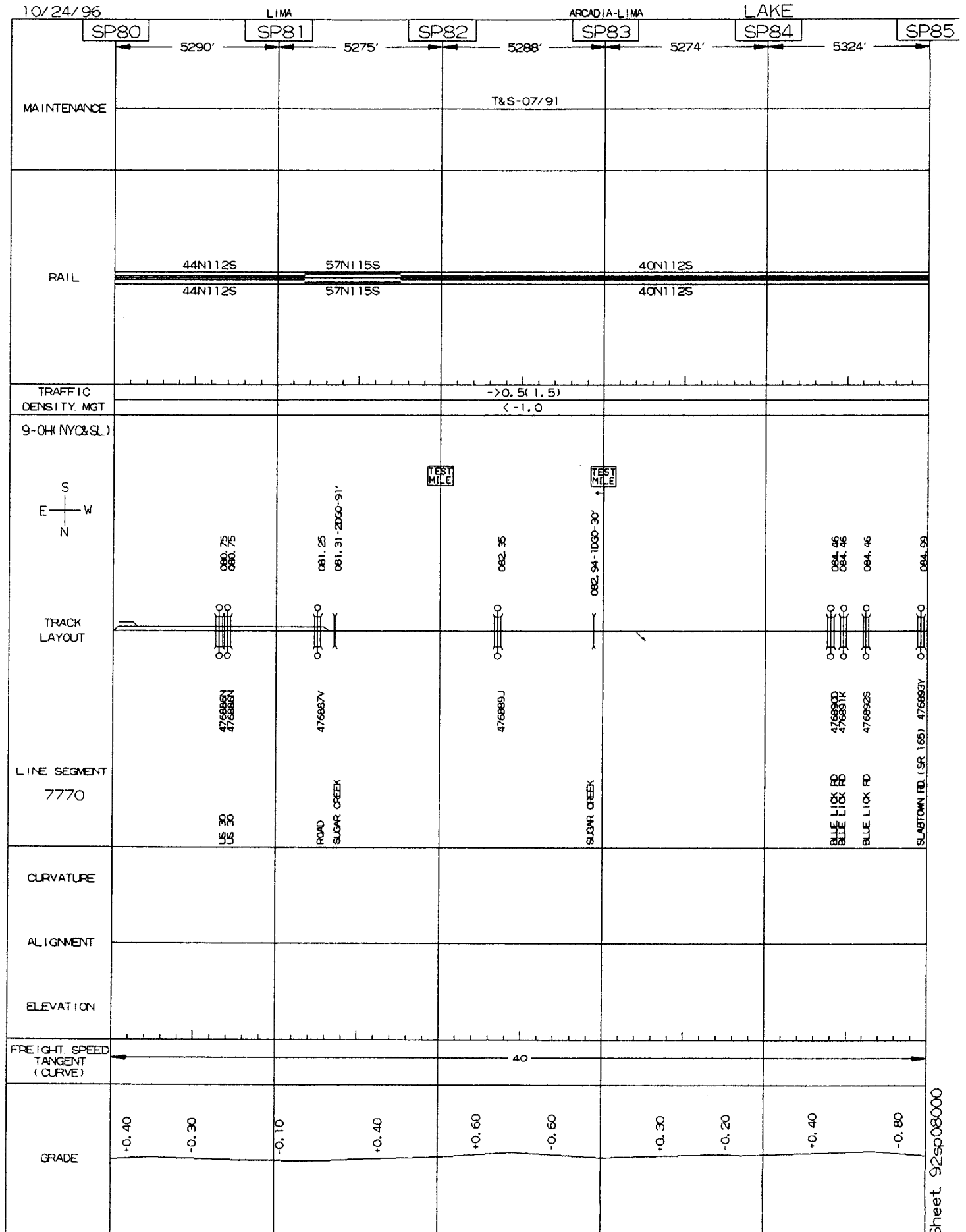




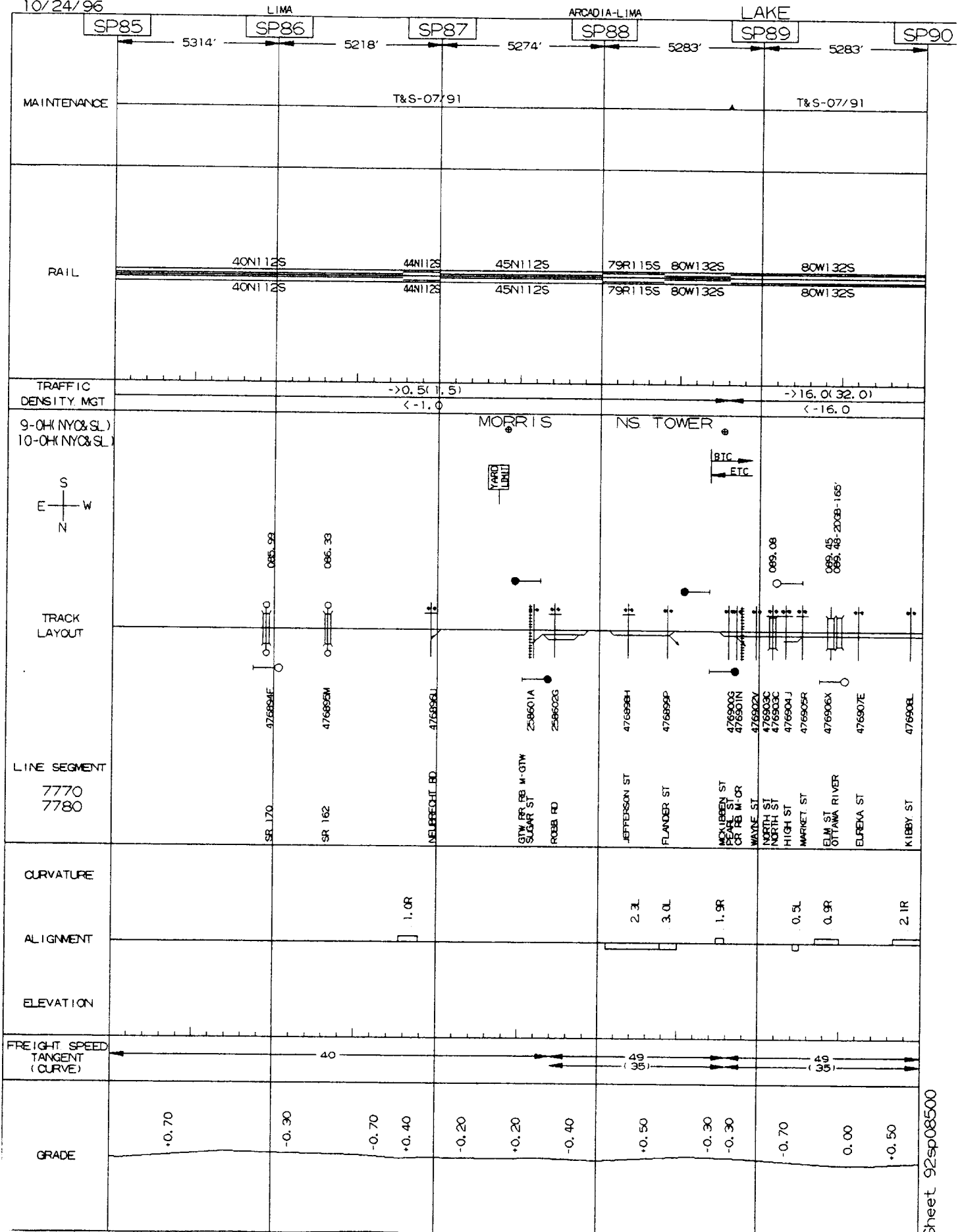


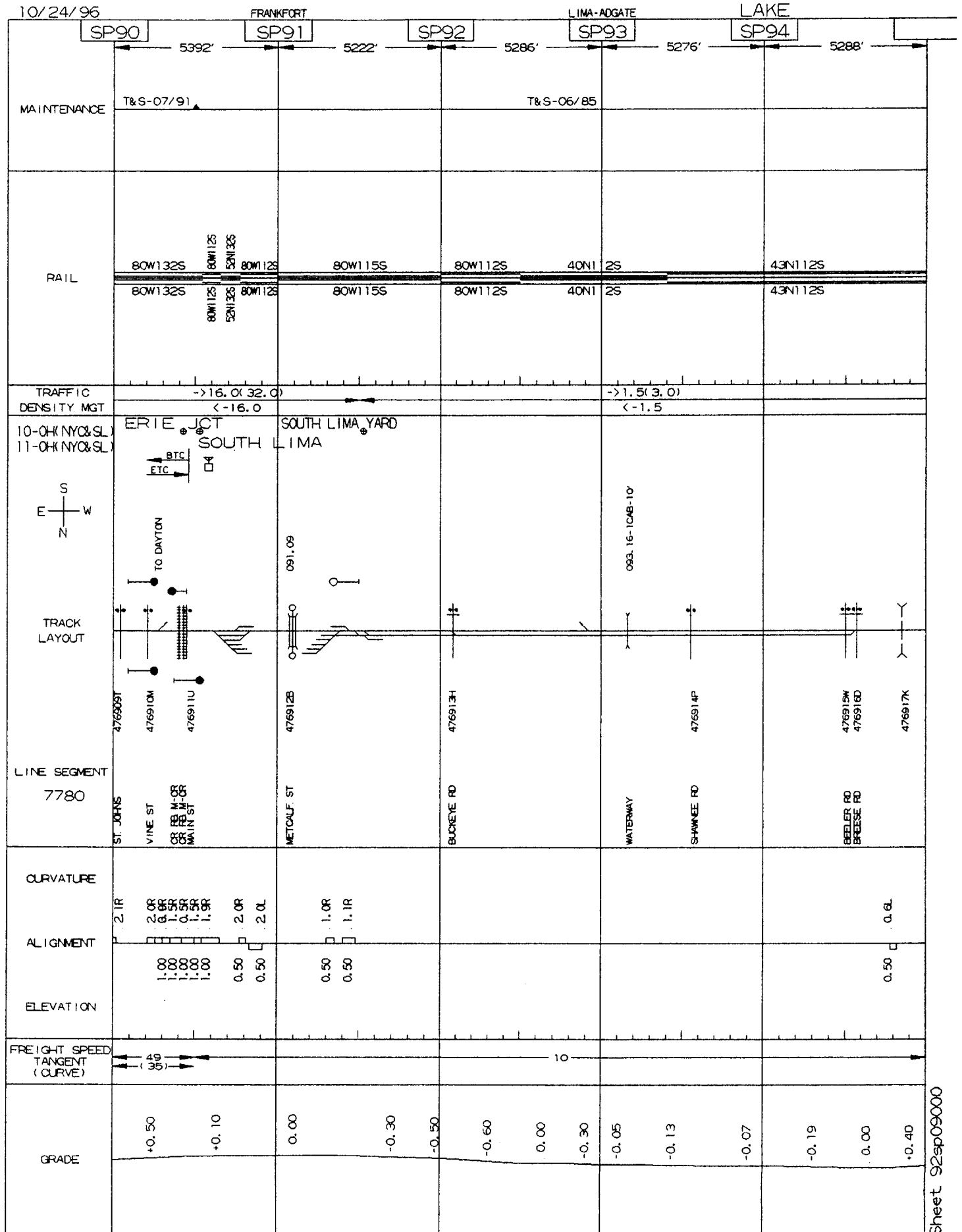
10/24/96

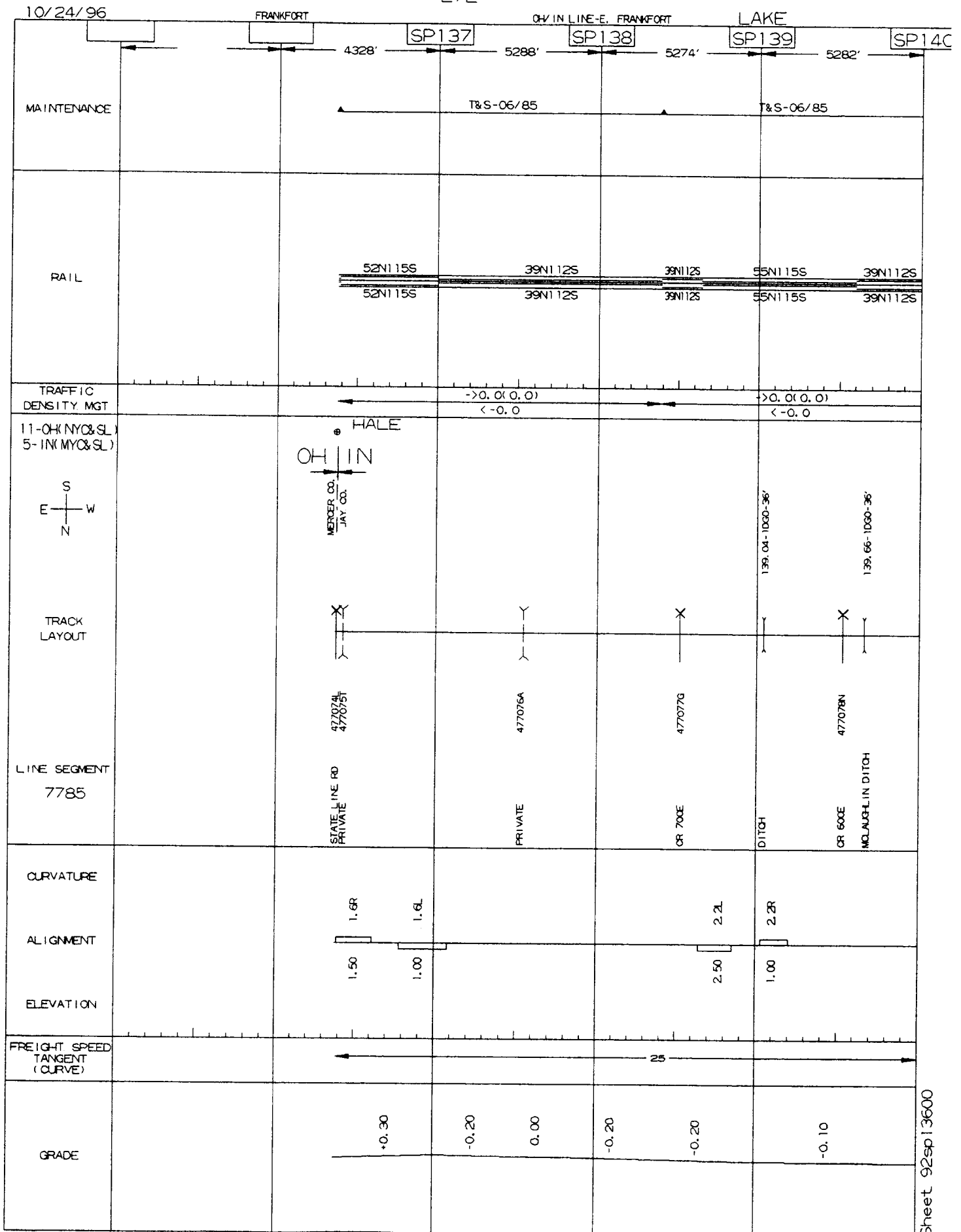




10/24/96





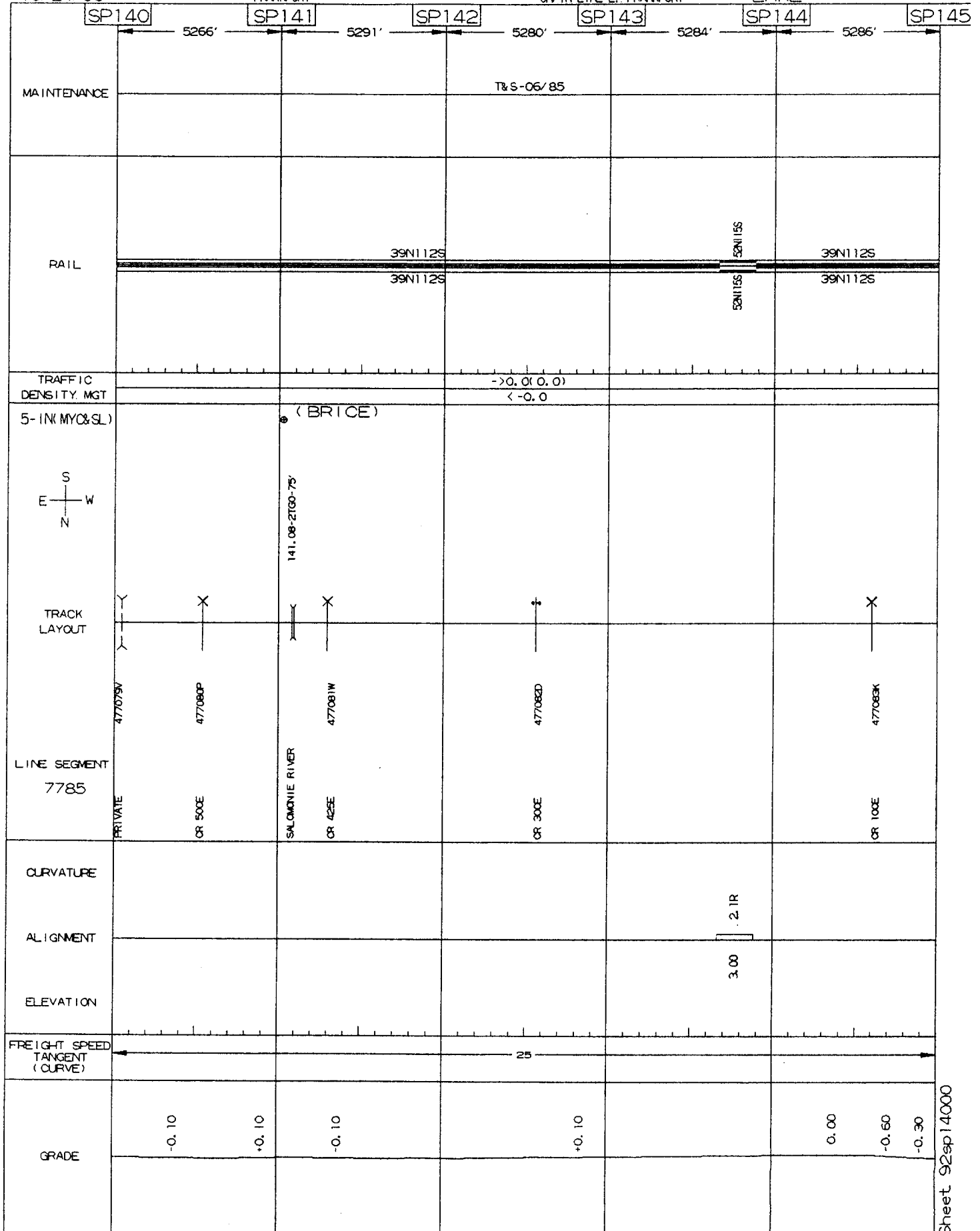


10/24/96

FRANKFORT

ON IN LINE-E. FRANKFORT

LAKE

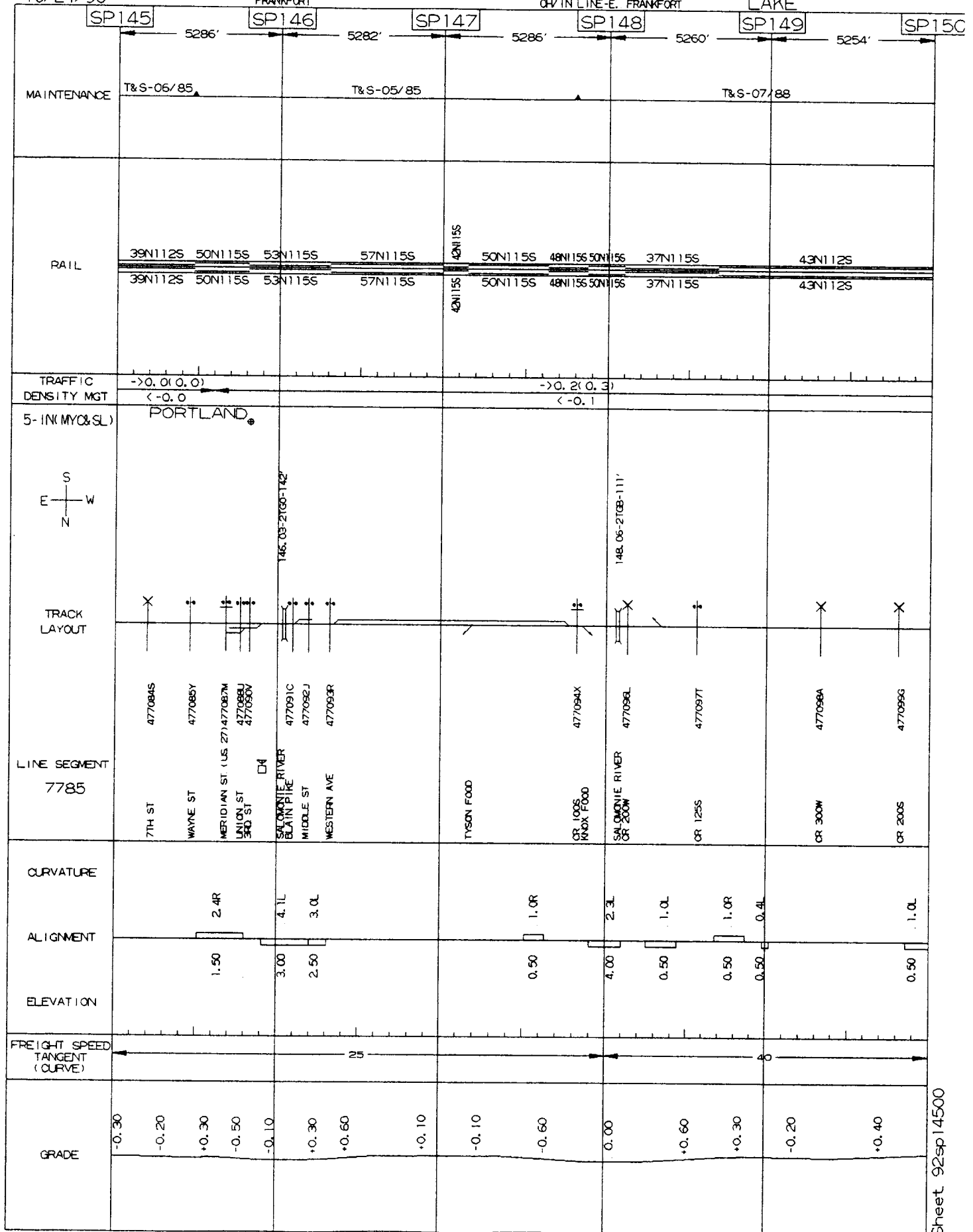


10/24/96

FRANKFORT

CH IN LINE-E. FRANKFORT

LAKE



10/24/96

FRANKFORT

OH/IN LINE-E. FRANKFORT

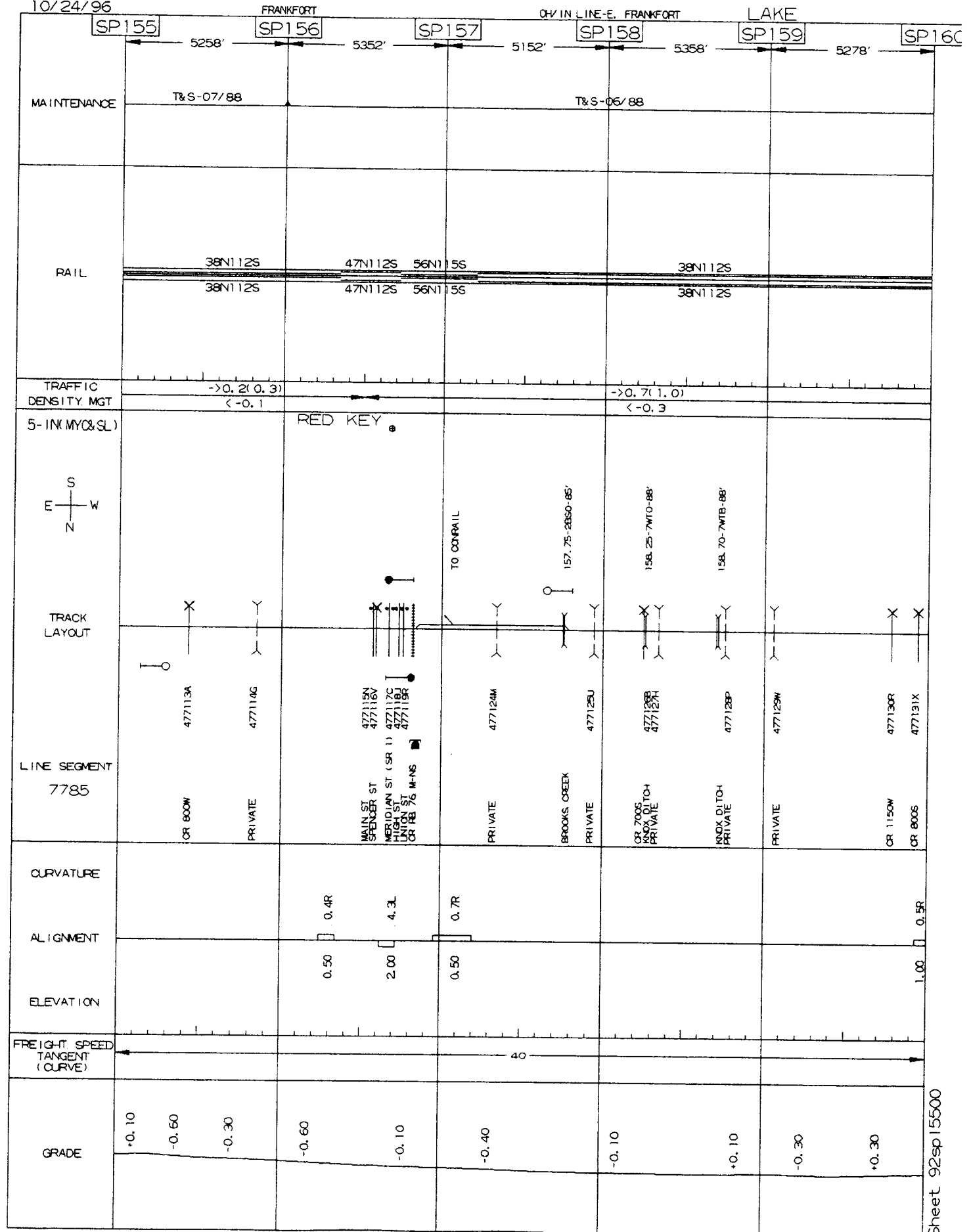
LAKE

	SP150	SP151	SP152	SP153	SP154	SP155
	5324'	5276'	5292'	5284'	5286'	
MAINTENANCE	T&S-07/88					
RAIL	43N112S		38N112S			
	43N112S		38N112S			
TRAFFIC DENSITY MGT	-> 0.2 (0.3) < -0.1					
5-IN (MYO&SL)						
TRACK LAYOUT	152 03-1008-64'					
LINE SEGMENT	477100Y PRIVATE	477101F CR 400W	477102M PRIVATE	477103J CR 500W	477104B CR 360S	477105H COMO RD
	477106P CR 400S	477107W PRIVATE	477108K PRIVATE	477110E CR 700W	477112T CR 500S	
CURVATURE	1.0L	1.0L	0.4R	1.1R		
ALIGNMENT	0.50	0.50	0.50	1.00		
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	40					
GRADE	-0.10 +0.20	-0.10 +0.40 -0.30	+0.50	0.00 +0.40	+0.30	+0.10

Sheet 92sp15000



10/24/96

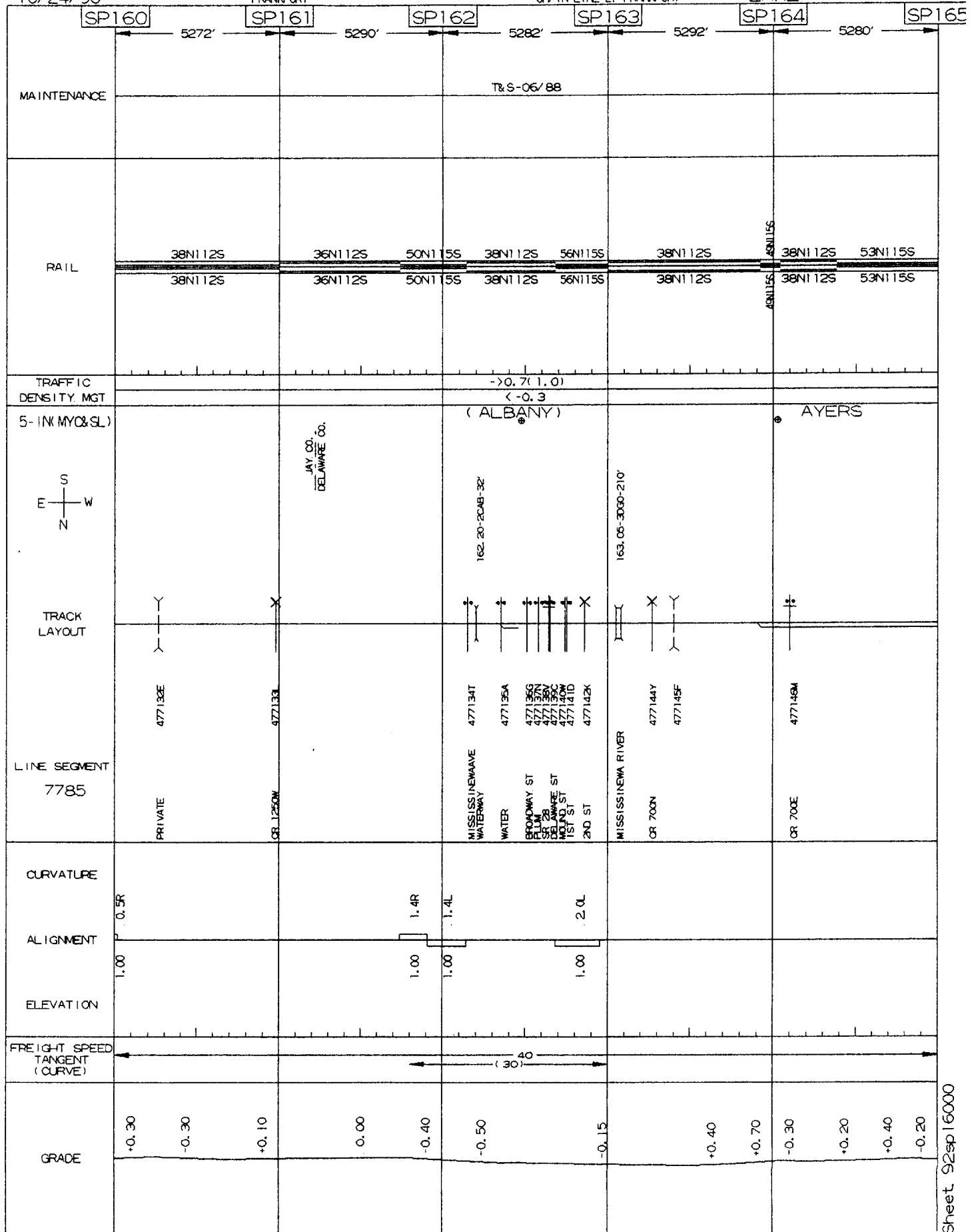


10/24/96

FRANKFORT

OH/IN LINE-E. FRANKFORT

LAKE



10/24/96

FRANKFORT

CH IN LINE-E. FRANKFORT

LAKE

	SP165	SP166	SP167	SP168	SP169	SP170
	5280'	5280'	5280'	5278'	5282'	
MAINTENANCE			T&S-06/88			
RAIL	53N115S 53N115S	37N112S 37N112S	53N115S 53N115S		38N112S 38N112S	
TRAFFIC DENSITY MGT			-> 0.7 (1.0) < -0.3			
5-IN (MY&SL)			(DE SOTO)			
<div style="text-align: center;">           S            E — W            N         </div>						
TRACK LAYOUT						
LINE SEGMENT	477147J OR 650E	477148B OR 500N	477150C OR 400N	477152R MAIN ST (OR 500E) OR 350E	477153X OR 350E	477154E OR 400E
CURVATURE						
ALIGNMENT		1.0R				
ELEVATION		1.00				
FREIGHT SPEED TANGENT (CURVE)			40			
GRADE	+0.10 +0.20	+0.10 +0.20 -0.40	+0.60 -0.50 +0.50	-0.20 +0.20	0.00 +0.50	

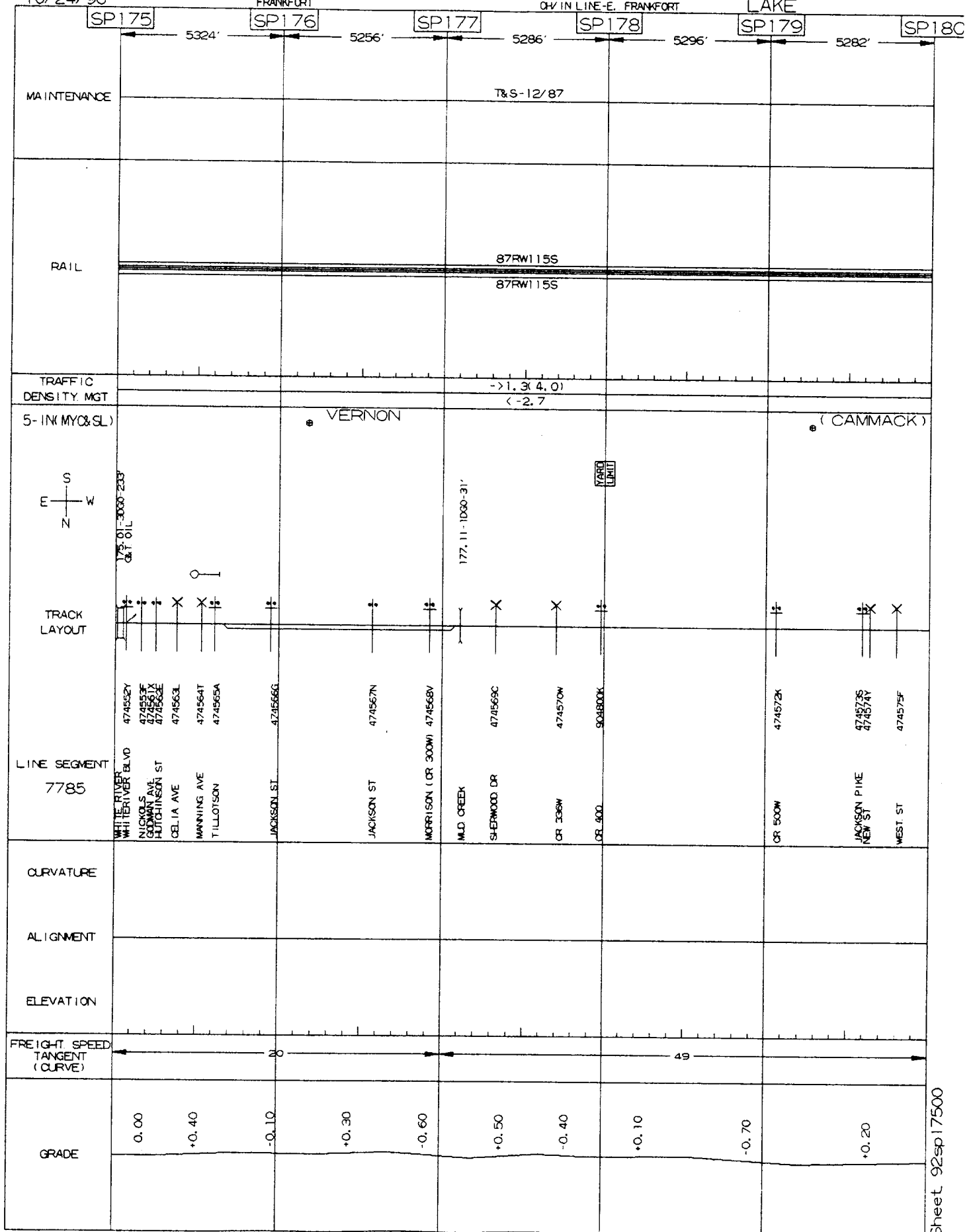
Sheet 92sp17000

10/24/96

FRANKFORT

CH/IN LINE-E. FRANKFORT

LAKE

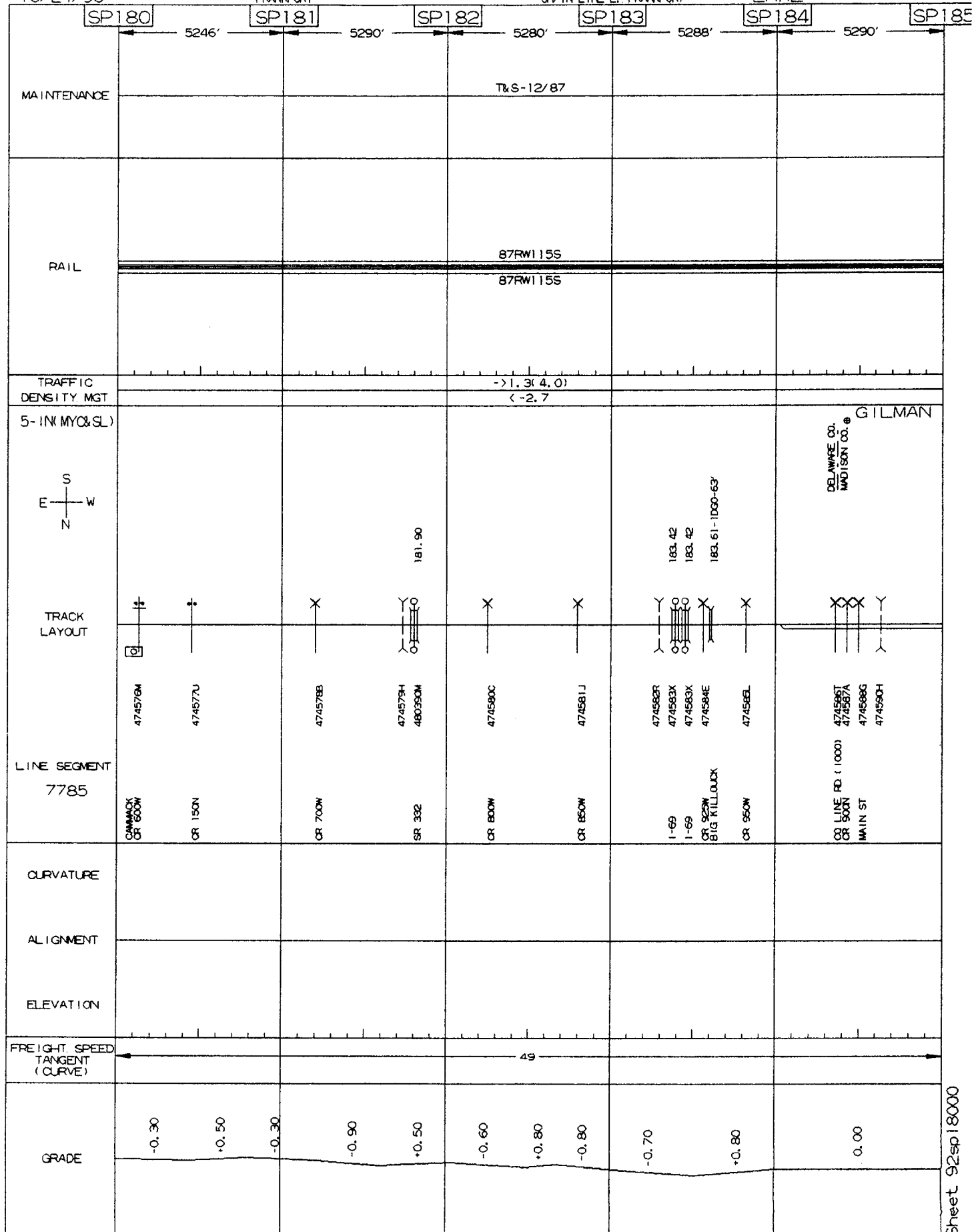


10/24/96

FRANKFORT

OH/IN LINE-E. FRANKFORT

LAKE



Sheet 92sp18500

10/24/96

FRANKFORT

ON/IN LINE-E. FRANKFORT

LAKE

	SP190	SP191	SP192	SP193	SP194	SP195
	5292'	5294'	5284'	5292'	5280'	
MAINTENANCE	T&S-12/87					
RAIL	<div>87RW115S</div> <div>87RW115S</div> <div>87R115S</div> <div>87R115S</div>					
TRAFFIC DENSITY, MGT	<div>-&gt;1.3(4.0)</div> <div>&lt;-2.7</div>					
5-IN MYO&SL)	<div>(ORESTES)</div> <div>(DUNDEE)</div>					
<div>S</div> <div>E—W</div> <div>N</div>						
TRACK LAYOUT	<div>190.88-2100-71'</div> <div>192.22-1148-12'</div>					
LINE SEGMENT	7785					
	BLACK ST CR 16-OR CENTRAL AVE	INDIANA AVE (100W) MID CREEK WASHINGTON ST	VIRGINIA AVE CR 200W	WATERWAY ONTARIO ST SUPERIOR ST (OR 300W)	CR 400W	CR 450W CR 500W
CURVATURE					0.7L	
ALIGNMENT					1.00	
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	49					
GRADE	-0.20 -0.50	+0.10 +0.80	-0.50 +0.20 +0.60	0.00 +0.20	0.00 -0.20	



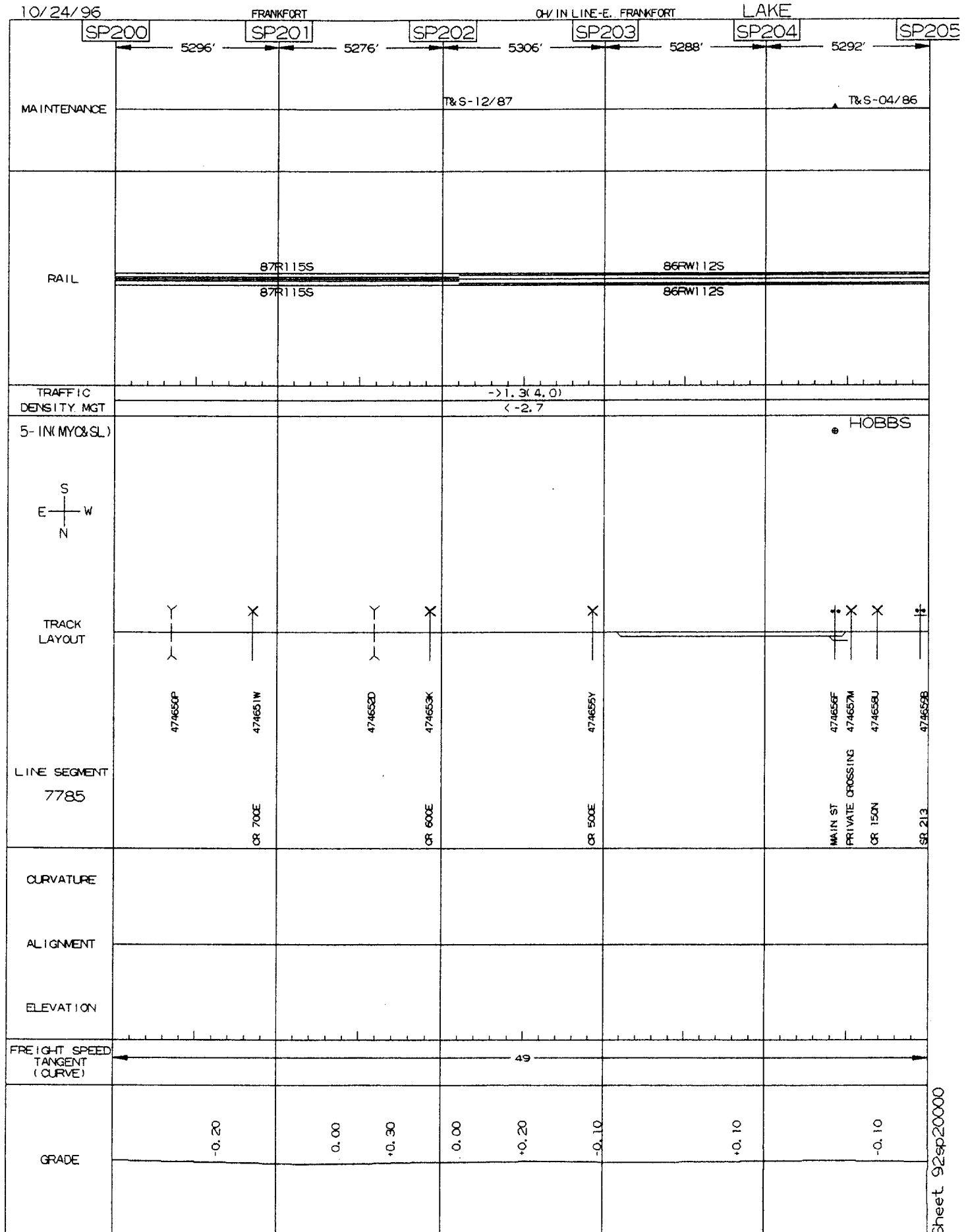
10/24/96

FRANKFORT

OH/IN LINE-E. FRANKFORT

LAKE

	SP195	SP196	SP197	SP198	SP199	SP200
	5292'	5294'	5298'	5291'	5292'	
MAINTENANCE			T&S-12/87			
RAIL			87R115S			
			87R115S			
TRAFFIC DENSITY MGT			-> 1.3 (4.0)			
			< -2.7			
5-IN (MYO&SL)		(WALLACE)		(ELWOOD)		MADISON CO. TIPTON CO.
E S W N						
TRACK LAYOUT	195.13-10AB-12'			RED GIANT	195.06-10B0-51'	
LINE SEGMENT	7785					
	WATERWAY CR 550W	CR 600W	CR 700W	SR 37 BRICK YD RD 30RD ST 31ST ST 28TH ST S 27TH ST 23TH ST 24TH ST 22ND ST	18TH ST ANDERSON ST ALLEY ALLEY ALLEY DOCK CREEK 9TH ST 8TH ST	3RD ST COUNTY LINE RD
CURVATURE						
ALIGNMENT				0.3L	0.4R	
ELEVATION				1.00	1.00	
FREIGHT SPEED TANGENT (CURVE)		49		30	49	
GRADE	-0.20 0.00	-0.30 0.00	-0.40 +0.40	-0.30 +0.10	-0.60 +0.40	-0.20

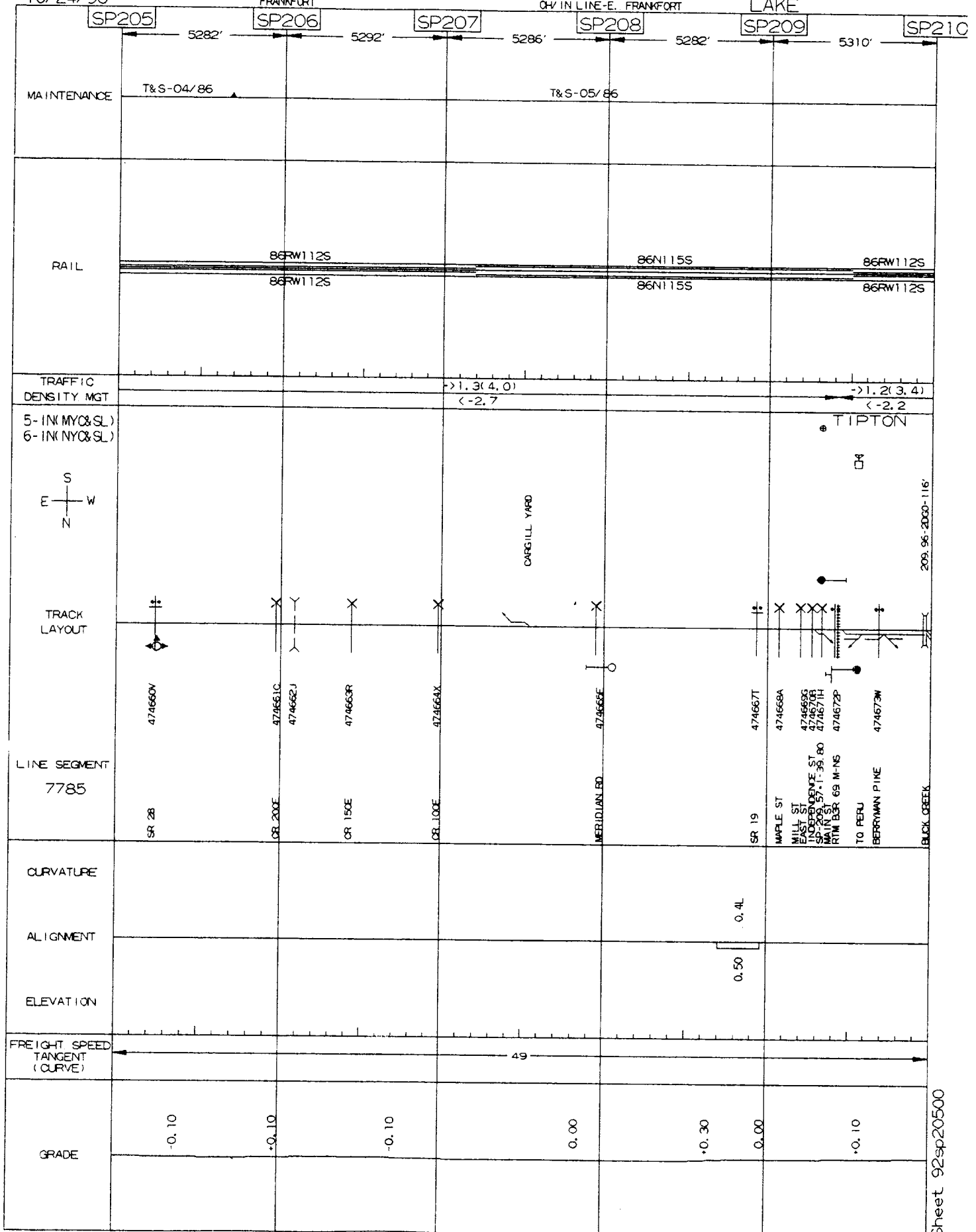


10/24/96

FRANKFORT

CH/ IN LINE-E. FRANKFORT

LAKE



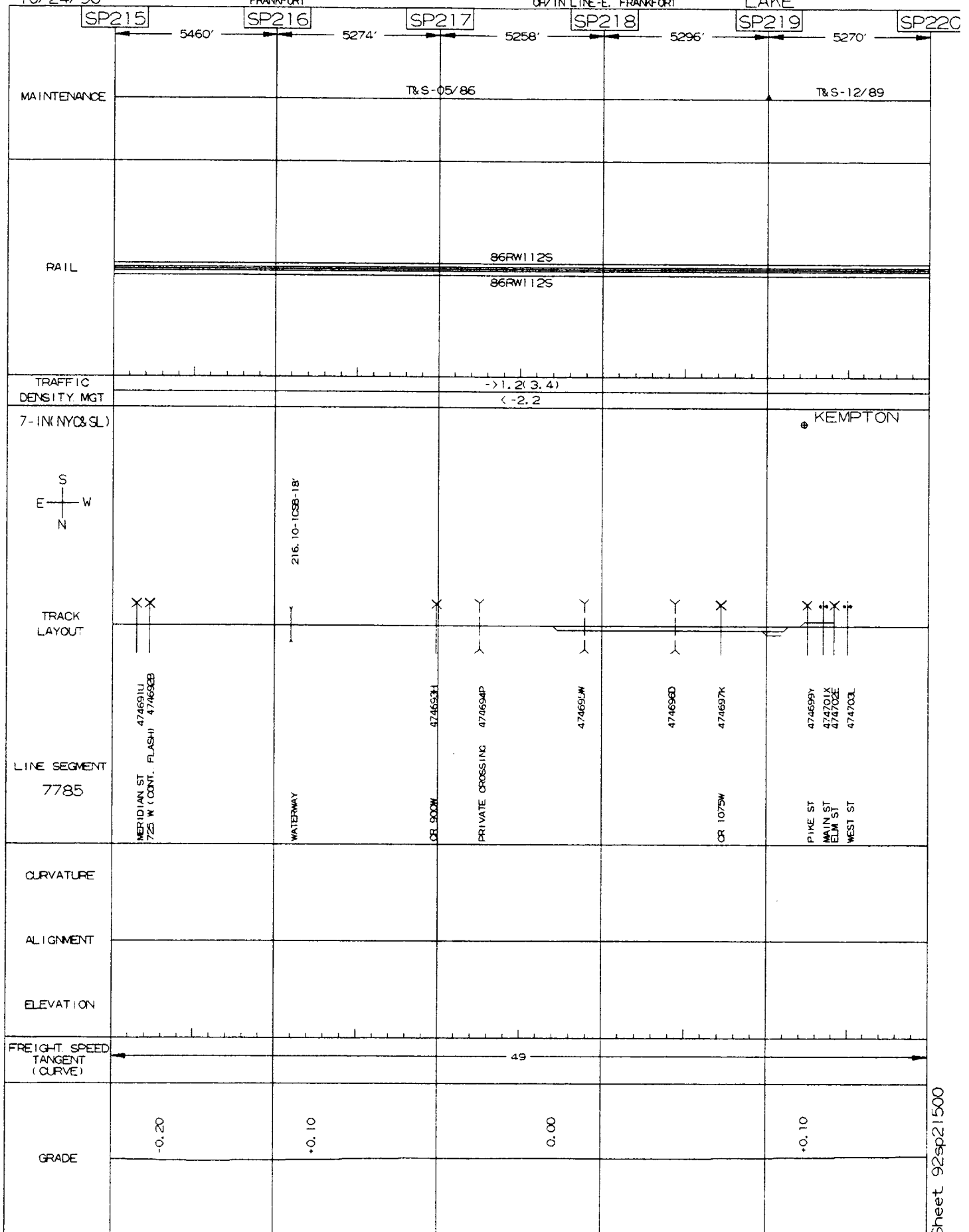
10/24/96

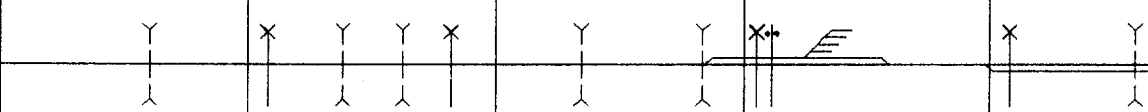
FRANKFORT

ON IN LINE-E. FRANKFORT

LAKE

	SP210	SP211	SP212	SP213	SP214	SP215
	5292'	5260'	5260'	5340'	5056'	
MAINTENANCE			T&S-05/86			
RAIL			86RW112S			
			86RW112S			
TRAFFIC DENSITY MGT			-> 1, 2, 3, 4			
			< -2.2			
6-IN(NY&SL) 7-IN(NY&SL)	ZA SIDING				(GOLDSMITH)	
<div style="text-align: center;">           S            E — W            N         </div>						
TRACK LAYOUT						
LINE SEGMENT						
7785						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			49			
GRADE	+0.10 +0.20	+0.10	+0.50	-0.10	+0.30	-0.20



10/24/96		FRANKFORT		QV IN LINE-E. FRANKFORT		LAKE					
SP220		SP221		SP222		SP223		SP224		SP225	
5278'		5280'		5267'		5267'		5296'			
MAINTENANCE		T&S-12/89									
RAIL		86RW112S 86RW112S									
TRAFFIC DENSITY MGT		-> 1, 2 (3, 4) < -2.2									
7-IN (NYC&SL)		-> 1, 2 (3, 2) < -2.0									
S E N W		(SCIRLEVILLE) Ⓢ									
TEST MILE		TEST MILE									
TRACK LAYOUT											
LINE SEGMENT 7785		474705A 474706G 474707N 474708V 474709C 474710W 474711D 474712K 474713S 474715F 474716M									
CURVATURE		PRVT OR 1300E PRVT PRVT OR 1230E PRVT PRVT MOLUNG ST OR 1100E OR 1000E PRVT									
ALIGNMENT		0.3R									
ELEVATION		0.50									
FREIGHT SPEED TANGENT (CURVE)		49									
GRADE		+0.10 -0.20 0.00 +0.30 0.00 -0.20 0.00 -0.20 +0.20 -0.10									

sheet 92sp22000

10/24/96

FRANKFORT

ON IN LINE-E. FRANKFORT

LAKE

	SP225	SP226	SP227	SP228	SP229	SP230
	5296'	5288'	5288'	5302'	5252'	
MAINTENANCE				T&S-11/89		
RAIL				86RW112S		
				86RW112S		
TRAFFIC DENSITY MGT				-> 1.2 (3.2)		
				< -2.0		
7-IN (NY&SL)	HILLSBURG			(BOYLESTON)		
				227.70-1088-10		
TRACK LAYOUT	X X X	X	Y Y	X	Y	Y
	474717U 474718B 474719H	474720C	474721J 474722R 474723X	474724E	474725L 474726T	474727A 474728G
LINE SEGMENT	OR 900E GREEN ST SPRING ST	OR 830E	PRVT PRVT PRVT	OR 700E WATERWAY	SR 29 PRVT	OR 500E PRVT
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	(35)			49		
GRADE	-0.10 -0.40 +0.20 -0.30	+0.10 0.00 +0.40 0.00	-0.40 -0.10	-0.08	0.00	

Sheet 92sp23000



10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE

1109

1110

5379'

5271'

MAINTENANCE

T&S-11/94

RAIL

29R090S 54R112S  
29R090S 49R112S  
54R112S 49R112S

TRAFFIC  
DENSITY MGT

-> 1.3 (2.3)  
< -1.0

15-IN (LE&W)

W  
S—N  
E

ARGOS  
MARSHALL CO.  
ARGOS CORP.

TRACK  
LAYOUT

LINE SEGMENT  
7760

1-109, 5/8-430.7  
NS MI 56 M-NS  
1-109, 8/8-430.8  
SOUTH ST

CURVATURE

ALIGNMENT

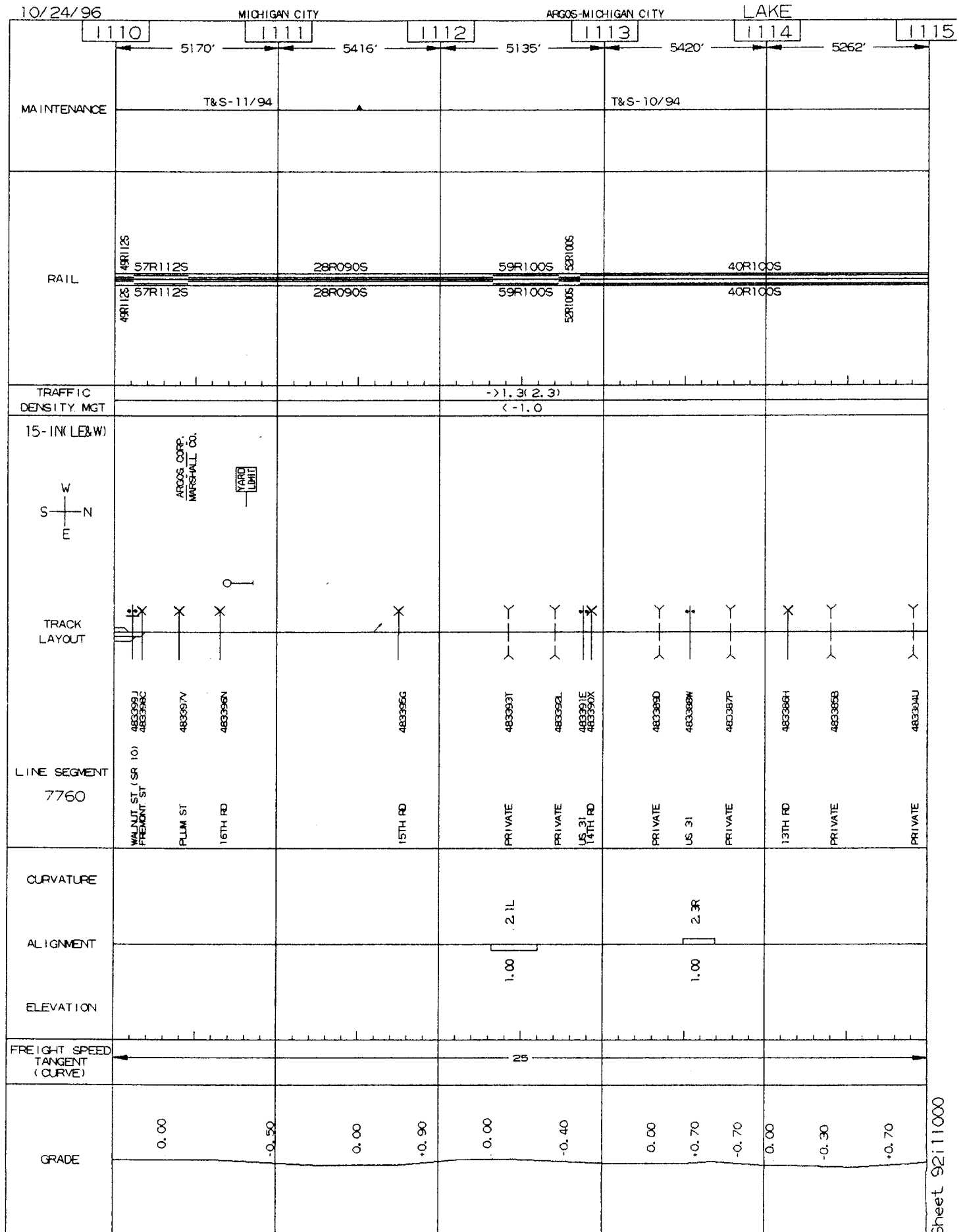
ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

10 25

GRADE

-1.00  
-0.50  
0.00

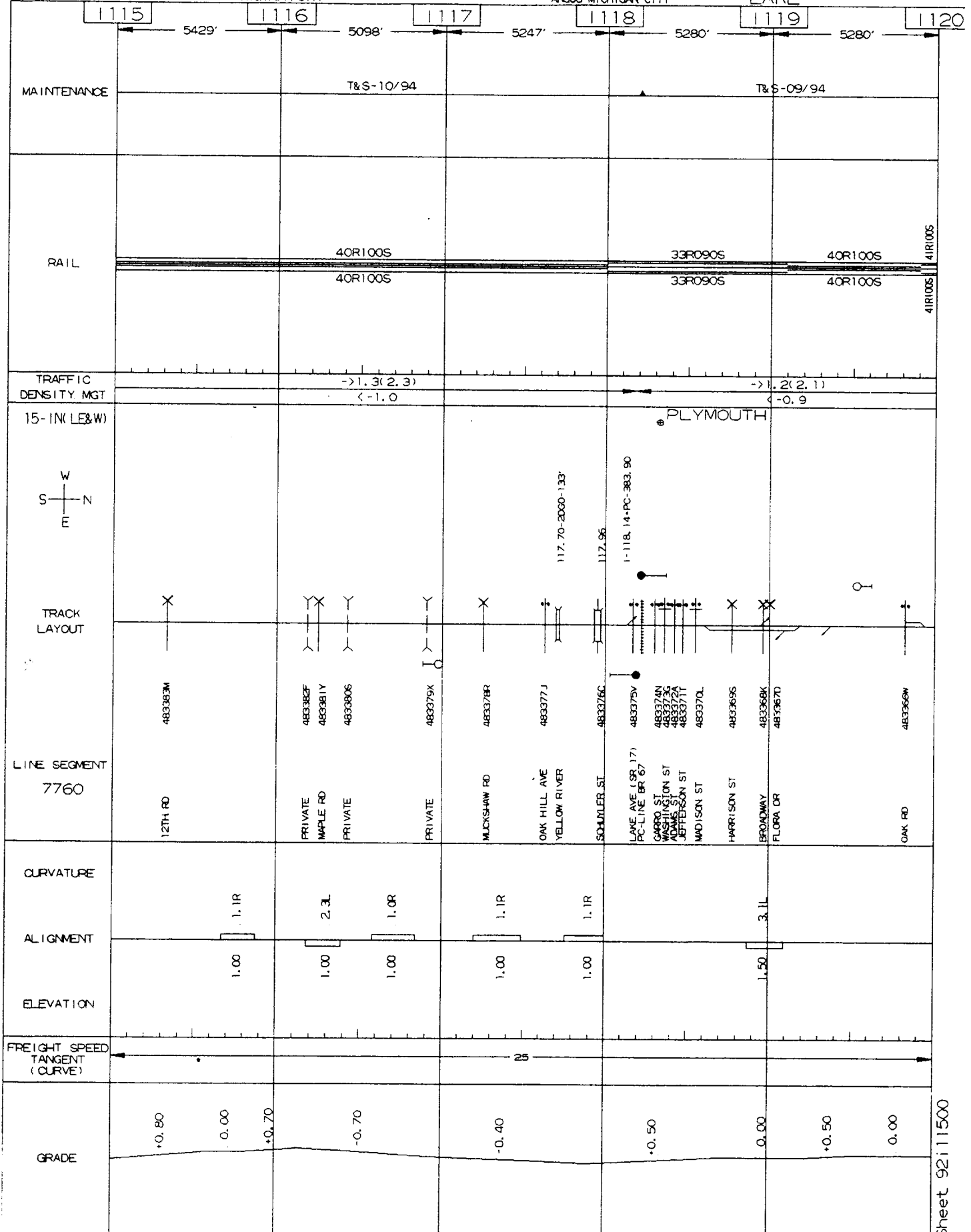


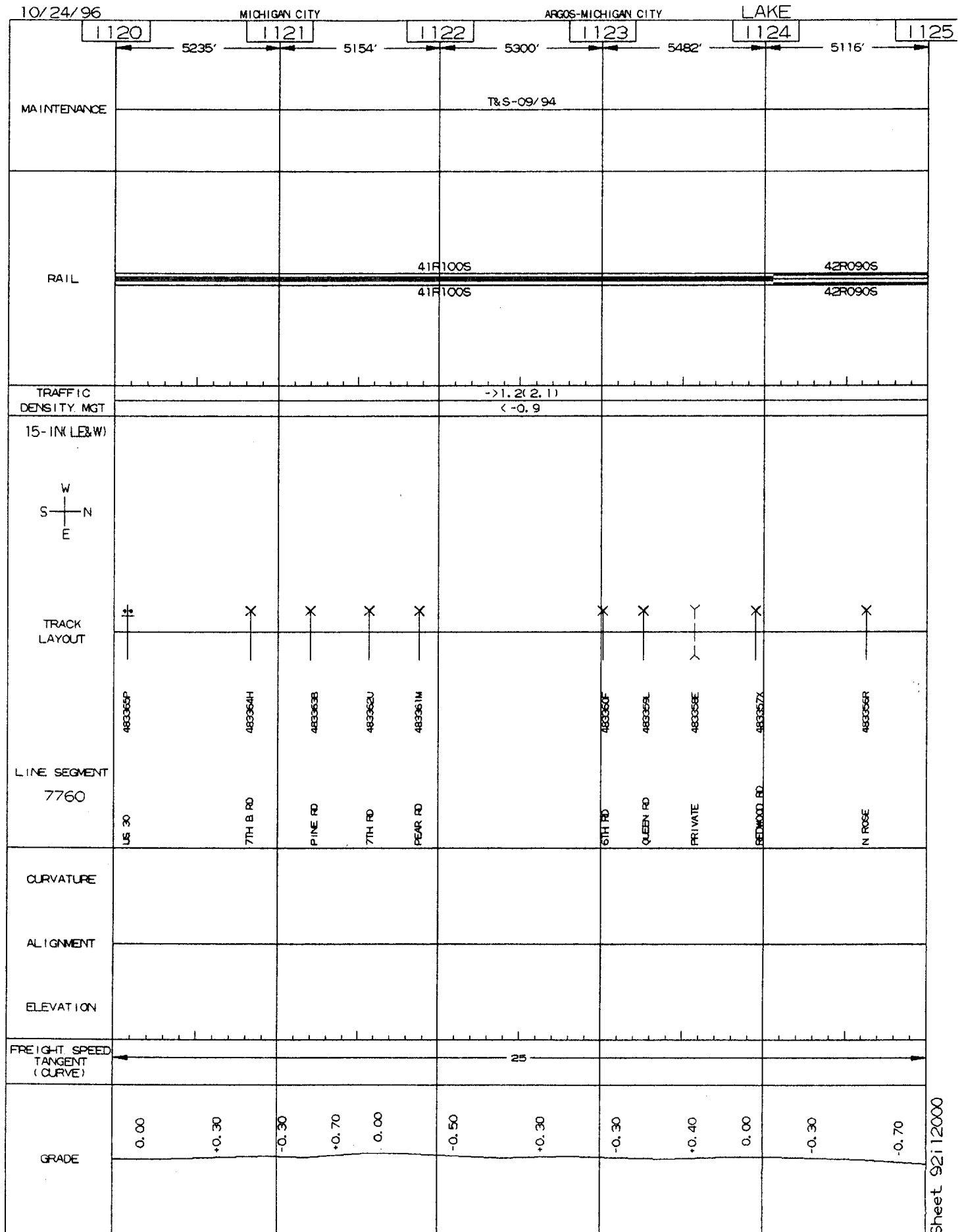
10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE



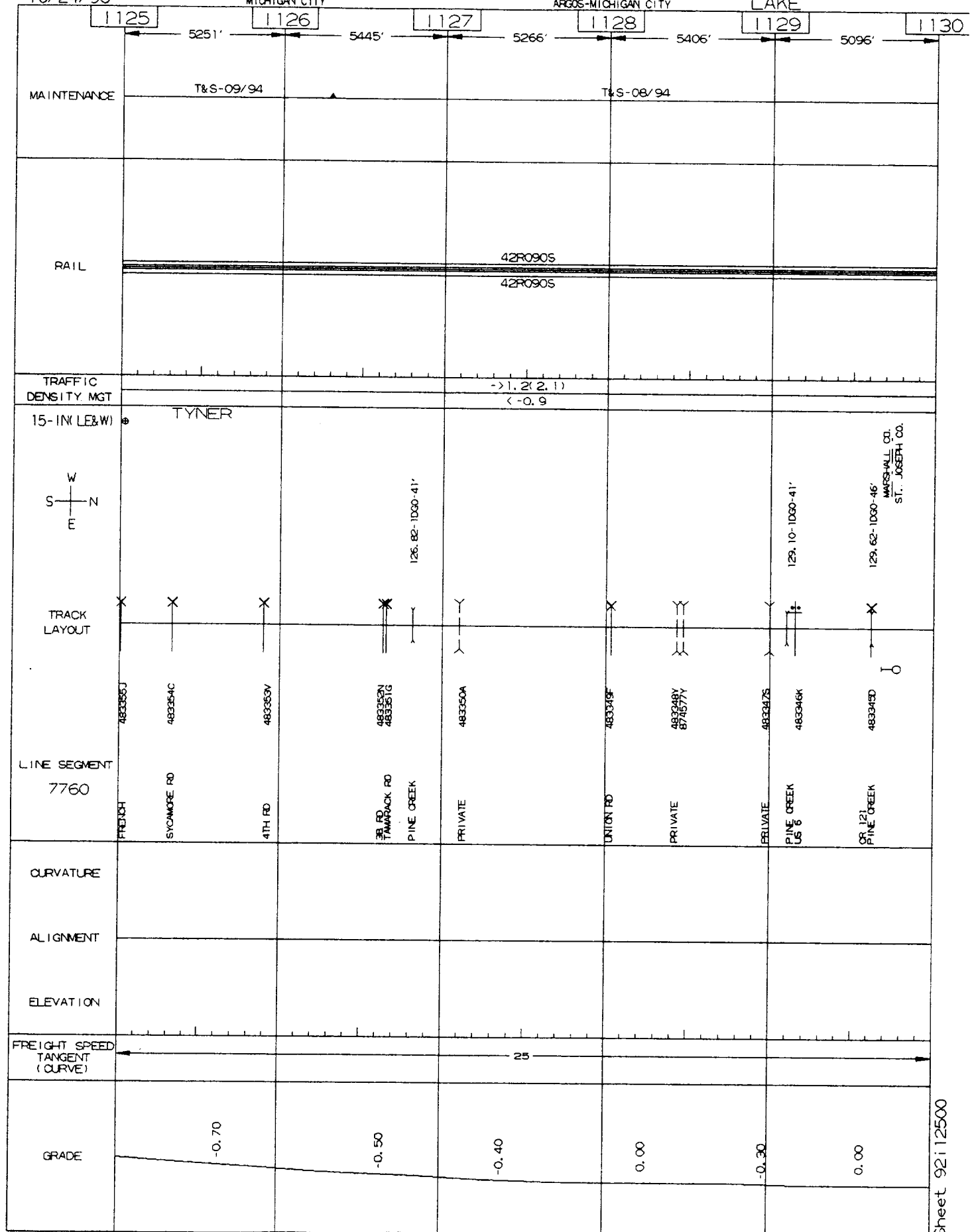


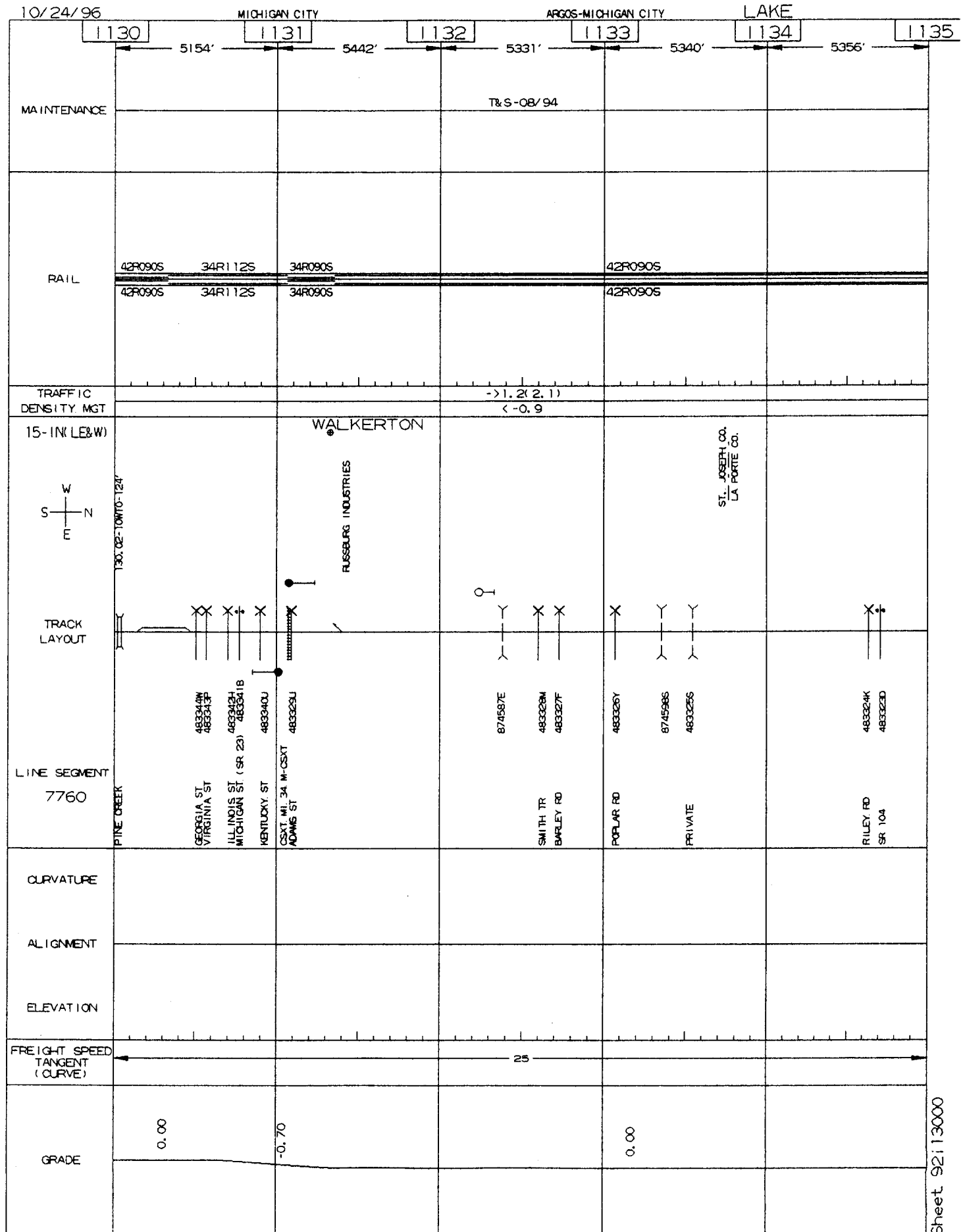
10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE



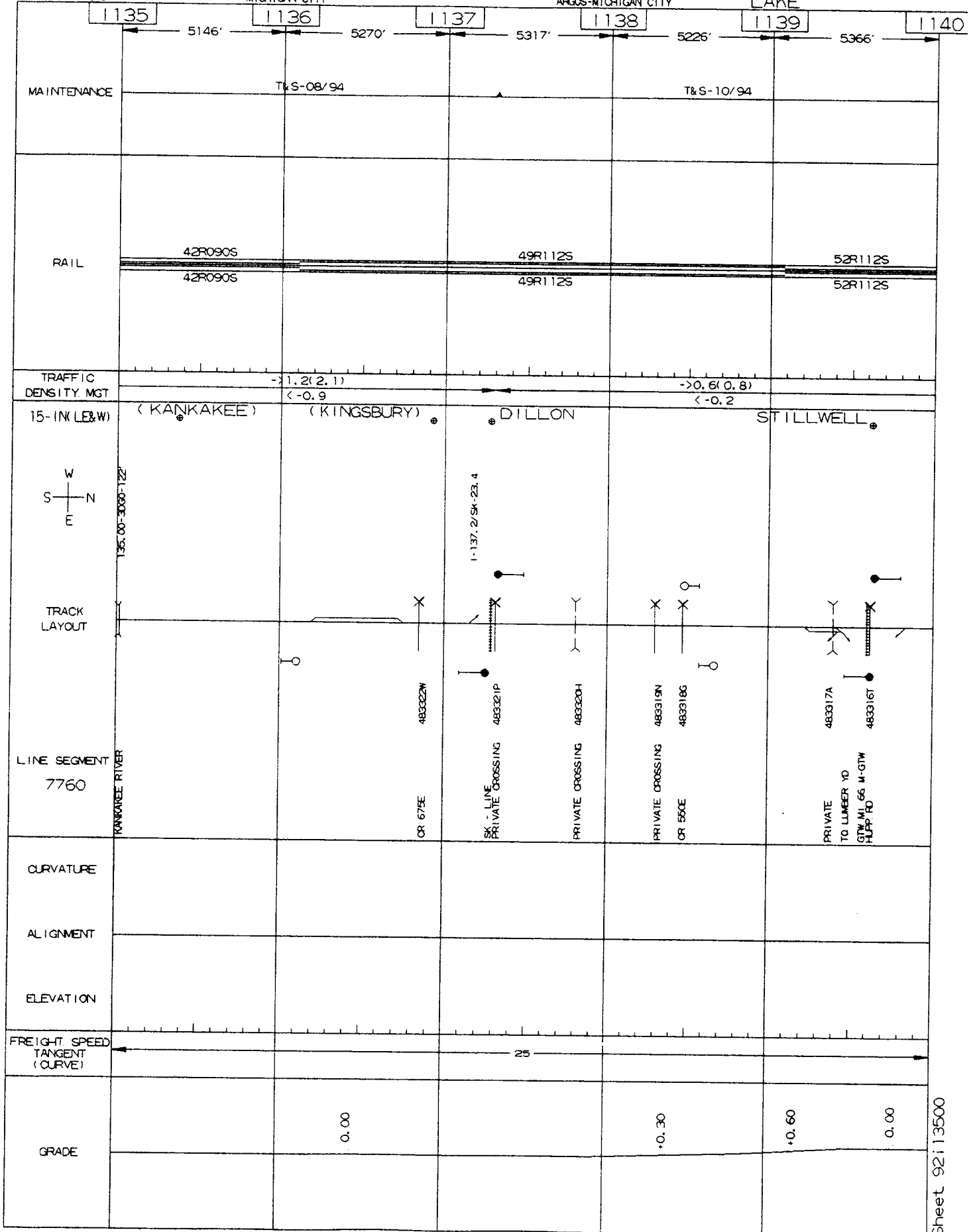


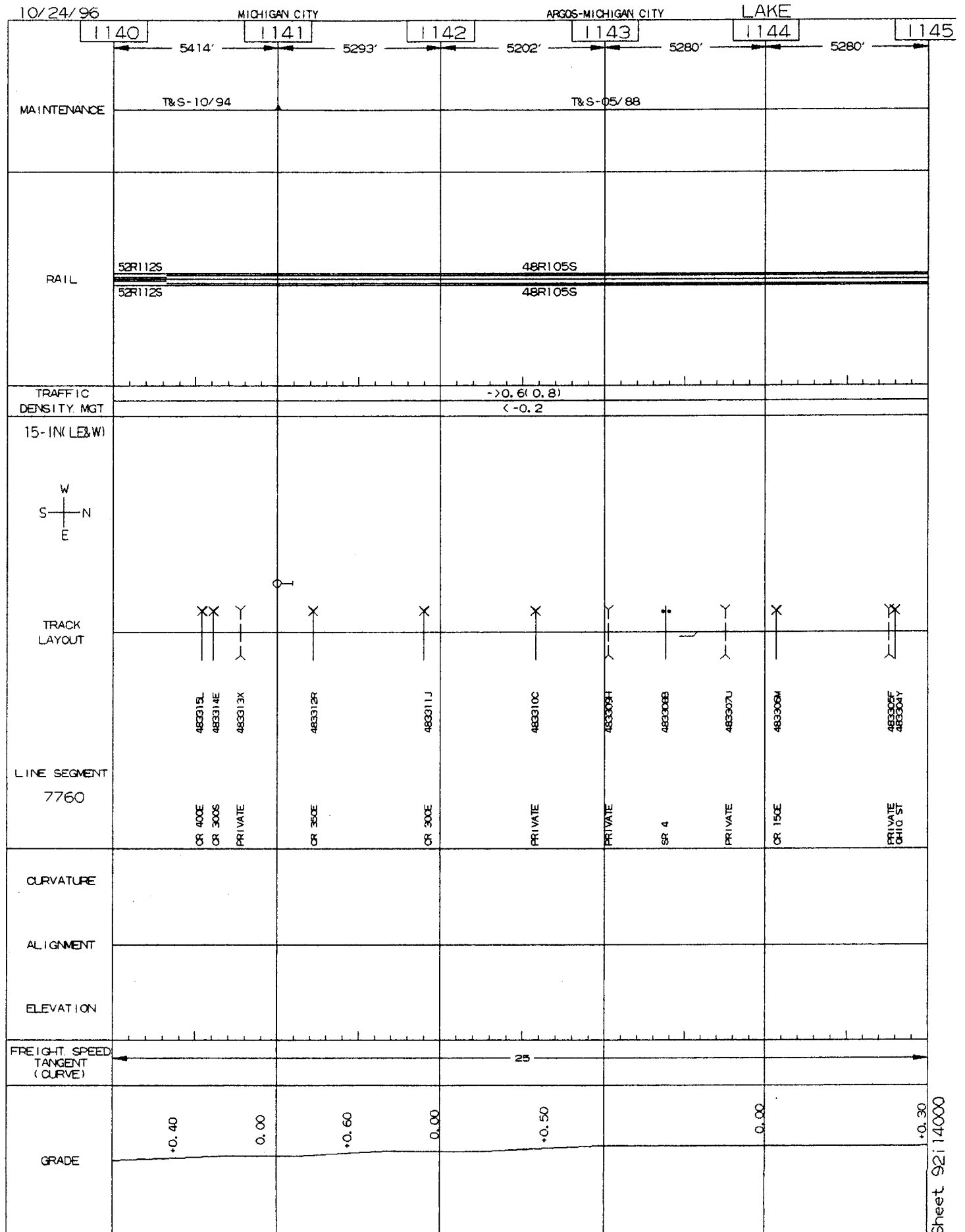
10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE





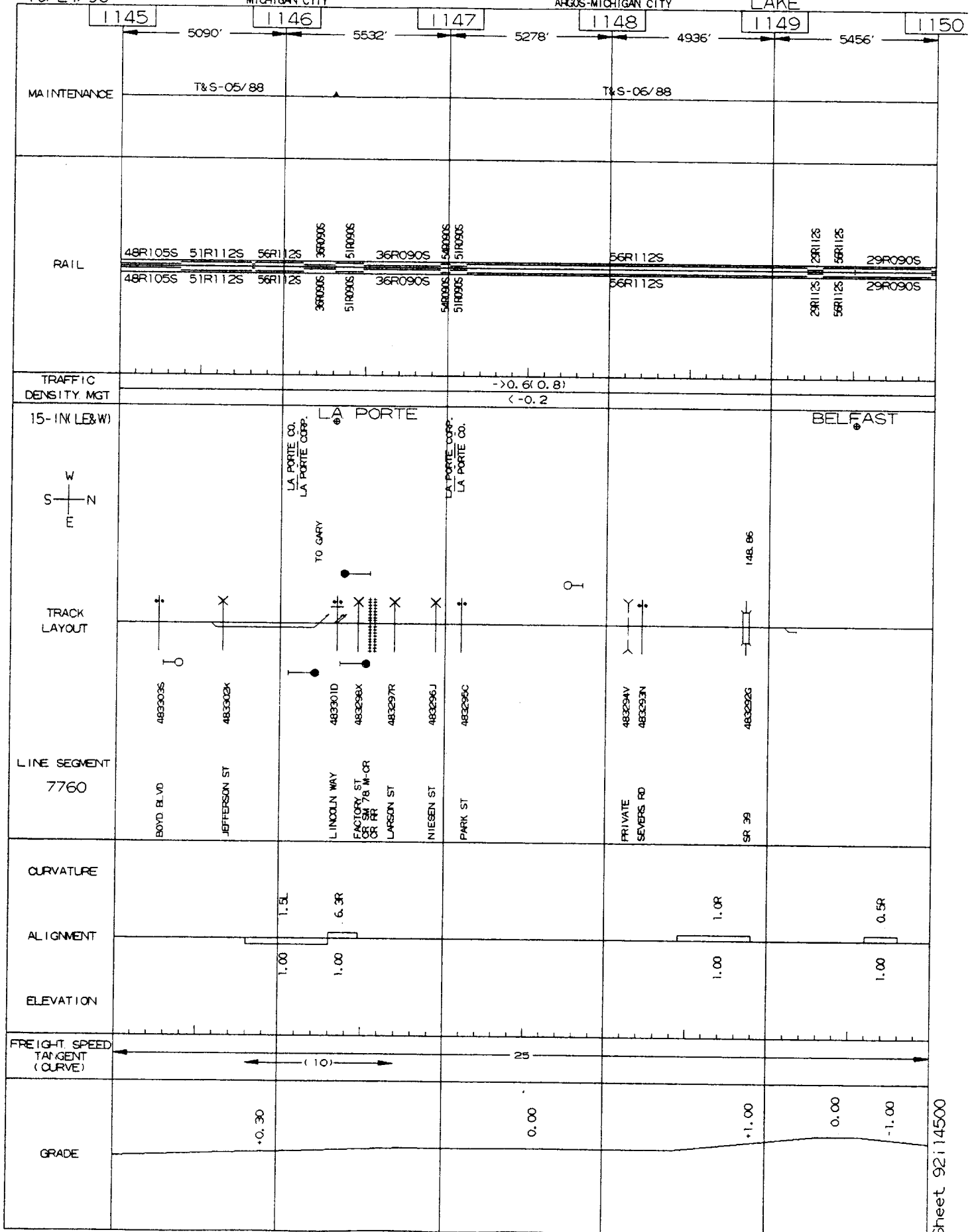


10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE

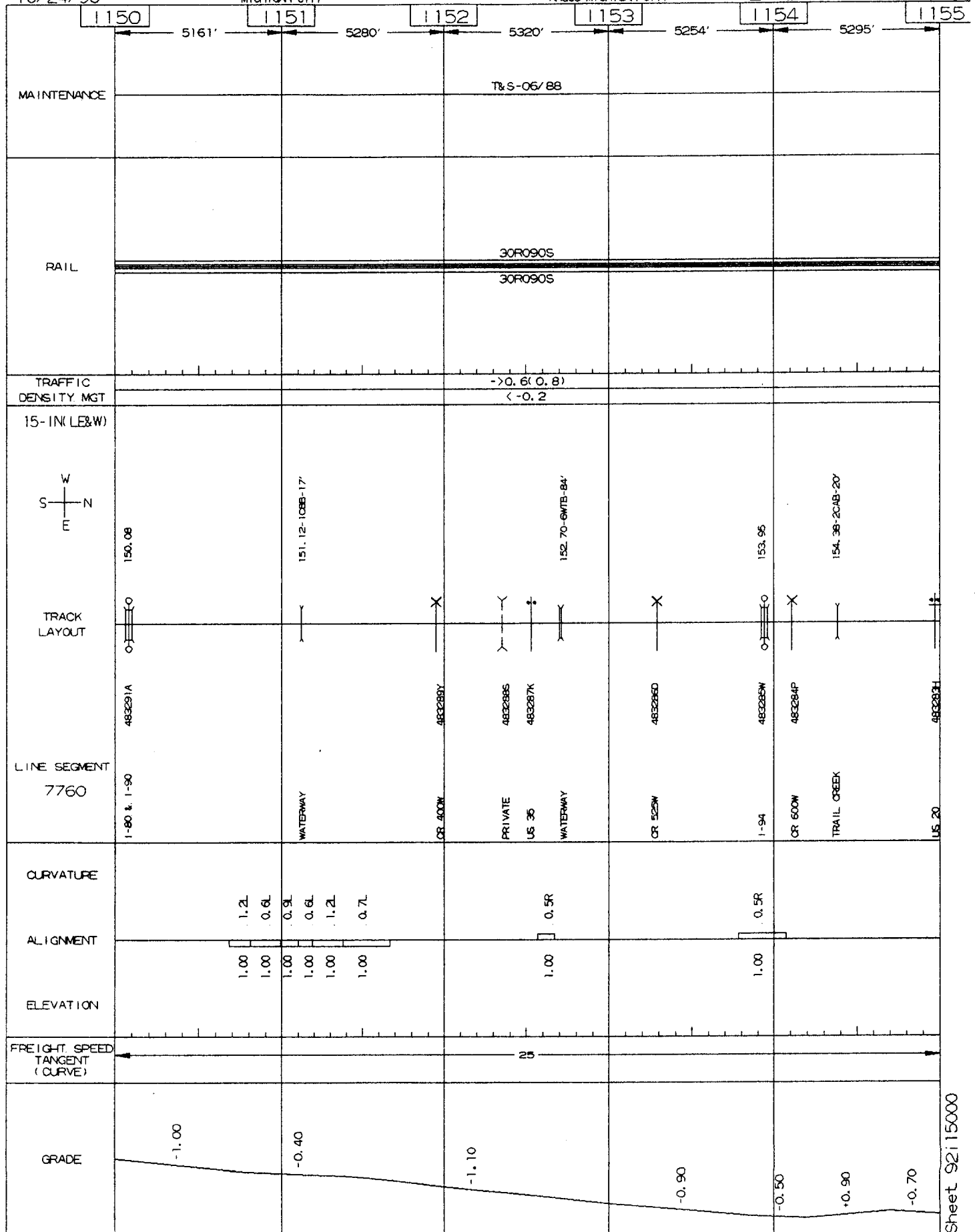


10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE

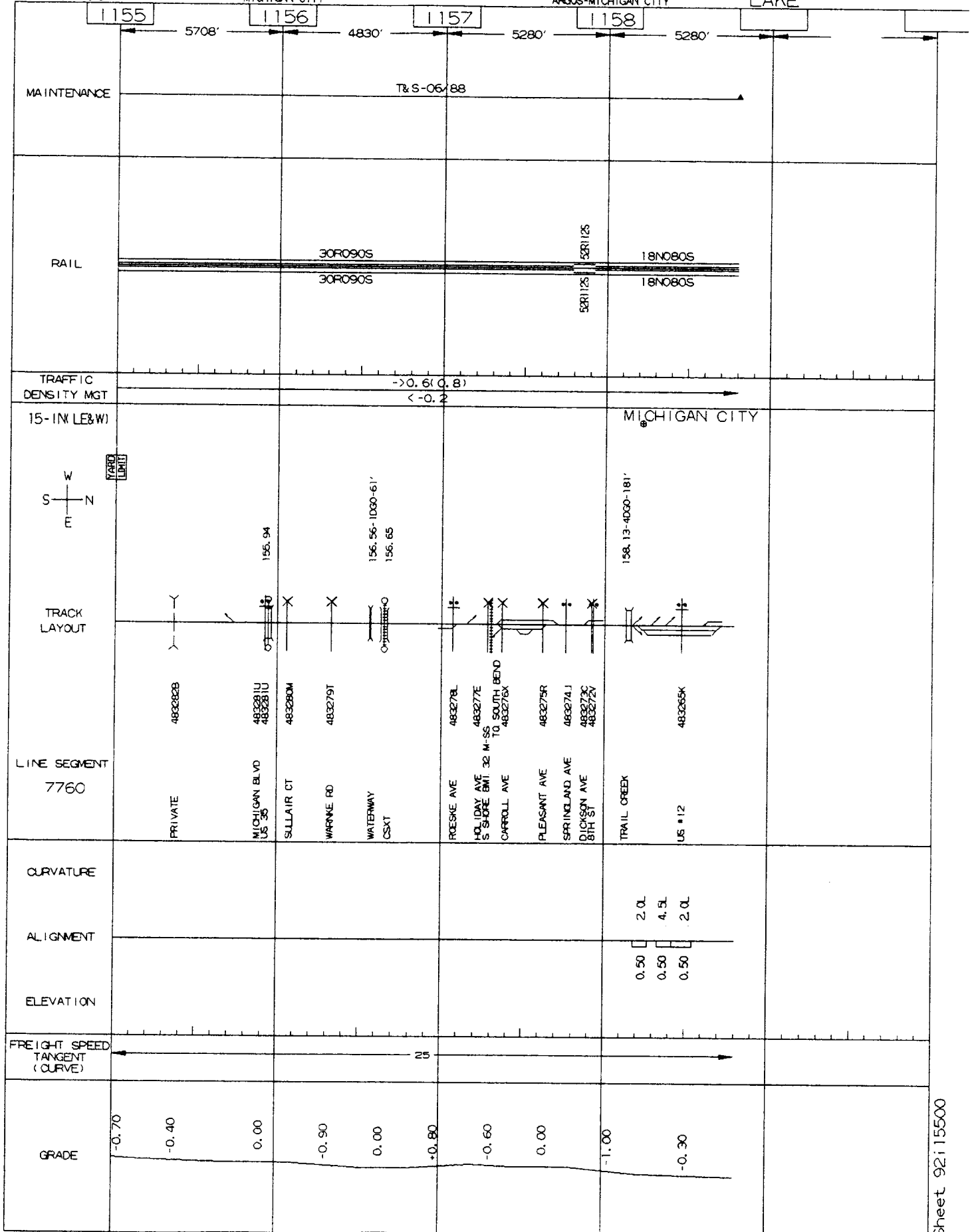


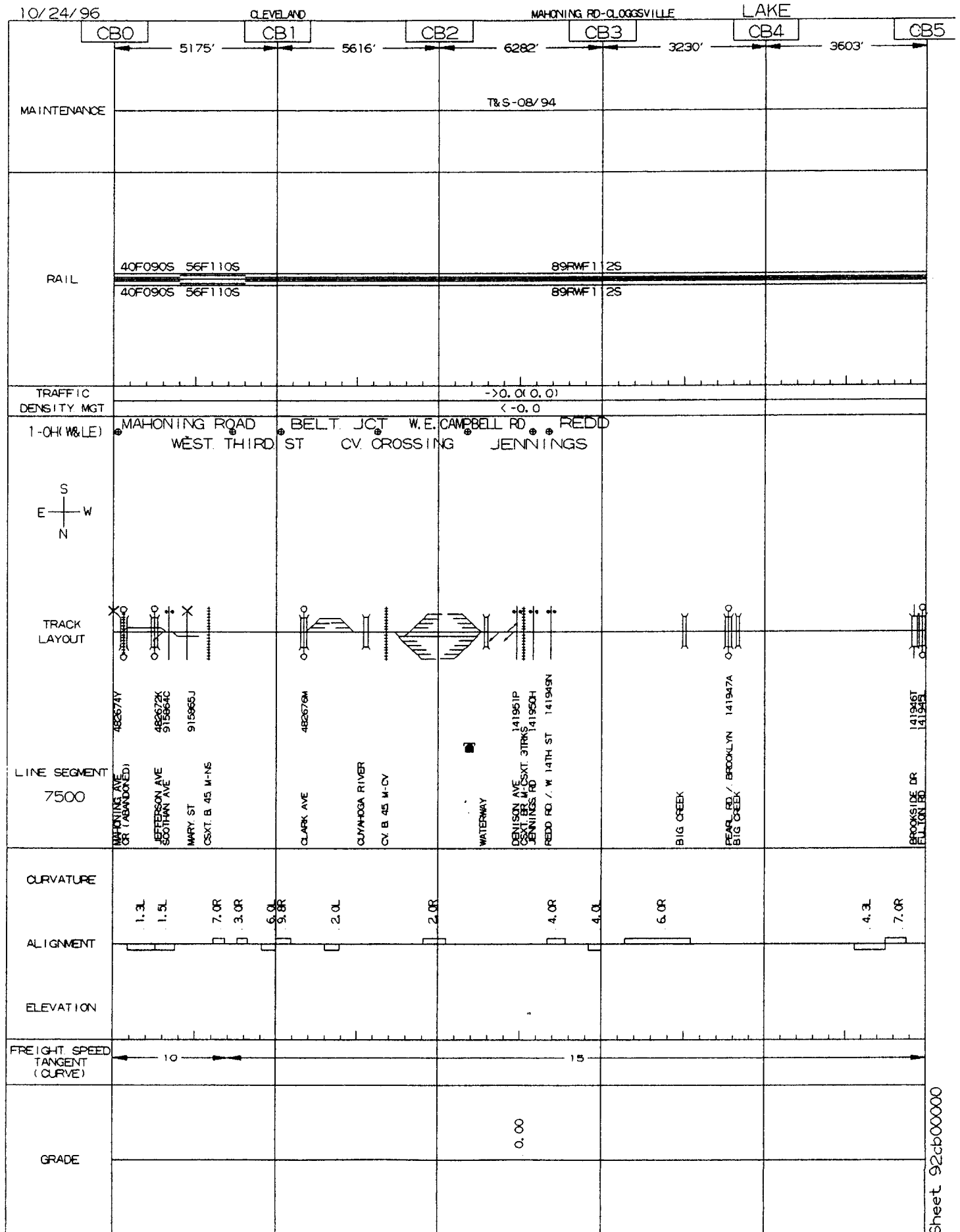
10/24/96

MICHIGAN CITY

ARGOS-MICHIGAN CITY

LAKE



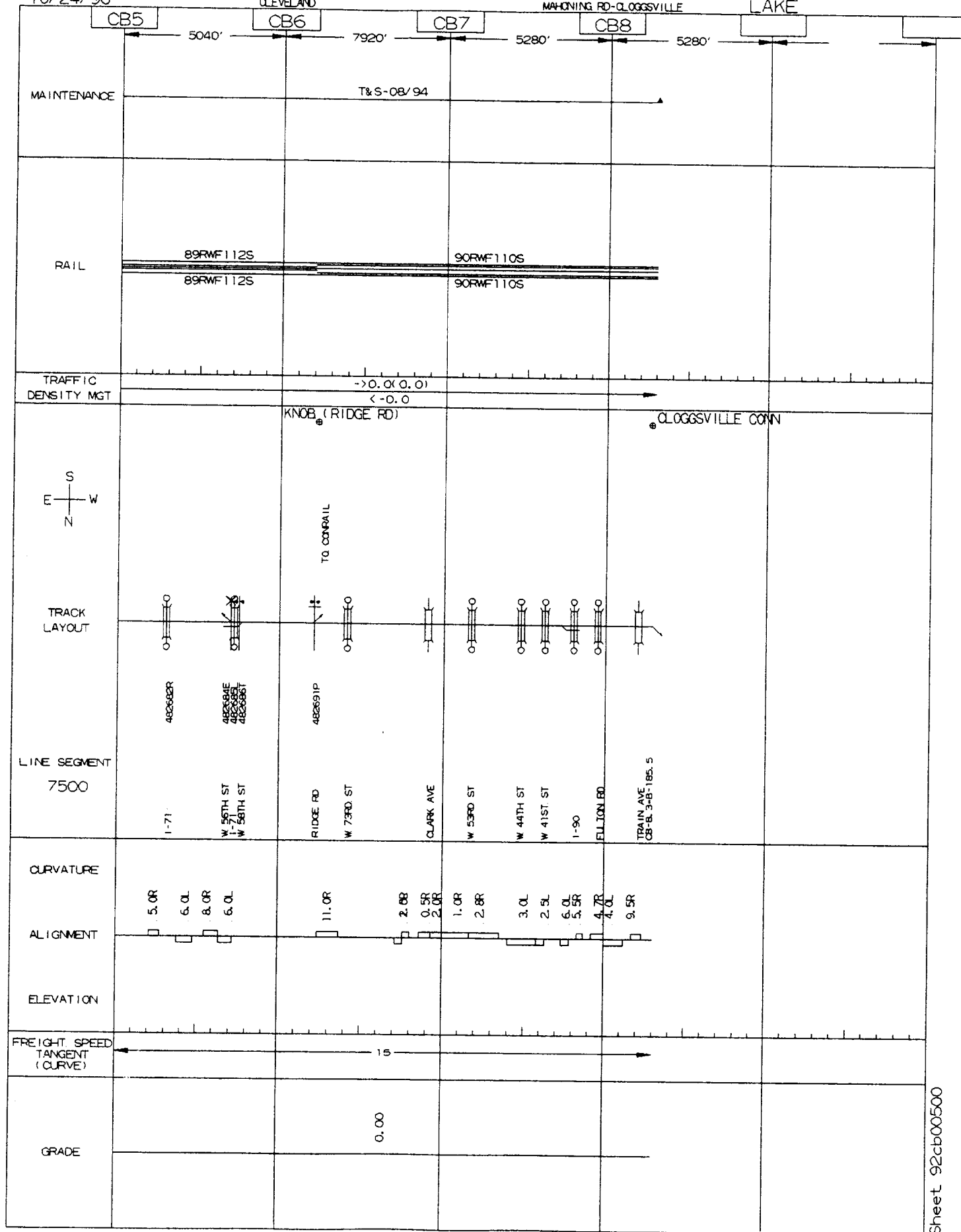


10/24/96

CLEVELAND

MAHONING RD-CLOGGSVILLE

LAKE

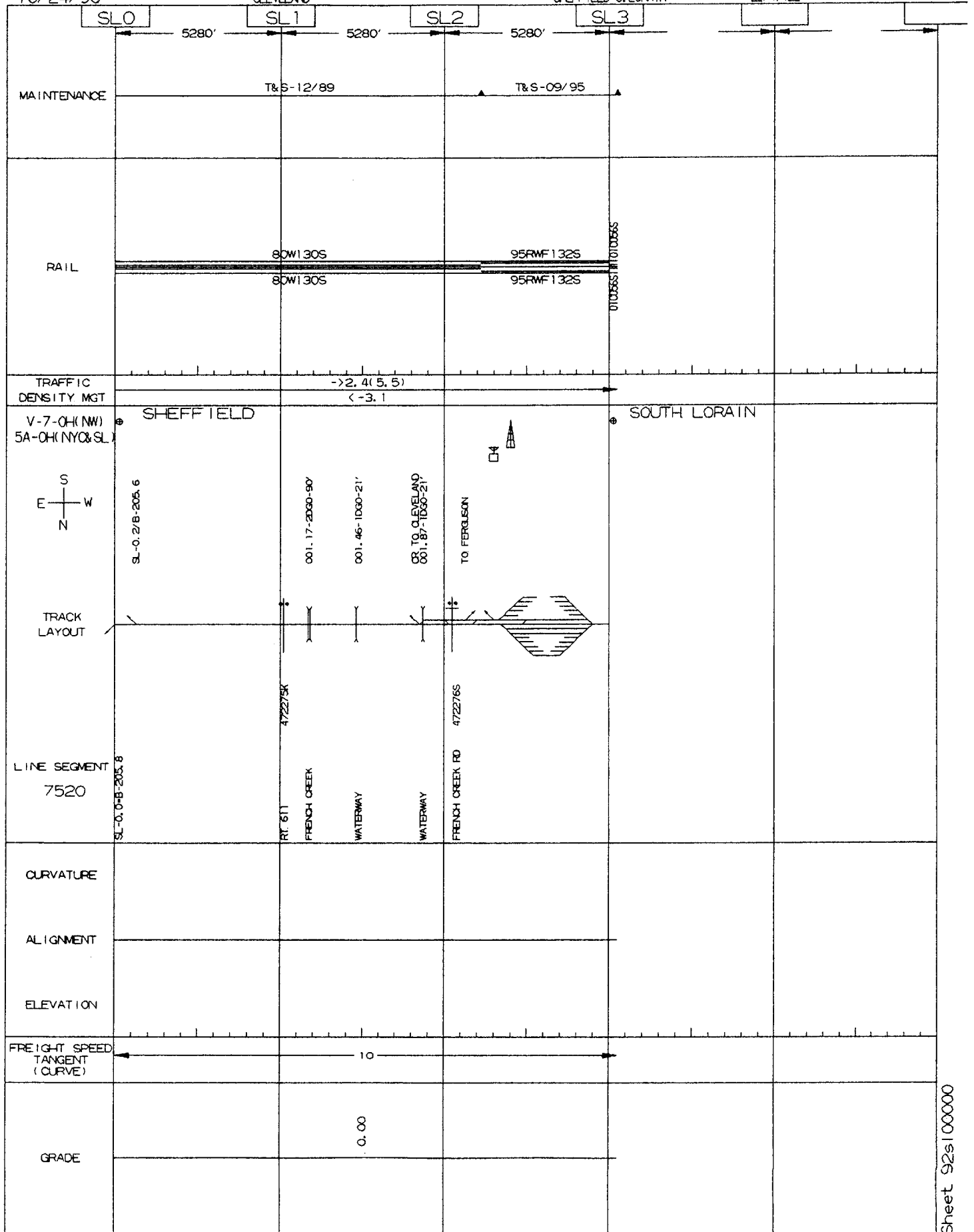


10/24/96

CLEVELAND

SHEFFIELD-S. LORAIN

LAKE



10/24/96

MAUMEE

WOODBURN-NEW HAVEN

LAKE

TN79

TN80

5346'

MAINTENANCE

T&S-06/90

RAIL

49R090S

49R090S

TRAFFIC  
DENSITY, MGT

-> 0.2 (0.4)

< -0.2

2B-1(N WAB)

WOODBURN

S  
E—W  
N

TRACK  
LAYOUT

X

X

LINE SEGMENT  
7870

WOODBURN RD  
4778100

477843Y

HETRICK

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

25

GRADE

+0.04

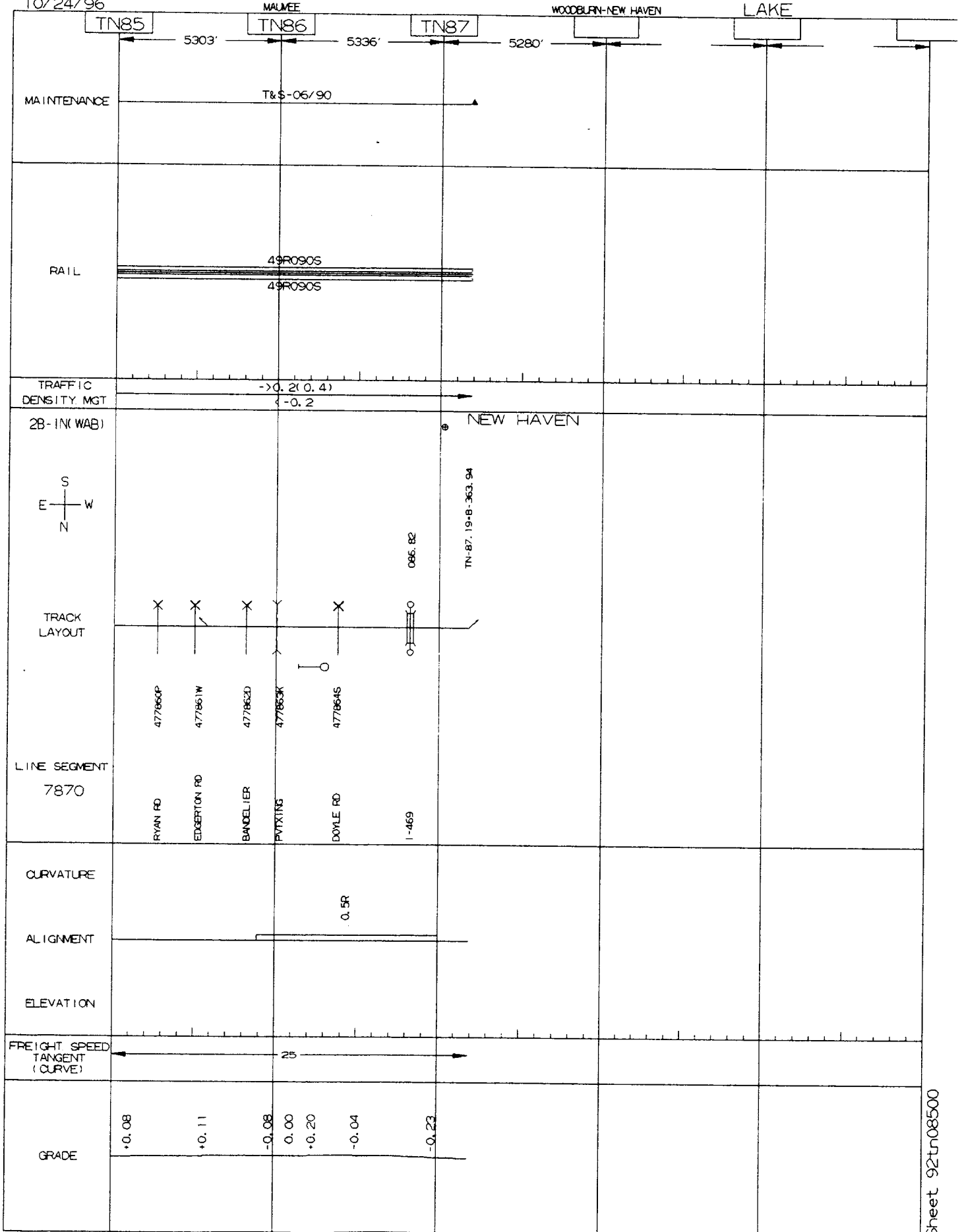
+0.07

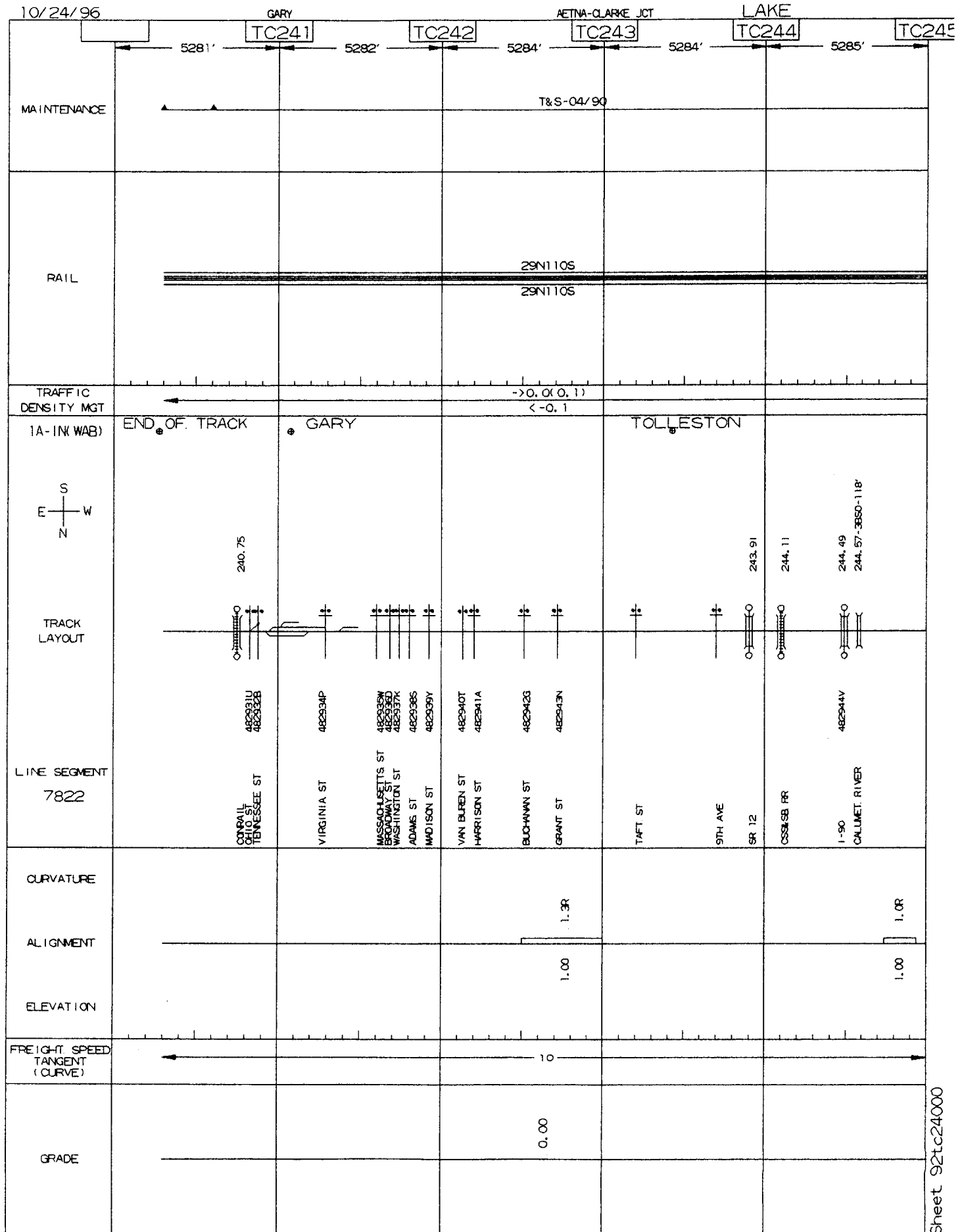
10/24/96		MALMEE		WOODBURN-NEW HAVEN		LAKE					
TN80		TN81		TN82		TN83		TN84		TN85	
5200'		5366'		5362'		5241'		5301'			
MAINTENANCE		T&S-06/90									
RAIL		49R0905									
TRAFFIC DENSITY MGT		-> 0.2 (0.4) < -0.2									
2B-IN(WAB)		( GARR CREEK )									
<div><div>S</div><div>E</div><div>N</div><div>W</div></div>											
TRACK LAYOUT		<div><div>080.10-3MT0-40'</div><div>080.70-3MT0-36'</div><div>082.16-2MT0-28'</div><div>083.56-3MT0-70'</div></div>									
LINE SEGMENT 7870		<div><div>477844F</div><div>477845M</div><div>477846J</div><div>477847B</div><div>477851R</div><div>477853E</div><div>477850J</div><div>477854L</div><div>477852X</div><div>477855T</div><div>477856A</div><div>477857G</div><div>477858N</div><div>477859V</div></div>									
WATERWAY PVTXING		SAMPSON RD		TO GOODRICH PLANT		WATERWAY BREMER		PVTXING		WEBSTER	
EDGERTON DITCH		PVTXING		ROUSEY		PVTXING		GARR CREEK RD		GARR CREEK	
BERTHALD		HARGER RD		PVTXING		PVTXING					
CURVATURE											
ALIGNMENT											
ELEVATION											
FREIGHT SPEED TANGENT (CURVE)		25									
GRADE		<div><div>+0.07</div><div>-0.12</div><div>0.00</div><div>+0.04</div><div>0.00</div><div>+0.10</div><div>0.00</div><div>+0.07</div><div>0.00</div><div>+0.08</div></div>									

Sheet 92tn08000



10/24/96





10/24/96

GARY

AETNA-CLARKE JCT

LAKE

TC245

TC246

5282'

5280'

MAINTENANCE

T&S-04/90

RAIL

29N110S

29N110S

TRAFFIC  
DENSITY MGT

-> 0.0 (0.1)

< -0.1

1A-1IN(WAB)

CLARKE<sub>8</sub> JCT

S  
E—W  
N

TRACK  
LAYOUT

482345C

246.30

0.000000

TO CHICAGO VIA BOCT

LINE SEGMENT  
7822

CLARK RD

ELMER RD

CURVATURE

ALIGNMENT

ELEVATION

3.9L

1.00

FREIGHT SPEED  
TANGENT  
(CURVE)

10

GRADE

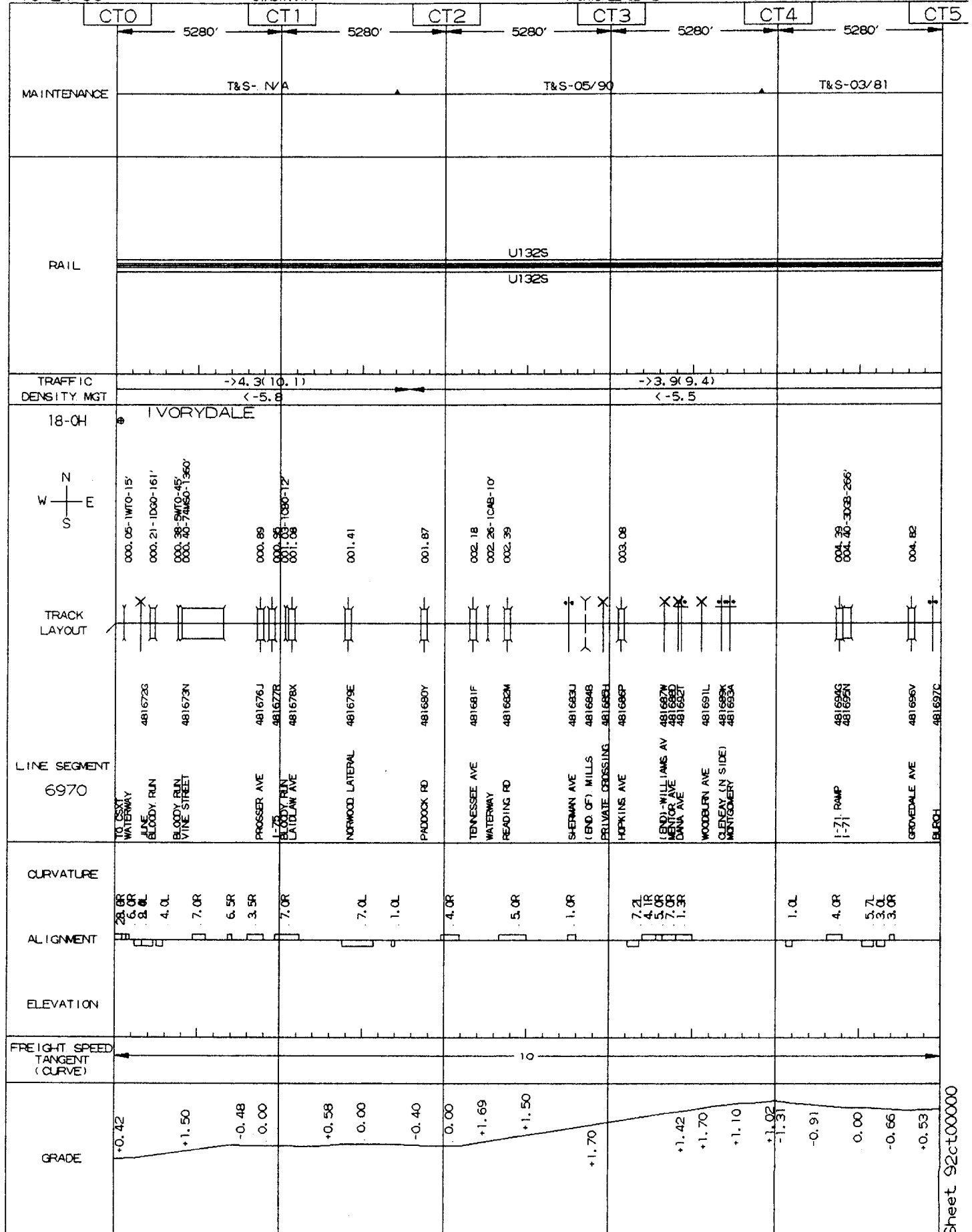
0.00

10/24/96

CINCINNATI

IVORYDALE-CLARE

LAKE

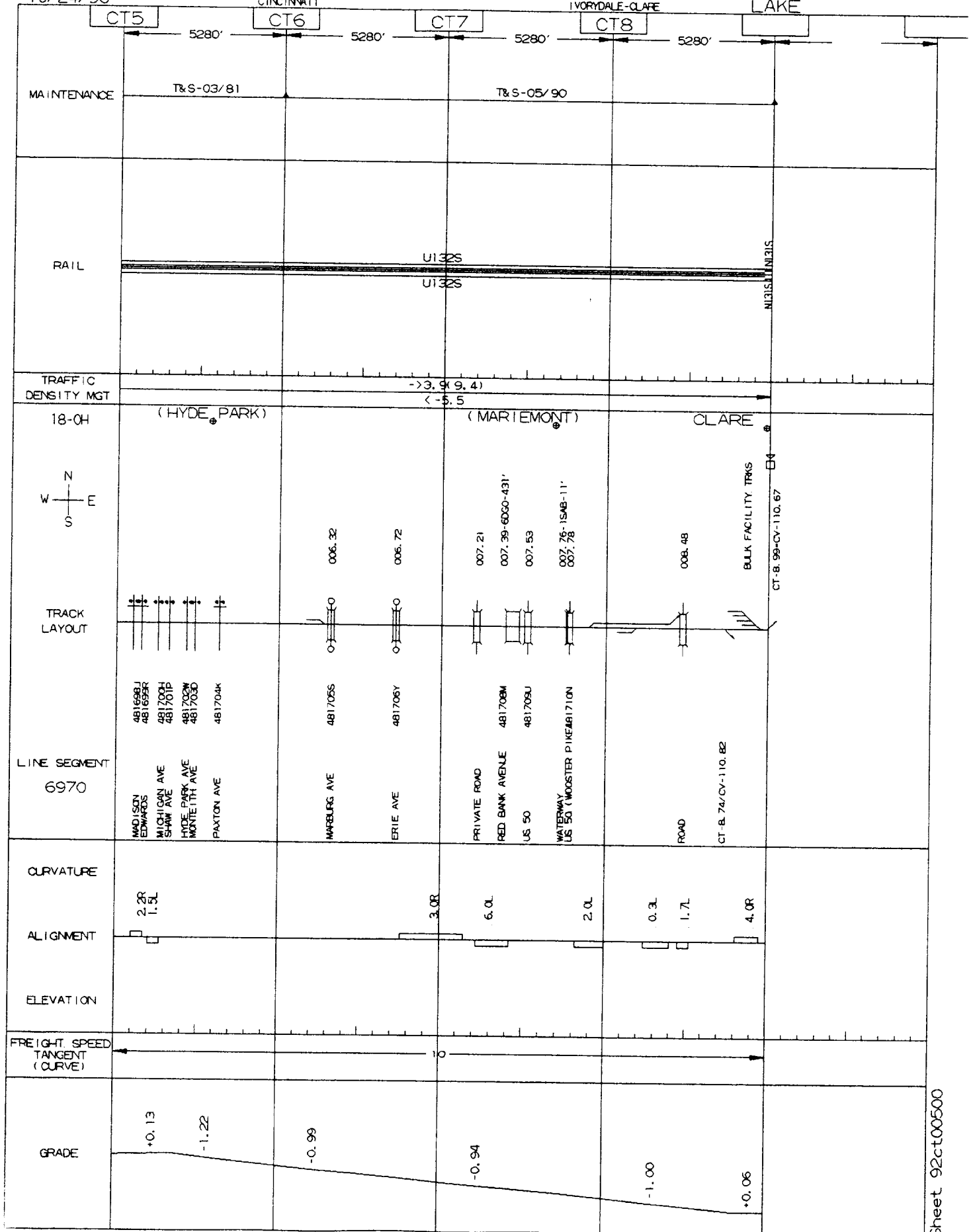


10/24/96

CINCINNATI

IVORYDALE-CLARE

LAKE

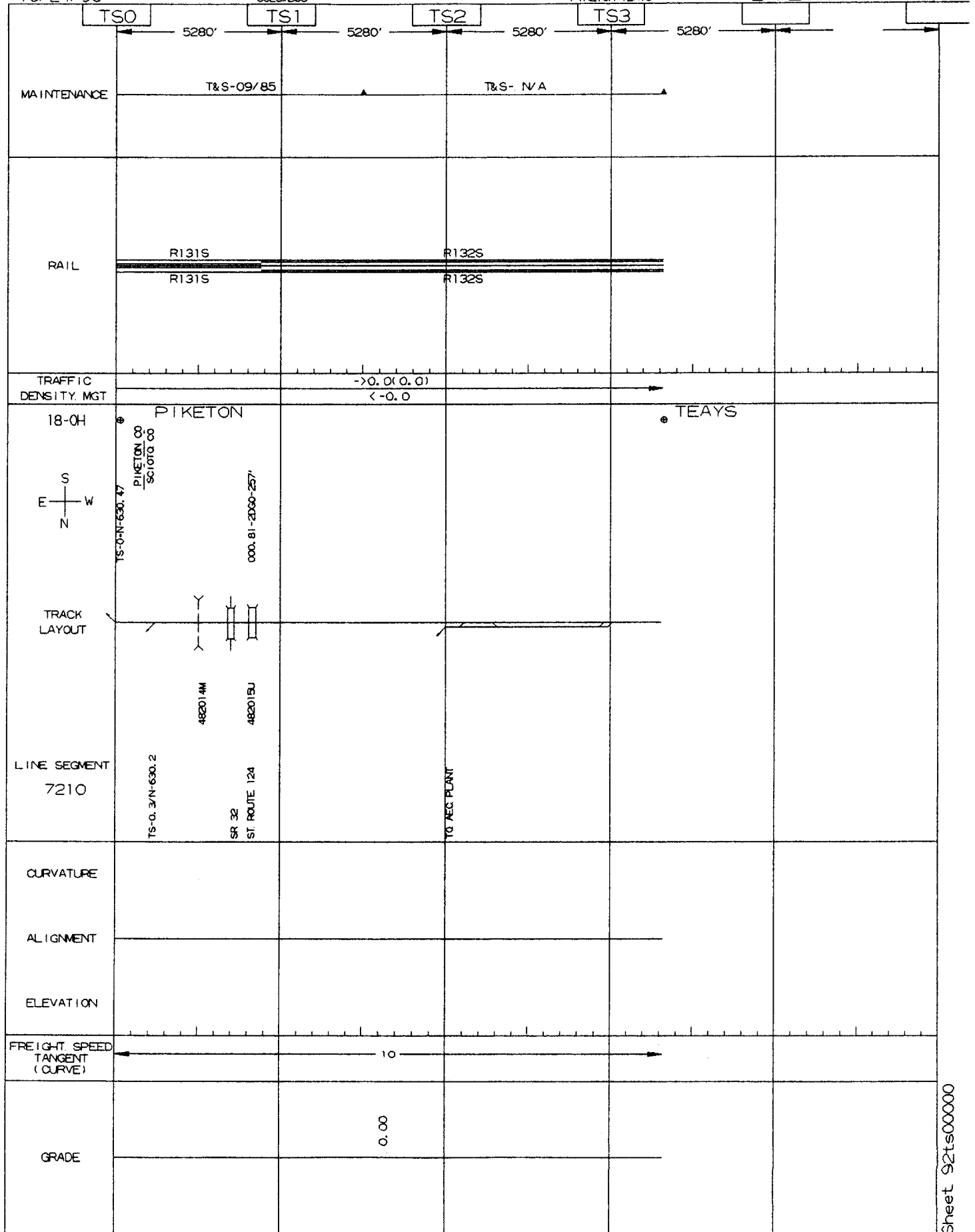


10/24/96

COLUMBUS

PIKETON-TEAYS

LAKE

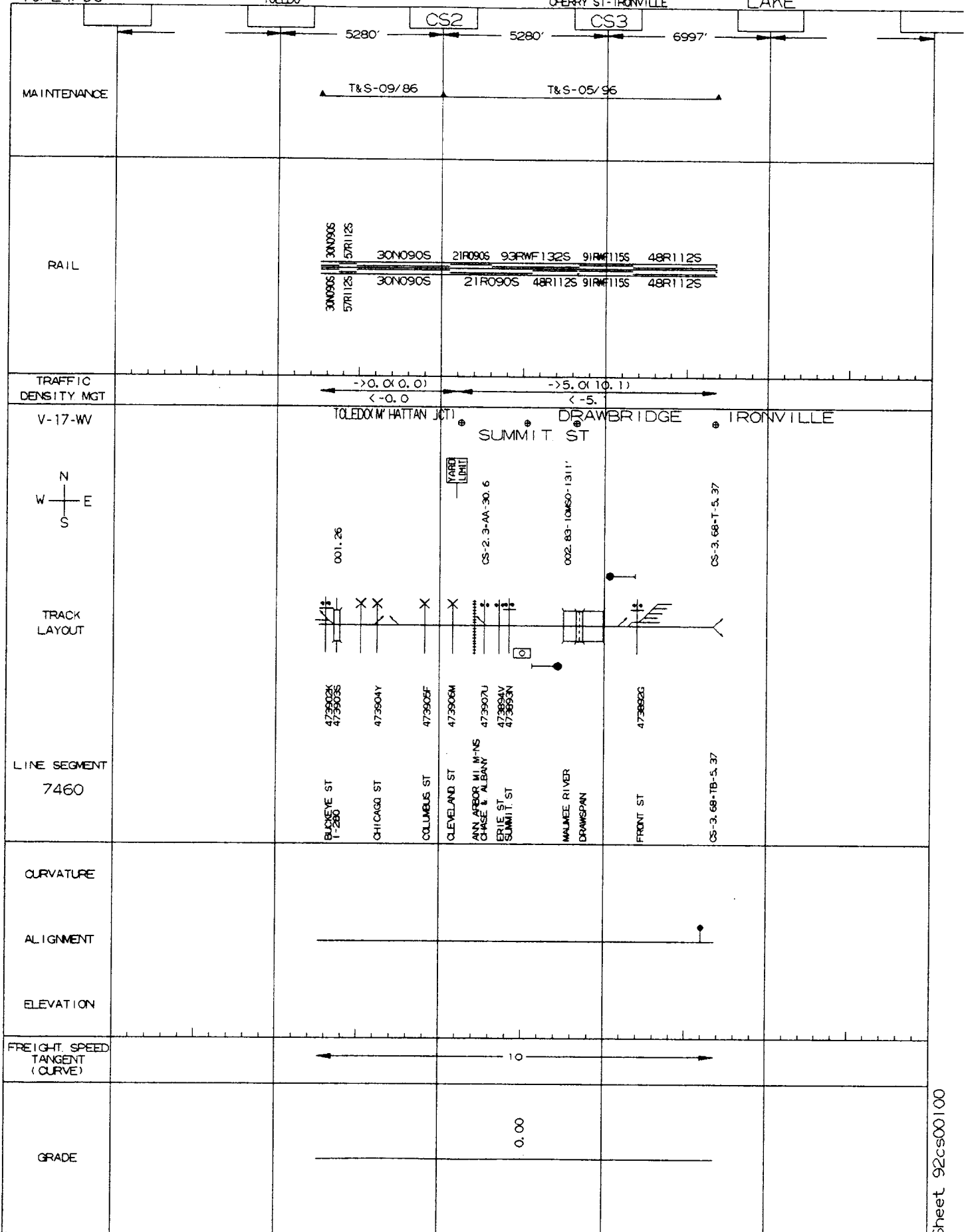


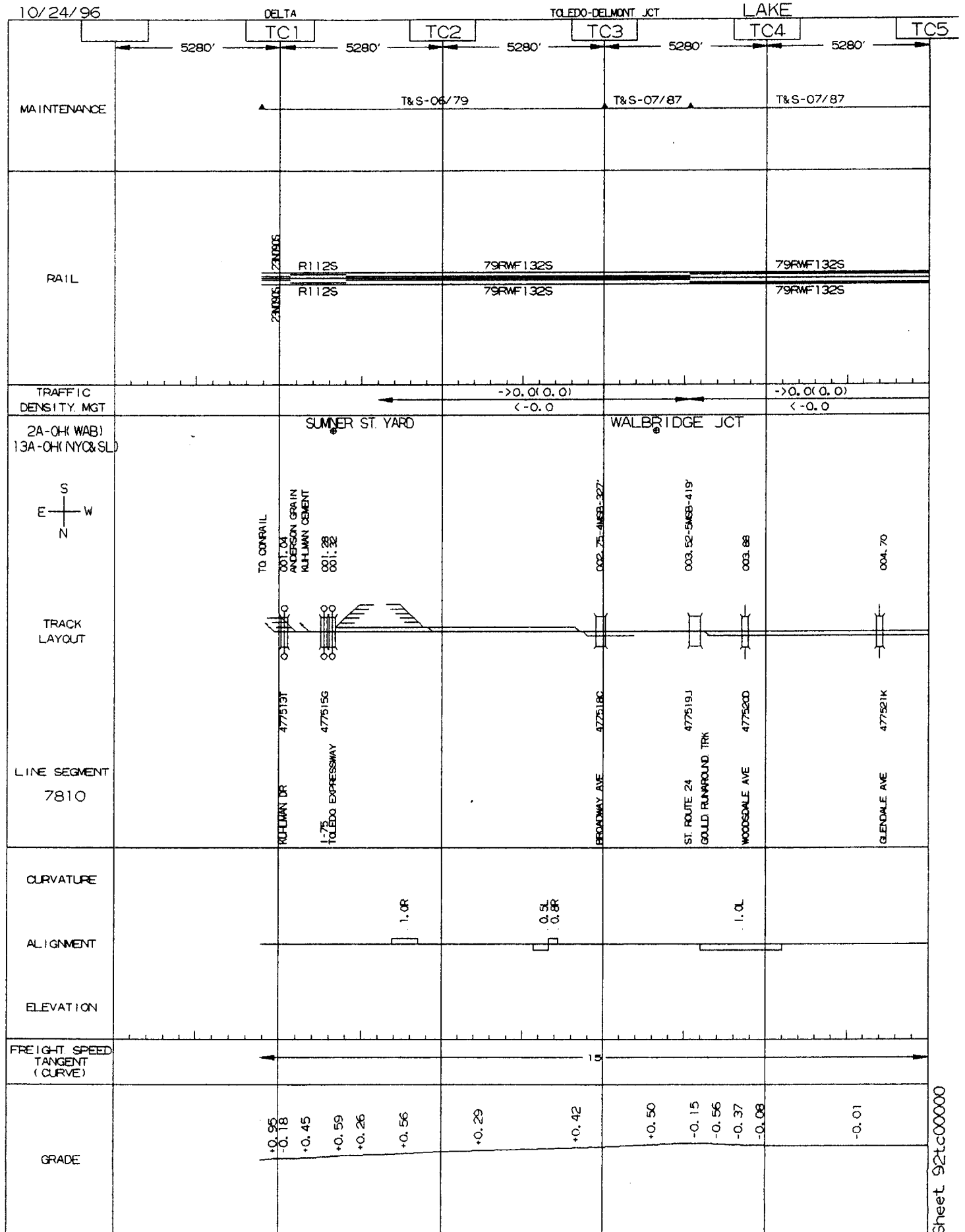
10/24/96

TOLEDO

CHERRY ST-IRONVILLE

LAKE





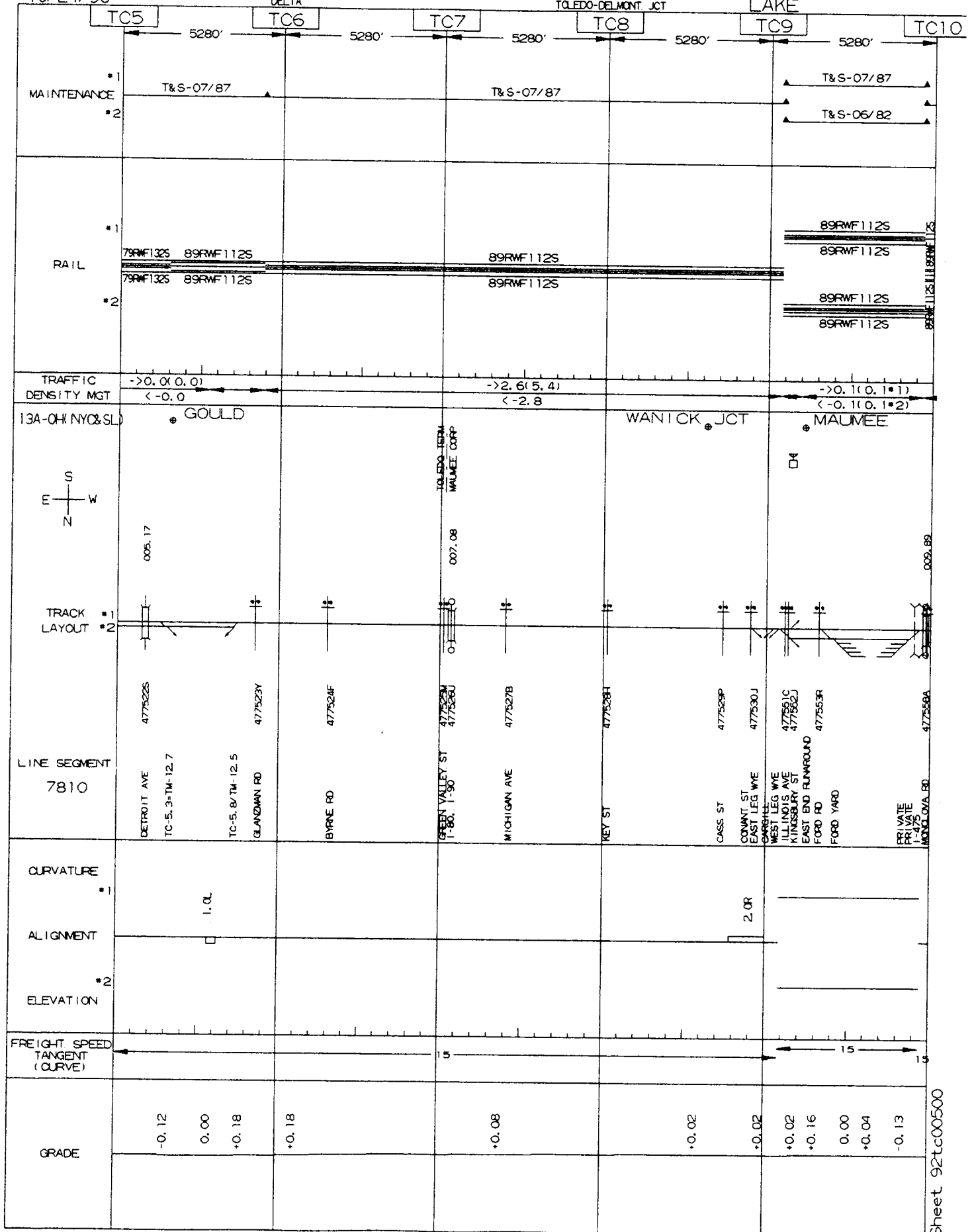


10/24/96

DELTA

TOLEDO-DELMONT JCT

LAKE

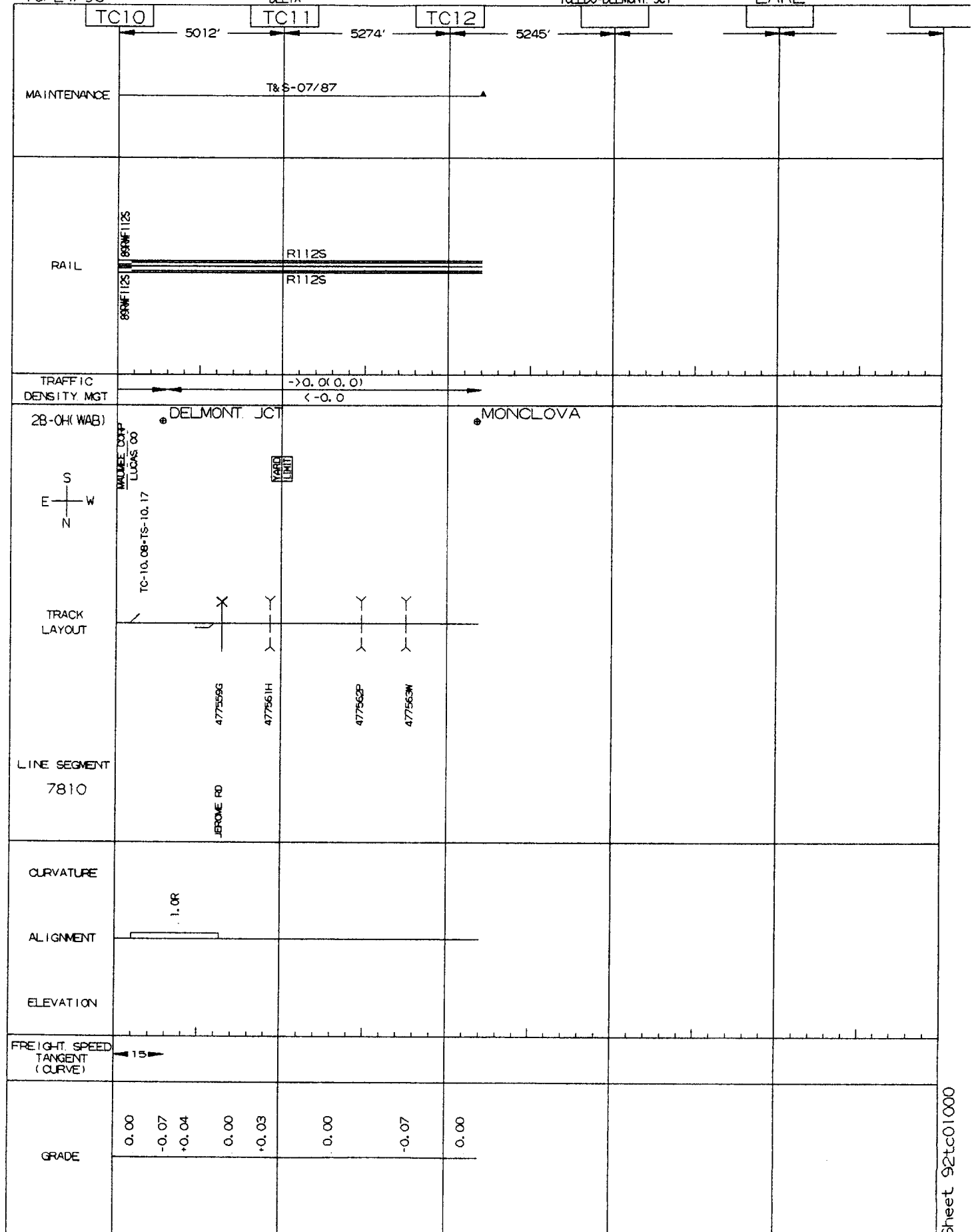


10/24/96

DELTA

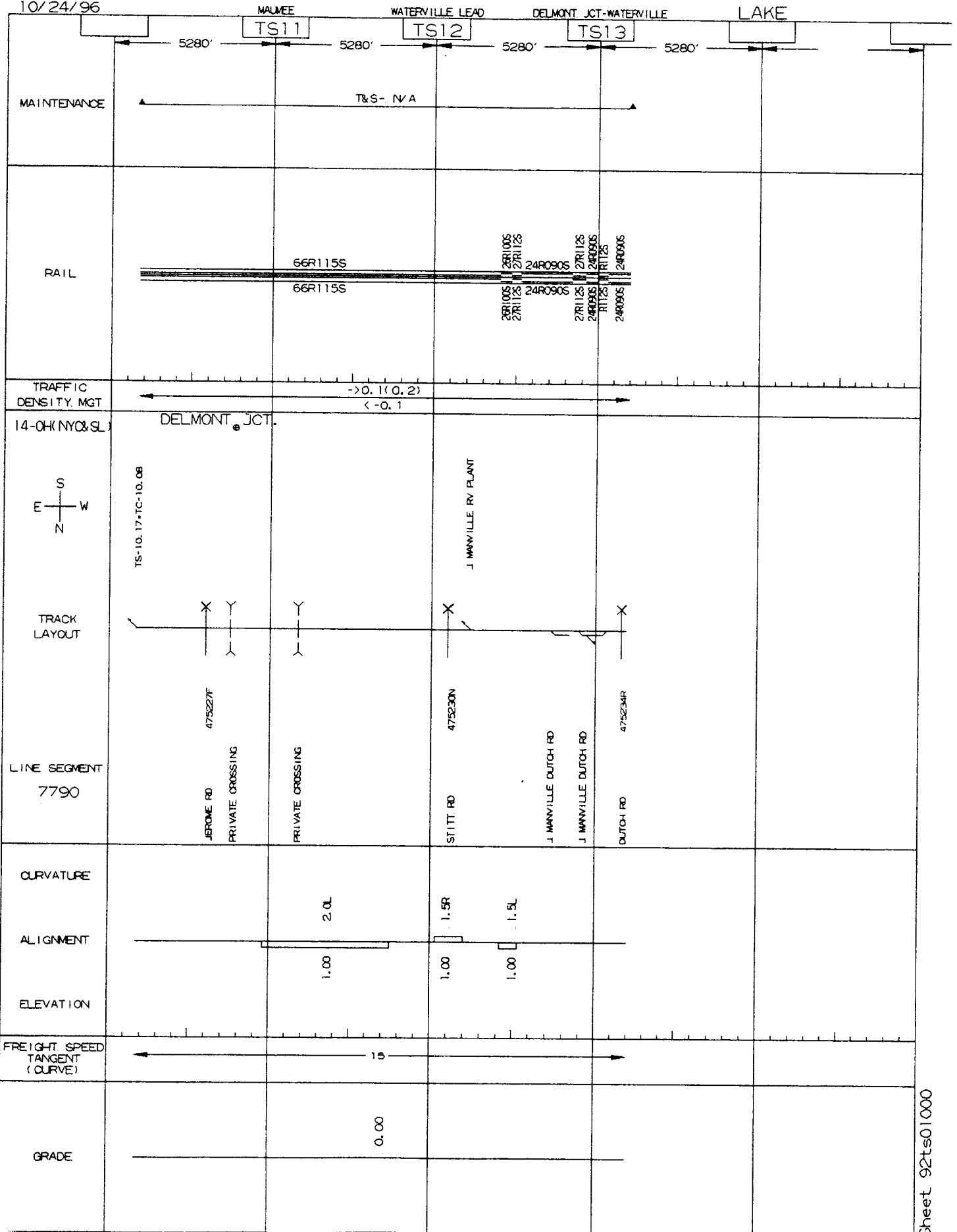
TOLEDO-DELMONT. JCT

LAKE

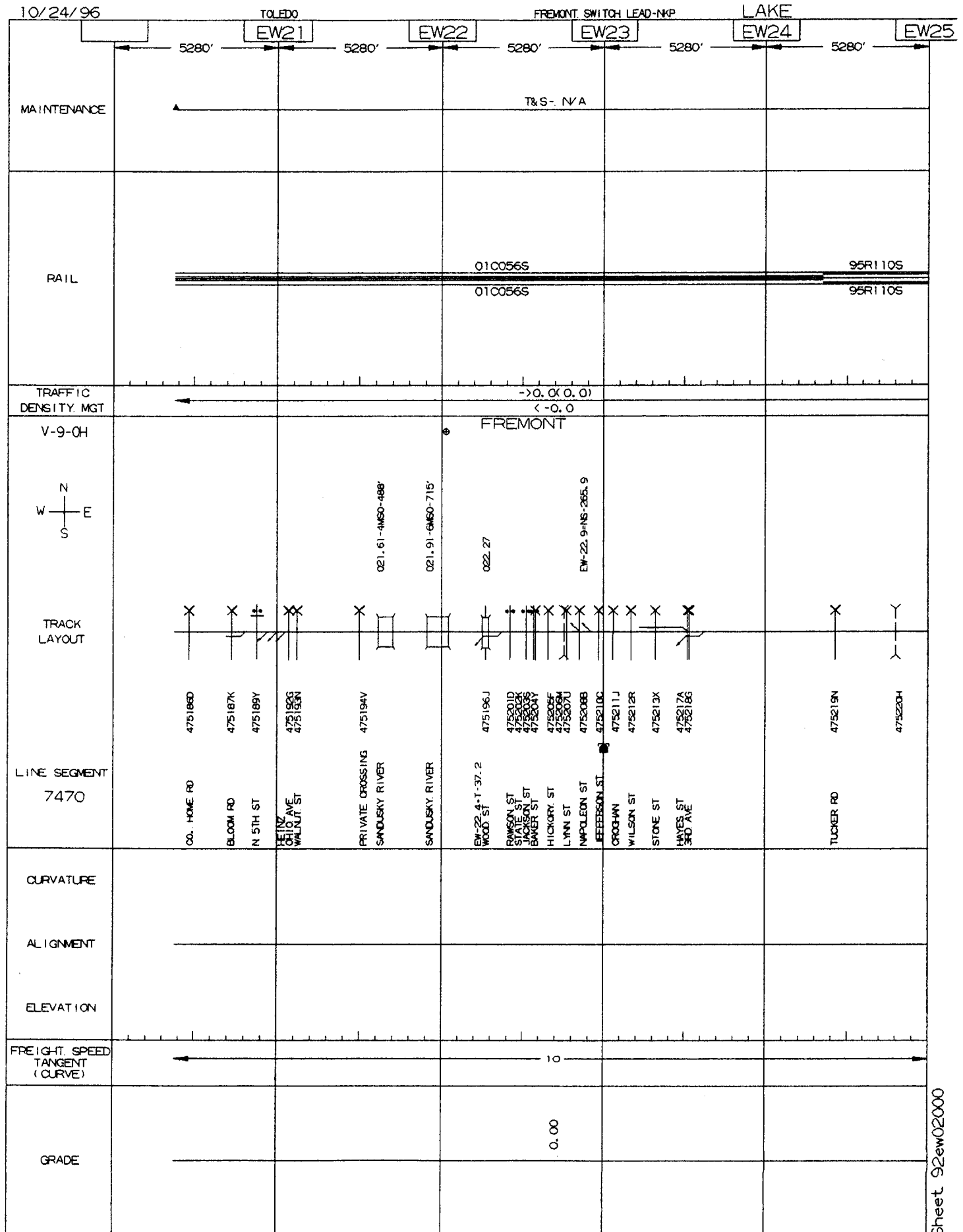


318

10/24/96



Sheet 92ts01000




320

10/24/96

TOLEDO

FREMONT SWITCH LEAD-NWP

LAKE

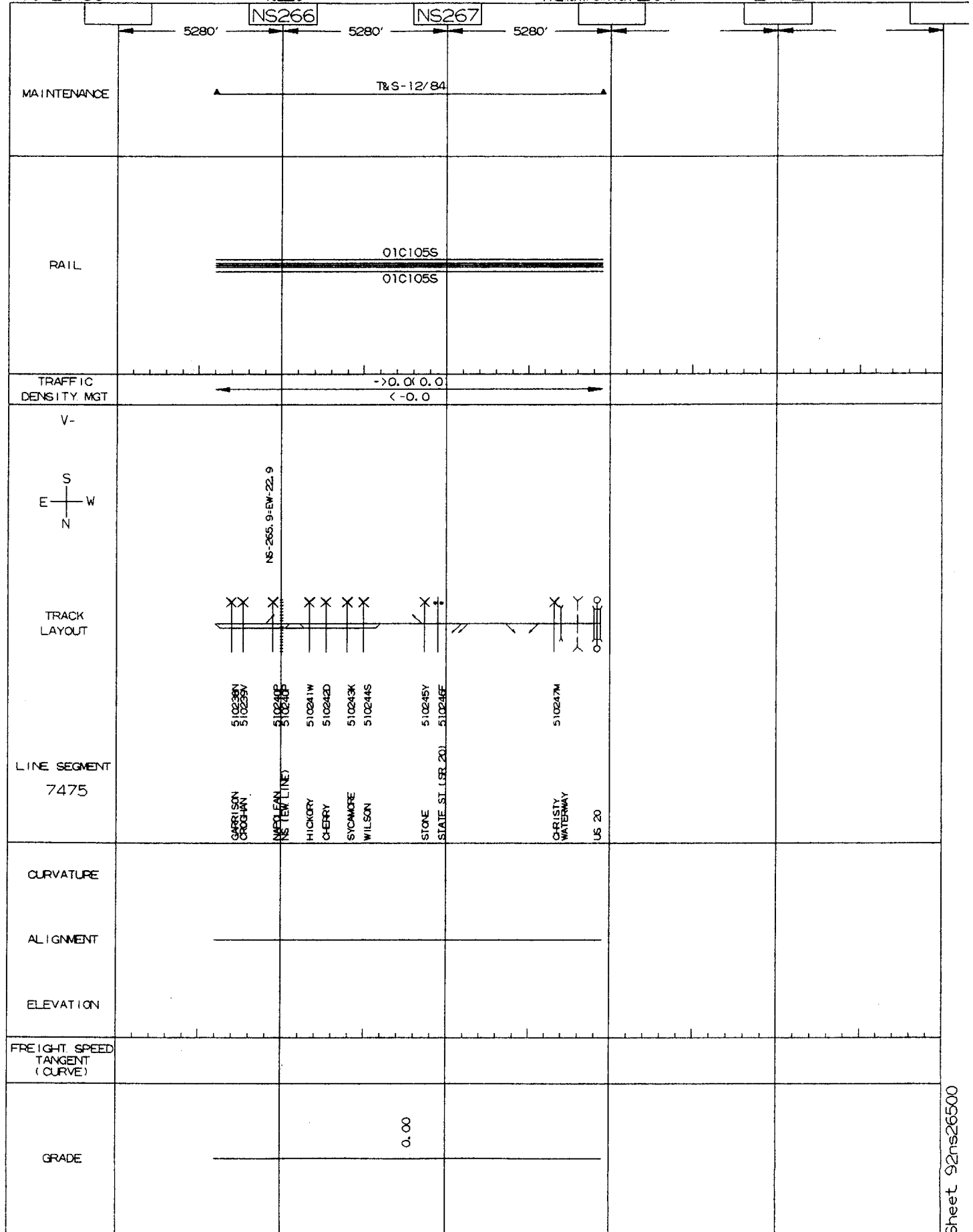
	EW25				
MAINTENANCE	T&S- N/A				
RAIL	01C075S 01C075S				
TRAFFIC DENSITY MGT	->0.0 (0.0) <-0.0				
DAVIDSON					
<div> <div>N W—E S</div> <div>           TRACK LAYOUT   </div> </div>					
LINE SEGMENT 7470					
CURVATURE					
ALIGNMENT					
ELEVATION					
FREIGHT SPEED TANGENT (CURVE)	10				
GRADE	0.00				

10/24/96

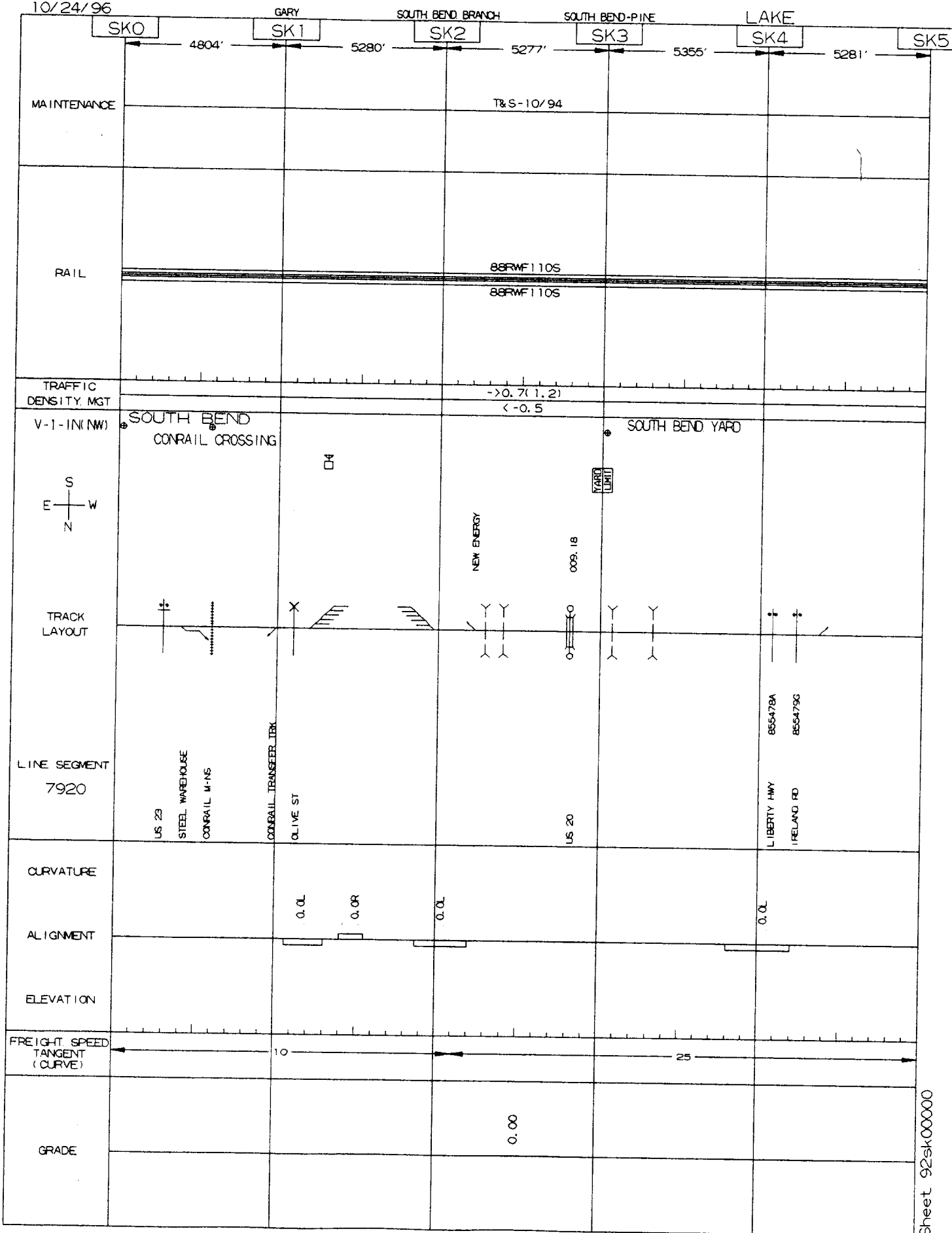
TOLEDO

FREMONT SWITCH LEAD-CR

LAKE



10/24/96



10/24/96

GARY

SOUTH BEND BRANCH

SOUTH BEND-PINE

LAKE

SK5

SK6

SK7

SK8

SK9

SK10

5278'

4731'

5174'

4809'

2243'

MAINTENANCE

T&amp;S-10/94

RAIL

88RWF110S

88RWF110S

88RWF110S

TRAFFIC  
DENSITY MGT

-&gt;0.7(1.2)

&lt;-0.5

V-1-IN(NW)

S  
E—W  
N

(YOST)

(SWEENEYS)

TRACK  
LAYOUT

Y

Y

+

Y

X

X

Y

X

Y

Y

002.32

LINE SEGMENT

7920  
7820865481H  
KERN RD865484D  
ROOSEVELT RD865485K  
MADISON RD

PRIVATE CROSSING

873077X  
NEW RD

PRIVATE CROSSING

OSBORNE RD

CURVATURE

0.0R

0.0L

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

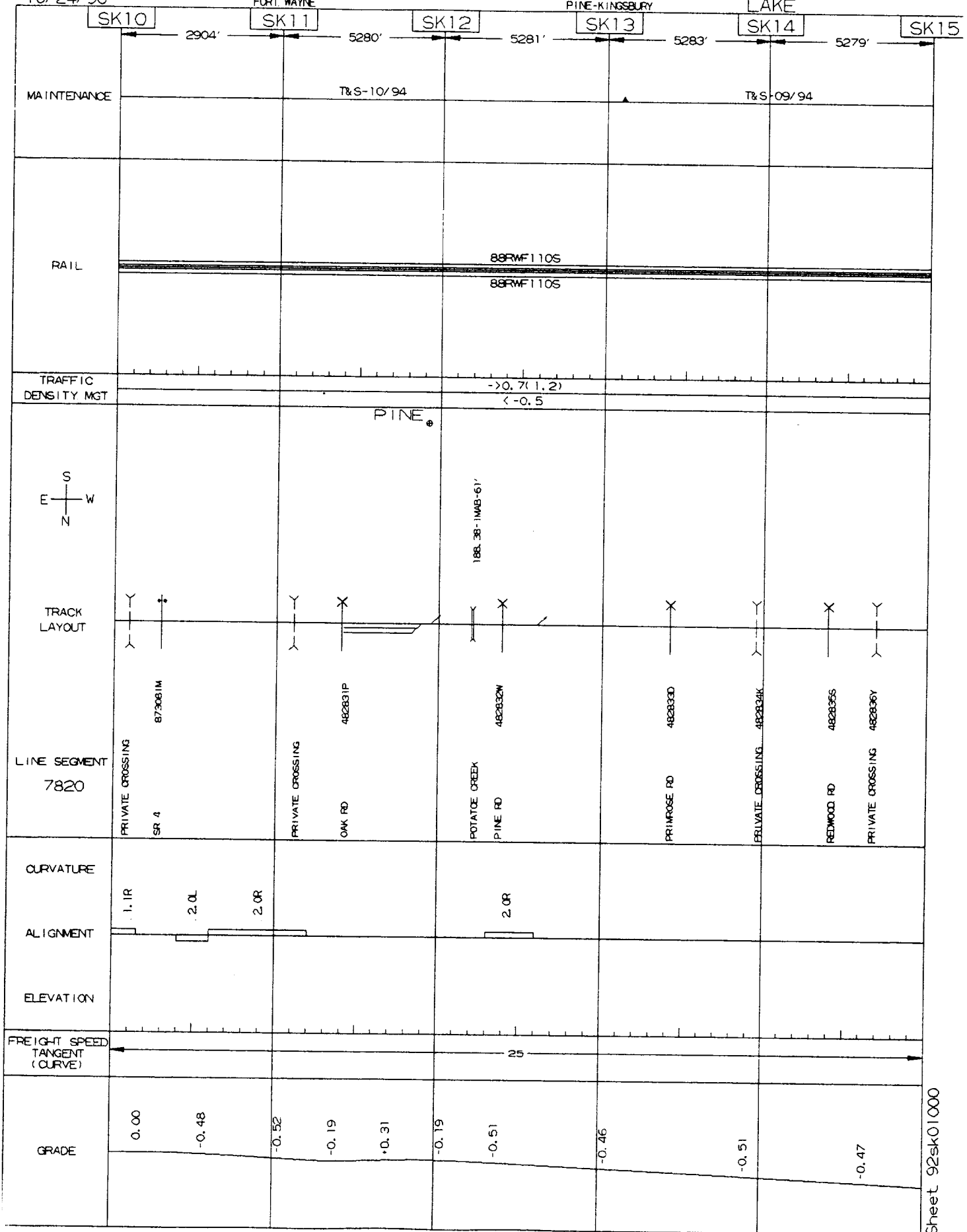
25

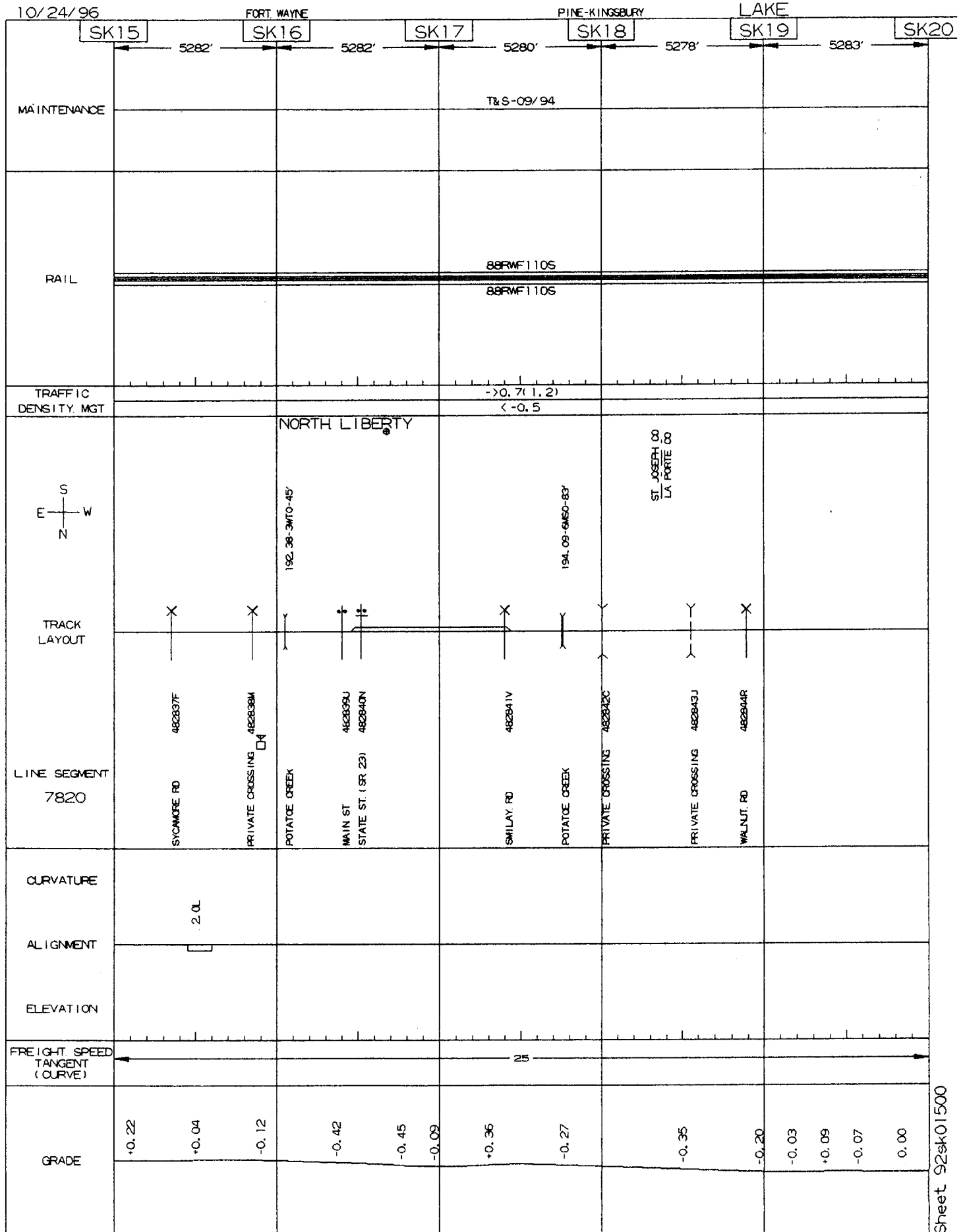
GRADE

0.00

0.00





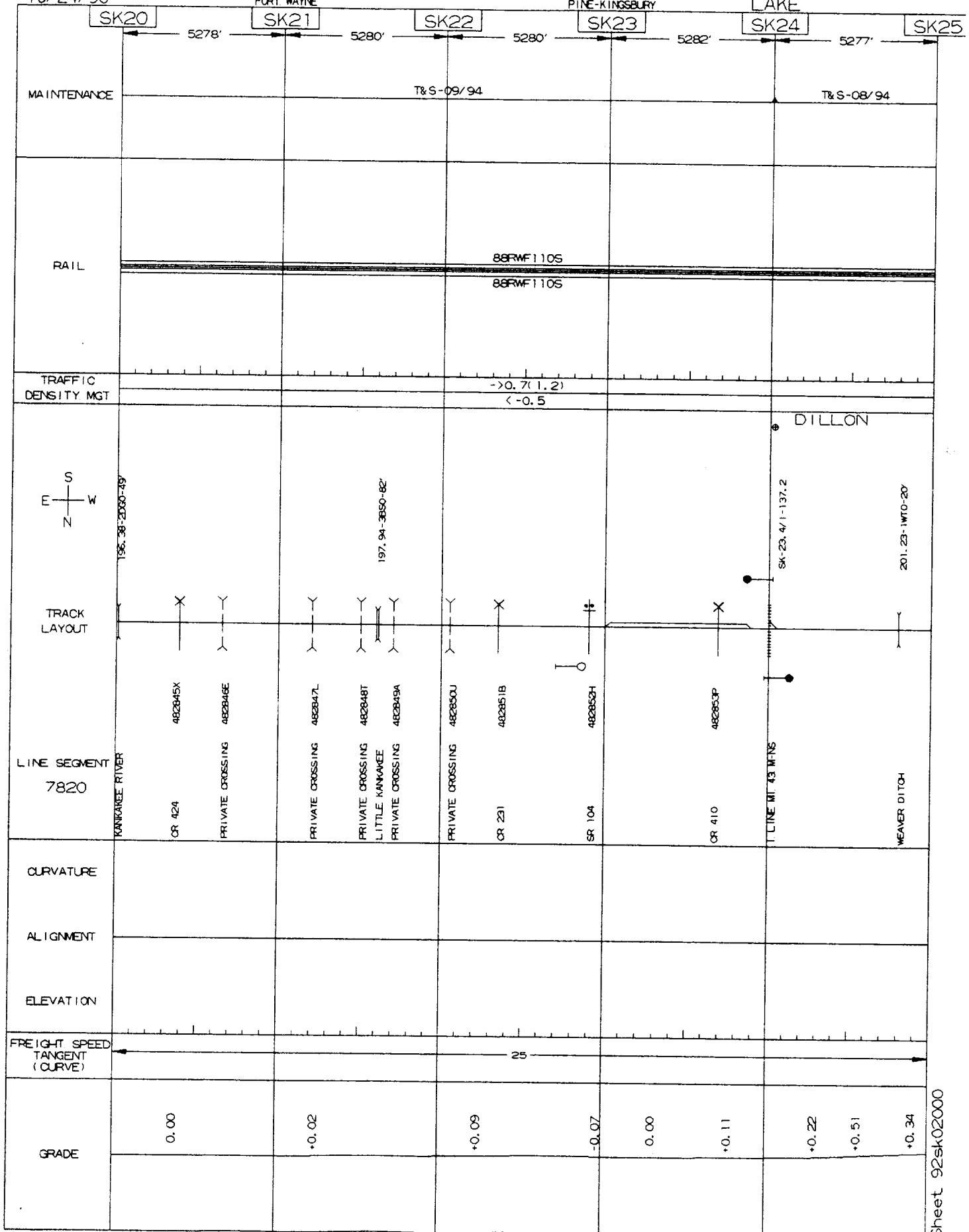


10/24/96

FORT WAYNE

PINE-KINGSBURY

LAKE

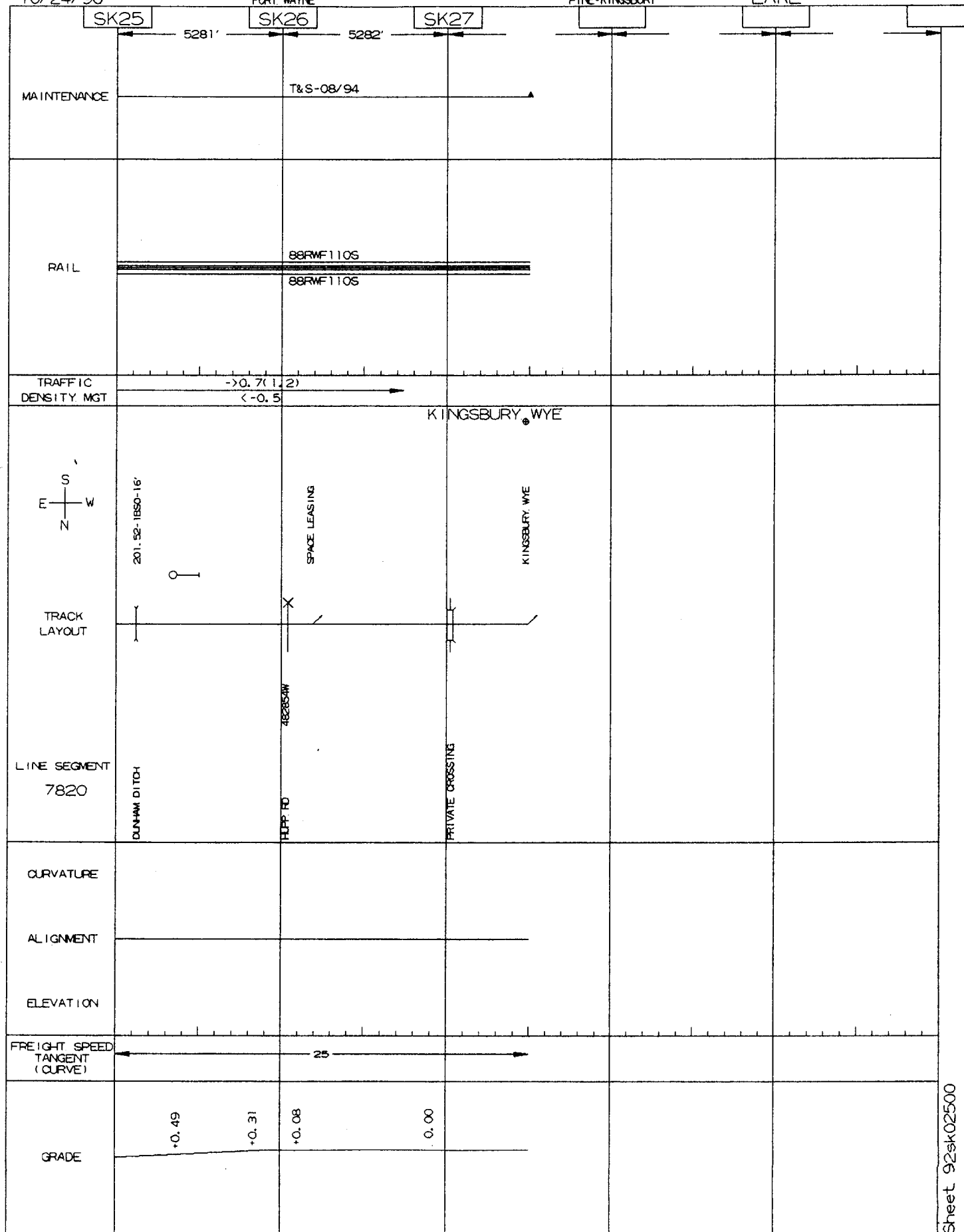


10/24/96

FORT WAYNE

PINE-KINGSBURY

LAKE



10/29/96

CHICAGO

MIKE-GARY

LAKE

PC320

MAINTENANCE

T&S-12/85

RAIL

49R133S

49R133S

TRAFFIC  
DENSITY MGT

->0.1 (0.3)

<-0.2

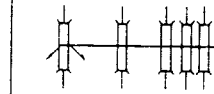
3-IN(PFW&C)

MIKE

(FT. WAYNE)

S  
E—W  
N

TRACK  
LAYOUT



LINE SEGMENT  
7701

PC-319, 20+0-146.17

532841K

532843Y

532845M

532846J

532847B

HANNA ST

LAFAYETTE ST

CLINTON ST

CALHOUN ST

HARRISON ST

CURVATURE

ALIGNMENT

ELEVATION

4.4R

3.5R

4.3R

1.50

1.50

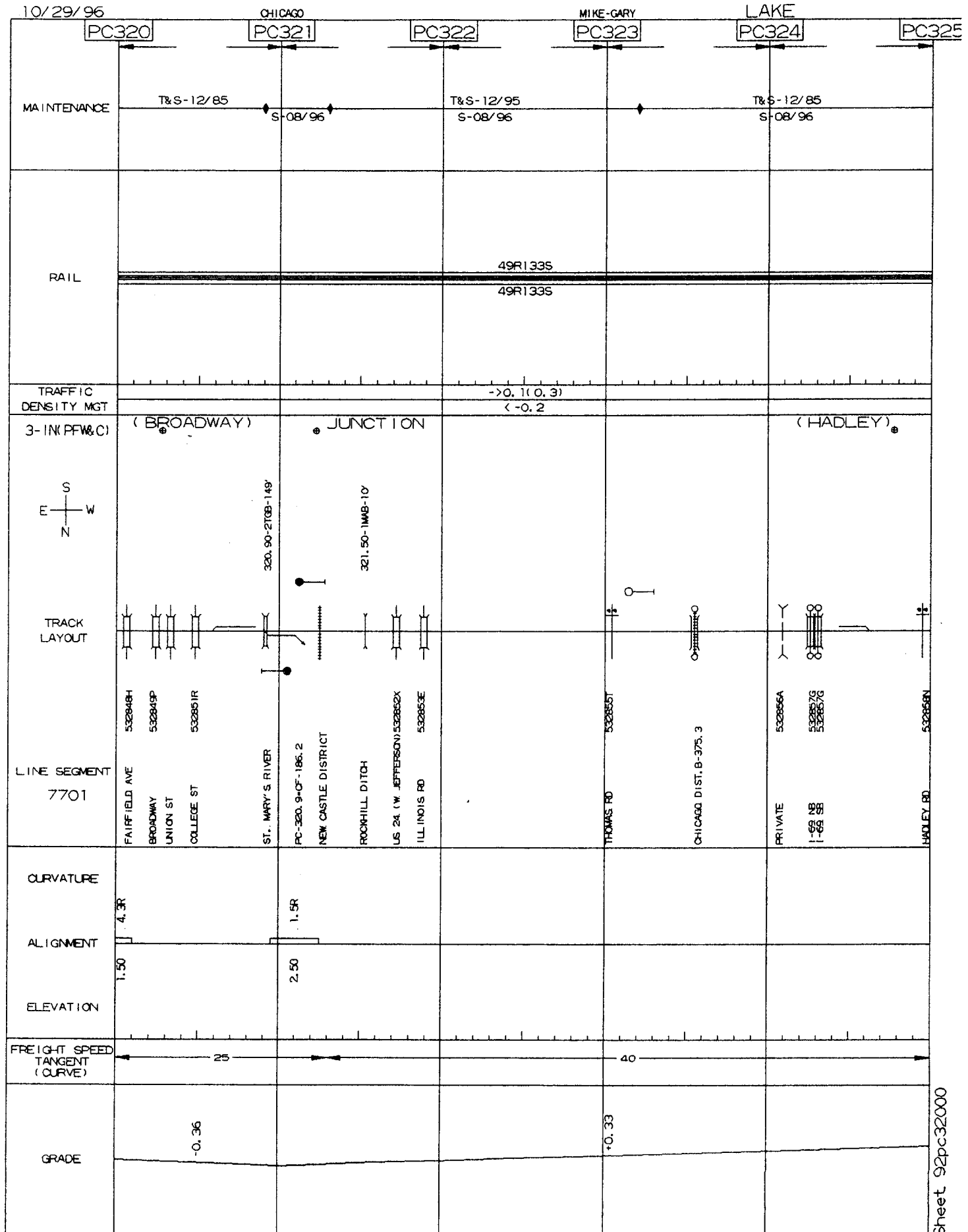
1.50

FREIGHT SPEED  
TANGENT  
(CURVE)

25

GRADE

-0.36



10/29/96

CHICAGO

MIKE-GARY

LAKE

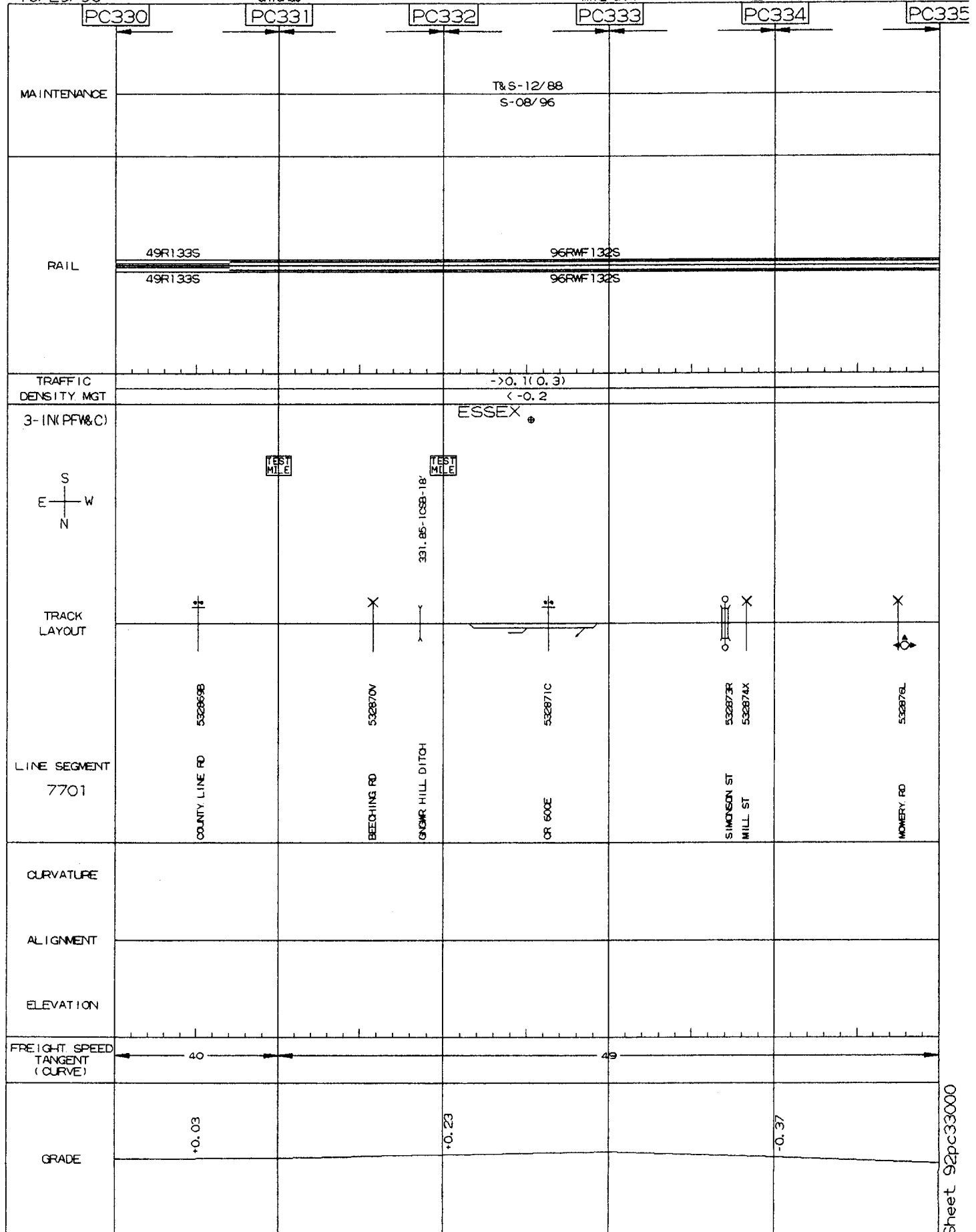
	PC325	PC326	PC327	PC328	PC329	PC330
MAINTENANCE		T&S-12/85 S-08/96			T&S-12/88 S-08/96	
RAIL			49R133S	49R133S		
TRAFFIC DENSITY MGT			->0.1 (0.3) <-0.2			
3-IN(PFW&C)				(ARCOLA)		
TRACK LAYOUT						
LINE SEGMENT 7701						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			40			
GRADE	+0.33	-0.21		+0.09	+0.03	

10/29/96

CHICAGO

MIKE-GARY

LAKE





10/29/96

CHICAGO

MIKE-GARY

LAKE

	PC335	PC336	PC337	PC338	PC339	PC340
MAINTENANCE				T&S-12/88 S-08/96		
RAIL				96RWF132S		
TRAFFIC DENSITY MGT				->0.1 (0.3) <-0.2		
3-IN (PF&C)				(COLUMBIA CITY)		
<div style="text-align: center;">           S            E — W            N         </div>						
TRACK LAYOUT						
LINE SEGMENT 7701						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)				49		
GRADE	+0.05	-0.16	+0.43	-0.16	+0.40	

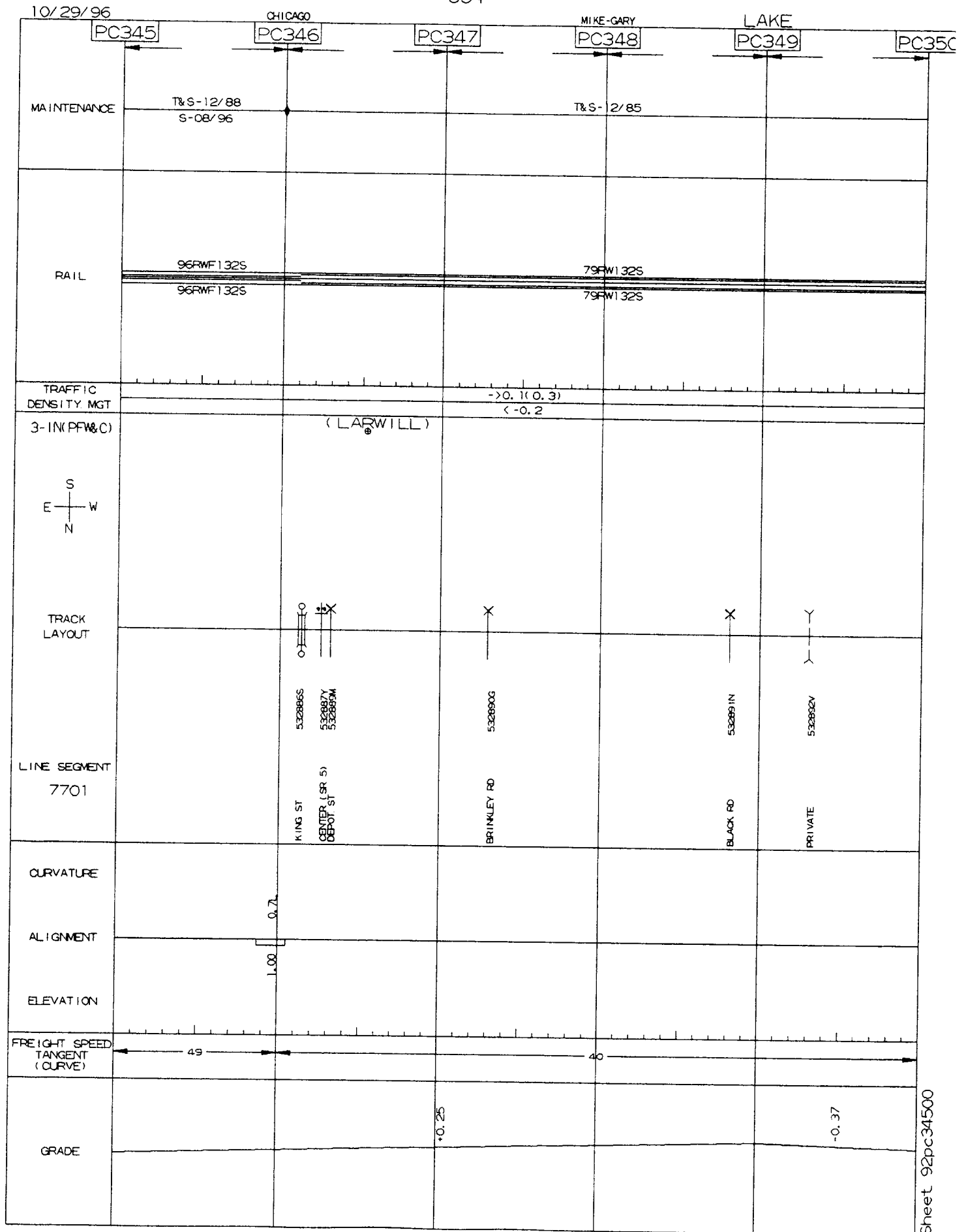
10/29/96

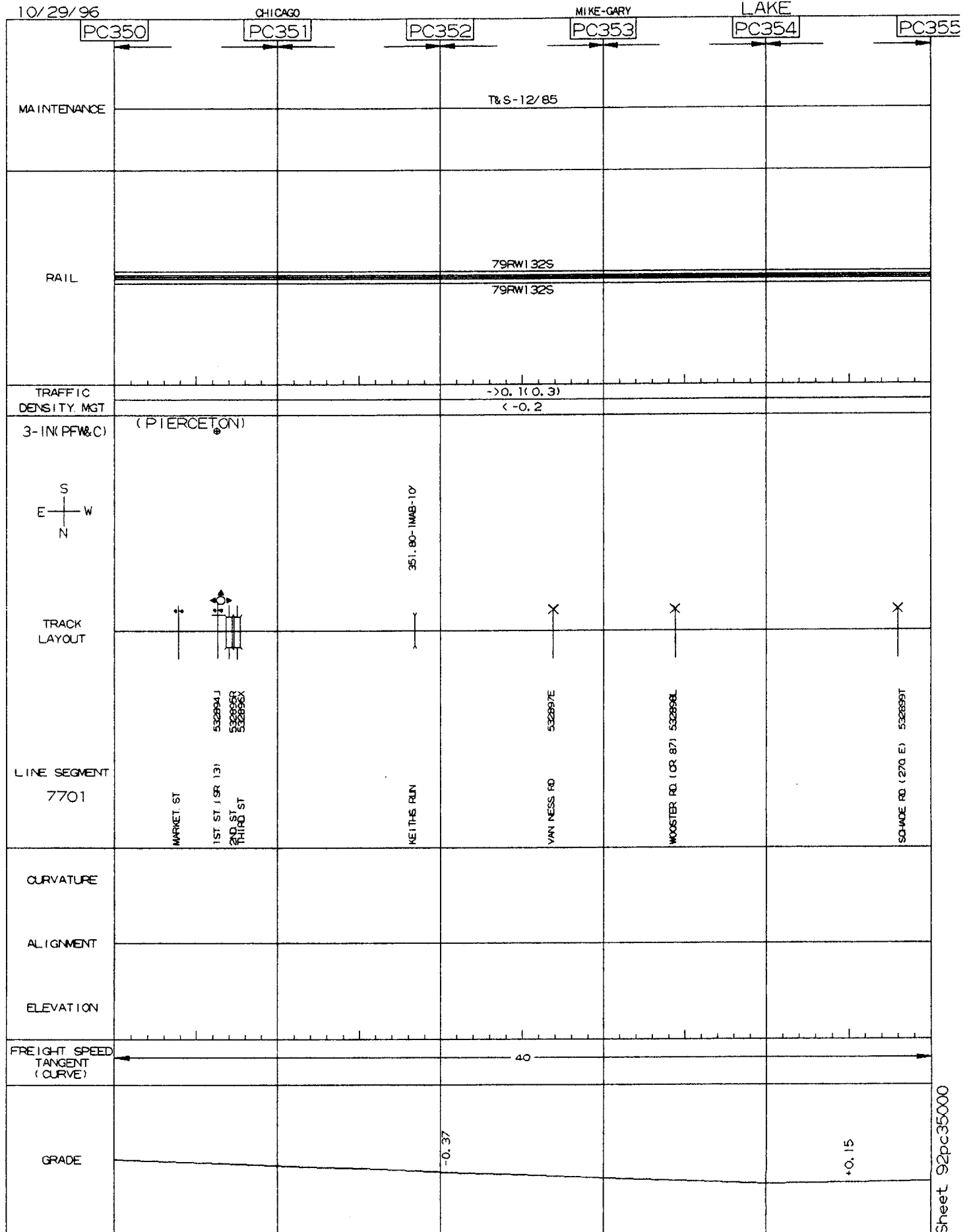
CHICAGO

MIKE-GARY

LAKE

	PC340	PC341	PC342	PC343	PC344	PC345
MAINTENANCE						
RAIL						
TRAFFIC DENSITY MGT						
3-IN (PF&C)						
TRACK LAYOUT						
LINE SEGMENT 7701						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)						
GRADE						





10/29/96

CHICAGO

MIKE-GARY

LAKE

PC355

PC356

PC357

PC358

PC359

PC360

MAINTENANCE

T&S-12/85

RAIL

79RW132S

79RW132S

TRAFFIC  
DENSITY MGT

-> 0.1 (0.3)  
< -0.2

3-IN (PFV&C)

WINONA

WARSAW



TRACK  
LAYOUT

LINE SEGMENT  
7701

GRANT ST (SR 25) 532900K

ARGONE RD  
FACTORY ENTRANCE 532901S  
532902Y

BERNARD ST 532903F

MAPLE ST 532904M

SCOTT ST 532905J

CONRAIL ST (SR 15) 532906B

DETROIT ST 532907H

INDIANA ST 532908P

BLUE ISLAND ST 532909N

WASHINGTON ST 532910X

COLUMBIA ST 532911Z

UNION ST 532912L

WALNUT CREEK  
PALESTINE RD 532914T

359, 55-1MAB-20'

CONRAIL  
TRACK

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

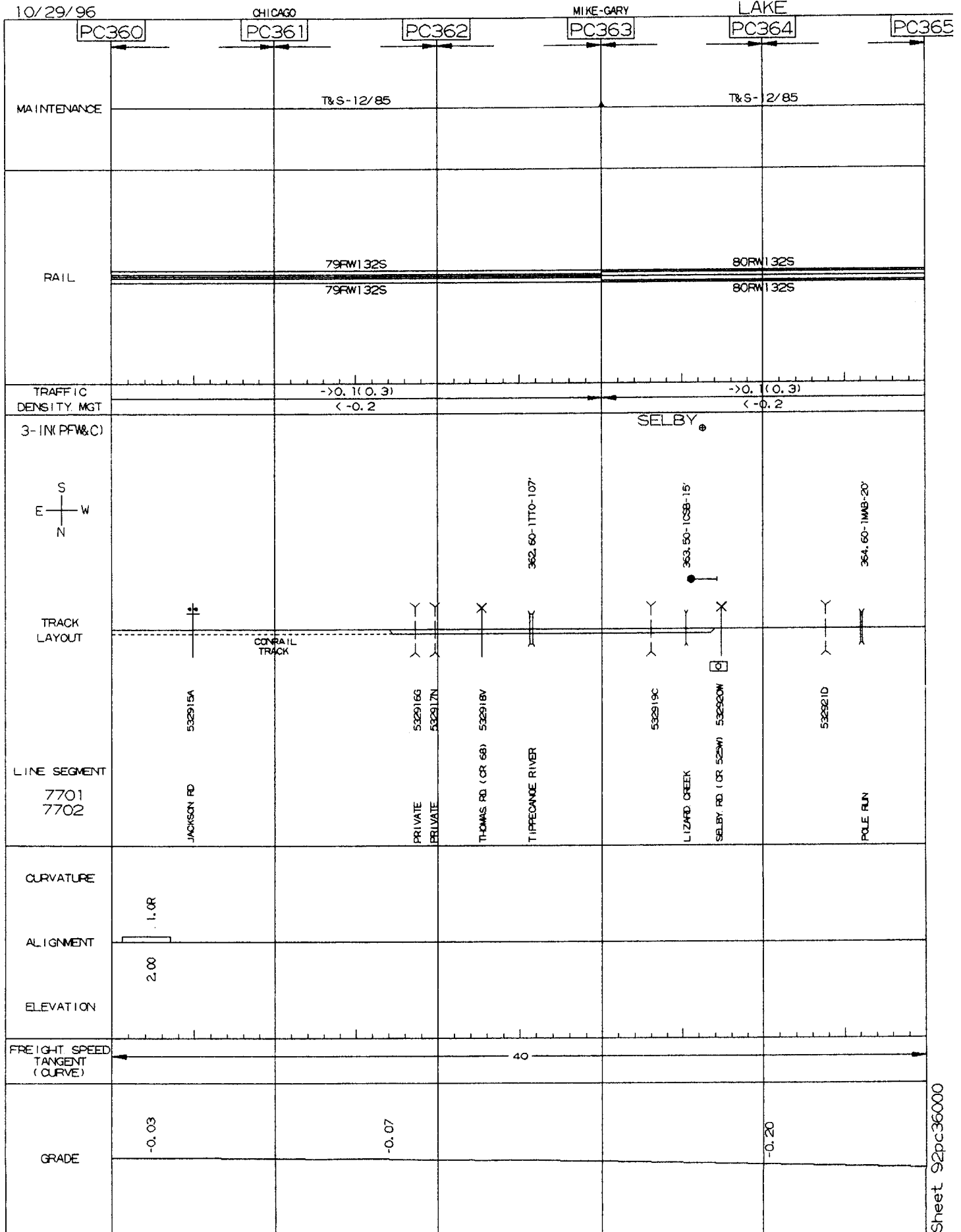
40

GRADE

-0.25

-0.09

-0.03



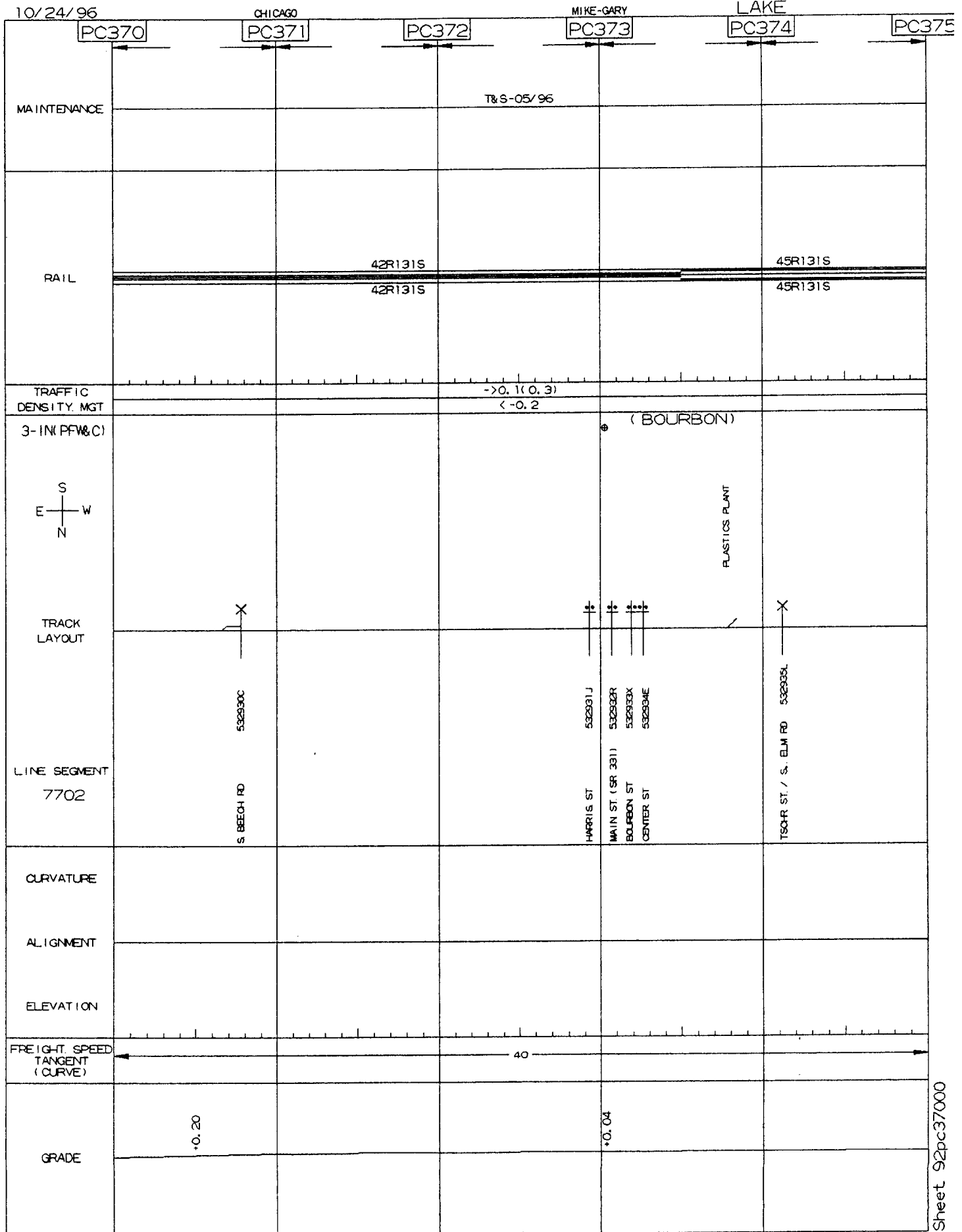
10/24/96

CHICAGO

MIKE-GARY

LAKE

	PC365	PC366	PC367	PC368	PC369	PC370
MAINTENANCE	T&S-12/85		T&S-04/96		T&S-05/96	
RAIL	80W132S 80W132S		42R131S 42R131S			
TRAFFIC DENSITY MGT			->0.1 (0.3) <-0.2			
3-IN (PF&C)	(ATWOOD)				(ETNA GREEN)	
<div style="text-align: center;">           S            E — W            N         </div>						
TRACK LAYOUT	X + X	X	X	X	X	X
LINE SEGMENT	532922K 532922S PRAIRIE ST HARRISON ST (ATWOOD)	PRIVATE RD	ROBINSON RD (800W) HOFFMAN OUTLET	SNYDER RD (OR 875W) CAMP CREEK	532926M 532927U COOK RD (OR 950W)	532928B 532929J WALNUT ST (SR 19) N PEARL ST
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			40			
GRADE	-0.20			+0.20		





10/24/96

CHICAGO

MIKE-GARY

LAKE

PC375

PC376

PC377

PC378

PC379

PC380

MAINTENANCE

T&amp;S-05/96

RAIL

45R131S

69R140S

60R133S

45R131S

69R140S

60R133S

TRAFFIC  
DENSITY MGT

→ 0.1 (0.3)

← -0.2

3-IN (PFV&amp;C)

(INWOOD)

S  
E — W  
N

TRACK  
LAYOUT

532936T

532937A

532938G

532939N

532940H

532941P

532942W

LINE SEGMENT  
7702

FIR RD

WYMER RD (OR 73)

COMPTON RD (OR 200)

HAWTHORNE RD

S. IRIS RD (OR 57)

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

40

GRADE

+0.04

0.00

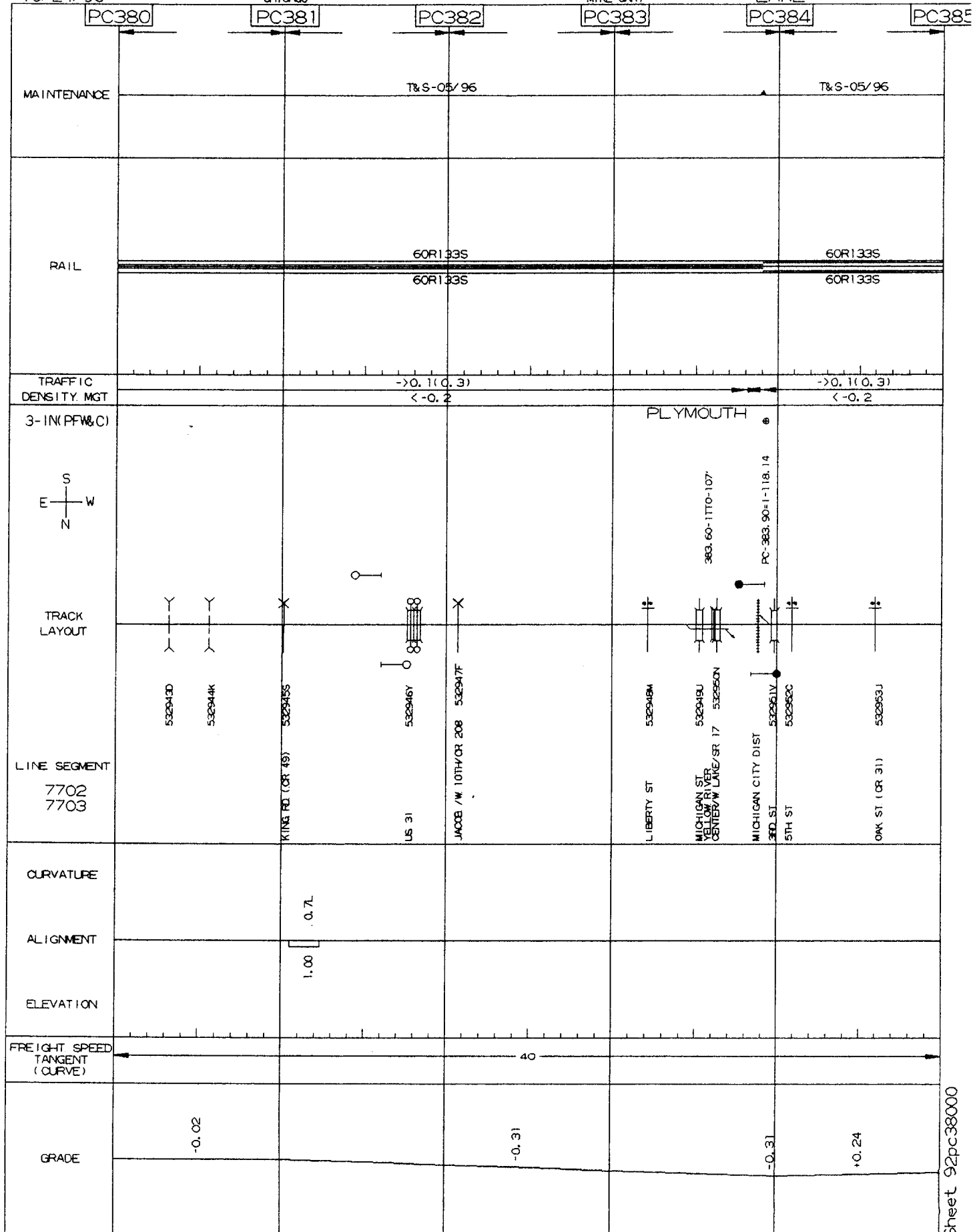
-0.02

10/24/96

CHICAGO

MIKE-GARY

LAKE

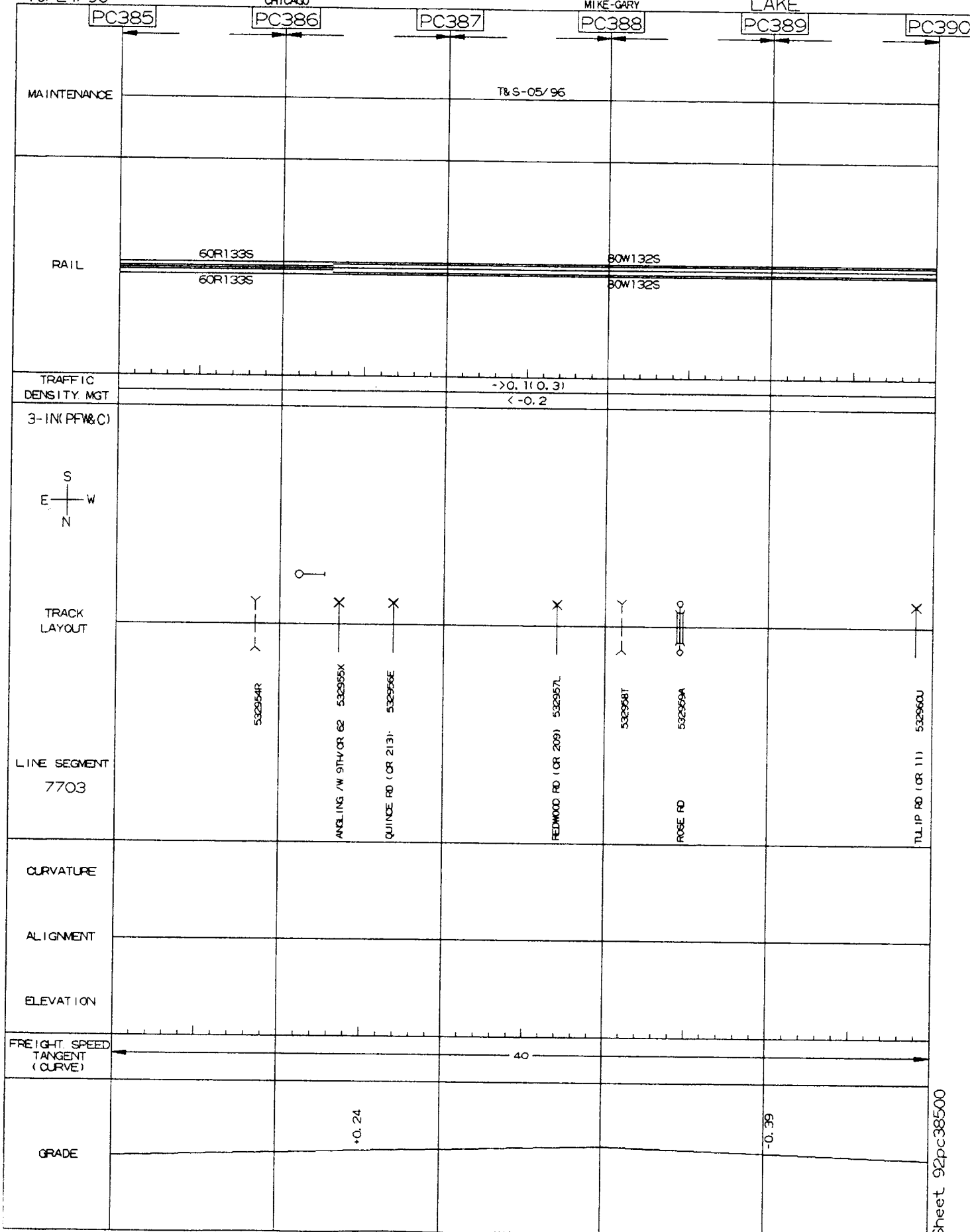


10/24/96

CHICAGO

MIKE-GARY

LAKE

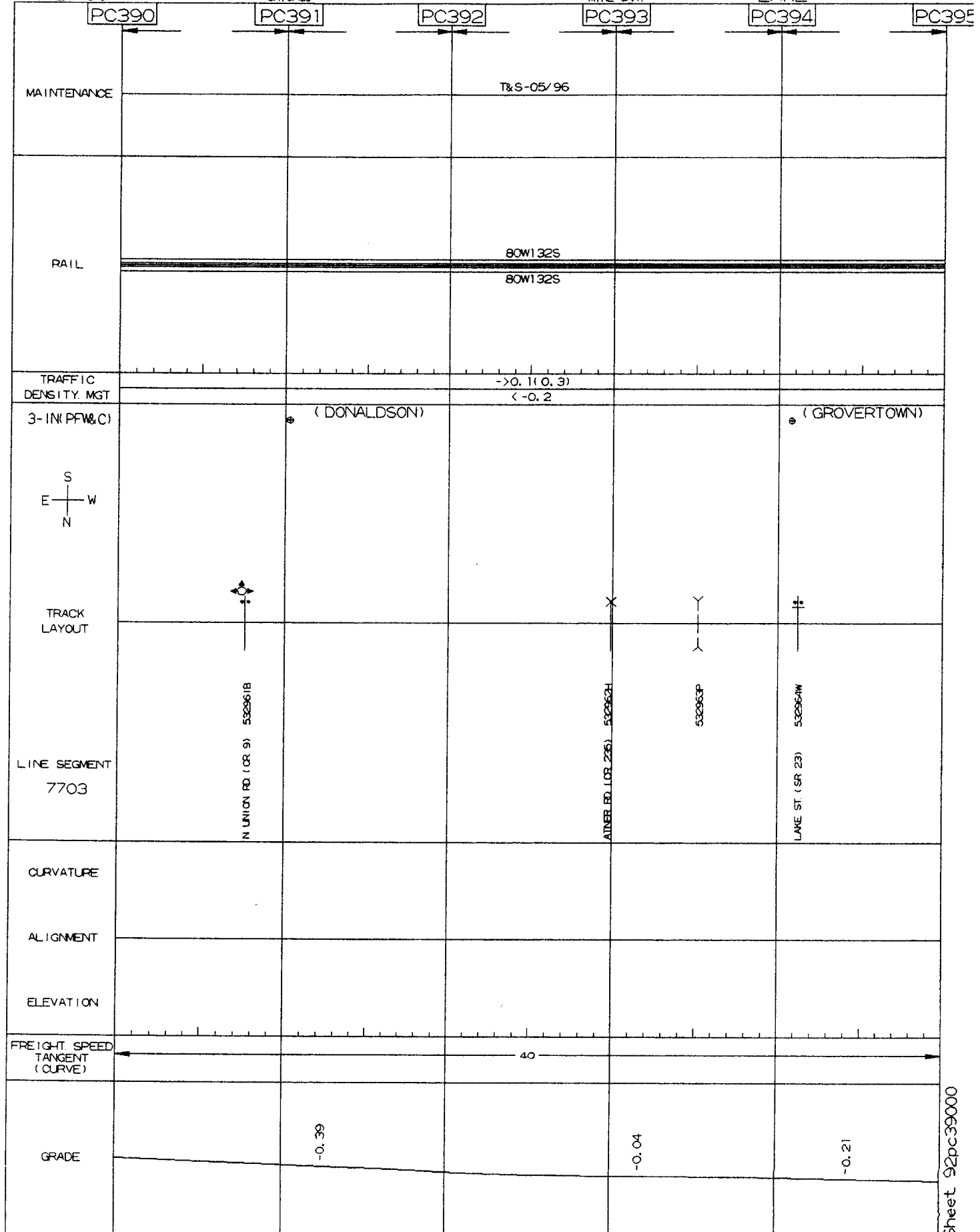


10/24/96

CHICAGO

MIKE-GARY

LAKE



10/24/96

CHICAGO

MIKE-GARY

LAKE

	PC395	PC396	PC397	PC398	PC399	PC400
MAINTENANCE						
RAIL						
TRAFFIC DENSITY MGT						
3-IN (PF&C)						
TRACK LAYOUT						
LINE SEGMENT						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)						
GRADE						

T&S-05/96

80W132S

80W132S

TRAFFIC DENSITY MGT

->0.1 (0.3)  
<-0.2

->0.0 (0.0)  
<-0.0

HAMLET

S  
E—W  
N

396.10-1850-30'

396.80-1838-19'

X

X

X

X

X

WALSH RD (OR 71) 532965D

JAIN DITCH

HALEY (OR 201) 532966K

KELLER DITCH

STARKE ST 532967S

JEFFERSON ST 532968Y

CHILDS ST (OR 500E) 532969F

LINE SEGMENT  
7703

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

40

GRADE

-0.21

-0.04

10/24/96

CHICAGO

MIKE-GARY

LAKE

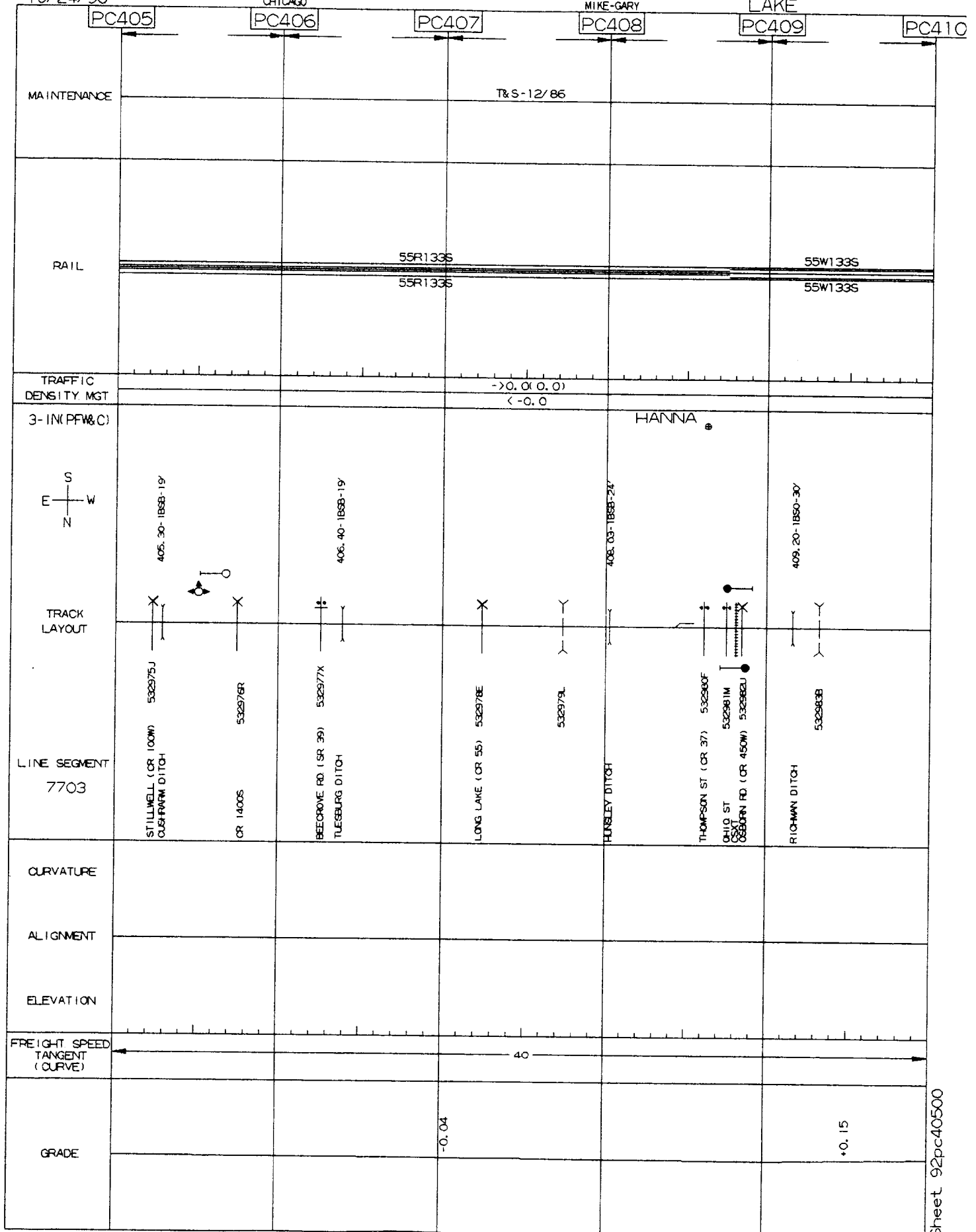
	PC400	PC401	PC402	PC403	PC404	PC405
MAINTENANCE				T&S-12/86		
RAIL	80W132S 80W132S			55R133S 55R133S		
TRAFFIC DENSITY MGT				->0.0(0.0) <-0.0		
3-IN(PFW&C)					(DAVIS)	
<div style="text-align: center;">           S            E — W            N         </div>	400.10-185B-11'	400.50-1700-56'	401.65-185B-19'	402.60-105B-22'		404.40-270B-131'
TRACK LAYOUT						
LINE SEGMENT	GRASSHOP DITCH US 30	ROBBINS DITCH	SO-HOOVER (OR 49)	STATE DITCH	SHEARIN DITCH	SO-KEDDER (OR 29)
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)				40		
GRADE				-0.04		

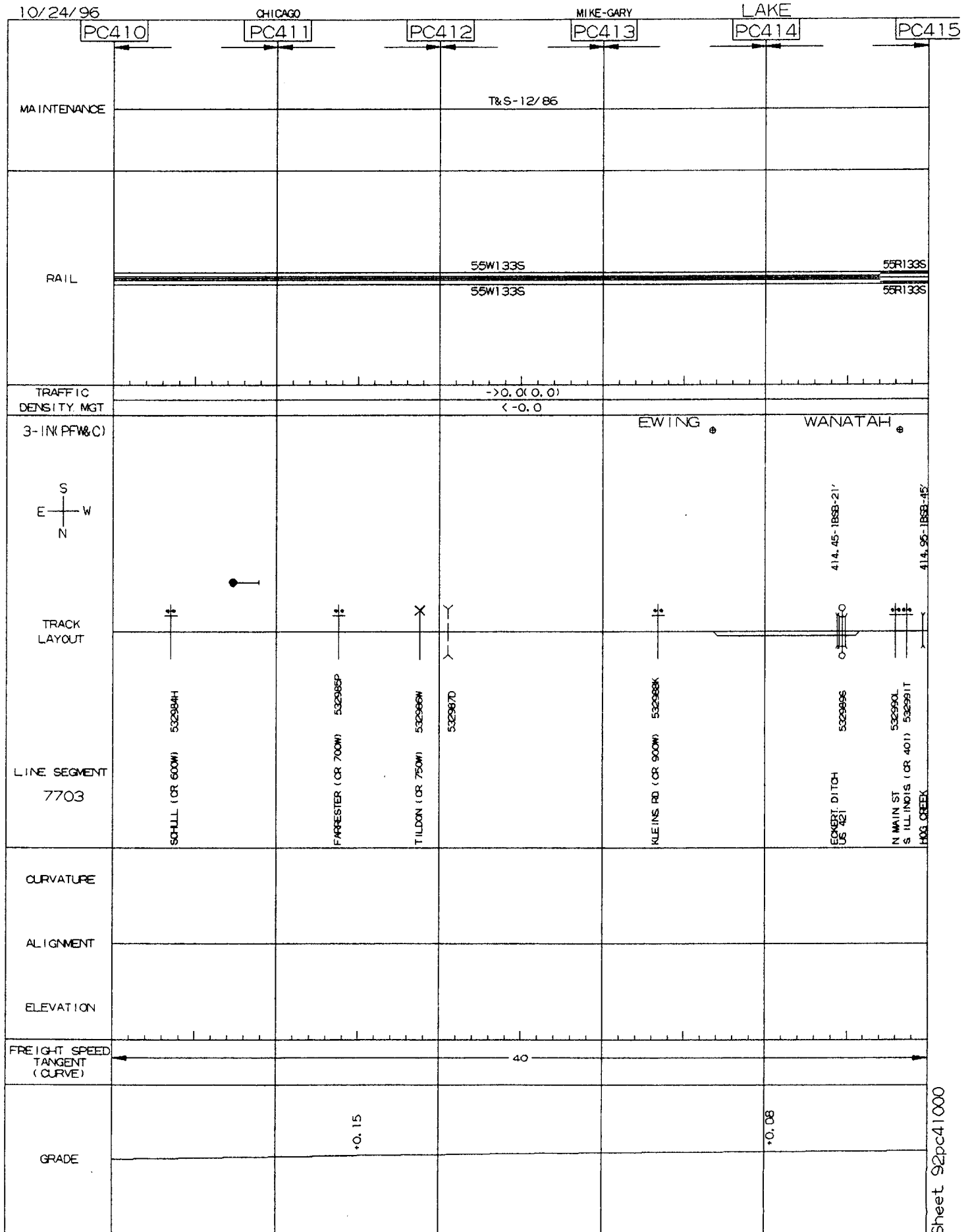
10/24/96

CHICAGO

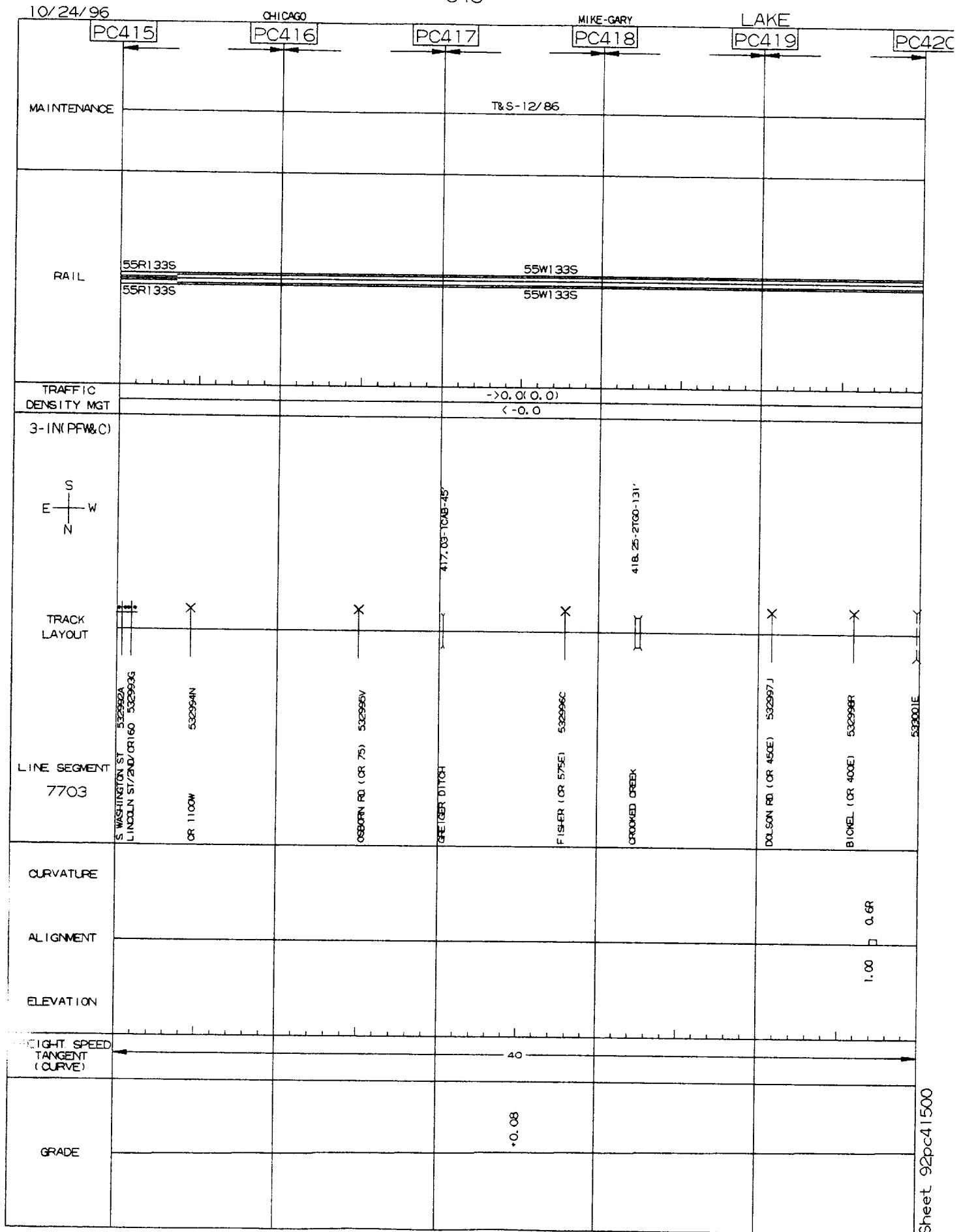
MIKE-GARY

LAKE









10/24/96

CHICAGO

MIKE-GARY

LAKE

	PC420	PC421	PC422	PC423	PC424	PC425
MAINTENANCE						
RAIL						
TRAFFIC DENSITY MGT						
3-IN (PF&C)						
TRACK LAYOUT						
LINE SEGMENT						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)						
GRADE						

T&amp;S-12/86

55W133S

55R133S

55W133S

55R133S

-&gt; 0.0 (0.0)

&lt; -0.0

(VALPARAISO)

S  
E—W  
N

420.30-1038-10'

TRACK  
LAYOUT

LINE SEGMENT  
7703

CATTLE PASS  
MONTAILE RD OR 325E1 533002L

PRIVATE RD 533003T

SR 49  
SR 49

CEMETERY RD (OR 175) 533005G

LINCOLN HWY (US 30) 533006N

GREENWICH ST 522864P

AXE ST 522865W

FRANKLIN ST 522866D

WASHINGTON ST 522867K

LAFFETTE ST 522868S

NAPOLÉON ST 522869Y

JOLIET ST 522870T

CLIFFORD KING OR 391 522871A

1.0R

1.0L

2.00

2.00

40

+0.08

-0.22

-0.30

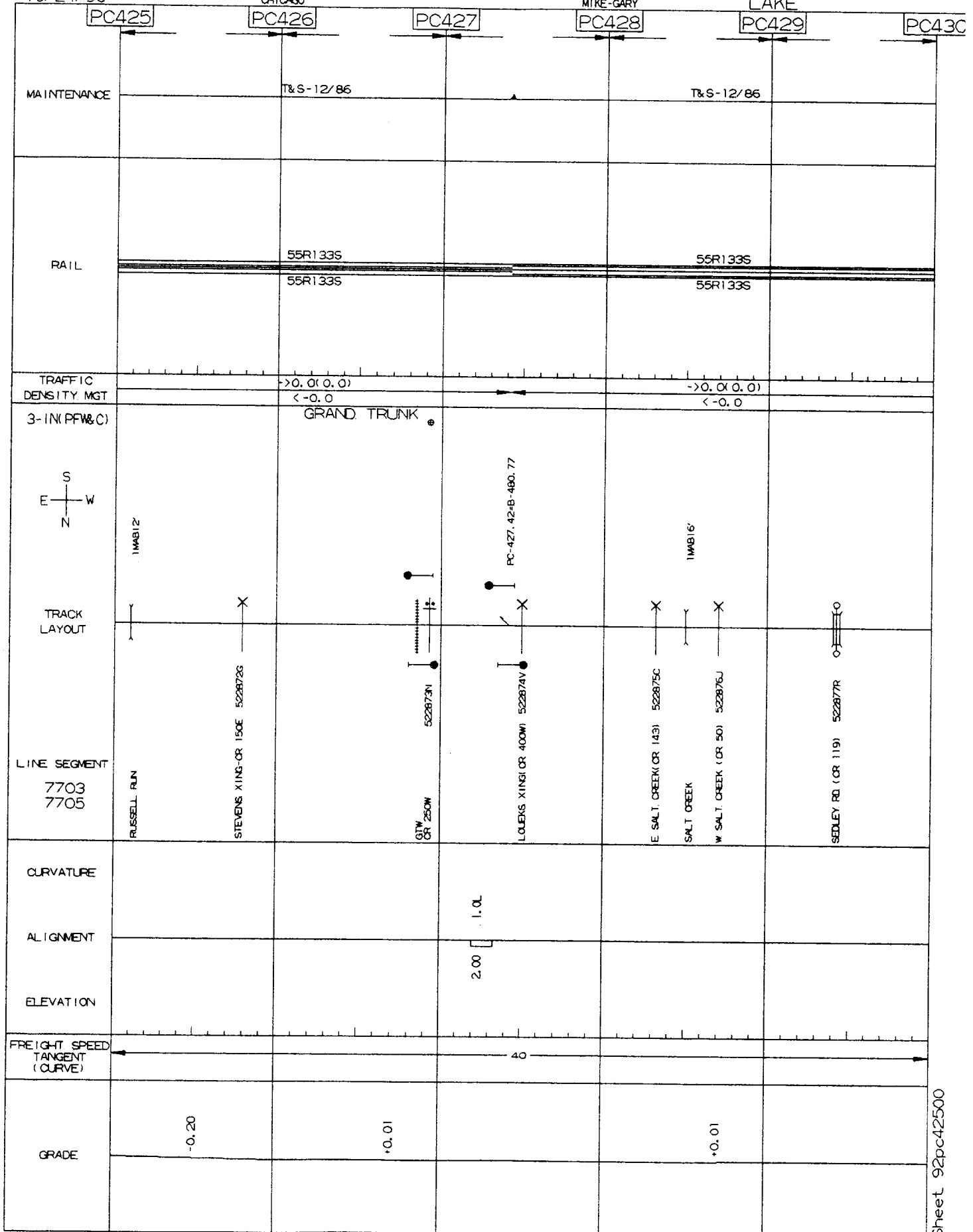
-0.20

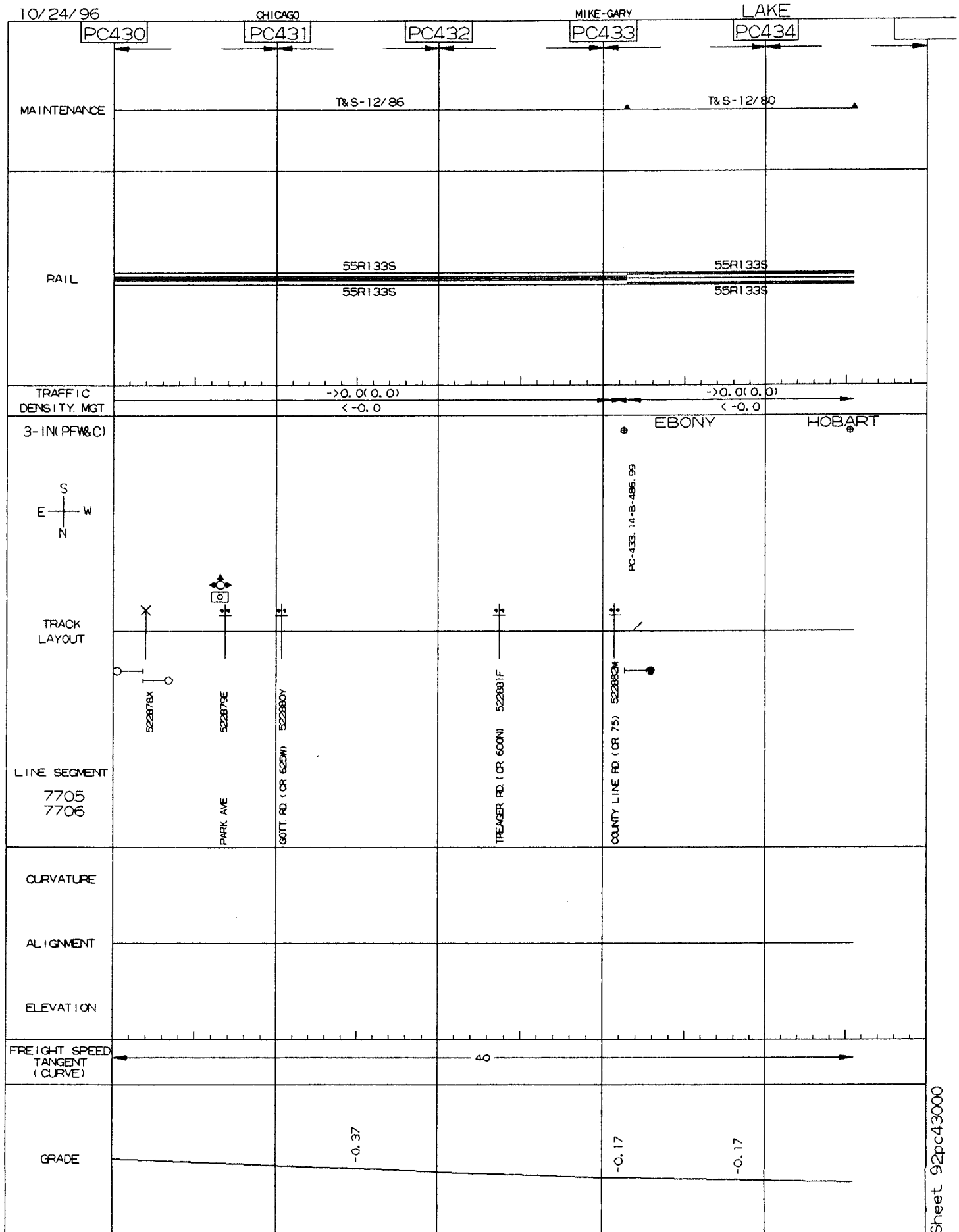
10/24/96

CHICAGO

MIKE-GARY

LAKE



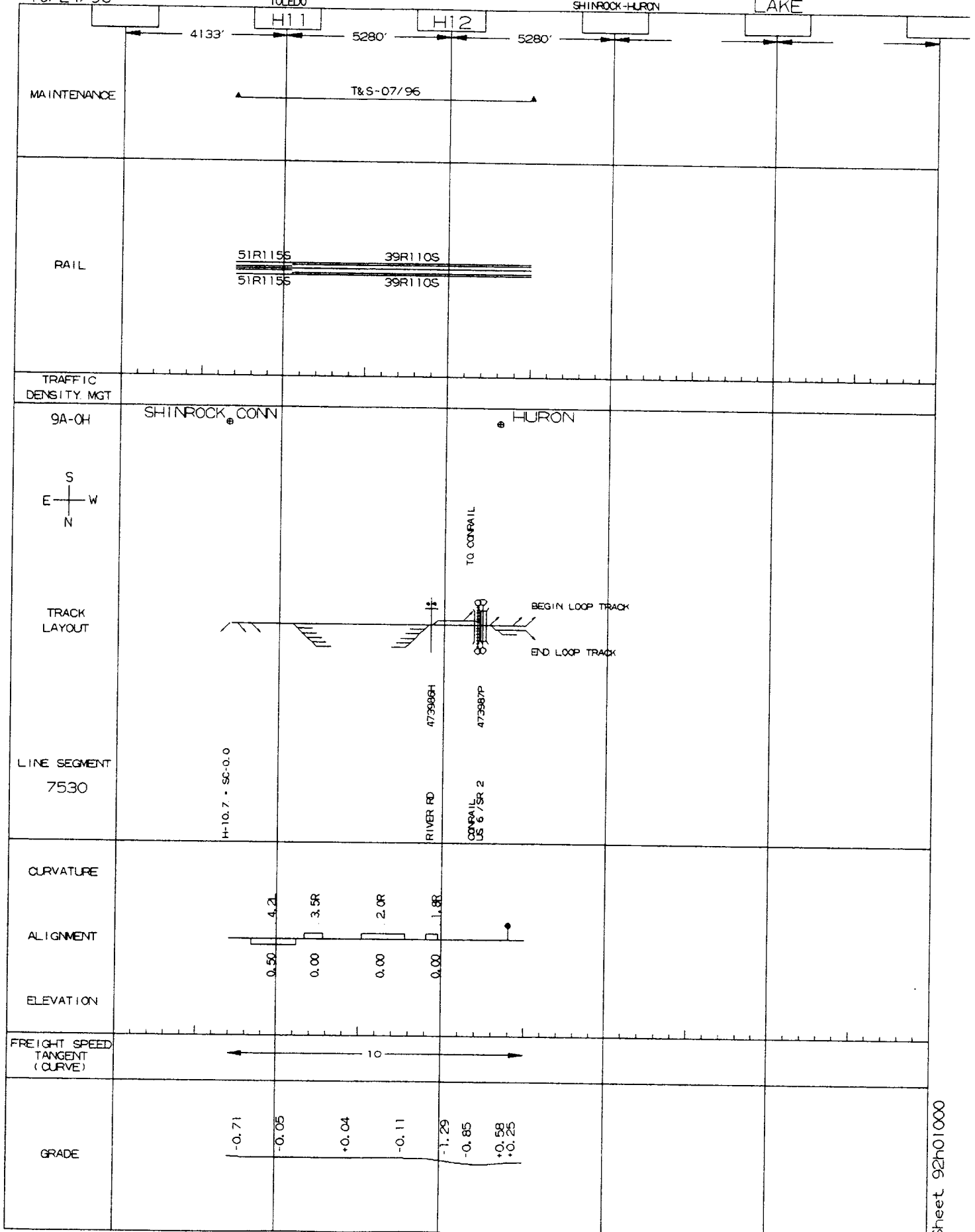


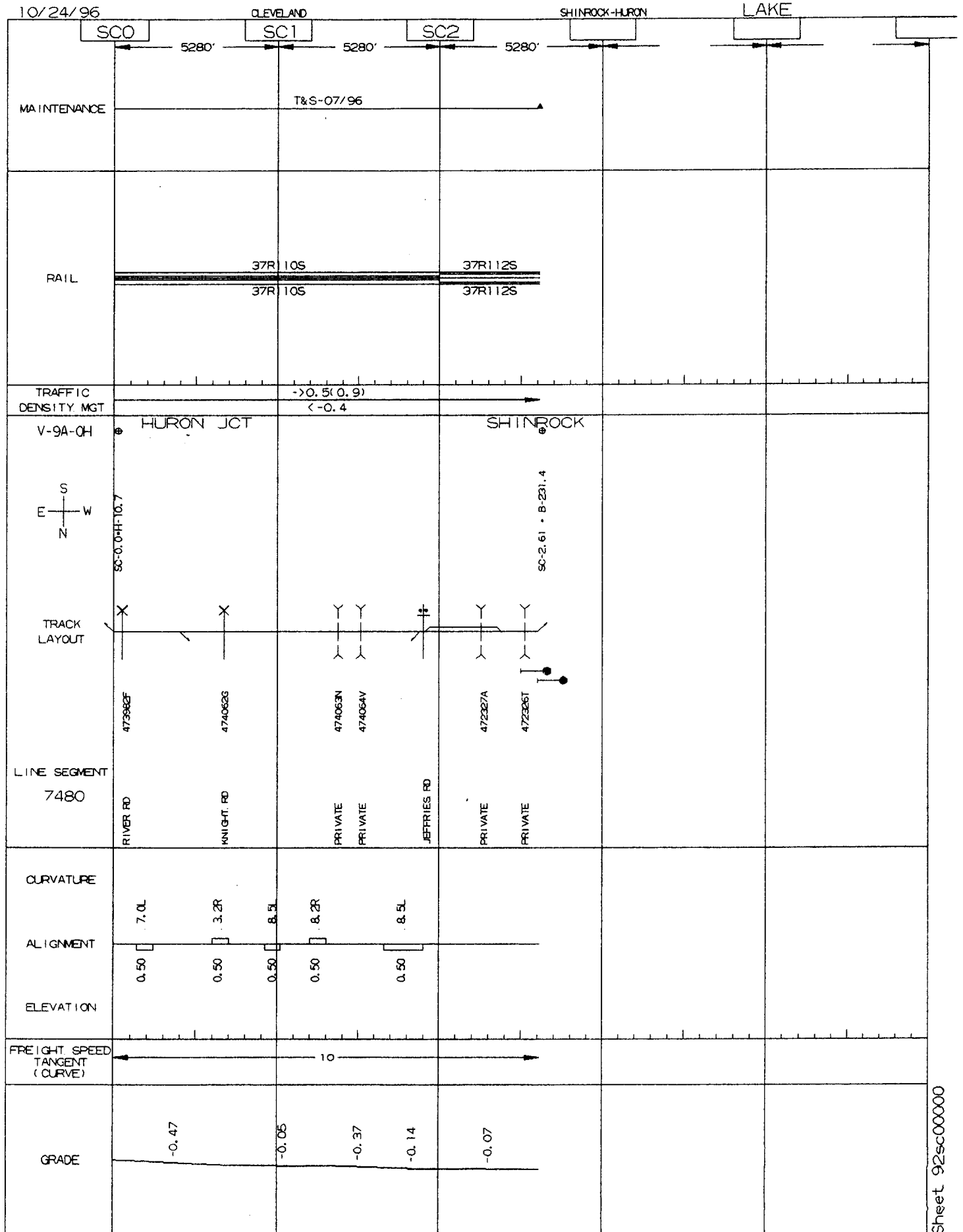
10/24/96

TOLEDO

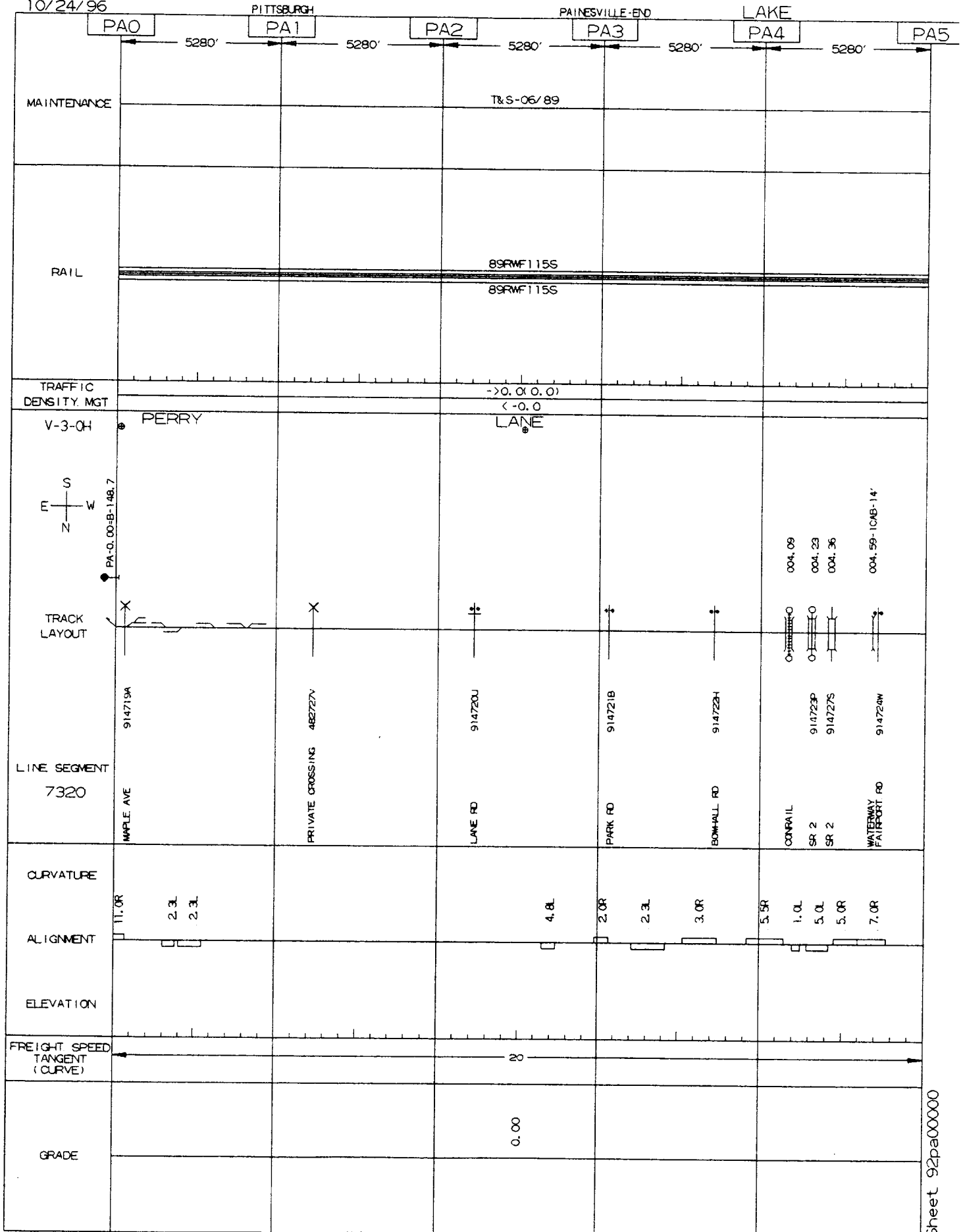
SHINROCK-HURON

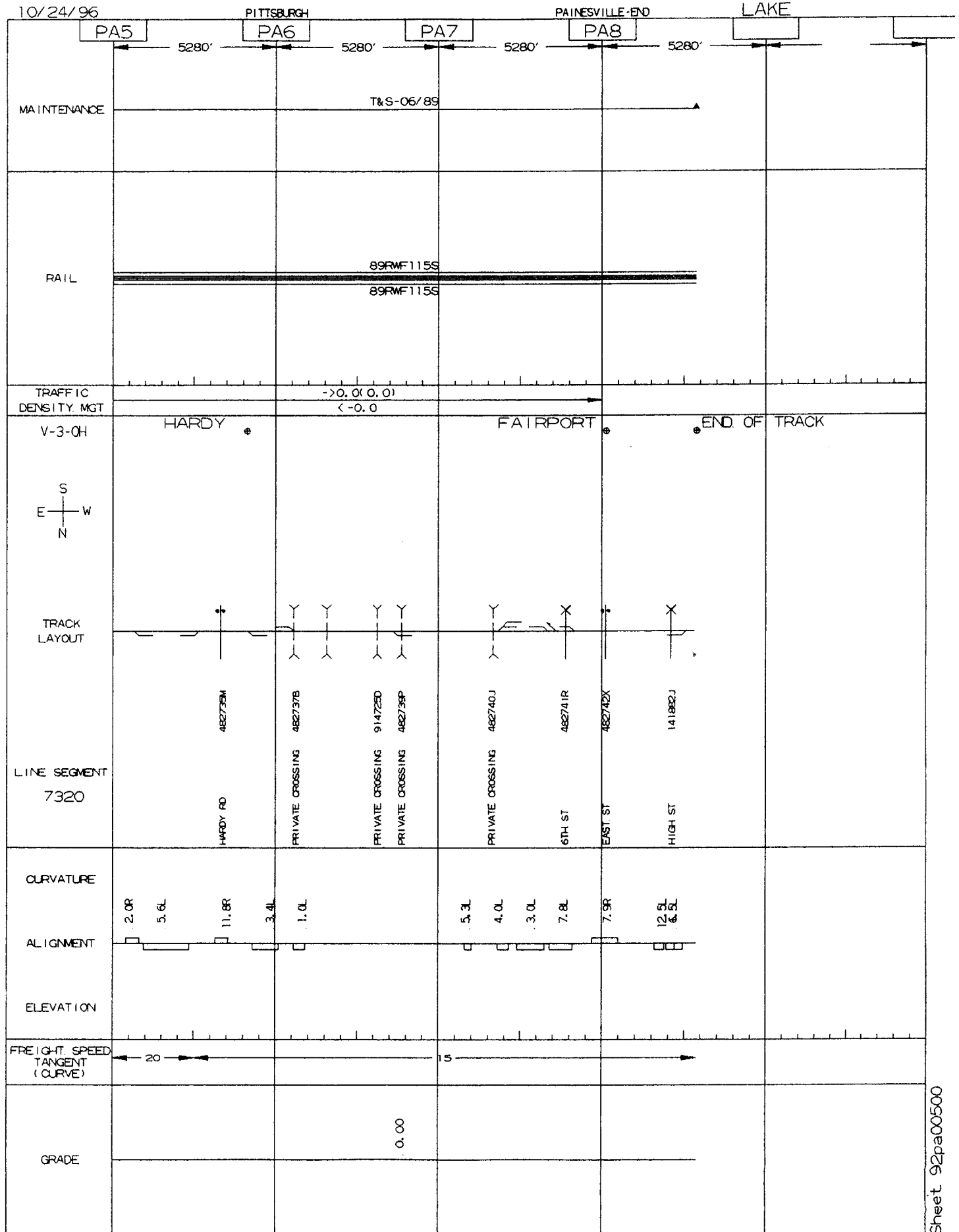
LAKE





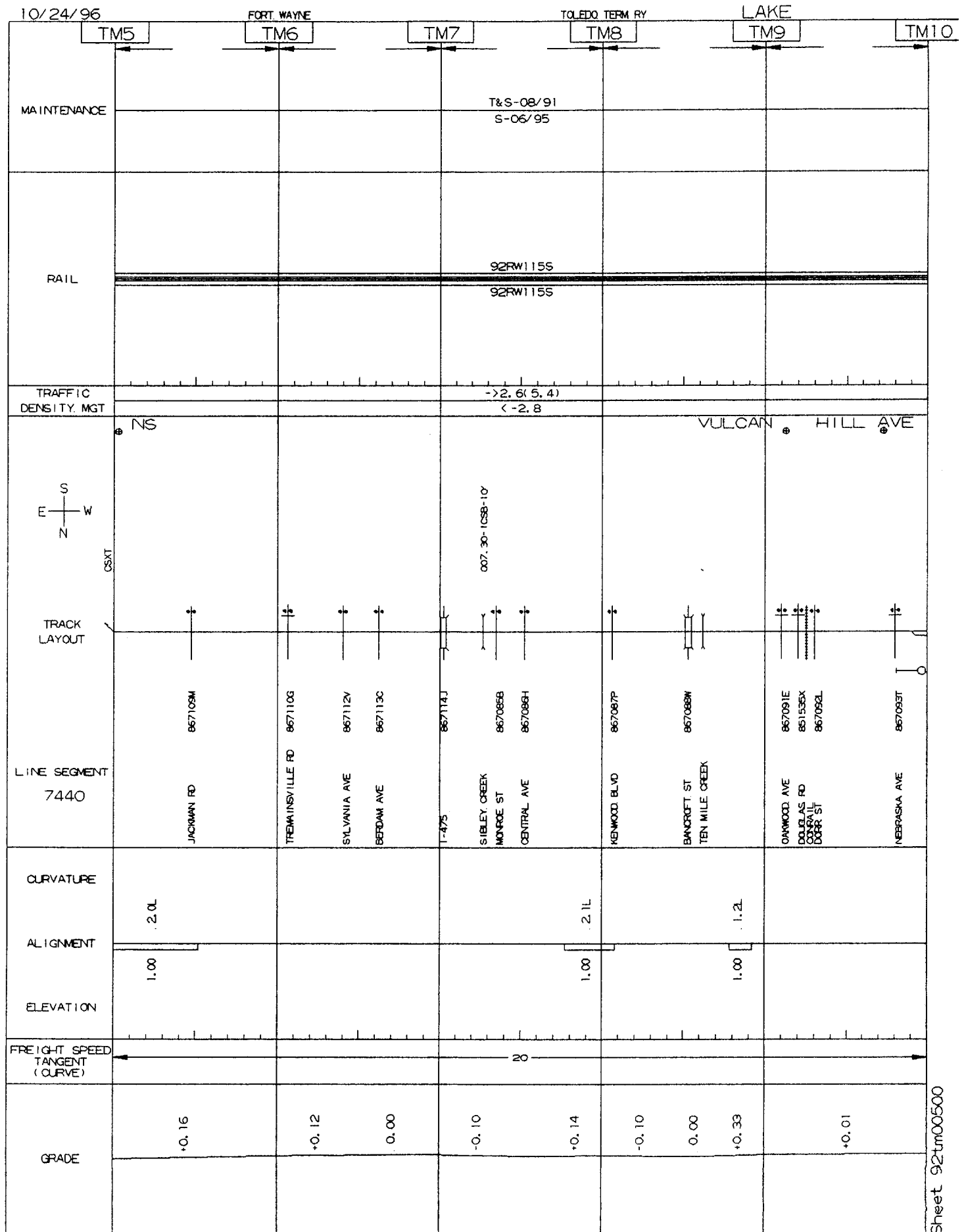
10/24/96







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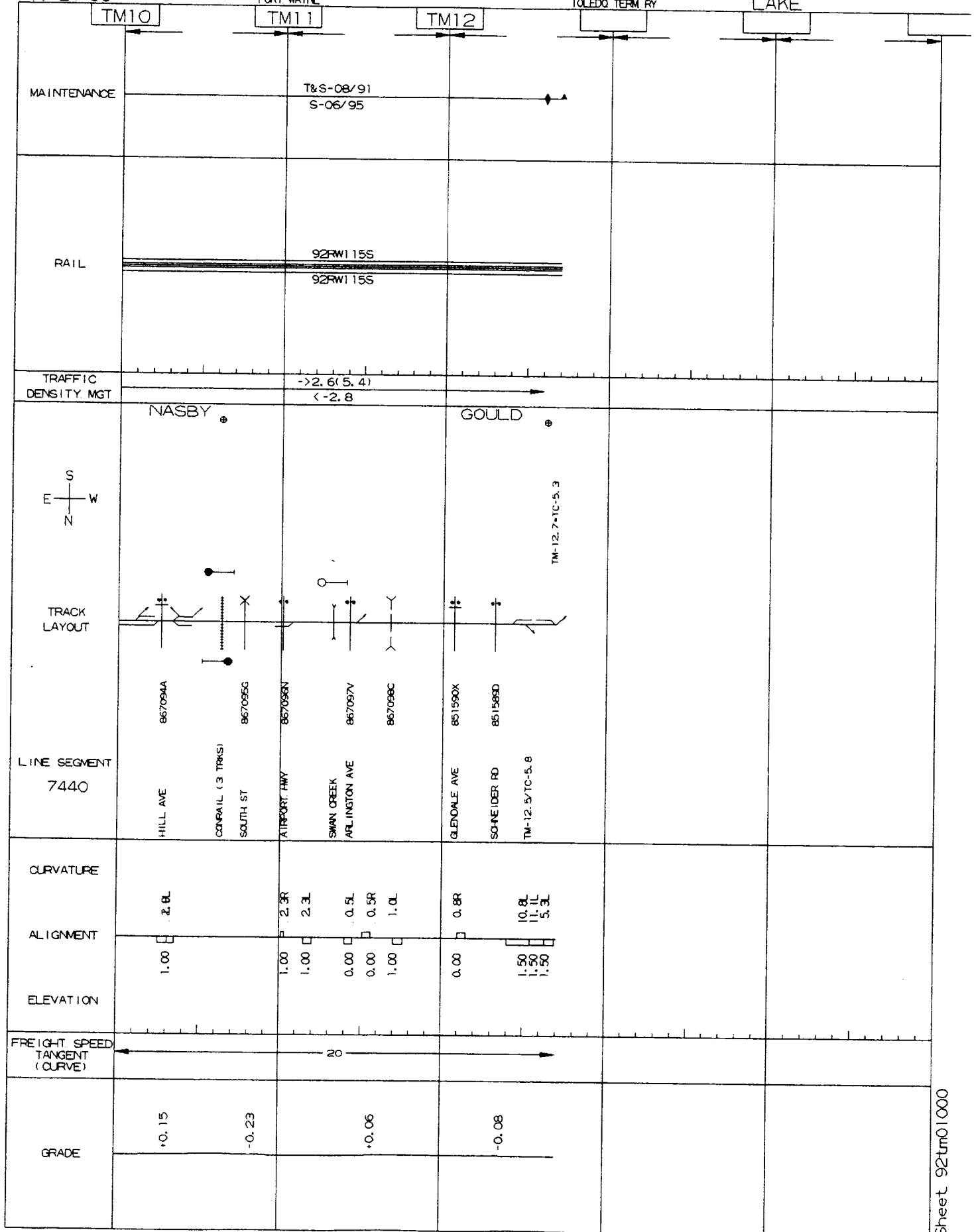


10/24/96

FORT WAYNE

TOLEDO TERM. RY

LAKE



10/24/96

TOLEDO

DAVIS BESSE LD

LAKE

	TE0	TE1	TE2	TE3	TE4	TE5
	5280'	5280'	5280'	5280'	5280'	5280'
MAINTENANCE			T&S- N/A			
RAIL			01R115S			
			01R115S			
TRAFFIC DENSITY MGT						
V-8-OH						
<div style="text-align: center;">           S            E — W            N         </div>						
TE-0.0-T-20.0						
TRACK LAYOUT	Y Y	Y Y	Y Y	Y Y	Y Y	Y Y
	473780H 473781P	473783D 473784K	473785S 473786Y	473788M 473789U 473790N	473792C 473793J 473794R	473796E 473798T 473799A 473800S
LINE SEGMENT 7310						PRIVATE CROSSING
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)			10			
GRADE			0.00			

10/24/96

TOLEDO

DAVIS BESSE LD

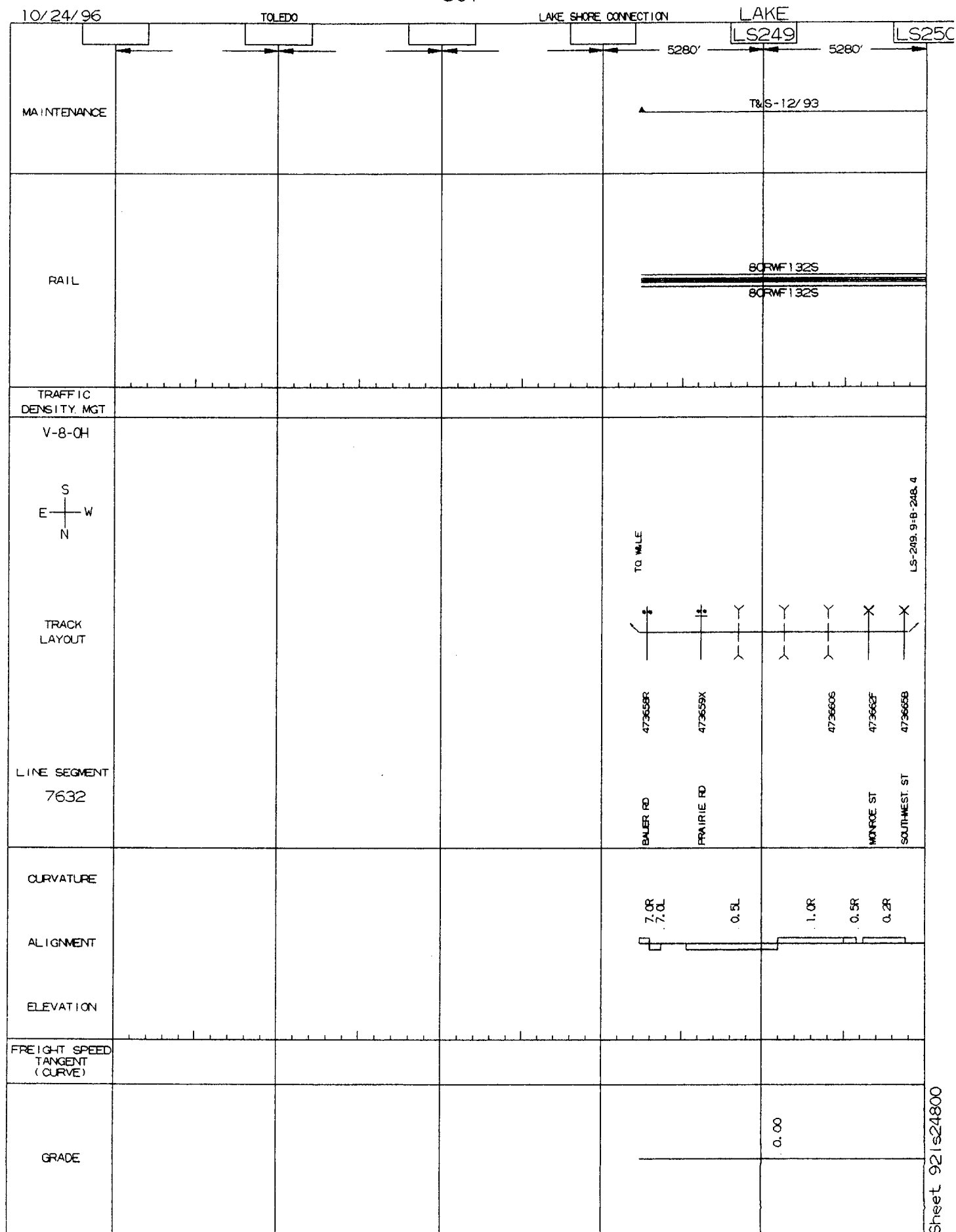
LAKE

	TE5	5280'	TE6	5280'	TE7	5280'			
MAINTENANCE	T&S- N/A								
RAIL	01R115S 01R115S								
TRAFFIC DENSITY MGT									
V-8-OH									
<div>S E — W N</div>									
TRACK LAYOUT	<div>Y Y</div>	<div>Y Y Y</div>	<div>Y Y Y</div>	<div>Y Y Y</div>	<div>Y Y Y</div>	<div>Y Y Y</div>			
LINE SEGMENT	473802F 473803M	473807P 473808W 473809D	473815G 473816N 473818C						
7310									
CURVATURE									
ALIGNMENT									
ELEVATION									
FREIGHT SPEED TANGENT (CURVE)	10								
GRADE	0.00								

DAVIS BESSE LD

LAKE

Sheet 92te00500



10/24/96

TOLEDO

LAKE SHORE CONNECTION

LAKE

LS250

5280'

MAINTENANCE

T&S-12/93

RAIL

800W132S  
800W132S  
800W132S

80C105S

80C105S

TRAFFIC  
DENSITY MGT

S  
E—W  
N

TRACK  
LAYOUT



473668W

LINE SEGMENT  
7632

KILBOURNE ST

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

GRADE

0.00

10/24/96

SANDUSKY

SANDUSKY

LAKE

	SP0	SP1				
MAINTENANCE	T&S- N/A					
RAIL	<div>00R090S</div> <div>00R090S</div>					
TRAFFIC DENSITY. MGT						
<div> <div>S</div> <div>E</div> <div>N</div> <div>W</div> </div> <div> <div>X</div> <div>+</div> <div>+</div> </div> <div> <div>47517IN</div> <div>475167Y</div> <div>475173C</div> </div> <div> <div>PRIVATE CAMP ST</div> <div>MILL ST</div> <div>MILL CREEK</div> </div>	<div>SANDUSKY</div> <div>100092</div> <div>SP-1, 09-S-109, 15</div>					
TRACK LAYOUT						
LINE SEGMENT 7765						
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	10					
GRADE	0.00					



10/24/96

CINCINNATI

OAKLEY JCT-BOND HILL

LAKE

	OB 11					
MAINTENANCE	T&S-N/A					
RAIL						
TRAFFIC DENSITY MGT	$\rightarrow > 20.4 (21.9)$ $\leftarrow < -1.5$					
<div data-bbox="107 892 189 976"> <p>S E — W N</p> </div> <div data-bbox="107 1102 189 1144"> <p>TRACK LAYOUT</p> </div> <div data-bbox="67 1323 211 1386"> <p>LINE SEGMENT 7000</p> </div>	<div data-bbox="247 798 446 829"> <p>OAKLEY JCT-SOUTH</p> </div> <div data-bbox="247 861 272 1039" style="writing-mode: vertical-rl; transform: rotate(180deg);"> <p>CONRAIL CONNECTION</p> </div> <div data-bbox="379 903 404 1039" style="writing-mode: vertical-rl; transform: rotate(180deg);"> <p>010.70-1858-607</p> </div> <div data-bbox="305 1270 454 1449" style="writing-mode: vertical-rl; transform: rotate(180deg);"> <p>LESTER RD TO OAKLEY JCT-NORTH 1-71 WATERWAY HARRIS AVE</p> </div>	<div data-bbox="503 798 693 829"> <p>EAST NORWOOD</p> </div> <div data-bbox="520 1312 545 1449" style="writing-mode: vertical-rl; transform: rotate(180deg);"> <p>CSXT CONNECTION</p> </div>				
CURVATURE						
ALIGNMENT						
ELEVATION						
FREIGHT SPEED TANGENT (CURVE)	10					
GRADE						

10/28/96

PEORIA

FRANKFORT-PEORIA

ILLINOIS

SP235

MAINTENANCE

RAIL

TRAFFIC  
DENSITY MGT

V-7-IN



TRACK  
LAYOUT

LINE SEGMENT  
7980

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

GRADE

CR CROSSING

LAKE DIVISION



ROSSVILLE AVE

55N15S  
77W131S  
55N15S  
77W131S

0.00

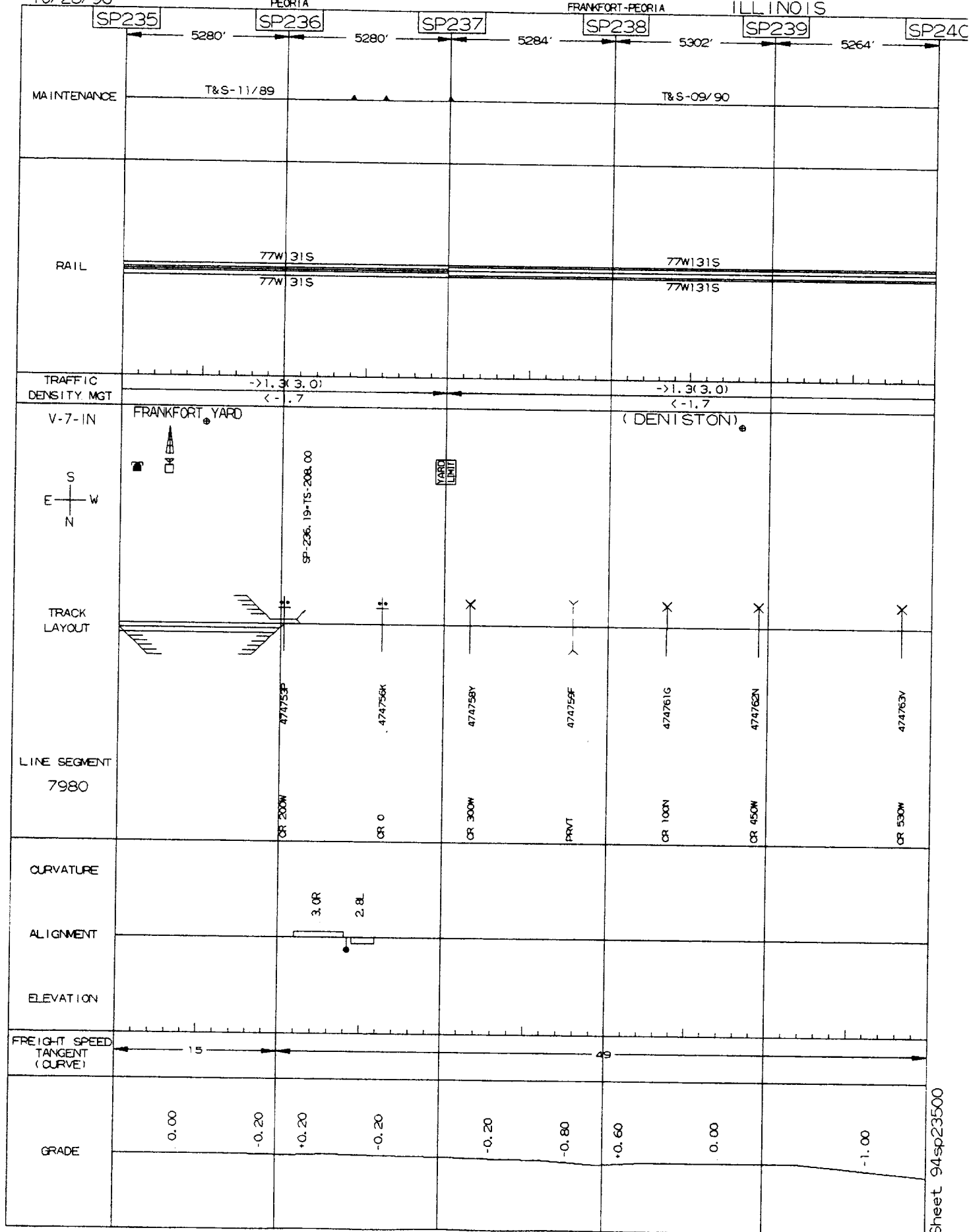
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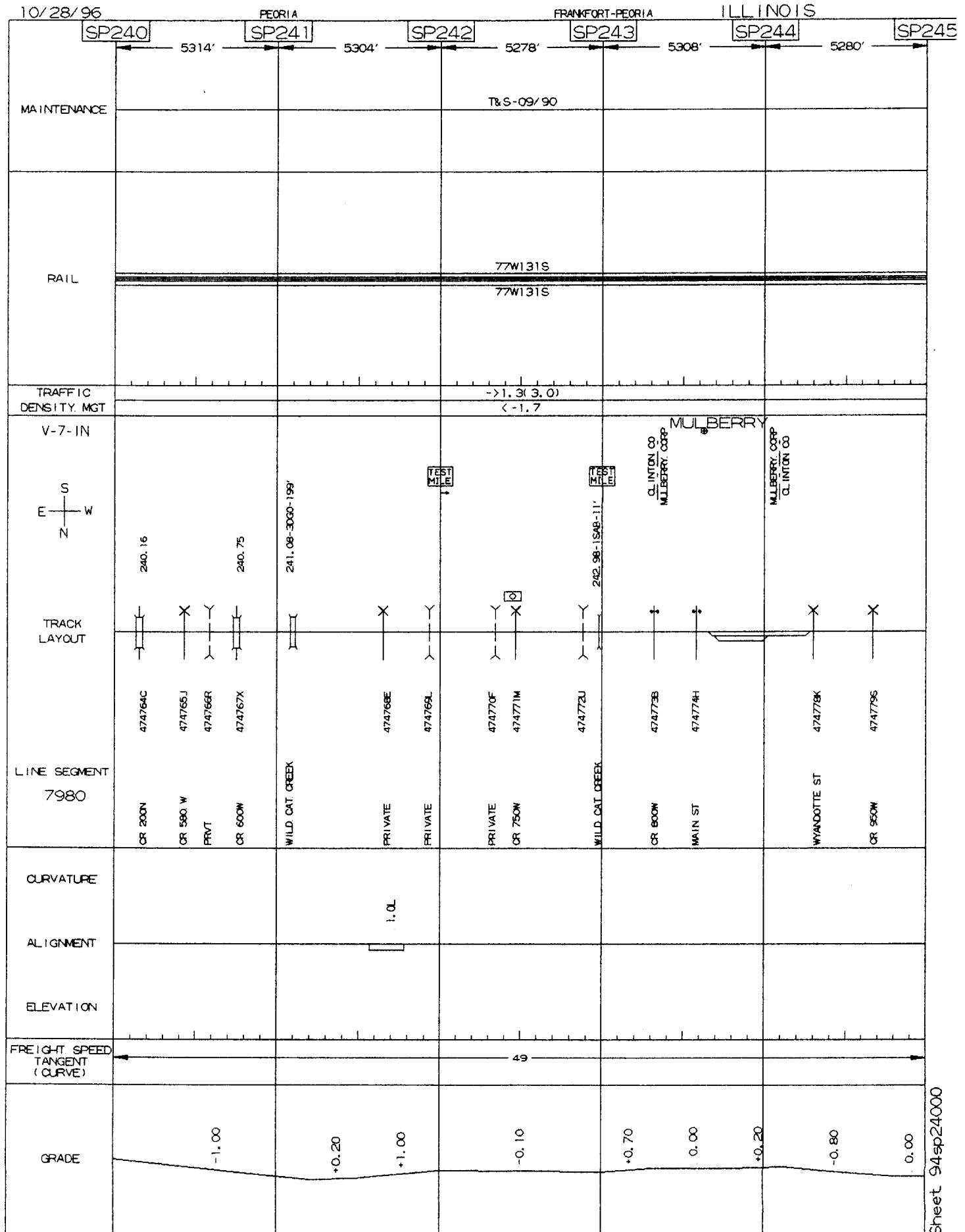
10/28/96

PEORIA

FRANKFORT-PEORIA

ILLINOIS



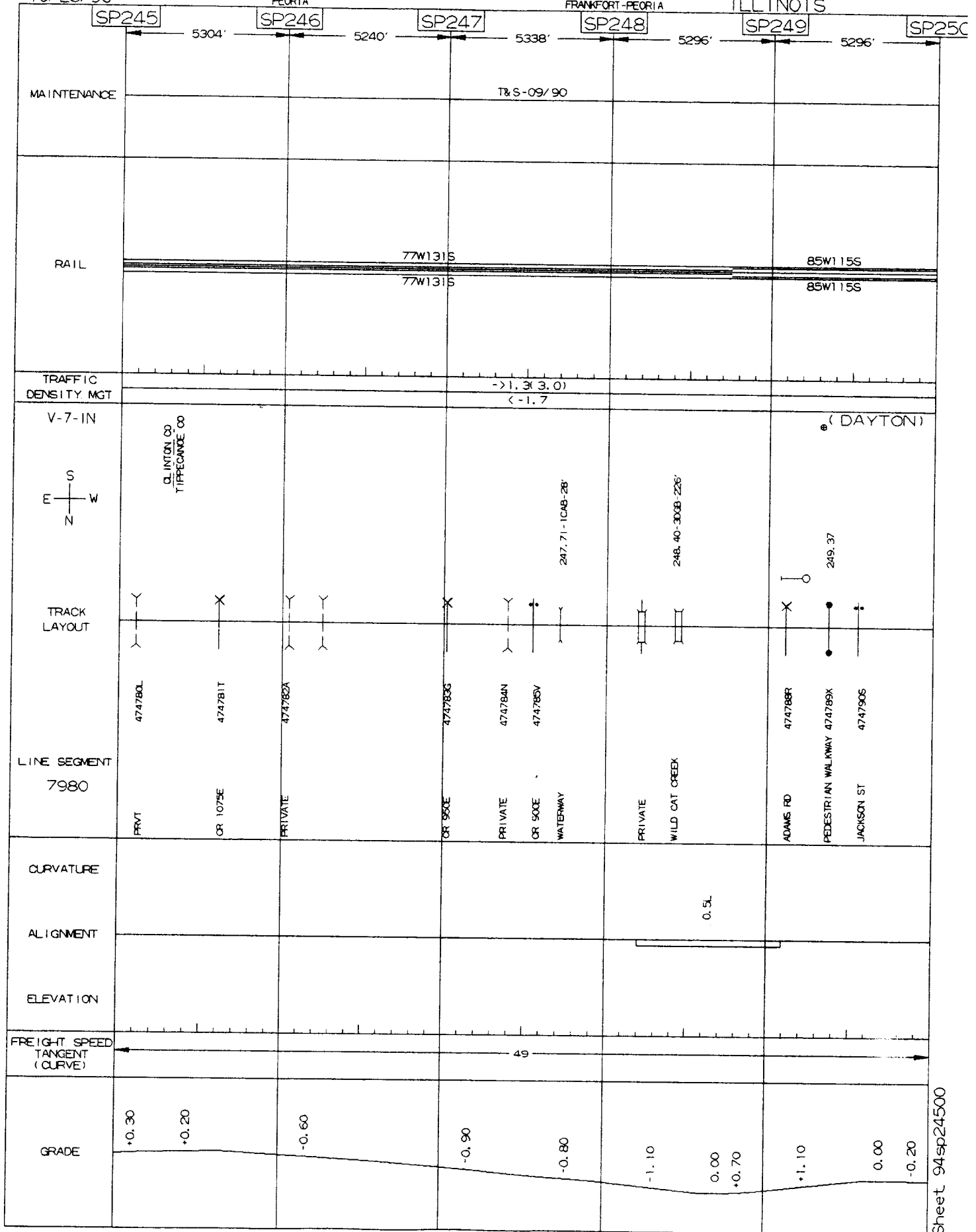


10/28/96

PEORIA

FRANKFORT-PEORIA

ILLINOIS

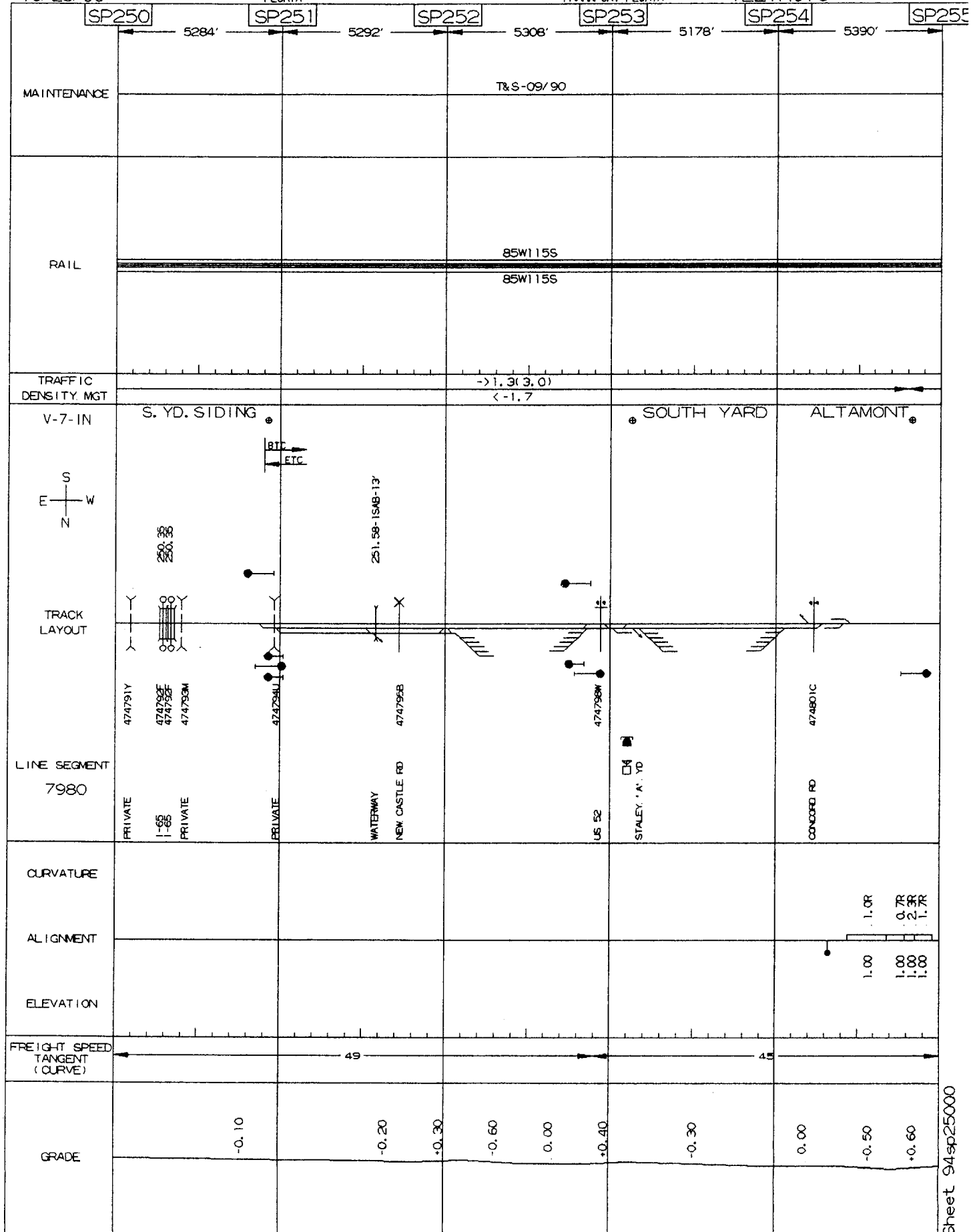


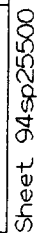
10/28/96

PEORIA

FRANKFORT-PEORIA

ILLINOIS



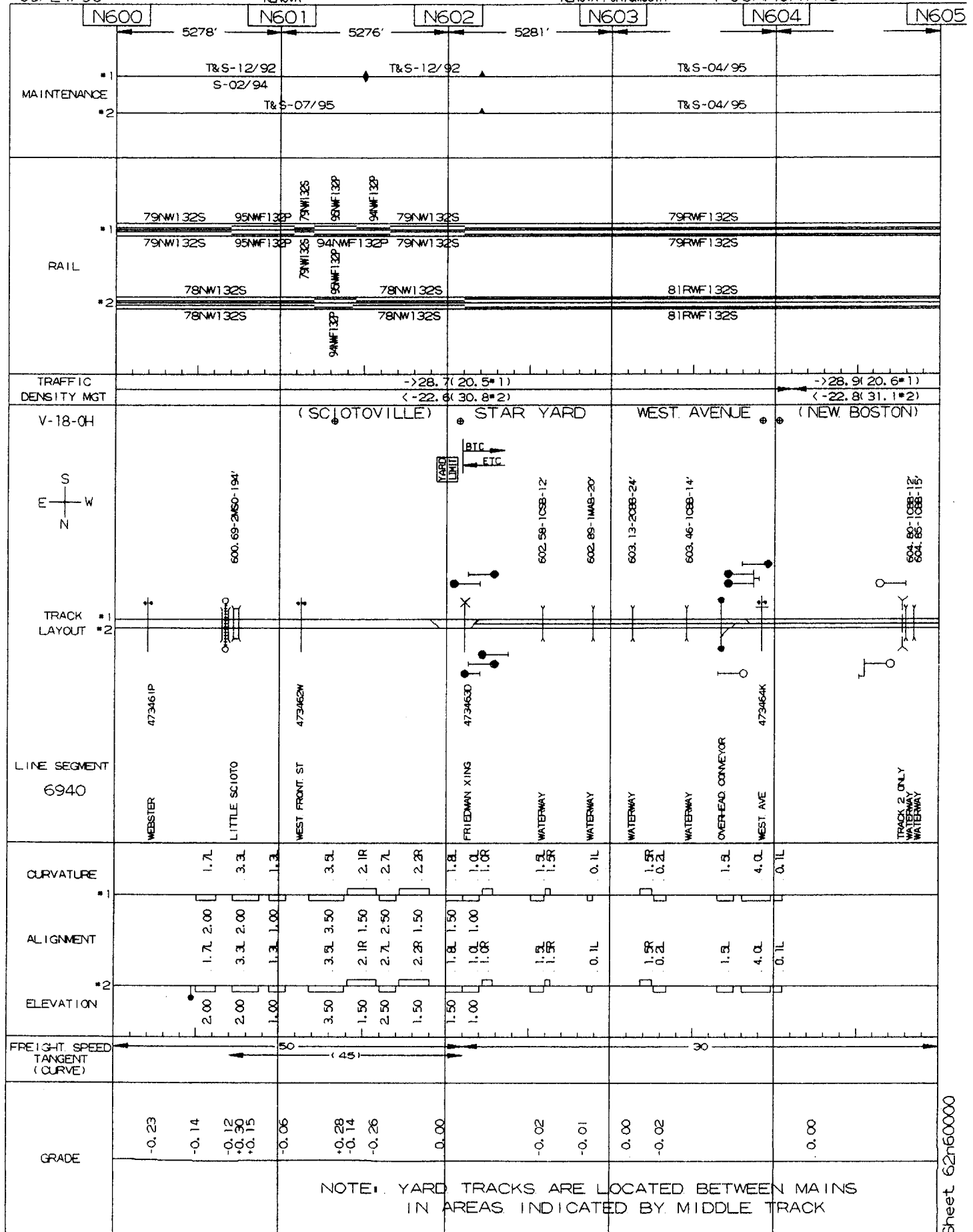


09/24/96

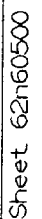
KENOVA

KENOVA-PORTSMOUTH

POCAHONTAS







12/03/94

TRACKAGE RIGHTS OVER I&O RR

373

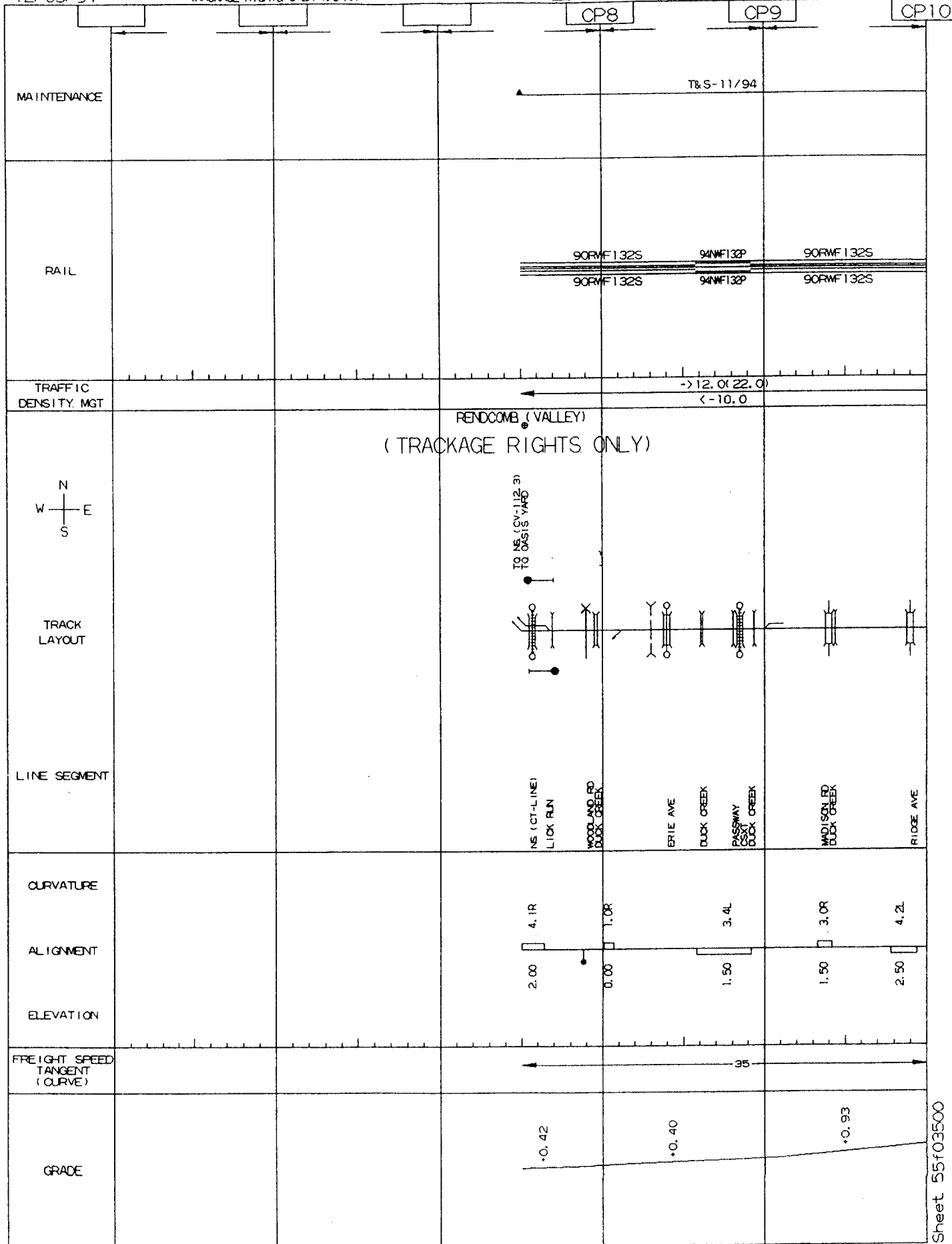
VALLEY-MILL (I&O RR)

LAKE

CP8

CP9

CP10



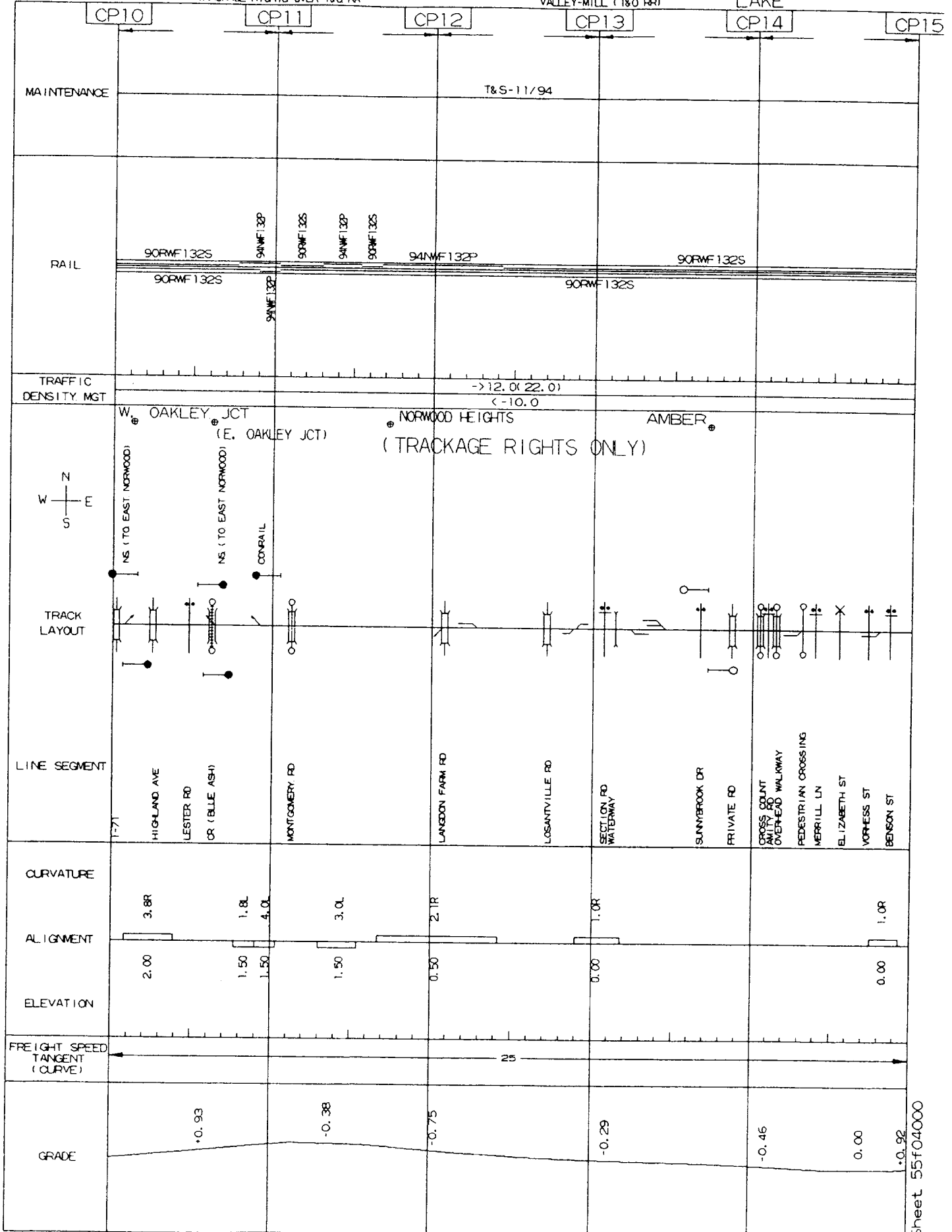
12/03/94

TRACKAGE RIGHTS OVER 180 RR

374

VALLEY-MILL (180 RR)

LAKE



Sheet 55f04000

LAKE

CP16

T&amp;S-11/94

RAIL

90RWF132S

90RWF132S

TRAFFIC  
DENSITY. MGT

	-> 12.0	(22.0)
	< -10.0	

⊕ MILL		
(TRUCKAGE RIGHTS ONLY)		

TQ NS (CF-16.5)

## TRACK LAYOUT

LINE SEGMENT

# VINE'S

MECHANICS ST  
COLUMBIA ST

READING BO

1  
2  
3  
4  
5

MILL CREEK

PRIVATE ROAD

## CURVATURE

## ALIGNMENT

ELEVATION

FREIGHT SPEED  
TANGENT  
(CURVE)

- 35

GRADE

0.92

-0.27

0.30

-0.40

0.00

•0.93

## Explanation of Graphic Display Conventions

### Top Margin - (Left to Right)

- 1) Data shown represents when drawing was prepared.
- 2) Items correspond to District, Branch and Spur when applicable.

or

Items correspond to Old Division name, and From-To station names.

- 3) Operating Division name.
- 4) Sheet number within Operating Division.

### Milepost Data Band -

- 1) Current milepost designation with prefix or suffix.
- 2) Actual distance between milepost markers in feet.

### Maintenance Section -

T&S date displayed above each main and surfacing data displayed below each main. Surfacing date is suppressed when prior to T&S date. Vertical tick-marks above or below each main show break points for T&S and/or surfacing.

### Rail Section -

Data displayed above the graphic for each main represents the left rail and data displayed below the graphic for each main represents the right rail (viewed in increasing milepost direction). Representation gives year laid, rail type, rail weight and premium (P) vs. standard (S) rail. Rail graphic change indicates break point in data values. Rail type codes are shown in Table 1.

### Traffic Density Section -

Annual density displayed in millions of gross tons rounded to tenths. Note this results in density less than 50,000 tons being rounded to zero. Direction of traffic is shown with arrows. Density by track is also shown with track identification.

### Track Layout Section -

- 1) Left hand margin gives valuation map I.D., track timetable direction (compass rose) and line segment reporting number.
- 2) Area 1 displays the station names and locations. Those stations shown in bold print (only) are the current timetable stations and their timetable locations. Those stations in parenthesis are not shown in the timetable.

- 3) Area 2 gives Territory markers (Terminal Control, Test Miles, Yard Limits, State Lines, County/Incorporated Lines), and Equipment Locations (Microwave Towers, Telephone, Radio Base Stations).
- 4) Area 3 gives Bridge and Crossing technical specifications. For Crossings at grade, this specification is AAR number (DOT number). For Bridges, this specification is Bridge number (in milepost format), number of spans, structure construction, and length of structure. See Table 2 for explanation of structure construction codes.
- 5) Area 4 (and Area 6) gives signals, signal structures, pole lines and detector locations.
- 6) Area 5 gives graphic representation of crossings (both private and public), underpasses, overpasses, bridges and tunnels.

Also displayed in Area 5 is a track diagram for main line, side tracks, crossovers, leads and yard track including types of turnouts between single main and double main territory. Detail of display is limited to switches on main track and adjacent track.

- 7) Area 6 gives the common name (street, highway, river) associated with the graphic symbols in Area 5 and the technical specifications in Area 3. For railroad crossings at grade the display convention is (when available): Owing Road, Crossing Type, Angle and Maintaining Road.

#### **Alignment Section -**

- 1) Graphic representation is given for curve direction and length for each main.  
(Note: Representation is currently provided for track 1 only. In double track sections track 2 is only a copy of track 1 and may not represent actual conditions.)
- 2) Curvature is specified to tenths of a degree above each main along with left/right indication.
- 3) Location of wheel flange lubricators are given along mains.

#### **Freight Speed Section -**

Curve and tangent speed limits taken from the timetables. The curve speed limit (shown within parentheses) is suppressed if it is the same as on tangent track. The speed limit on #2 track is suppressed if it is the same as on #1 track.

#### **Grade Section -**

Grade shown is based on ascending milepost direction.

Ruling grades are based upon determination made by Operations Research and are stated in the uphill direction, e.g. "Bluefield to Roanoke", regardless of whether the uphill direction is ascending milepost or descending milepost.

**TABLE 1**  
**RAIL TYPE CODES**

<b>NWF</b>	<b>New welded rail which has been field welded</b>
<b>NW</b>	<b>New welded rail which has not been field welded</b>
<b>RWF</b>	<b>Rewelded welded rail which has been field welded</b>
<b>RW</b>	<b>Rewelded welded rail which has not been field welded</b>
<b>WF</b>	<b>Relay welded rail which has been field welded</b>
<b>W</b>	<b>Relay welded rail which has not been field welded</b>
<b>NB</b>	<b>New butt welded (78') rail</b>
<b>N</b>	<b>New jointed rail</b>
<b>R</b>	<b>Relay jointed rail</b>
<b>RB</b>	<b>Relay butt welded (78') rail</b>
<b>C</b>	<b>Cropped jointed rail</b>

**TABLE 2**  
**BRIDGE TYPE CODES**

**Type of Bridge Structure**

**BS = Beam Span**  
**BA = Brick Arch**  
**CA = Concrete Arch**  
**CB = Concrete Box**  
**CS = Concrete Span**  
**DG = Deck Plate Girder**  
**DT = Deck Truss**  
**MA = Masonry Arch**  
**MS = Mixed Span**  
**SA = Structural Plate Arch**  
**TG = Through Plate Girder**  
**TT = Through Truss**  
**WT = Timber (Wood) Trestle**

**Deck Construction**

**O = Open Deck**  
**B = Ballast Deck**  
**C = Combination**