



**NORFOLK
SOUTHERN**

Virginia Division

2003

THIS BOOK IS INTENDED FOR GENERAL REFERENCE ONLY

THIS BOOK IS PREPARED AND PUBLISHED BY THE OFFICE OF ENGINEERING SYSTEMS-ATLANTA AND IS BASED ON INFORMATION CONTAINED IN TWO DISTINCT DATABASE SOURCE FILES, (1) THE ENGINEERING SYSTEMS DATA FILE AND (2) THE CORPORATE TRACK DATABASE FILE (CTRK).

ENGINEERING SYSTEMS DATABASE:

ENGINEERING SYSTEMS MAINTAINS A DEPARTMENTAL DATABASE, WHICH IS USED TO GENERATE THE TRACK LAYOUT SECTION OR CENTER PORTION OF THE TRACK CHART DIAGRAM. QUESTIONS OR INFORMATION CONCERNING CHANGES, CORRECTIONS, ADDITIONS, OR DELETIONS TO THIS SECTION SHOULD BE DIRECTED TO THE ENGINEERING SYSTEMS OFFICE AS FOLLOWS.

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MEMO: JCELLIOT PH. 529-1315

CTRK-NS CORPORATE DATABASE:

INFORMATION RELATED TO RAIL, T&S, SURFACING, CURVES, ELEVATION, SPEEDS AND SPEED RESTRICTIONS IS OBTAINED FROM THE CTRK DATABASE. VARIOUS DEPARTMENTS OWN AND MAINTAIN THESE FILES. QUESTIONS OR INFORMATION CONCERNING CHANGES, CORRECTIONS, ADDITIONS, OR DELETIONS TO THESE RECORDS SHOULD BE DIRECTED AS FOLLOWS:

**RAIL, T&S, SURFACING: MW&S DEPT.
 E.P.HATTEN, MGR. PROG & SCHD.
 MEMO: EPHATTEN PH. 529-1456**

**CURVES, ELEVATIONS MW&S DEPT
AND CURVE SPEEDS: A.I.PATEL, SYSTEM TRACK ANALYST
 MEMO: AIPATEL PH. 527-2513**

**OPERATING SPEEDS AND TRANSPORTATION DEPT.
SPEED RESTRICTIONS: K.L.RICKS, SYSTEM MGR TRANS.
 MEMO: KLRICKS PH. 529-2298**

VIRGINIA DIVISION

WV

VA

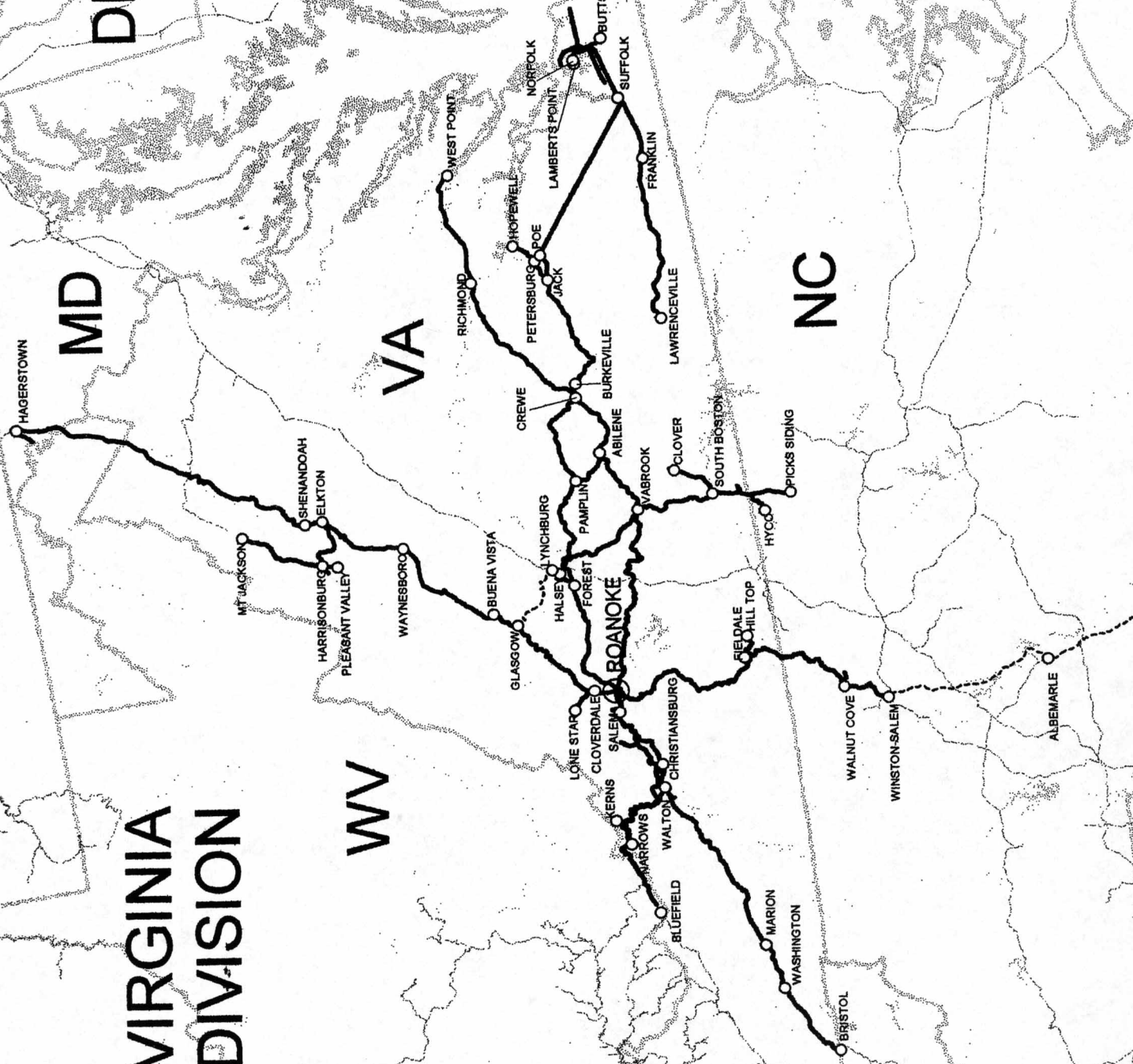
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DE

NC

KY

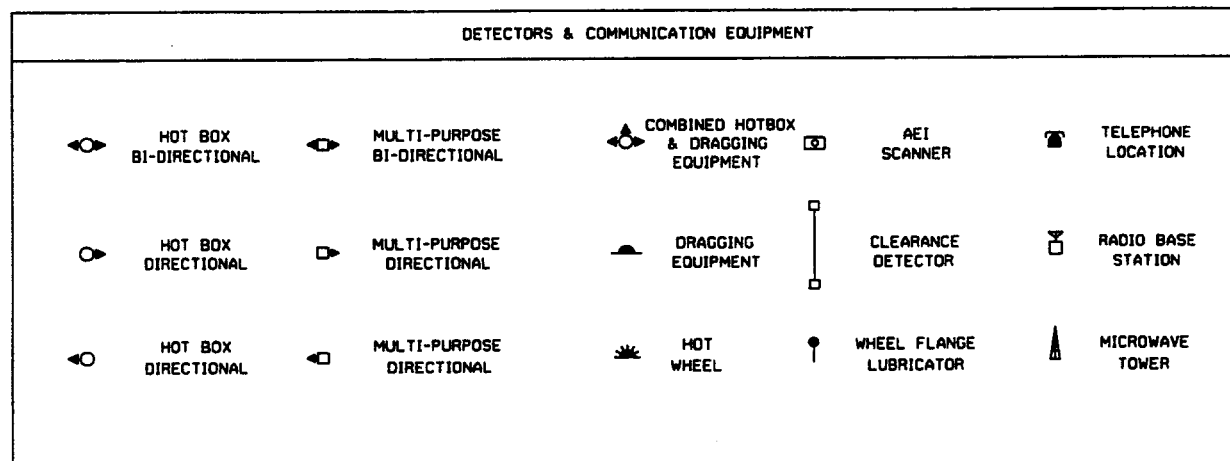
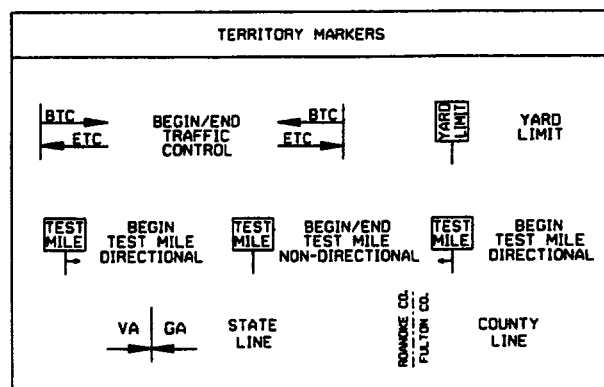
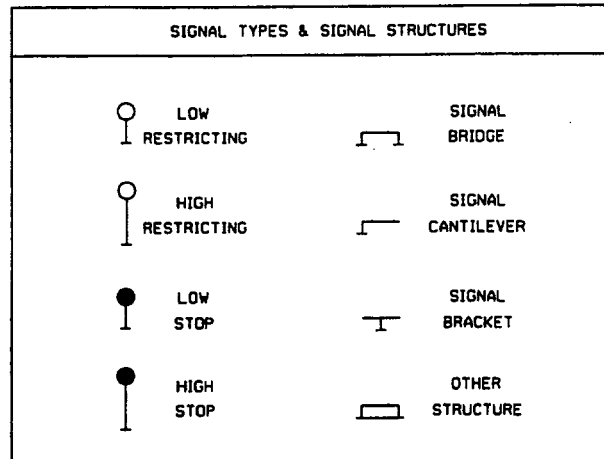
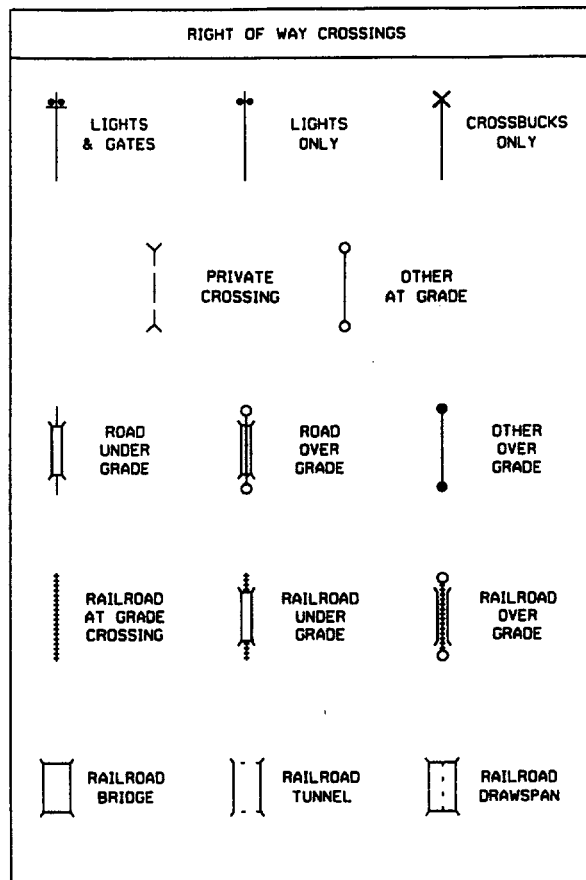
TN



VIRGINIA DIVISION
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FROM	TO			FROM	TO			
MAIN LINE								
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Poe - Jack		P	0.00	-	P	8.86	Norfolk	19
Poe - Petersburg - Jack		N	77.81	-	N	88.31	Norfolk	21
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TRACK CHART SYMBOL LEGEND



SINGLE TO DOUBLE MAIN WITH EQUILATERAL TURNOUT

SINGLE TO DOUBLE MAIN WITH LEFT HAND TURNOUT

SINGLE TO DOUBLE MAIN WITH RIGHT HAND TURNOUT

PASSING SIDING ABOVE SINGLE MAIN

PASSING SIDING BELOW SINGLE MAIN

PASSING SIDING ABOVE DOUBLE MAIN

PASSING SIDING BELOW DOUBLE MAIN

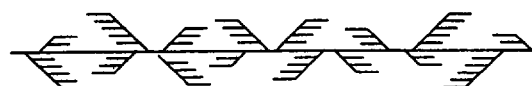
PASSING SIDING BETWEEN DOUBLE MAIN

INDUSTRY LEADS ABOVE AND BELOW DOUBLE MAIN

INDUSTRY LEADS ABOVE AND BELOW SINGLE MAIN

CROSSOVERS BETWEEN DOUBLE MAIN

YARD TRACKS ABOVE AND BELOW MAINS



001

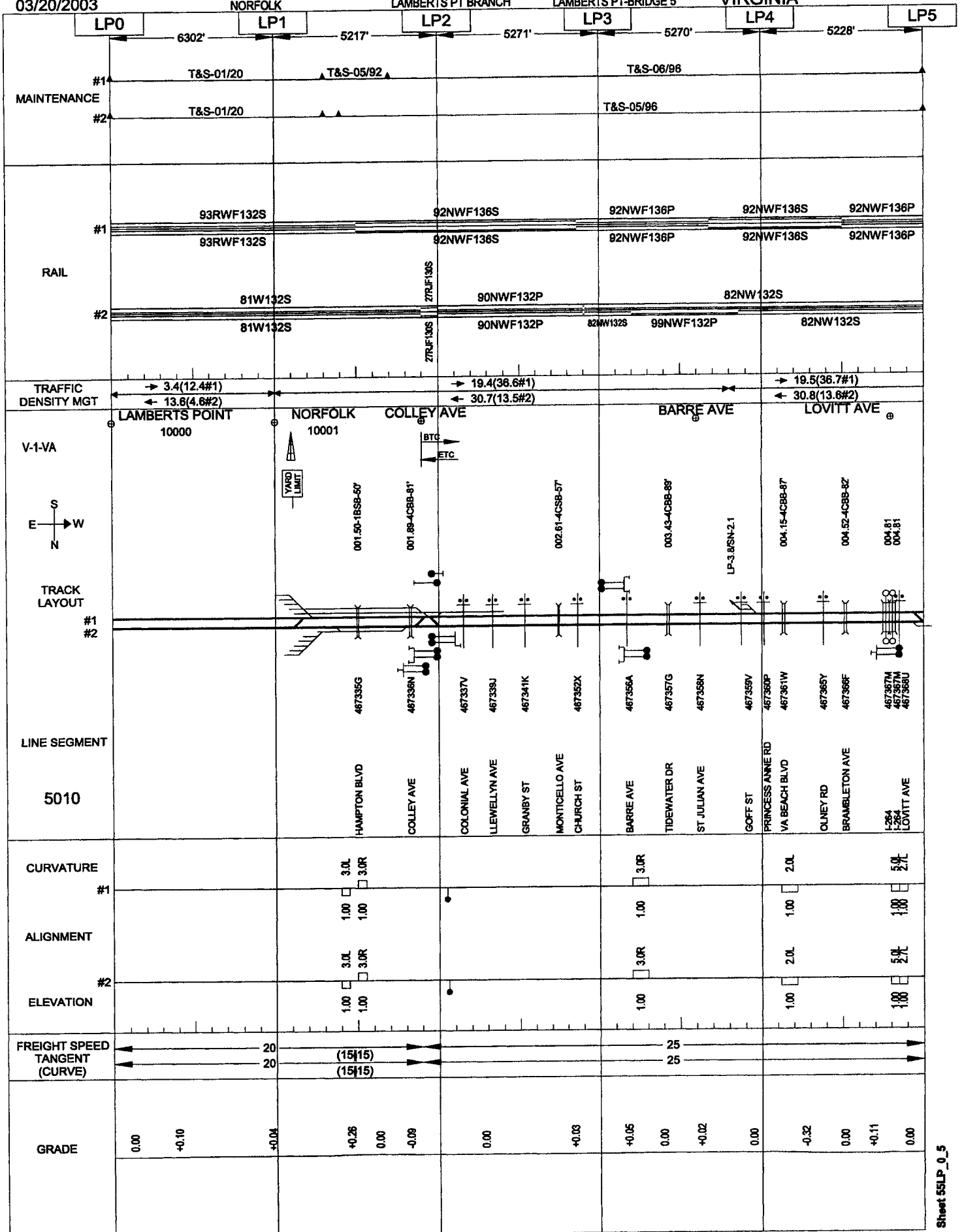
03/20/2003

NORFOLK

LAMBERTS PT BRANCH

LAMBERTS PT-BRIDGE 5

VIRGINIA



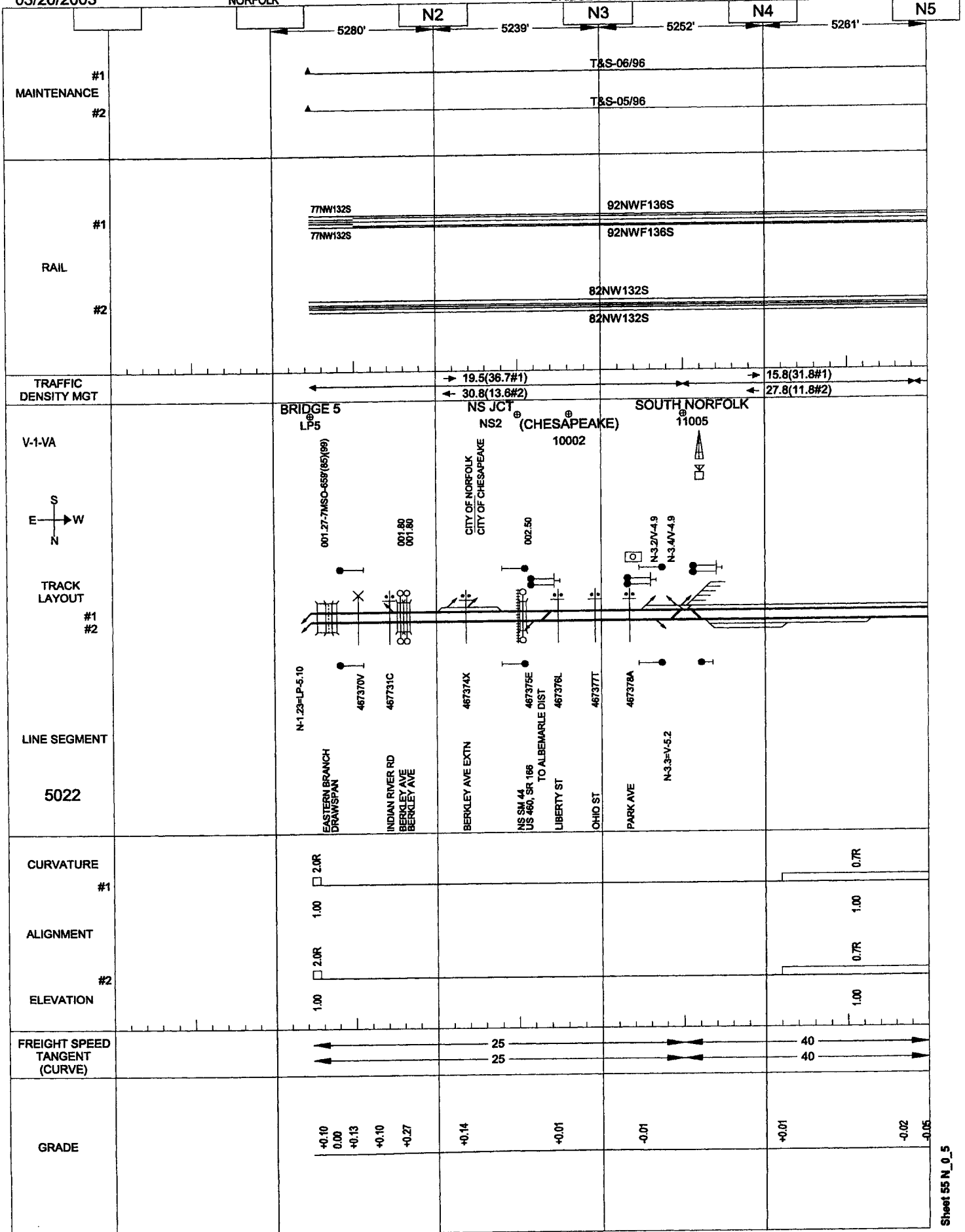
LP5	4557'					
MAINTENANCE	#1 #2					
RAIL	#1 #2					
TRAFFIC DENSITY MGT	→					
V-1-VA	BRIDGE 5 12055					
TRACK LAYOUT	LP-5.10-N-1.23					
LINE SEGMENT	#1 #2					
5010						
CURVATURE	#1					
ALIGNMENT						
ELEVATION	#2					
FREIGHT SPEED TANGENT (CURVE)	25 25					
GRADE	0.00					

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NORFOLK

BRIDGE 5-GILMERTON

VIRGINIA

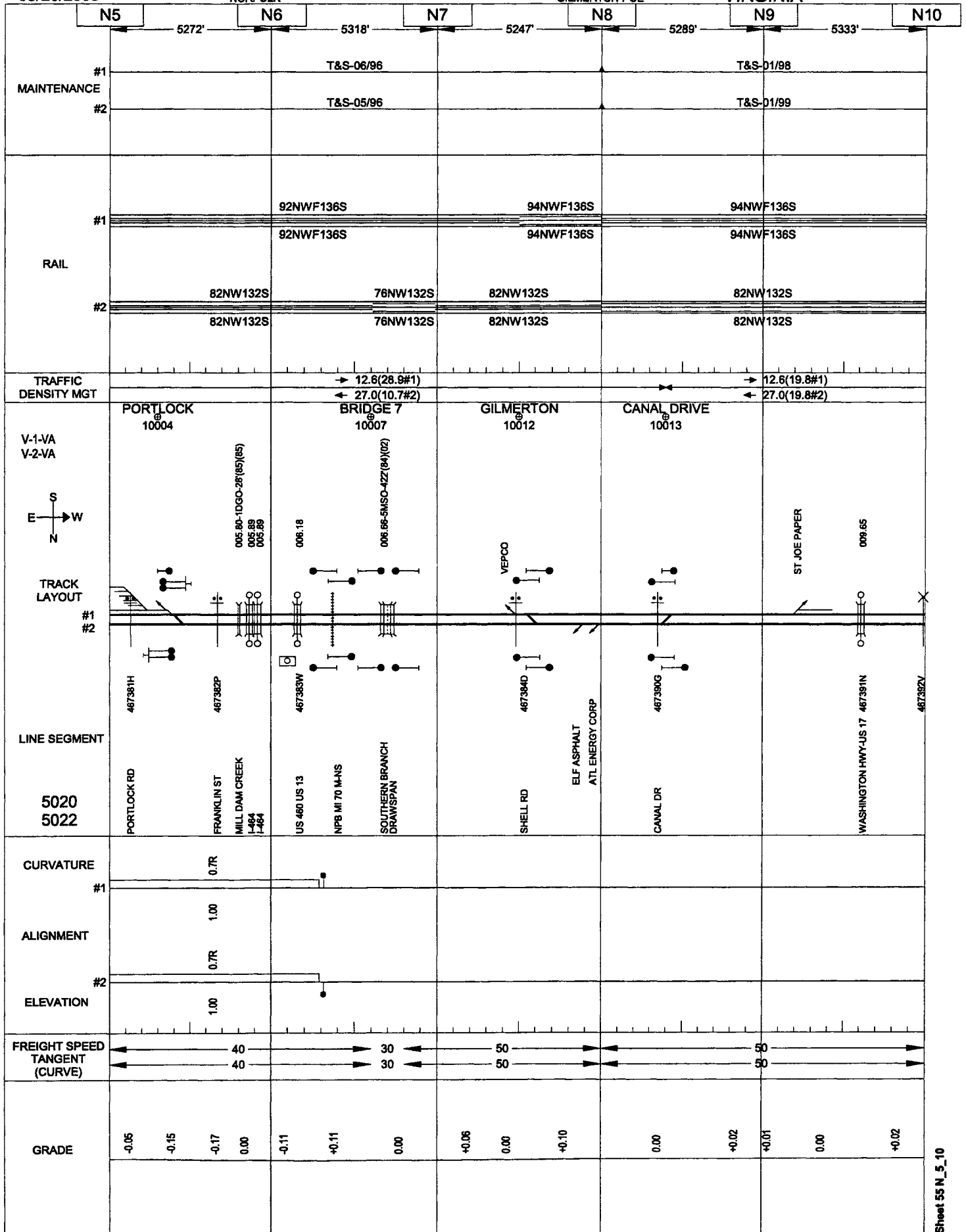


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NORFOLK

GILMERTON-POE

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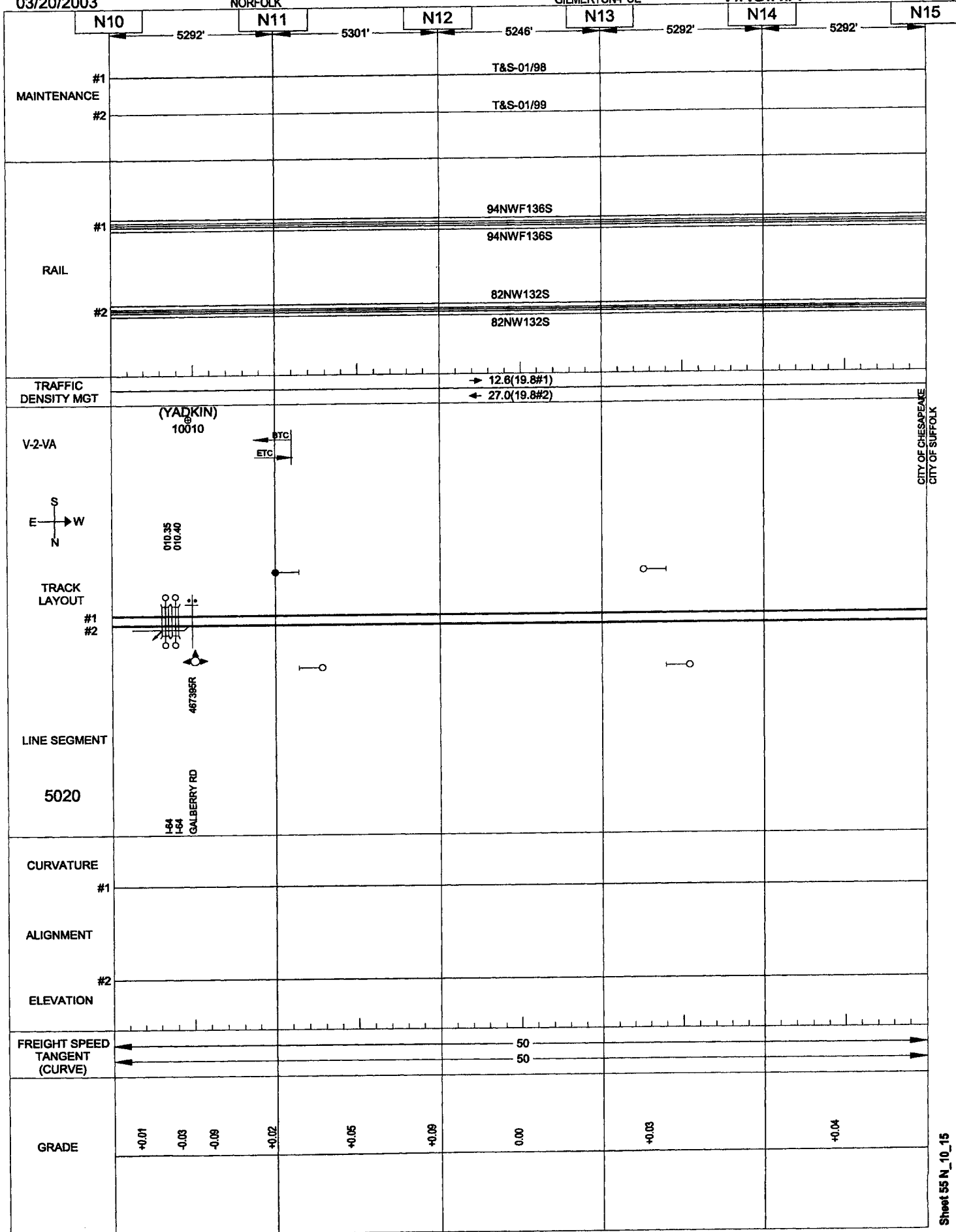


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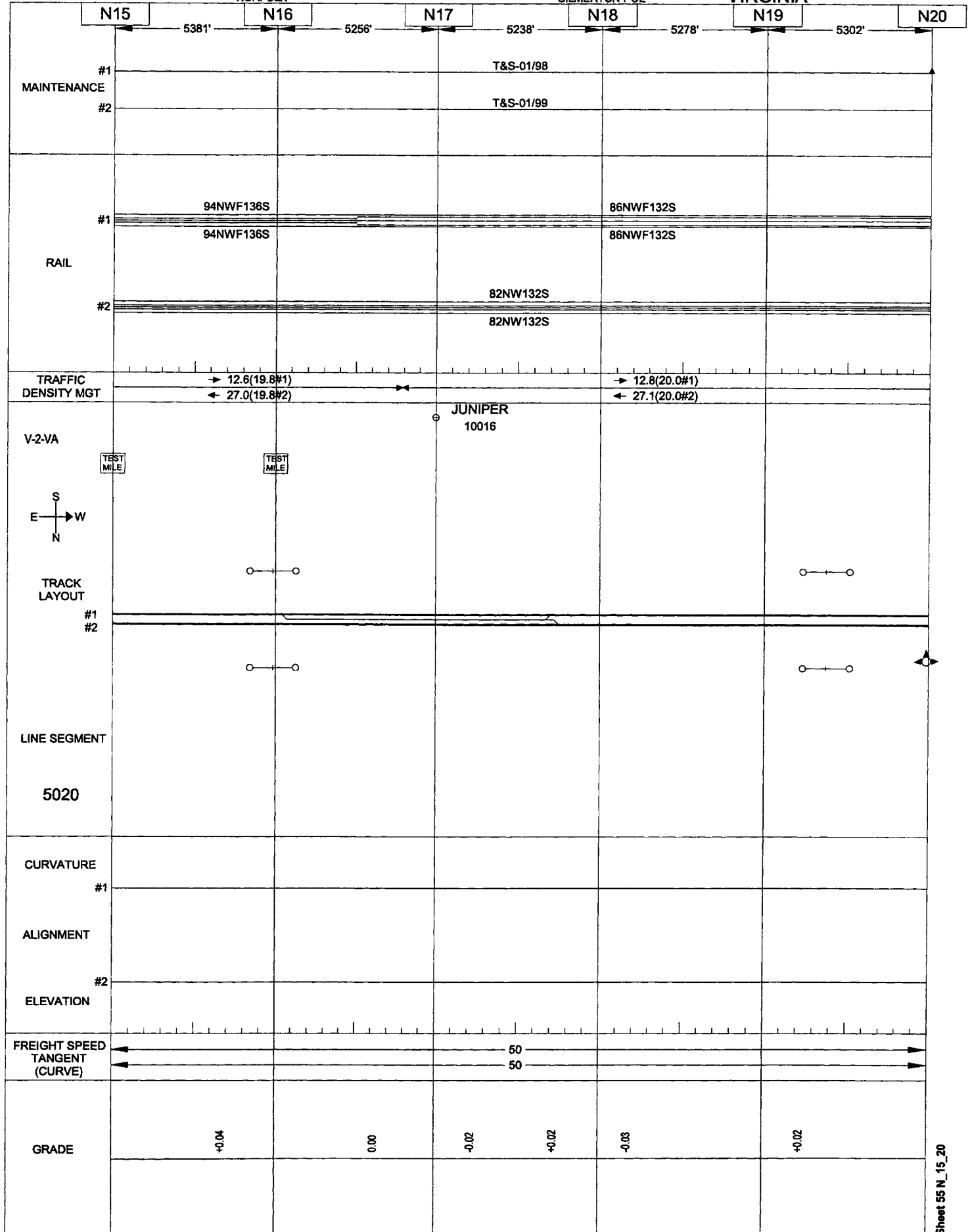


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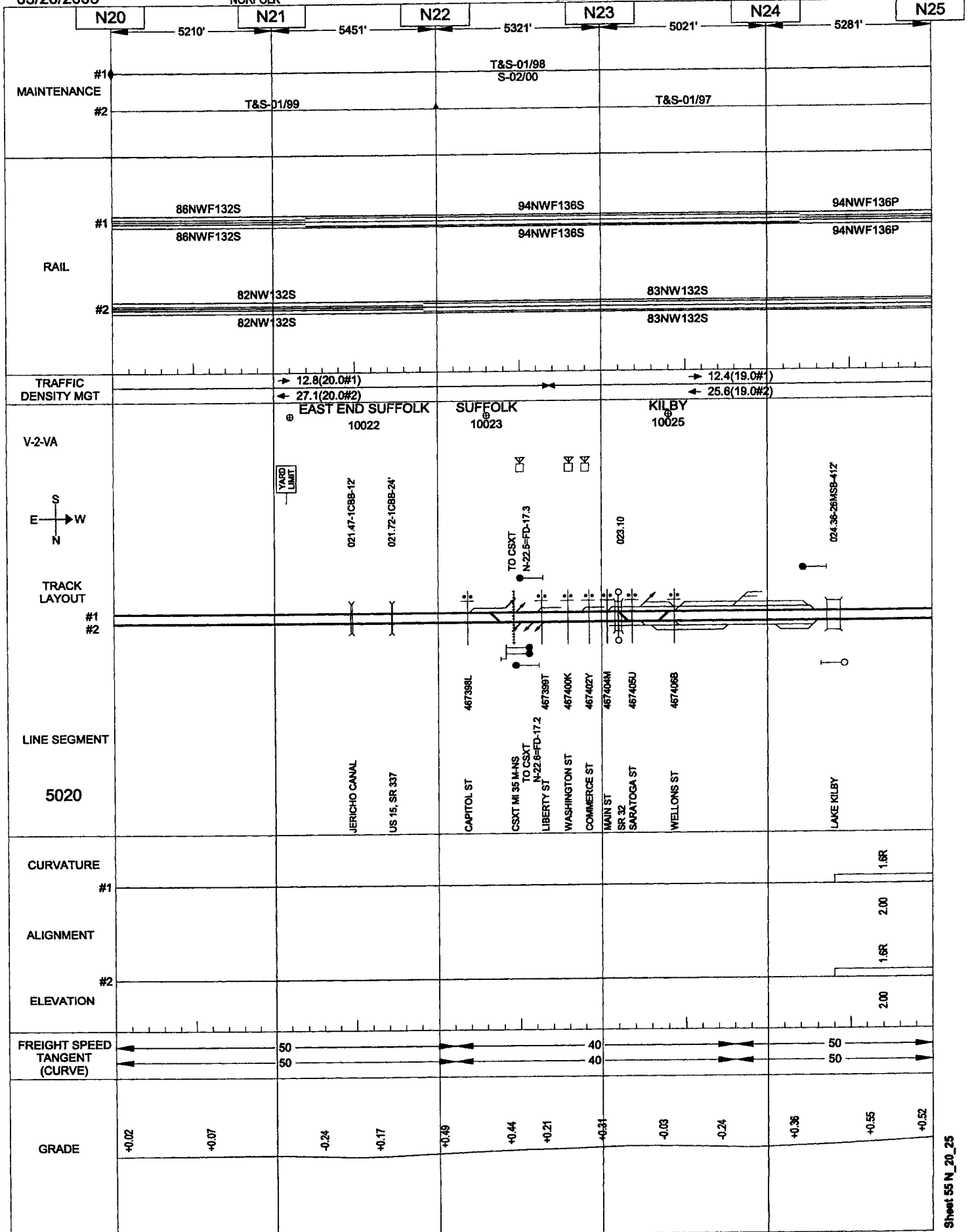


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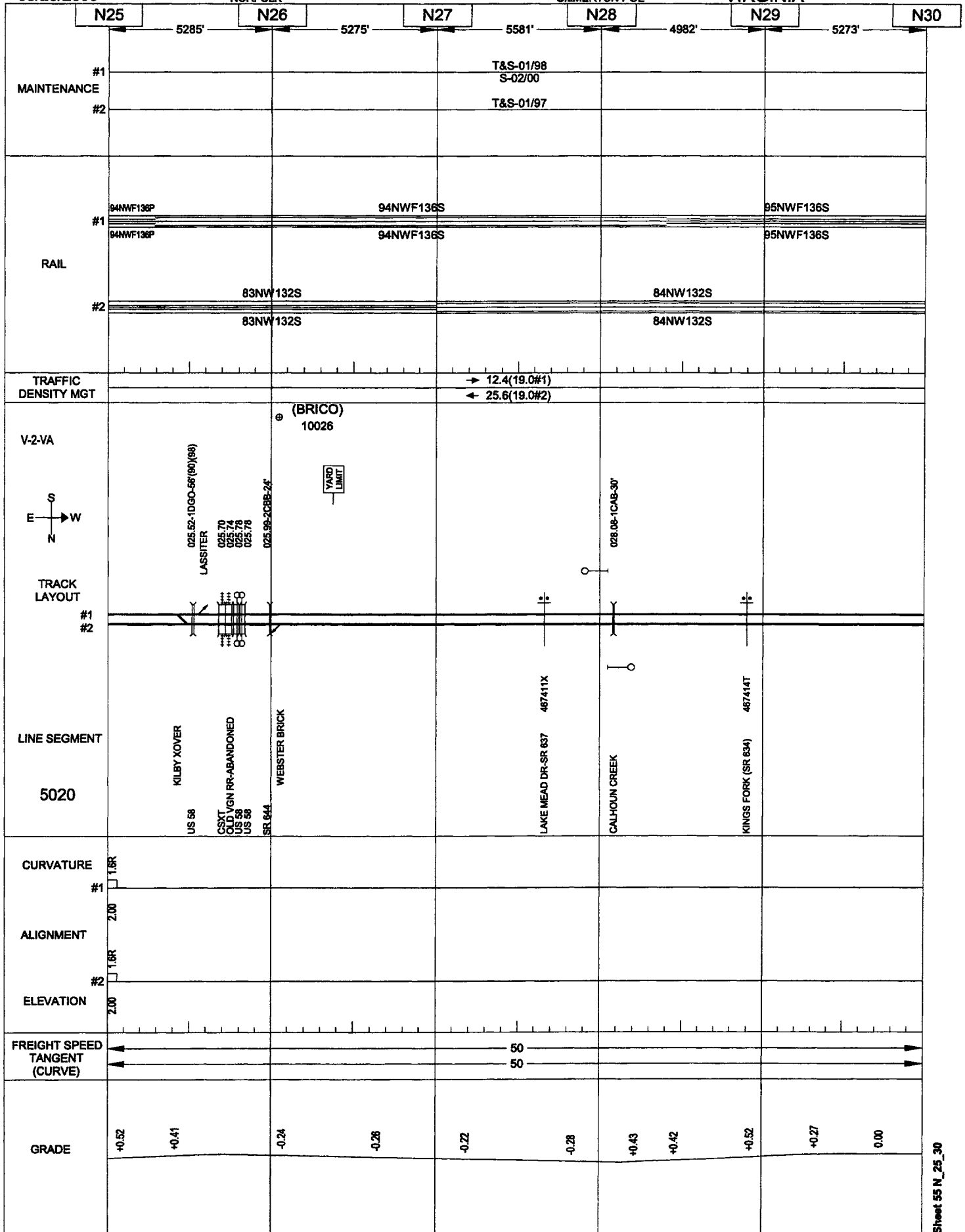


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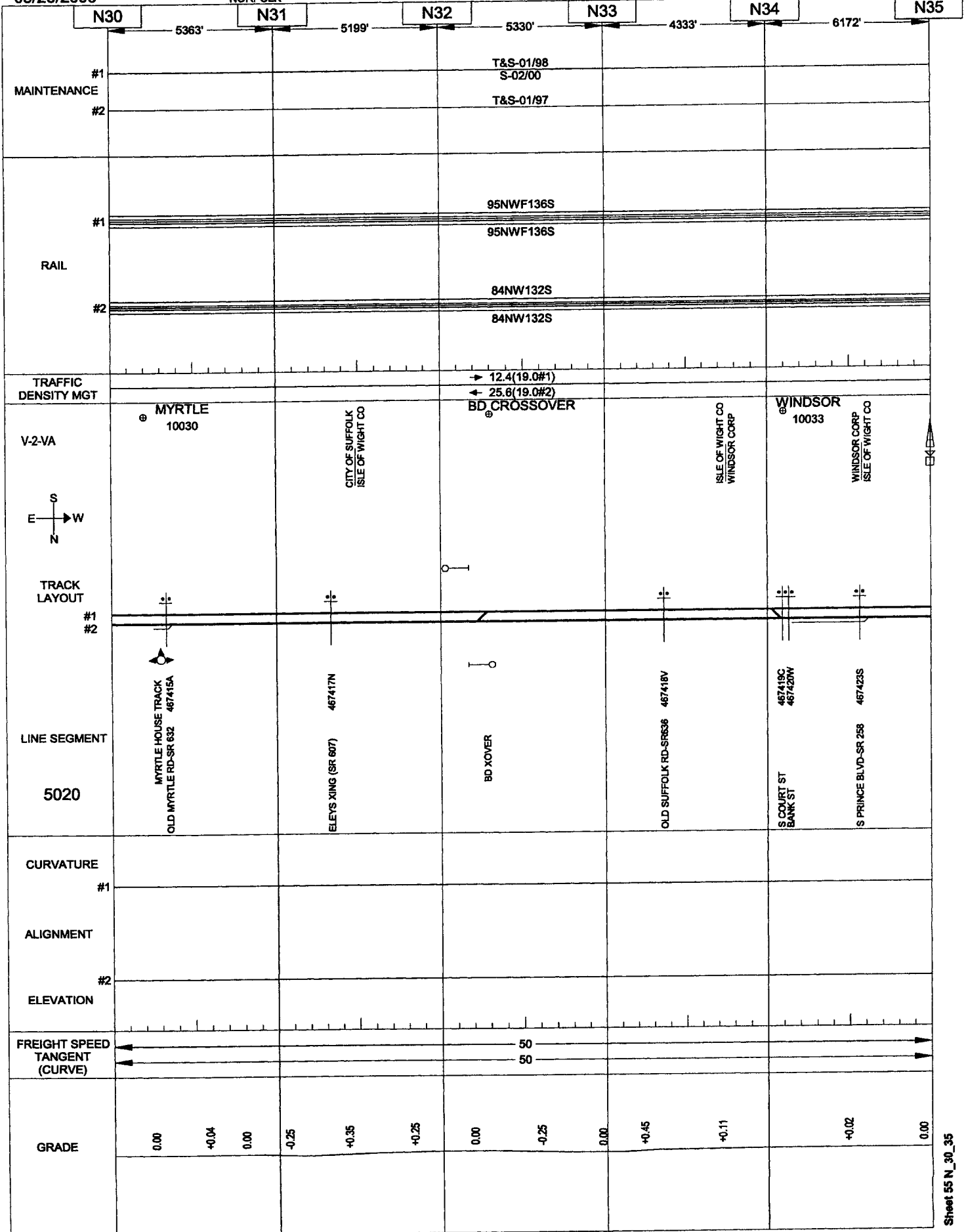


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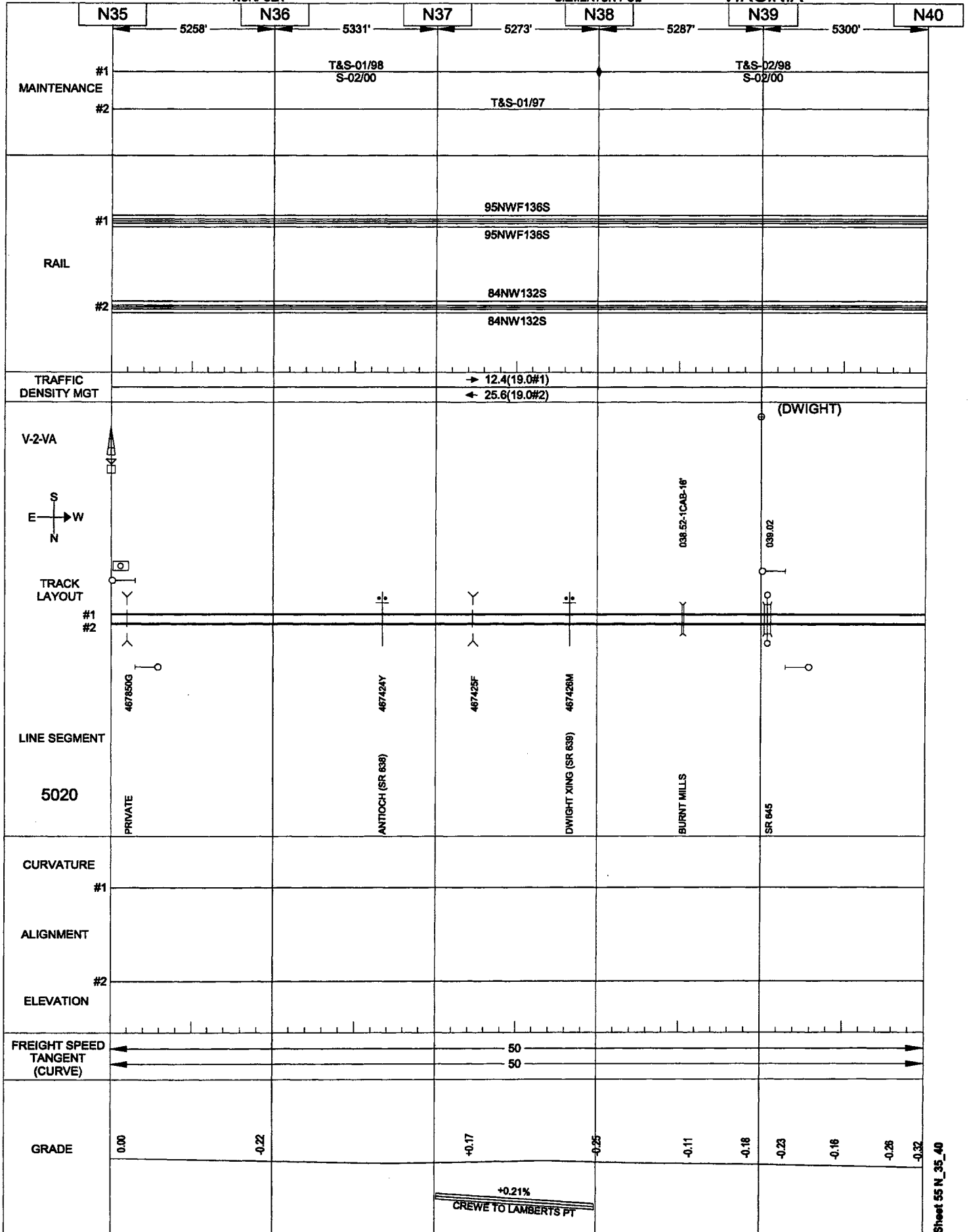


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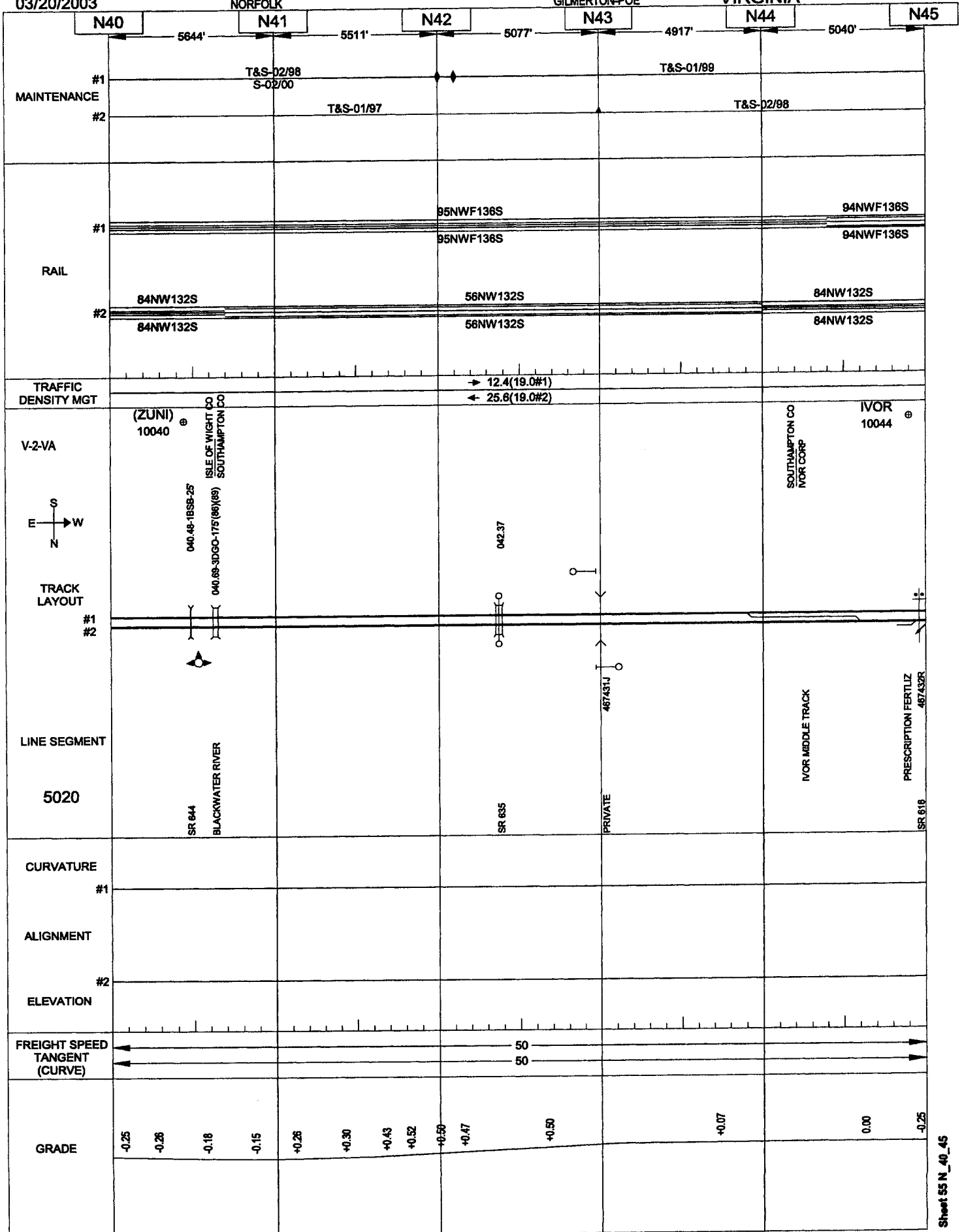


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NORFOLK

GILMERTON-POE

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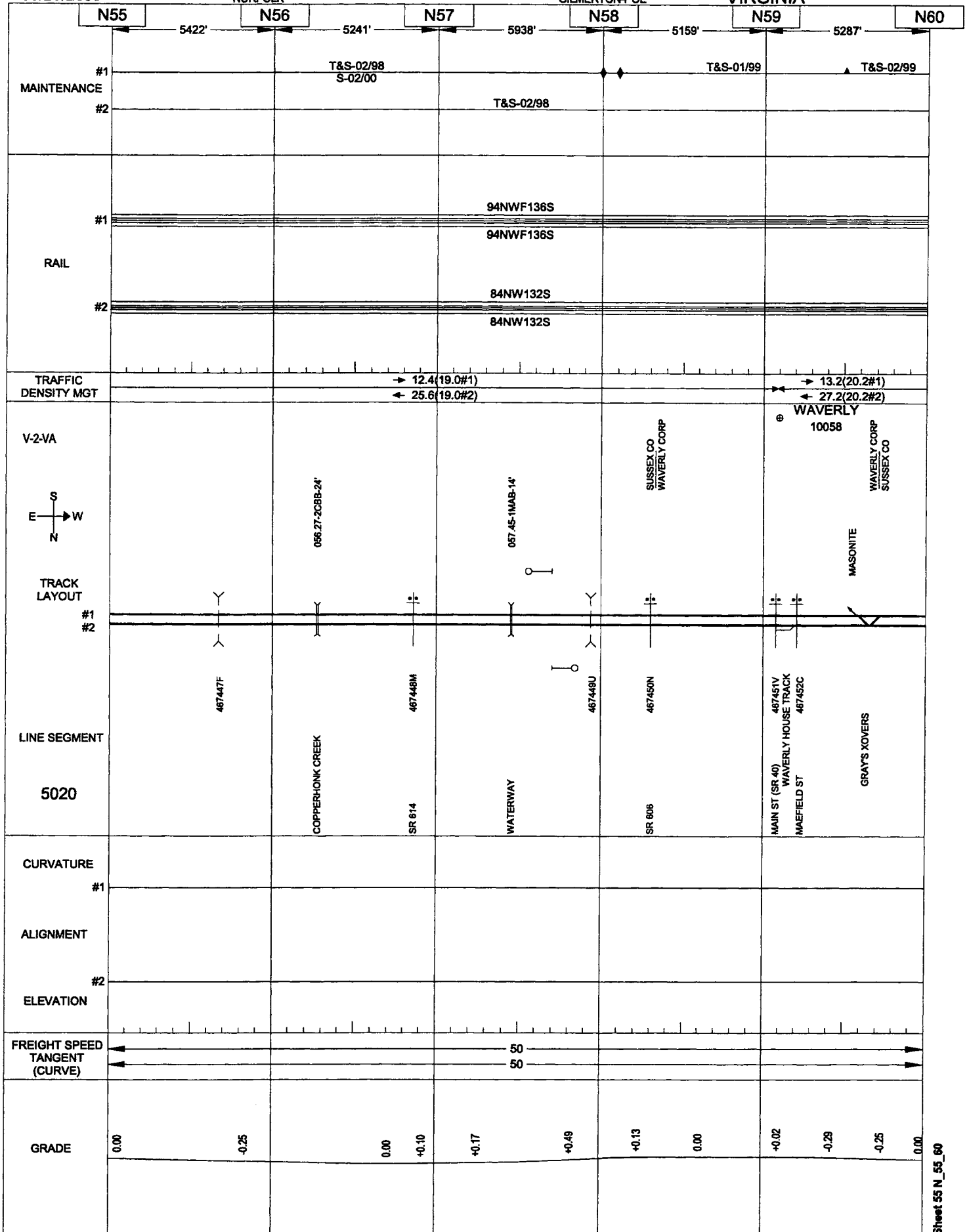
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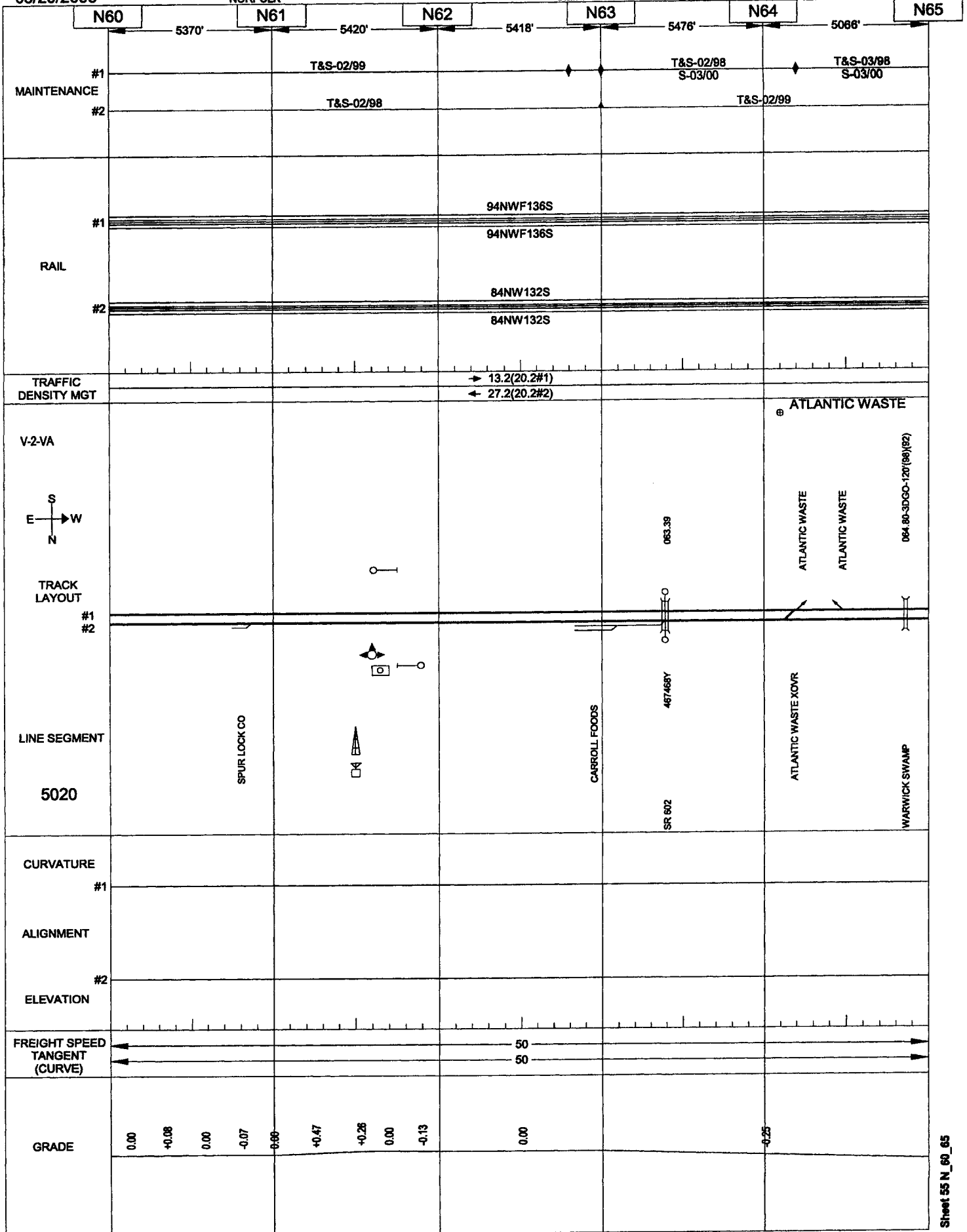


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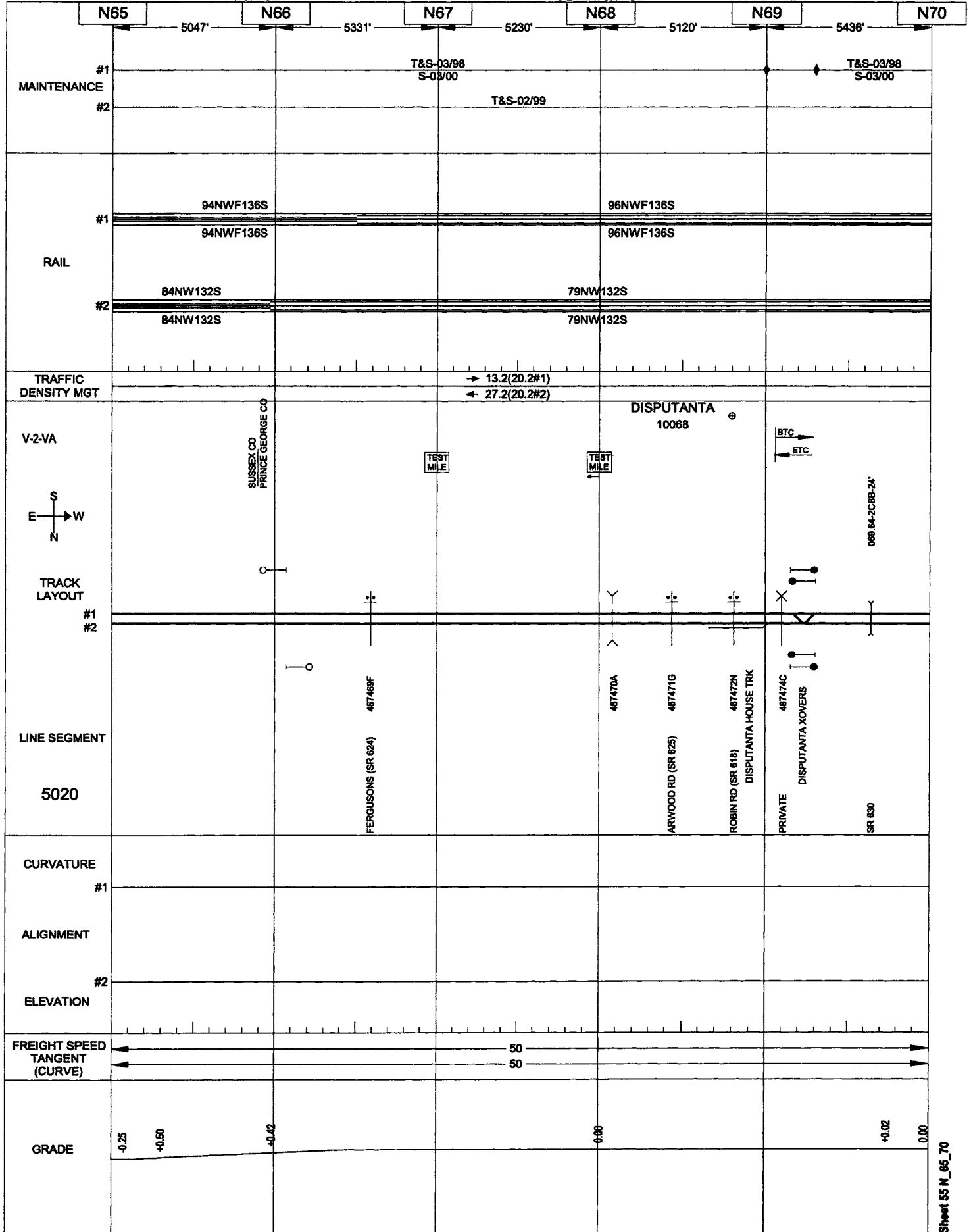


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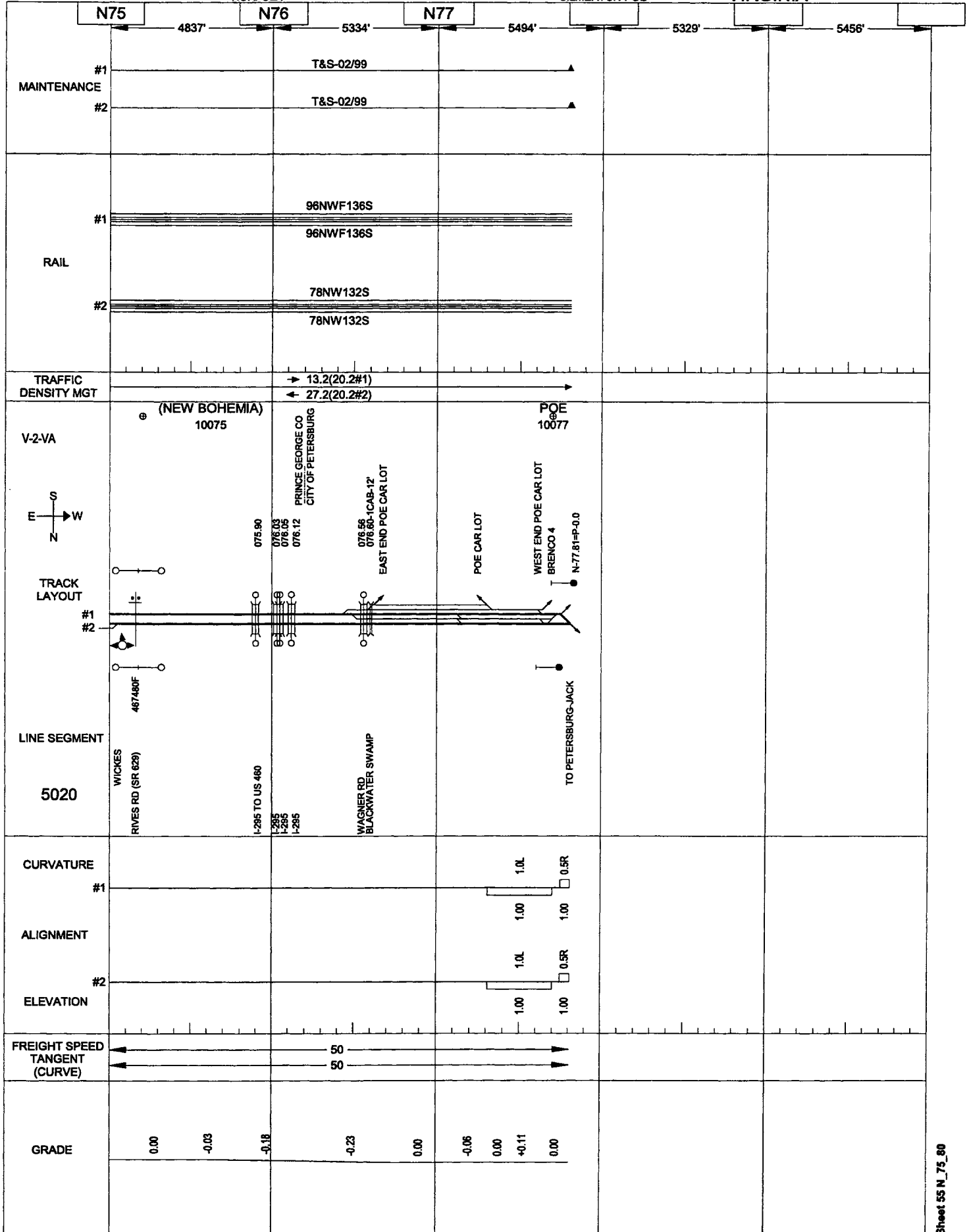
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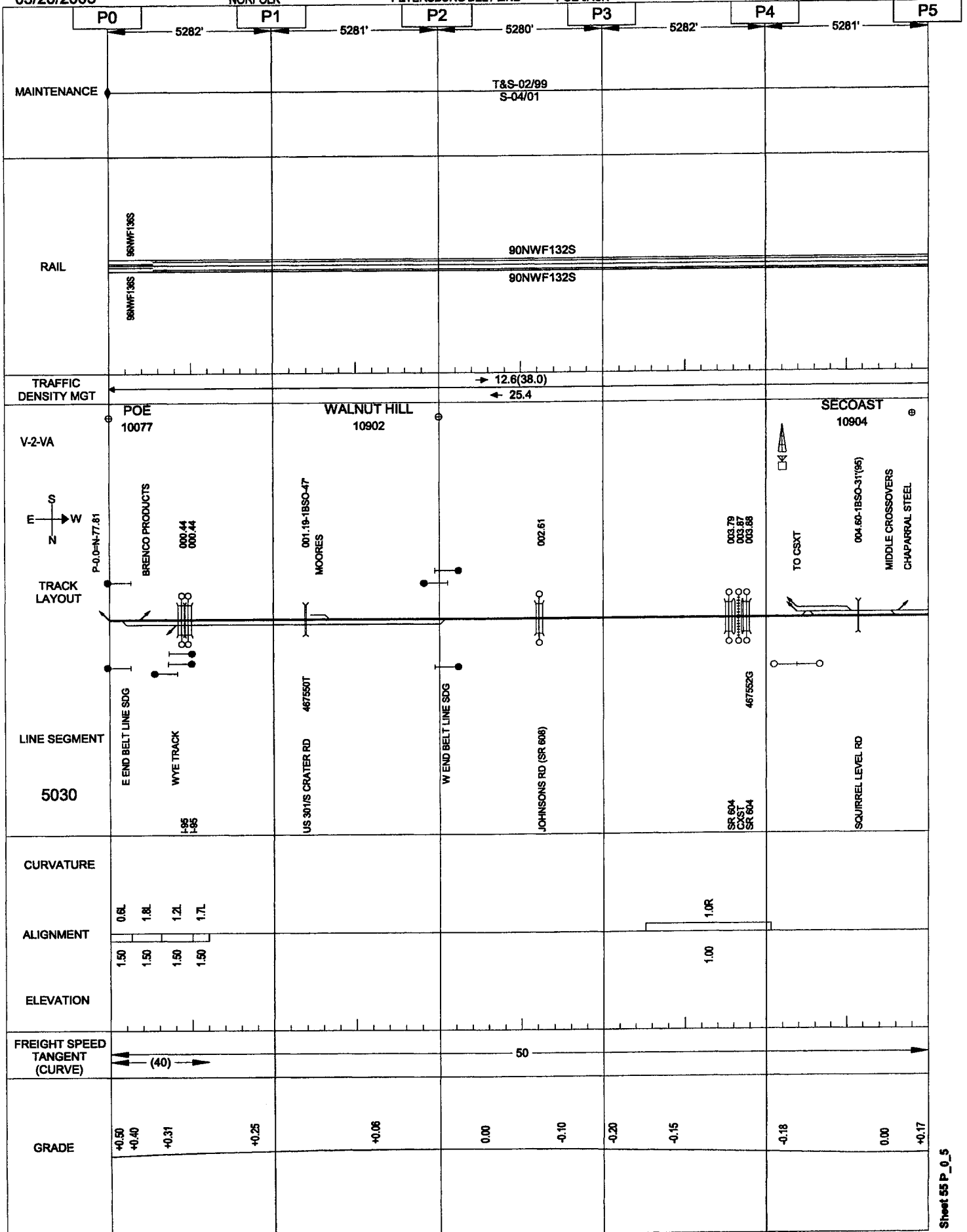
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NORFOLK

PETERSBURG BELT LINE

POE-JACK

VIRGINIA



020

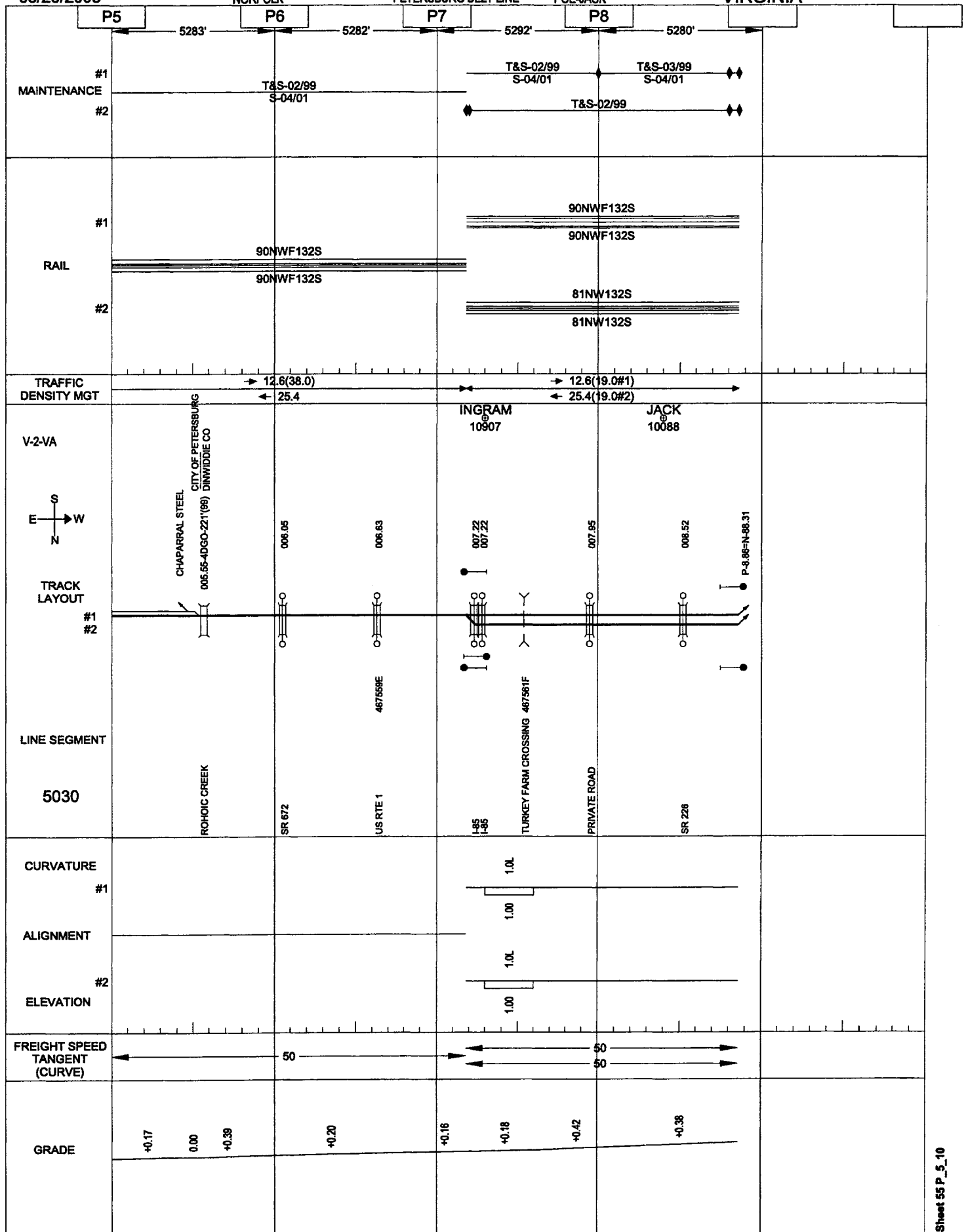
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NORFOLK

PETERSBURG BELT LINE

POE-JACK

VIRGINIA



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NORFOLK

POE-PETERSBURG-JACK

VIRGINIA

N78

N79

N80

4837'

5334'

5494'

5329'

5456'

MAINTENANCE

T&S-09/96

RAIL

83W132S

83W132S

TRAFFIC
DENSITY MGT

→ 0.9(3.1)

← 2.2

V-2-VA

TRACK
LAYOUTPOE
10077

LANE

WYE AT POE, VA

078.88
078.96

079.47-1CAB-12

LINE SEGMENT

TO NORFOLK

5050

US 460
CRATER RD

POOL CREEK

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

GRADE

1.3R

1.0R

0.6R

1.6L

1.50

1.50

1.50

1.50

40

-0.70

+0.75%

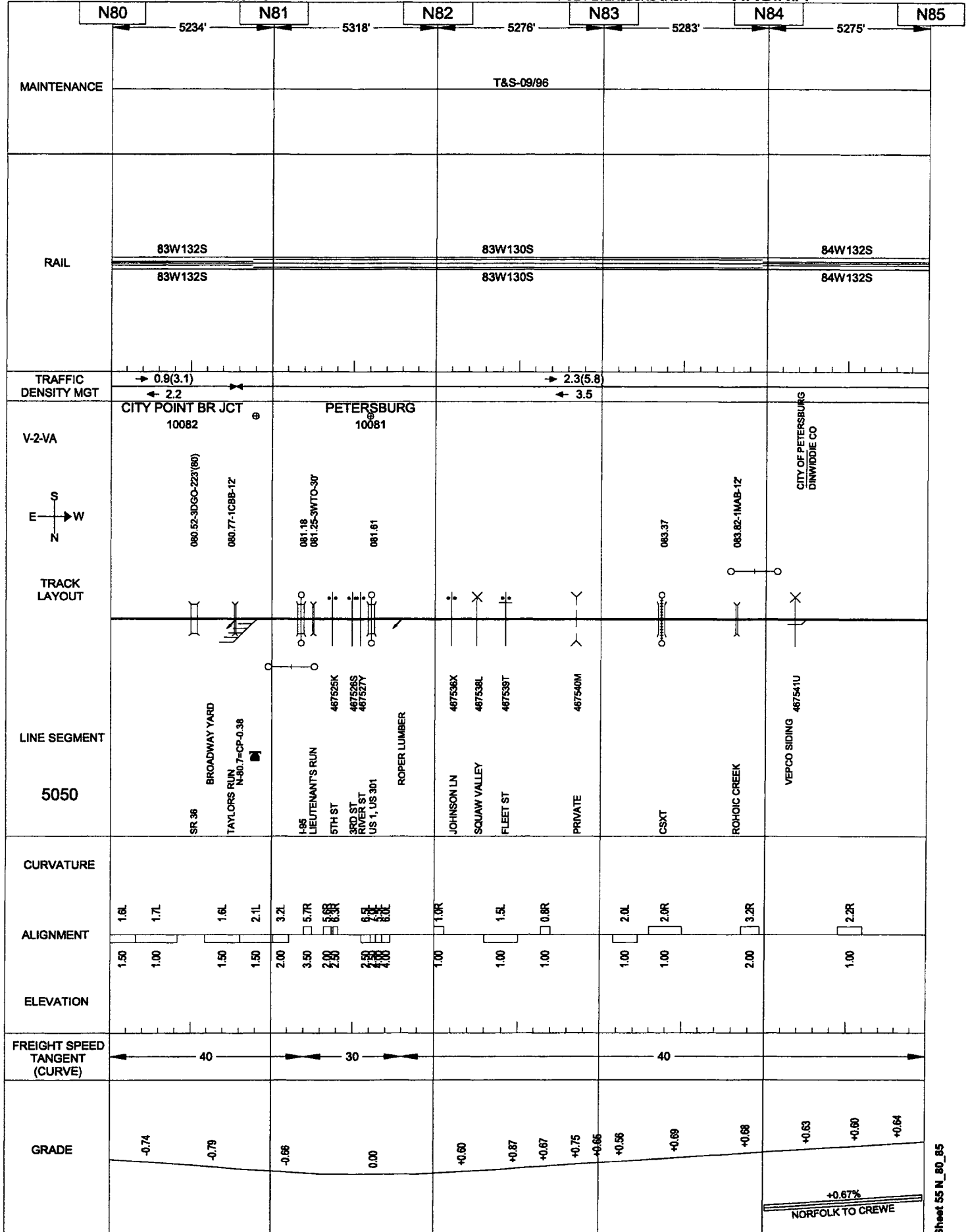
CREWE TO NORFOLK

03/20/2003

NORFOLK

POE-PETERSBURG-JACK

VIRGINIA

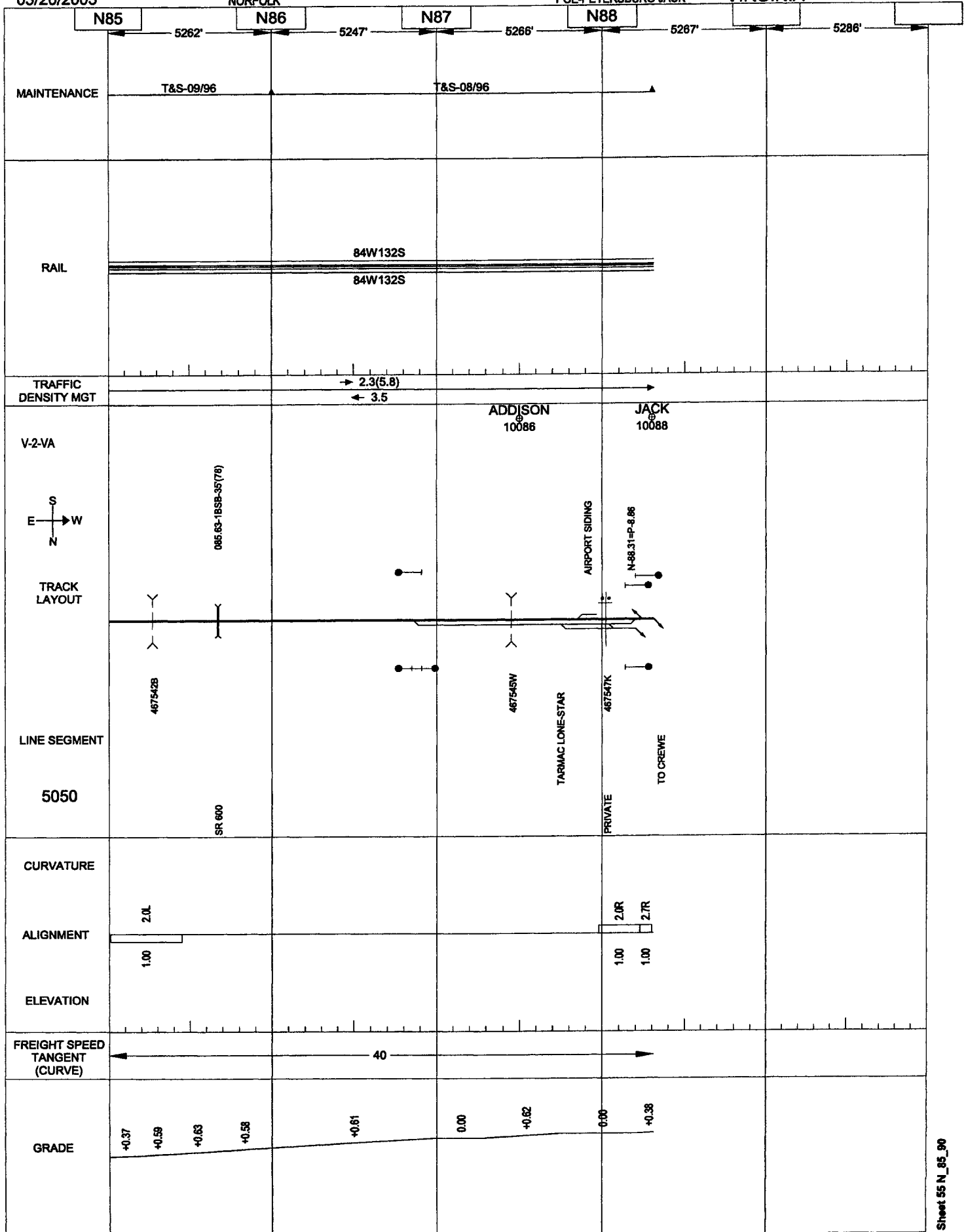


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NORFOLK

POE-PETERSBURG-JACK

VIRGINIA



024

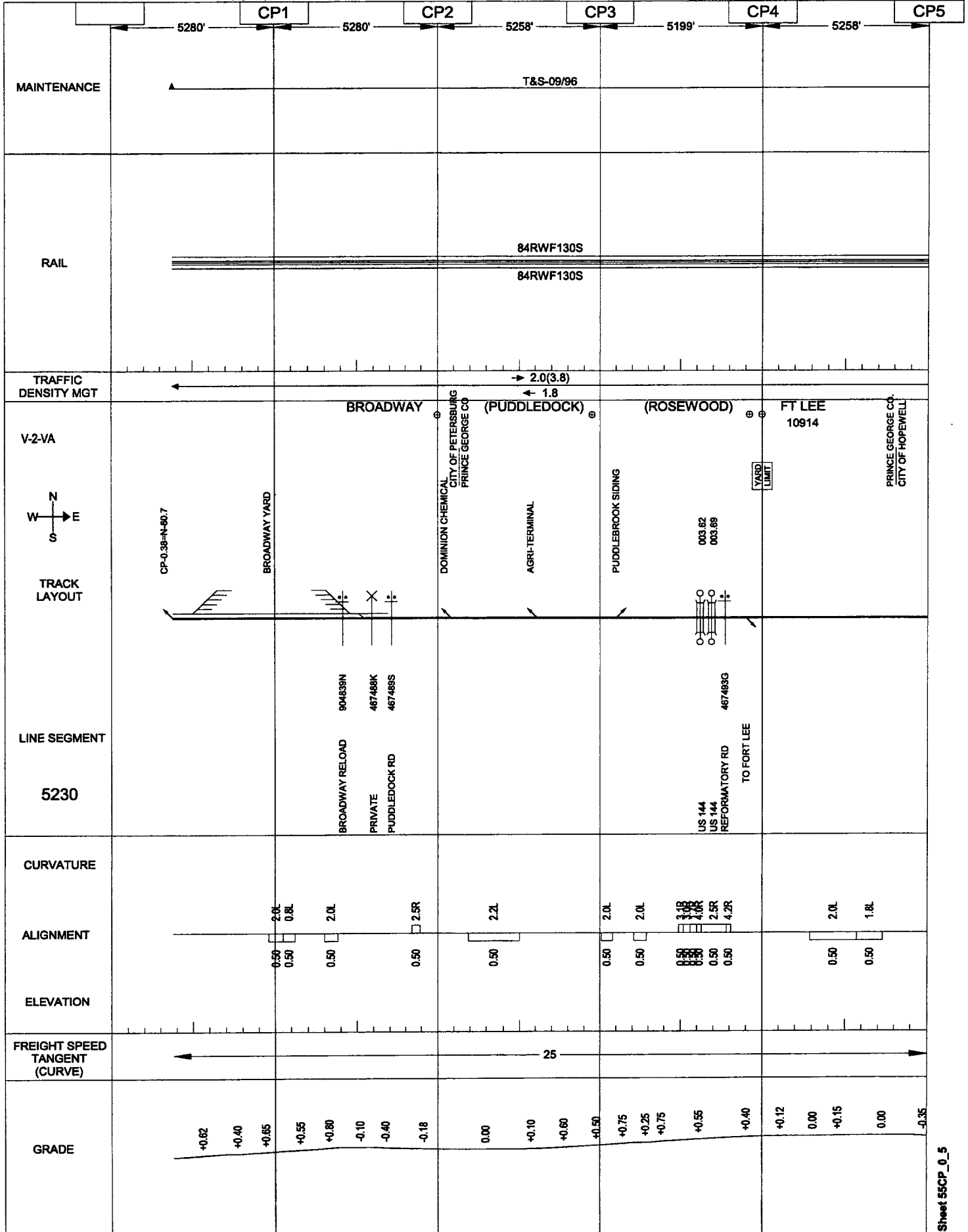
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NORFOLK

CITY POINT BRANCH

PETERSBURG-HOPEWELL

VIRGINIA



025

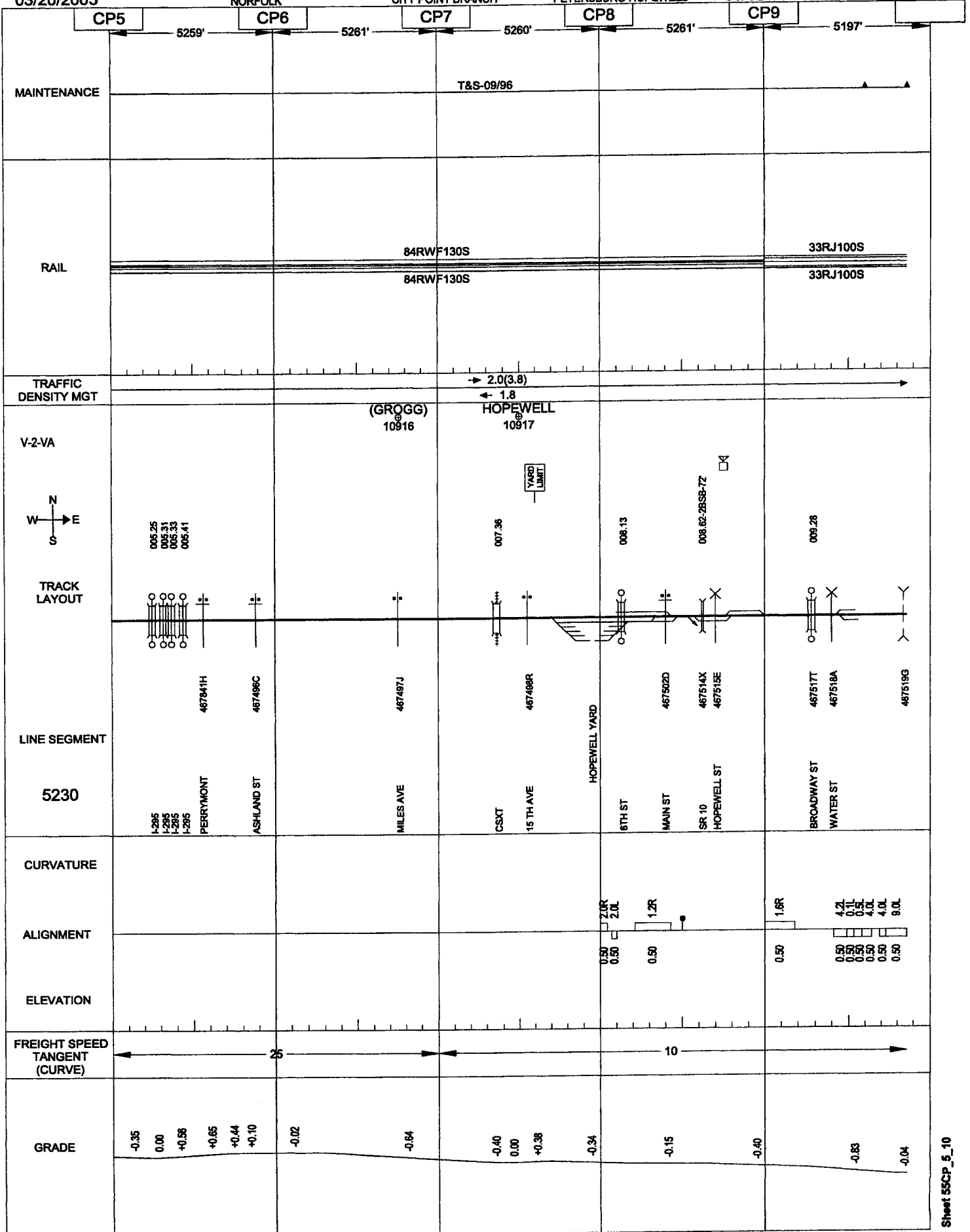
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NORFOLK

CITY POINT BRANCH

PETERSBURG-HOPEWELL

VIRGINIA



03/20/2003

026

NORFOLK

JACK-CREWE

VIRGINIA

N89

N90

5262'

5247'

5266'

5267'

5286'

#1
MAINTENANCE
#2

T&S-03/98

T&S-02/99

RAIL

#1

84NW132S

84NW132S

#2

84NWF132S

84NWF132S

TRAFFIC
DENSITY MGT

14.8(21.9#1)

28.9(21.9#2)

V-2-VA

S
E → W
N

TRACK
LAYOUT

#1
#2JACK
10088BTC
ETC

N-88.31-P-8.88

088.49-1MAB-12

LINE SEGMENT

5040

TO PETERSBURG

WATERWAY

OLGERS RD-SR 632

TINDALL CONCRETE

PRIVATE

487567W

487568D

CURVATURE

#1

ALIGNMENT

#2

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

+0.60

+0.65%

LAMBERTS PT TO CREWE

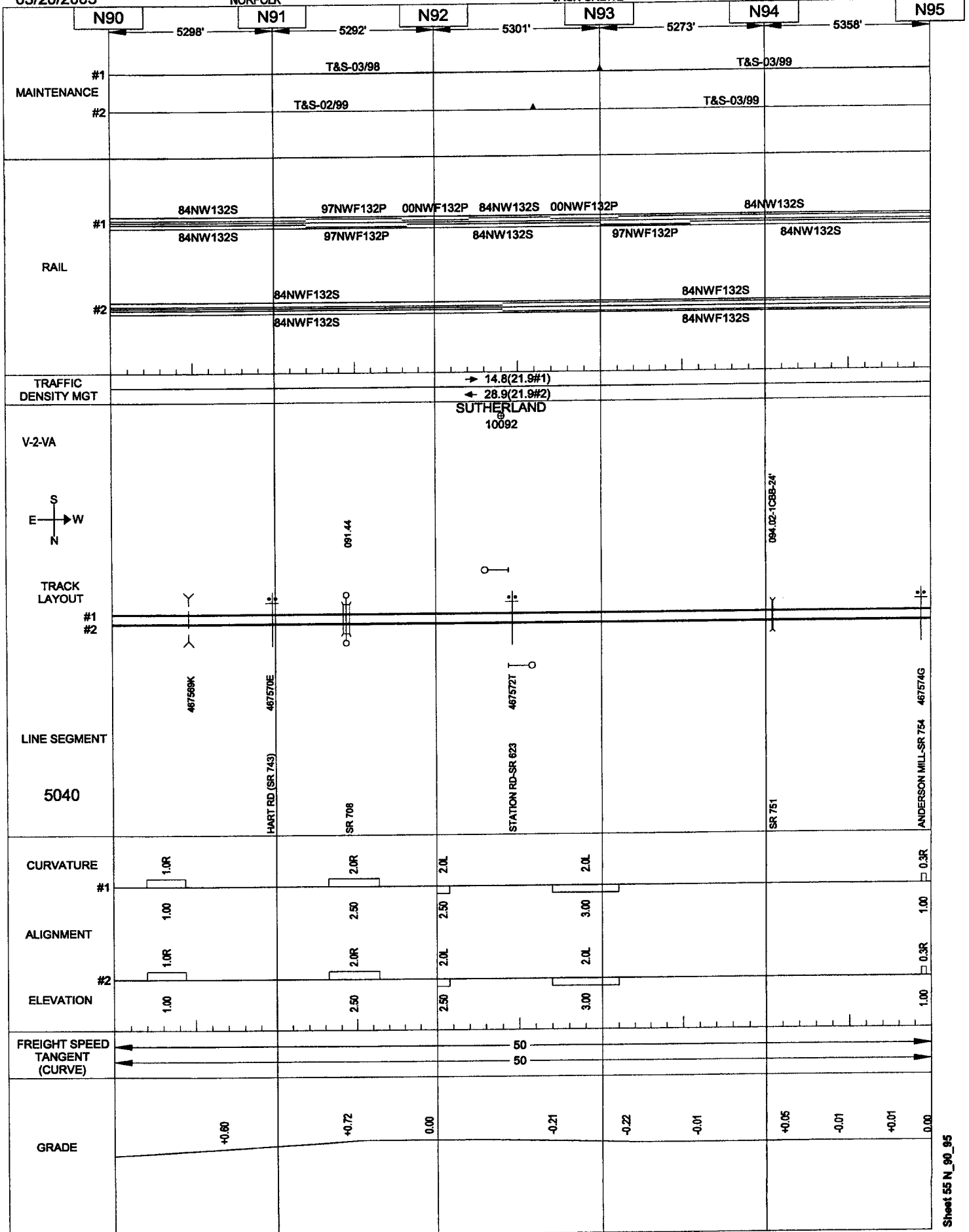
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NORFOLK

JACK-CREWE

VIRGINIA

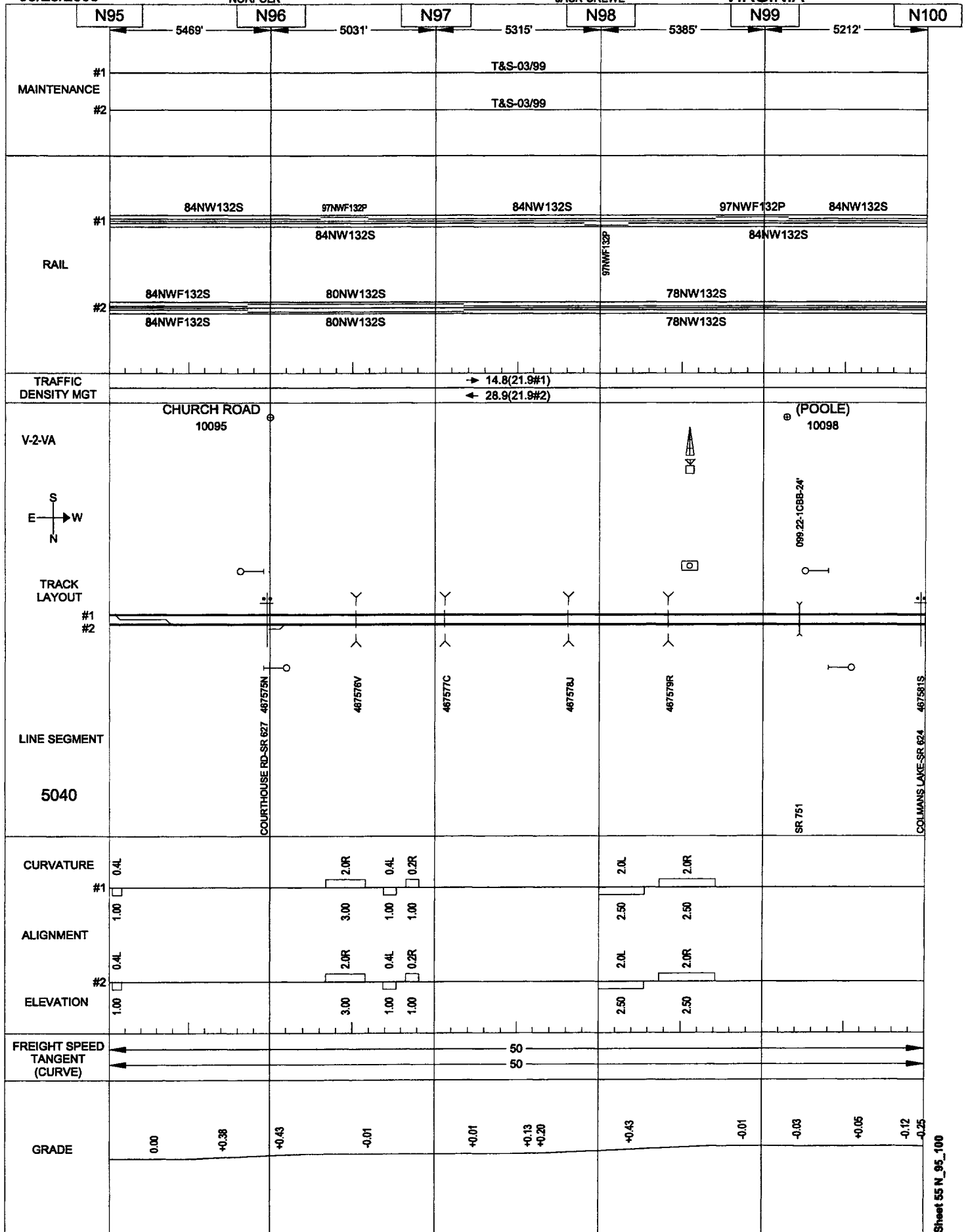


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NORFOLK

JACK-CREWE

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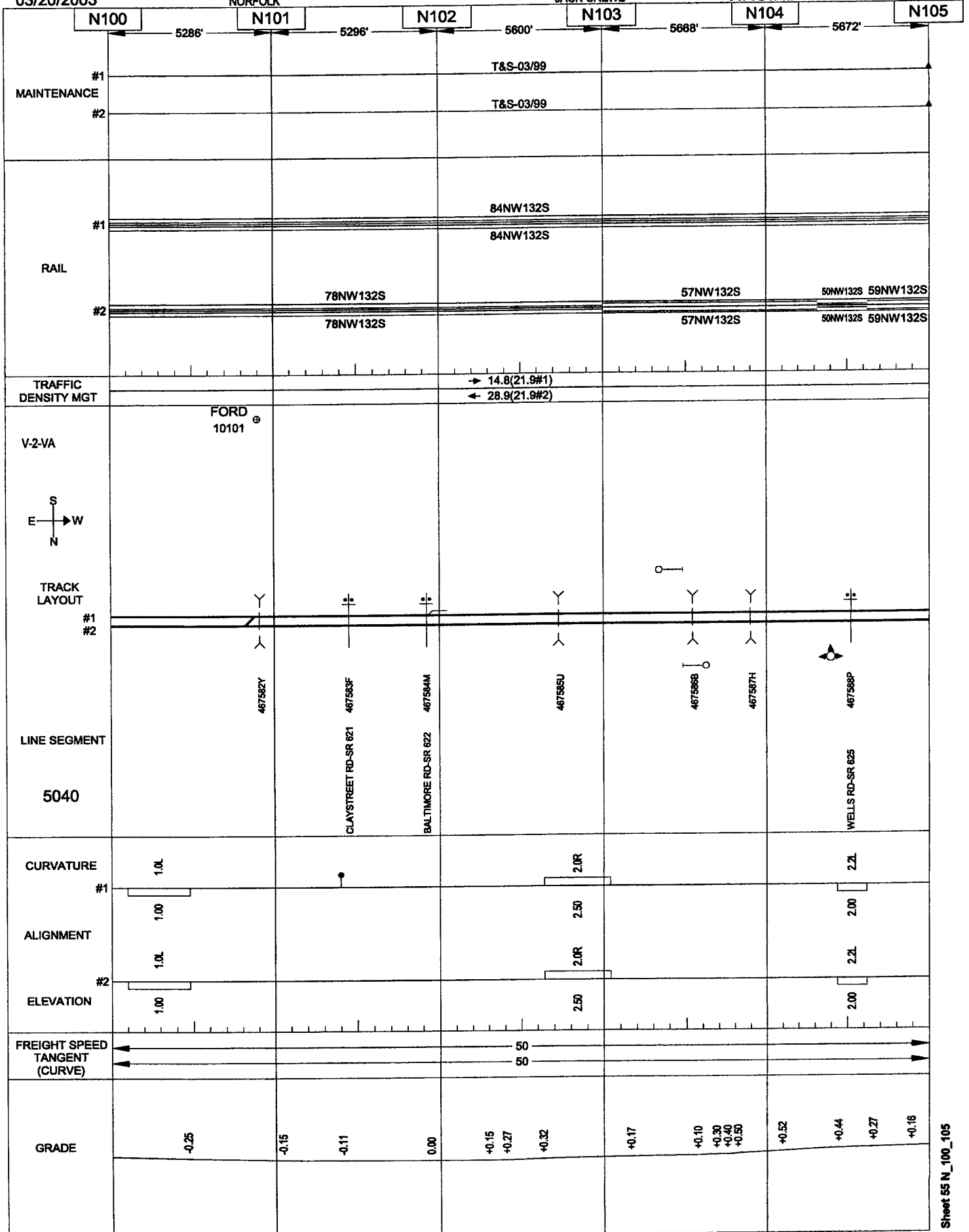


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NORFOLK

JACK-CREWE

VIRGINIA



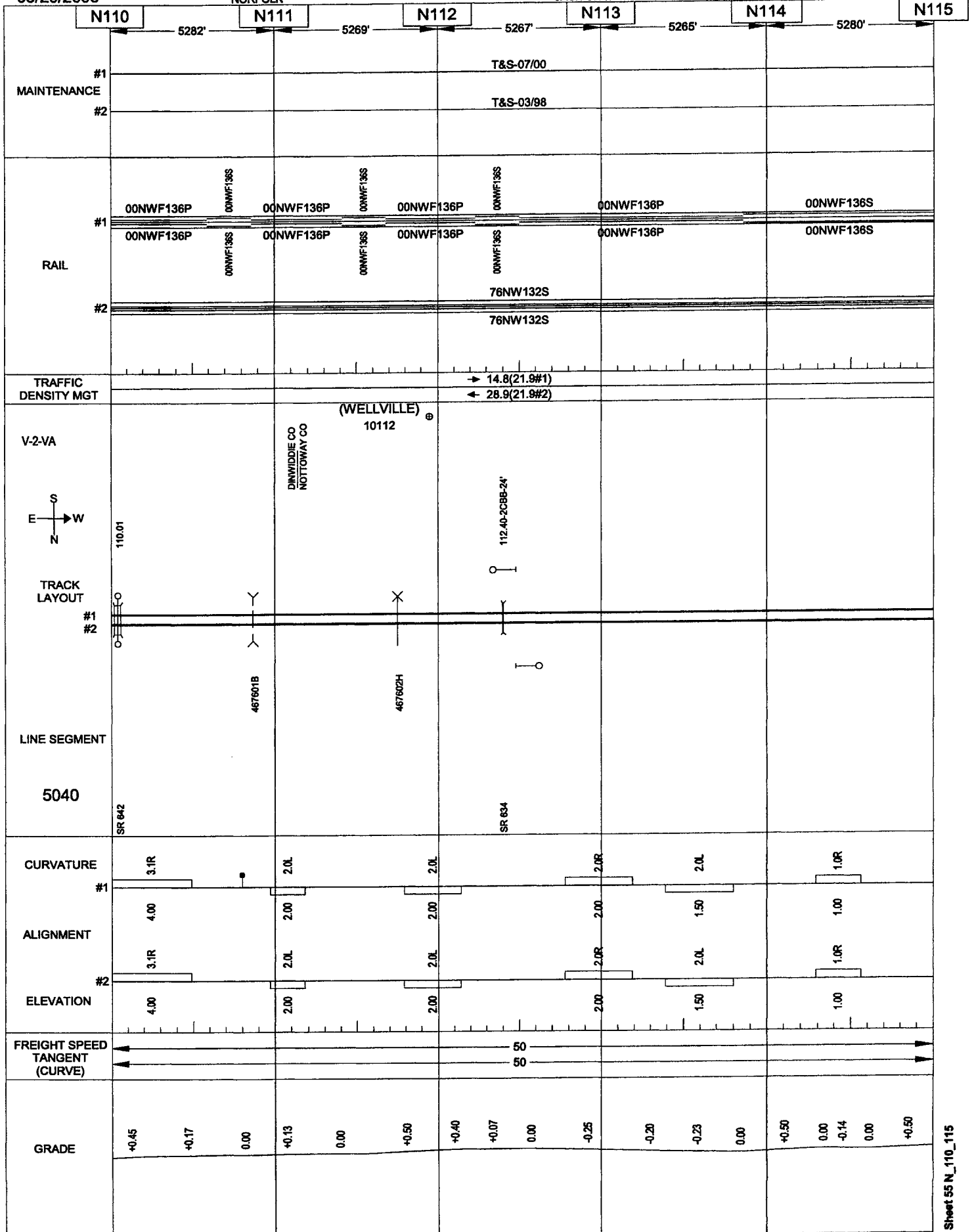
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NORFOLK

JACK-CREWE

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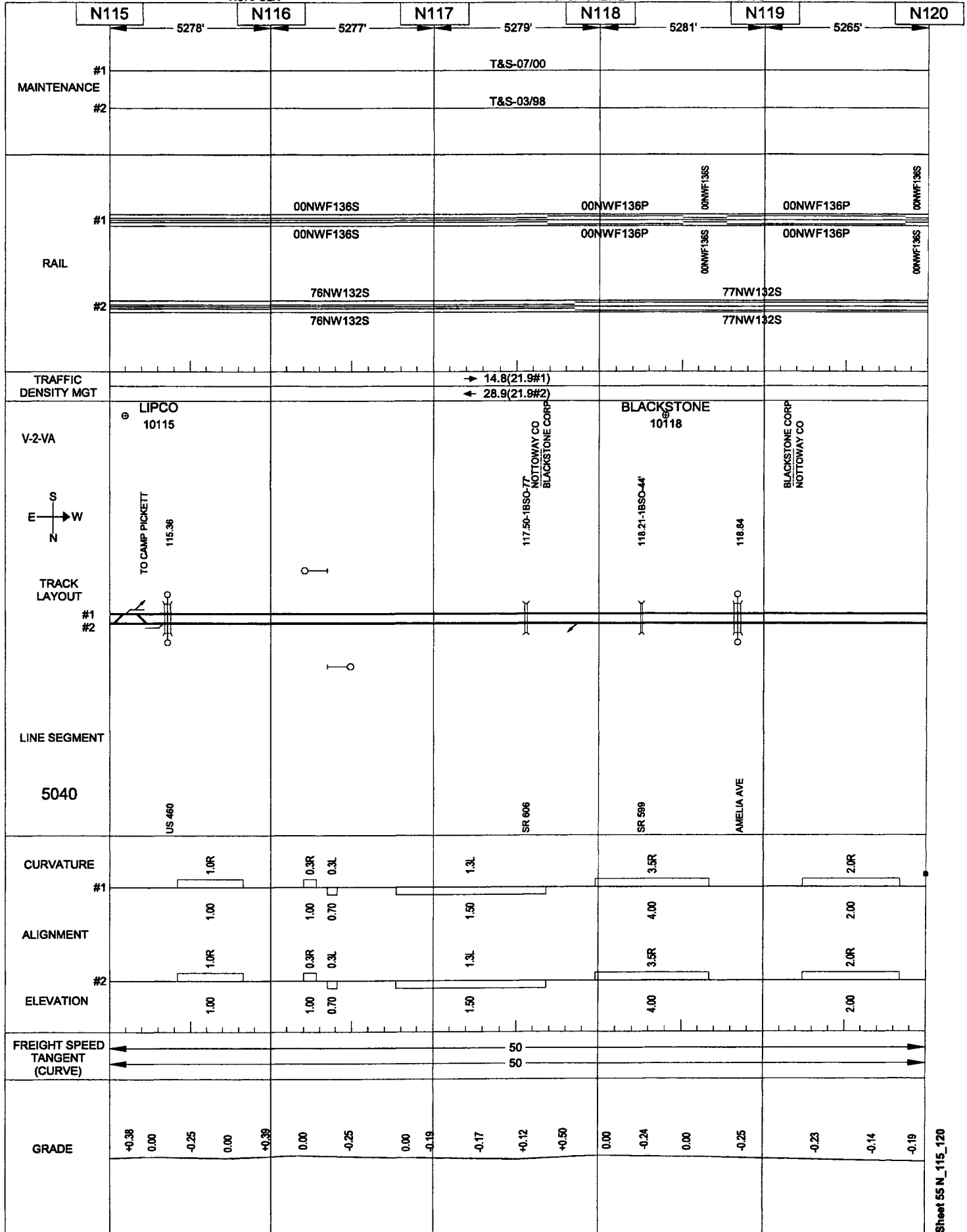


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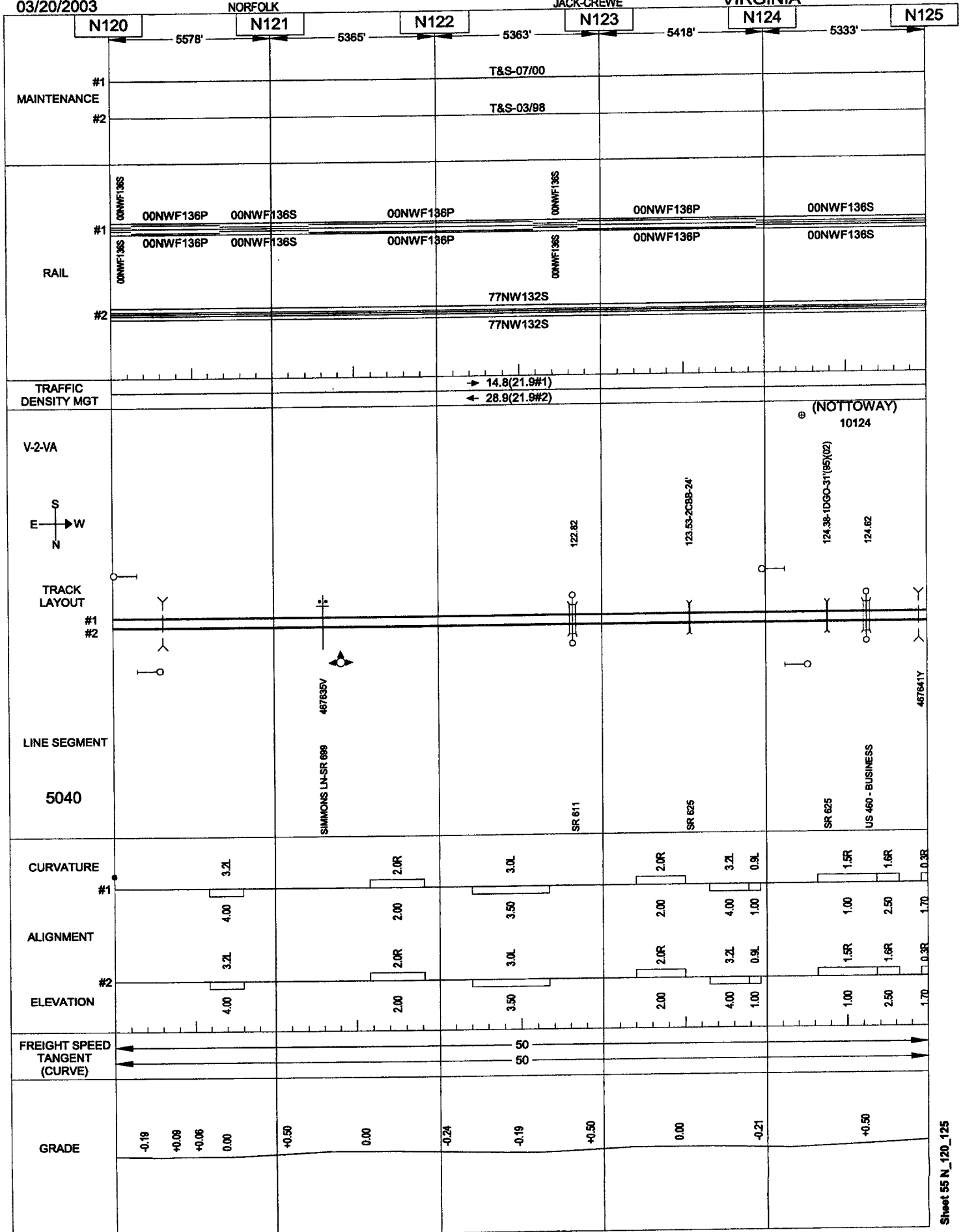


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JACK-CREWE

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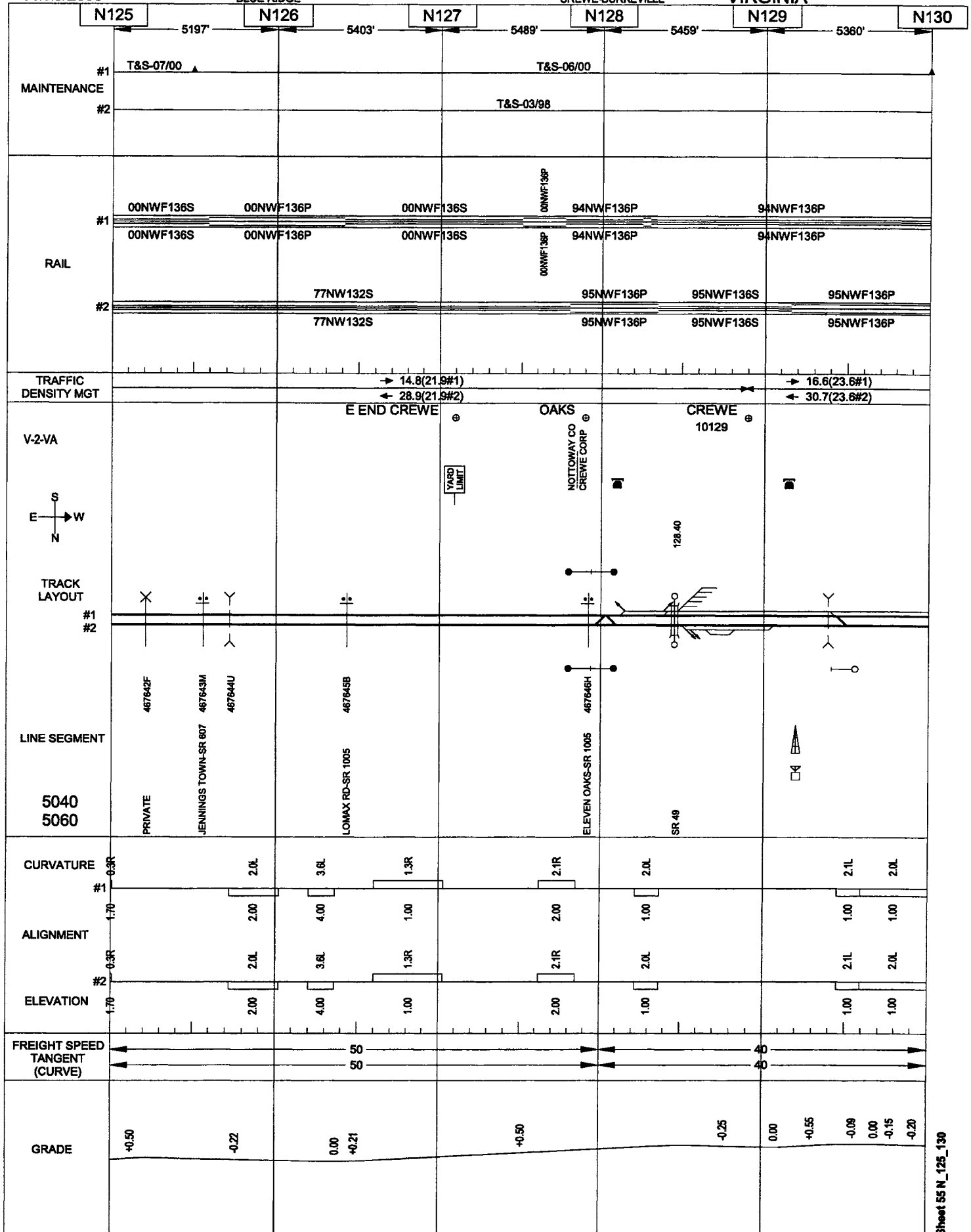


03/20/2003

BLUE RIDGE

CREWE-BURKEVILLE

VIRGINIA

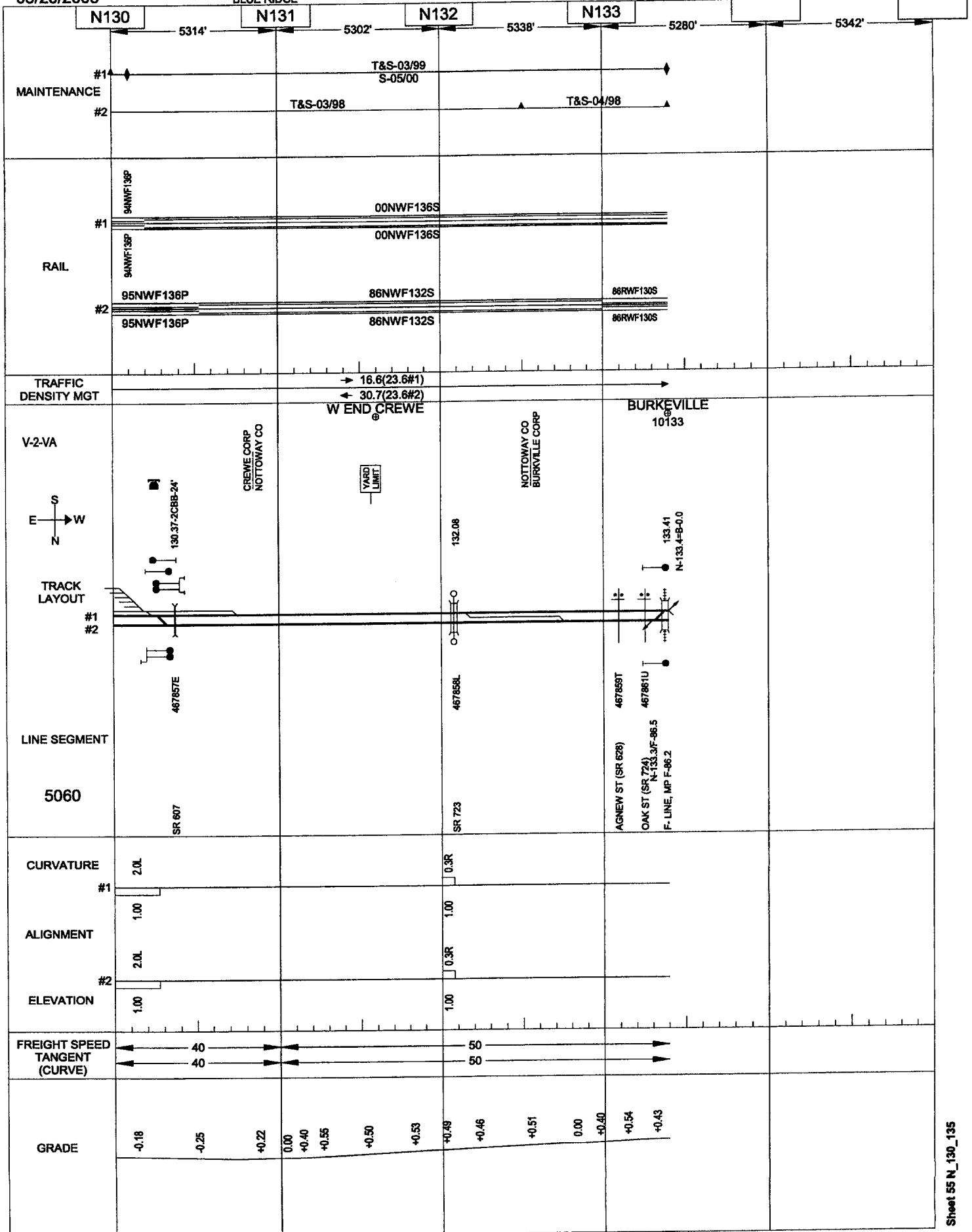


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BLUE RIDGE

CREWE-BURKEVILLE

VIRGINIA



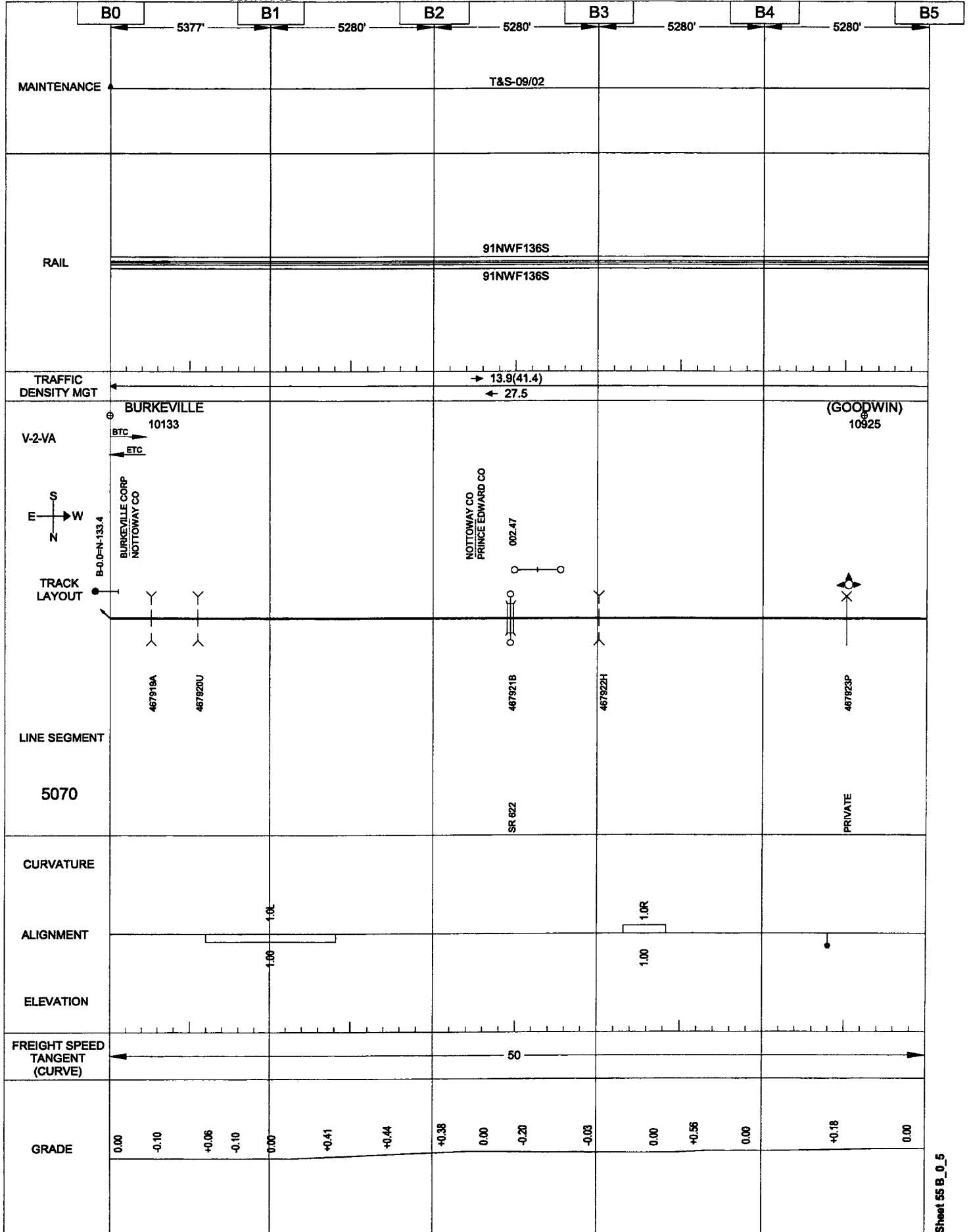
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



037

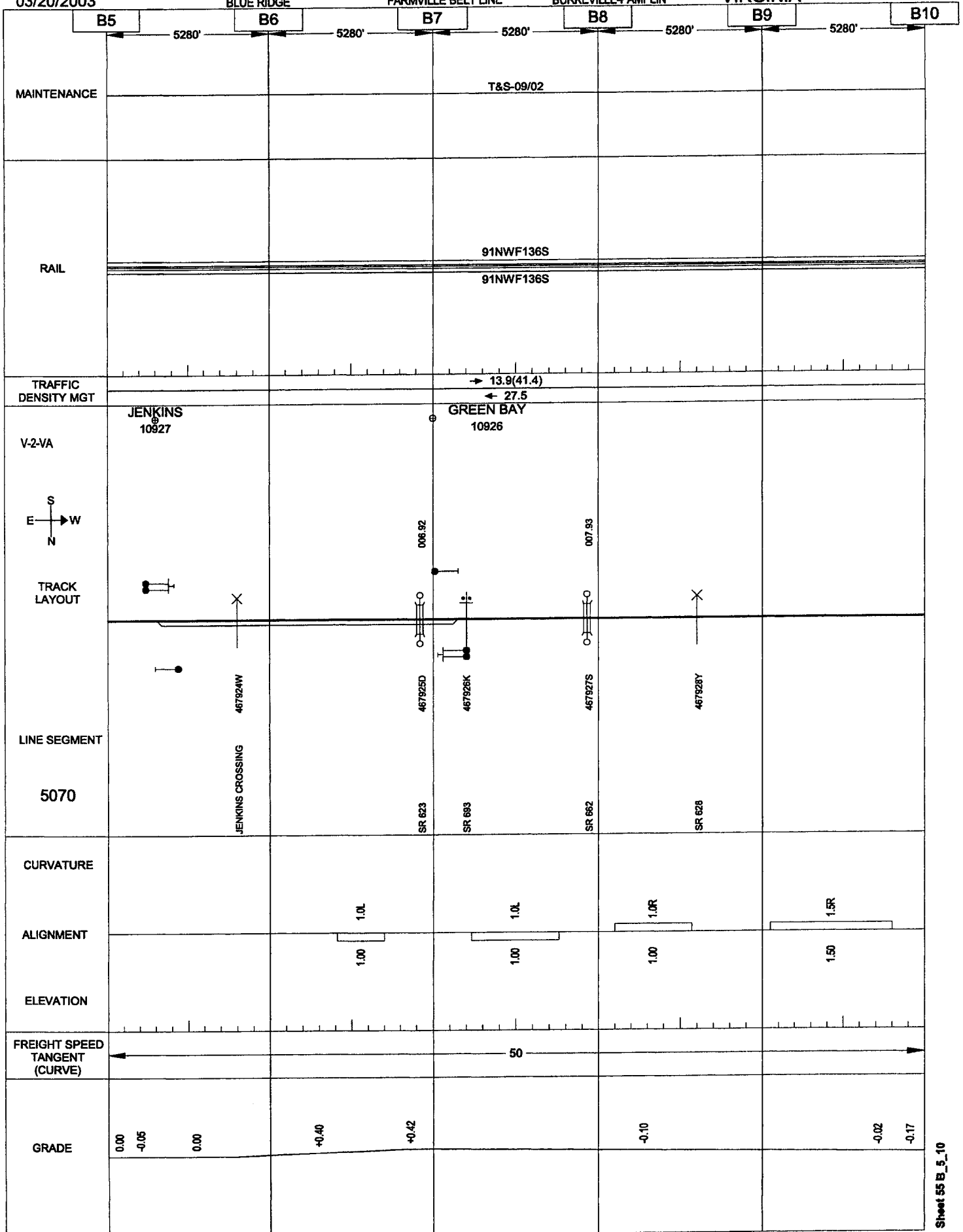
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FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



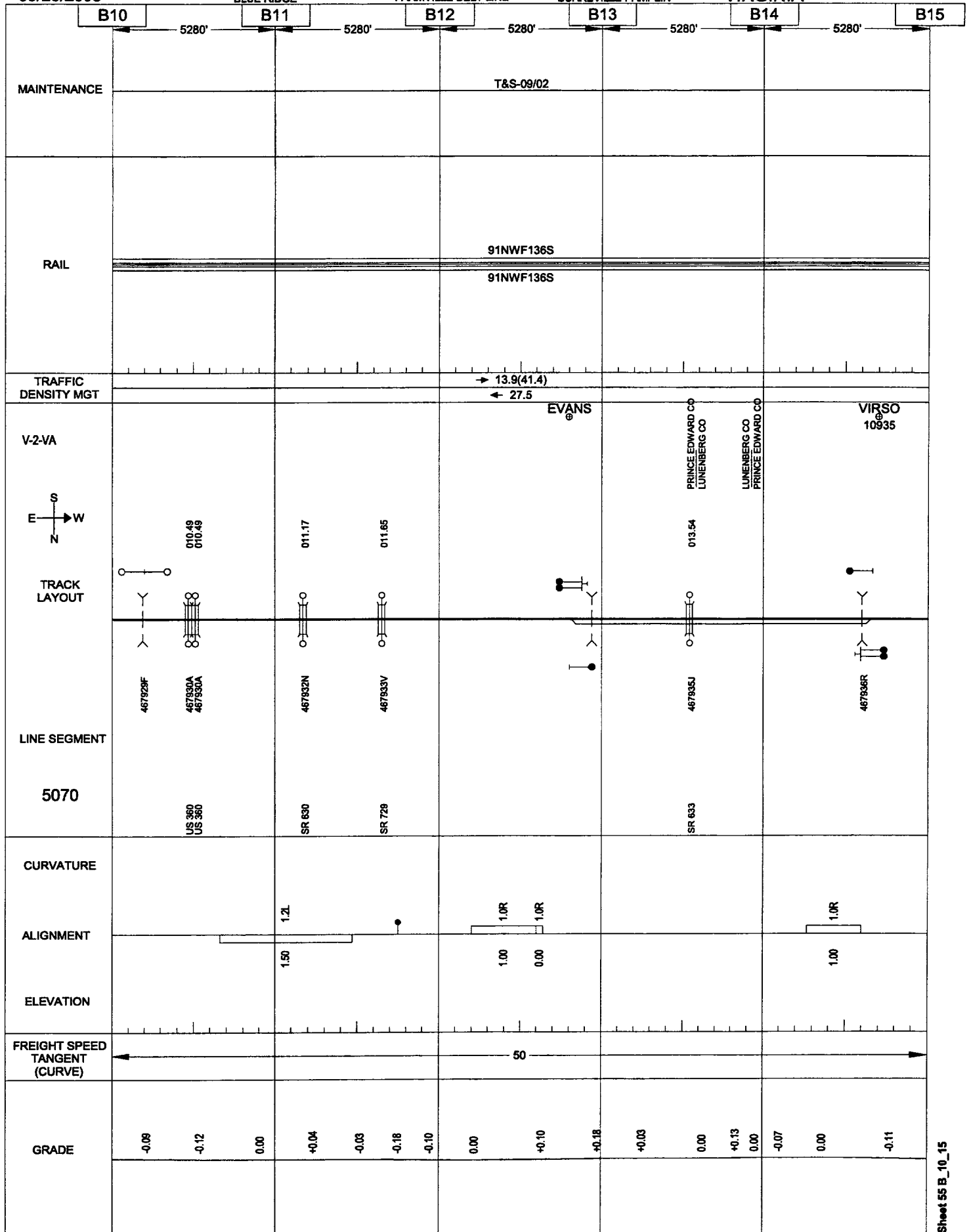
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



039

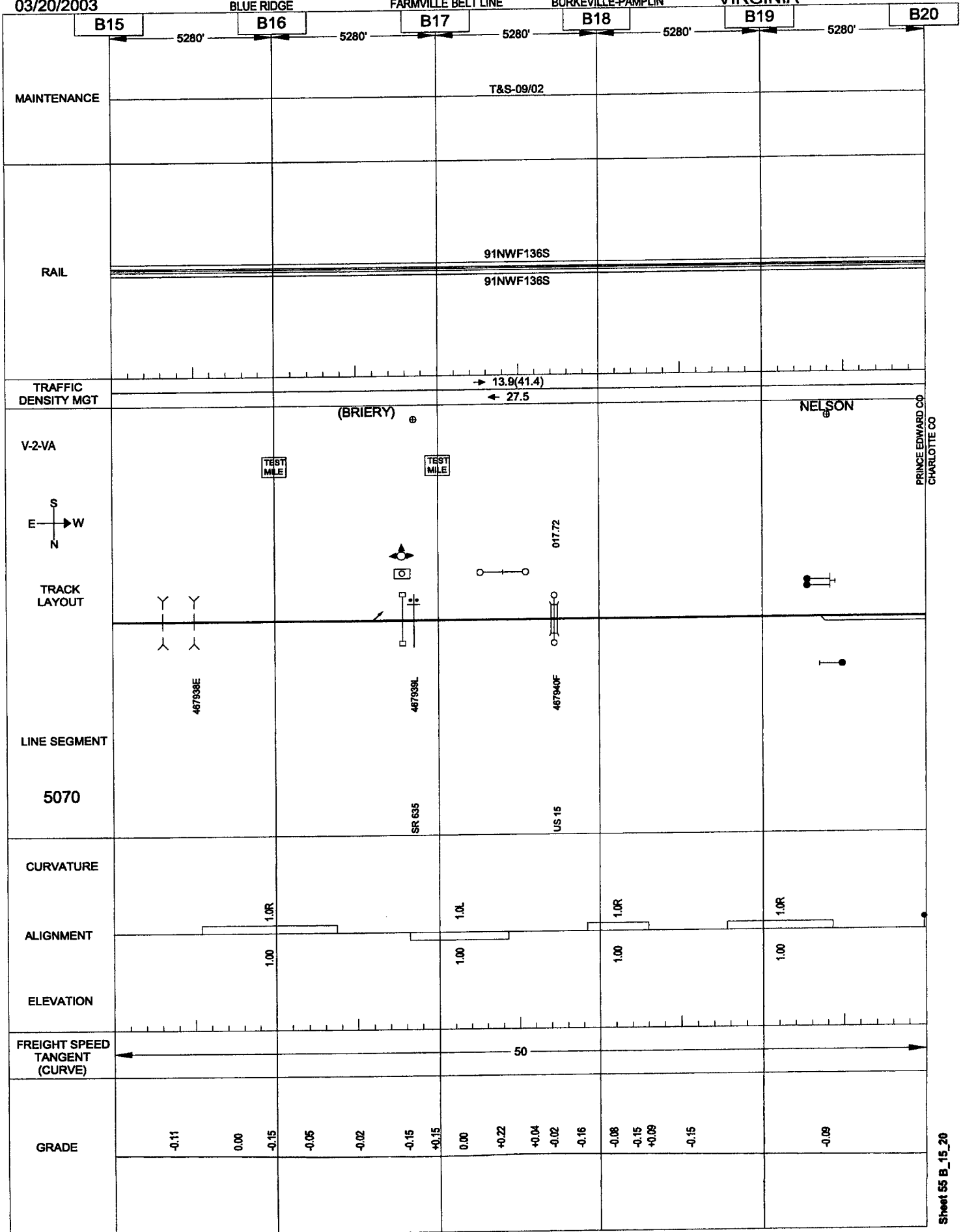
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



PRINCE EDWARD CO
CHARLOTTE CO

040

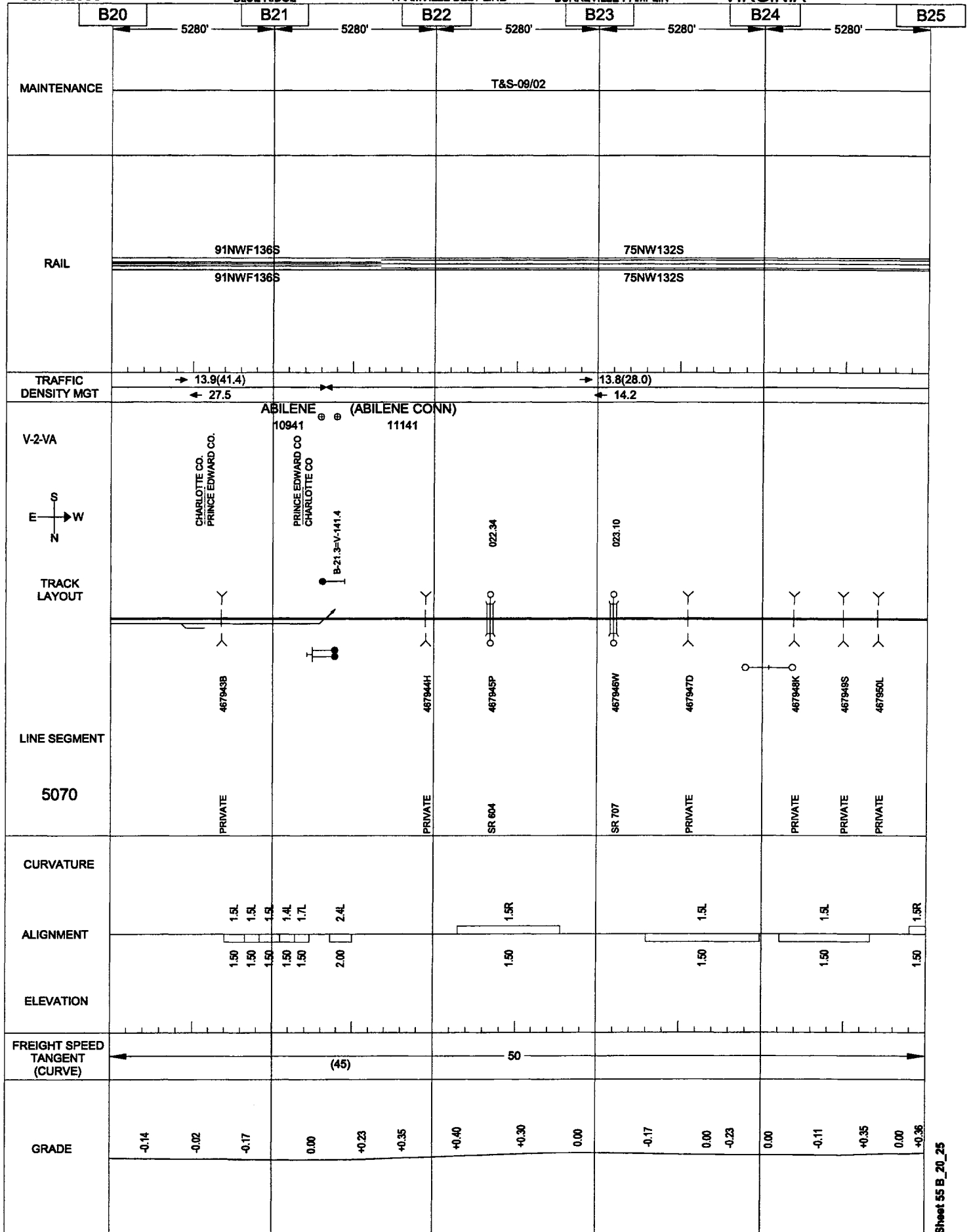
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



041

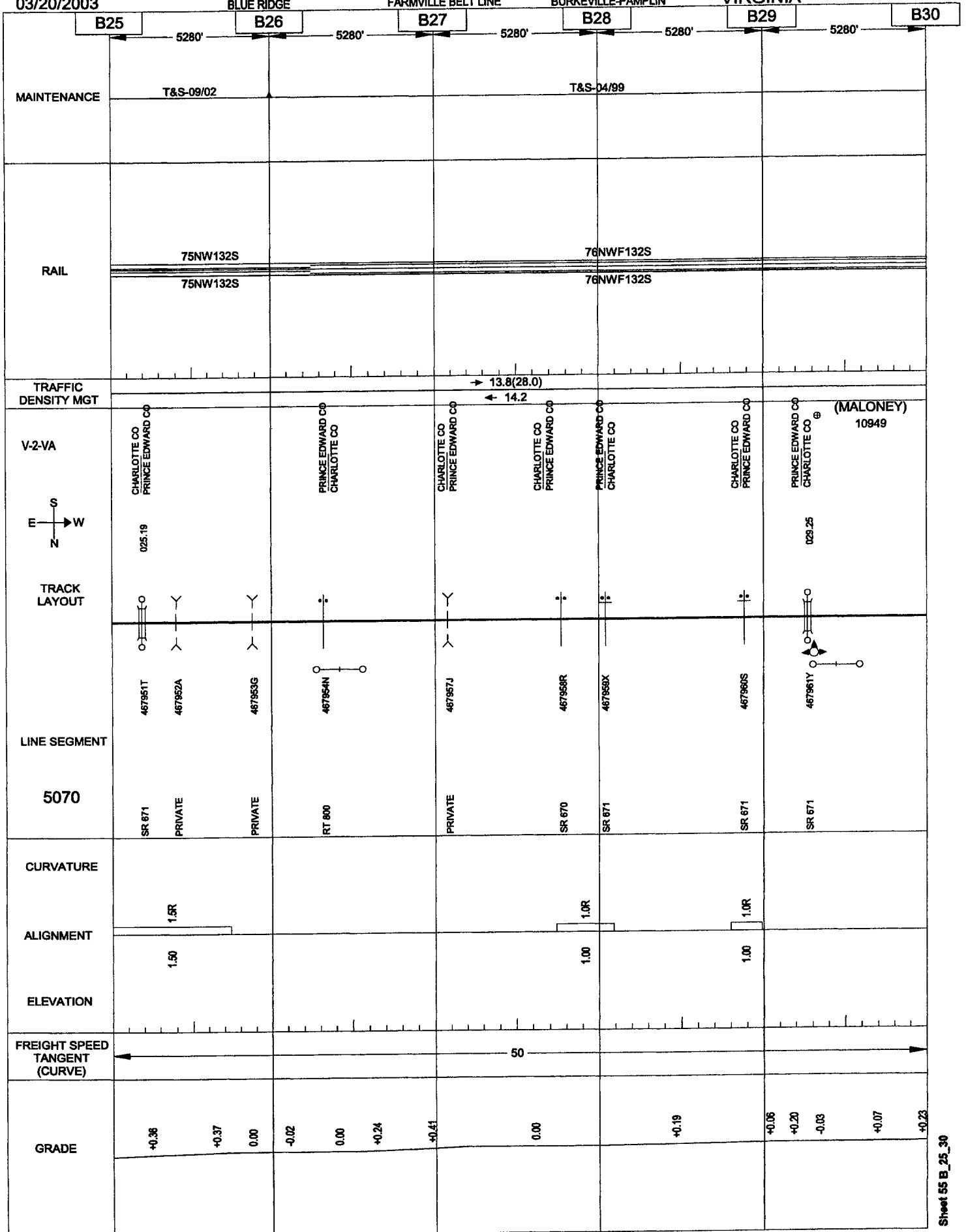
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



03/20/2003

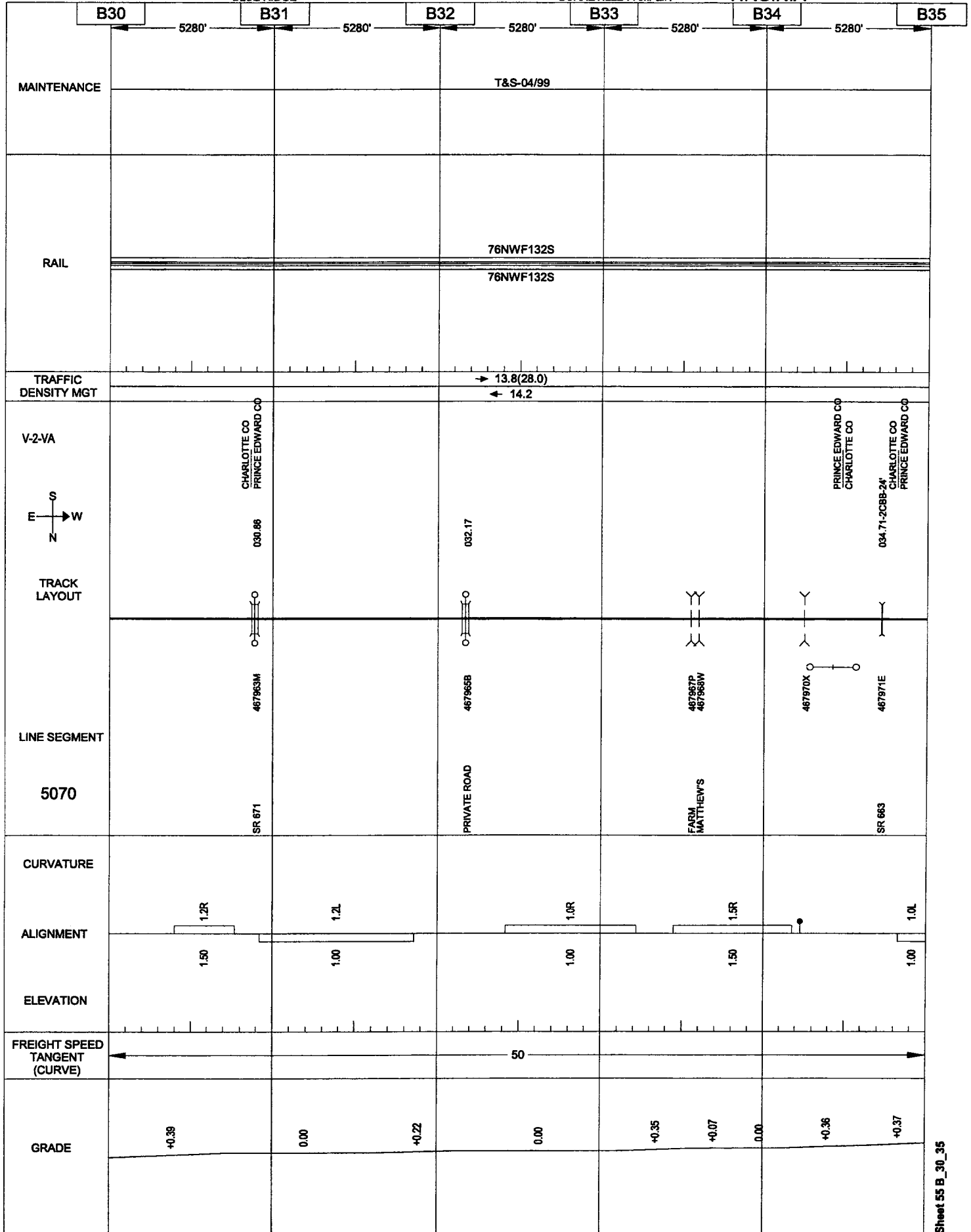
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



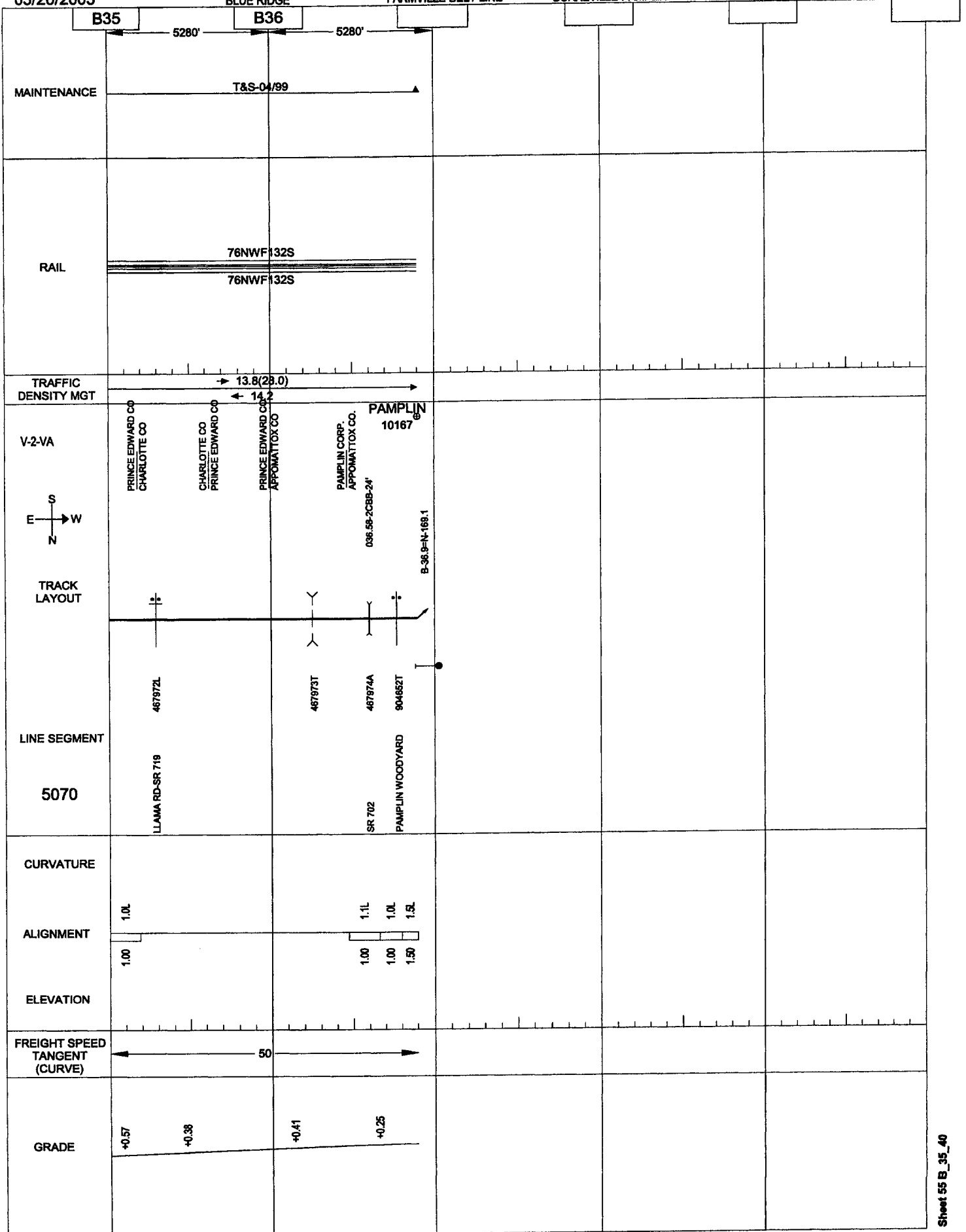
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BLUE RIDGE

FARMVILLE BELT LINE

BURKEVILLE-PAMPLIN

VIRGINIA



03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

N134

N135

5314'

5302'

5338'

5280'

5342'

MAINTENANCE

T&S-01/97

RAIL

00NWF130S
86NWF130S
86RWF130S
00NWF130S
86NWF130S
86RWF130S

TRAFFIC
DENSITY MGT

1.3(3.7)

2.4

V-2-VA

S
E → W
N

TRACK
LAYOUTBURKEVILLE
10133ATWOOD
10134

BURKEVILLE CORP
NOITOWAY CO
133.75-3DGO-75(78)

B-0.0 FARMVILLE BELT

134.18
134.18

TO CREWE

134.18
134.18

467862B

467863H
467863H

LINE SEGMENT

5120

OLD US 460

US 360
US 360

CURVATURE

ALIGNMENT

ELEVATION

2.0R

1.0R

1.00

1.00

FREIGHT SPEED
TANGENT
(CURVE)

40

GRADE

0.00

-0.23

-0.25

0.00

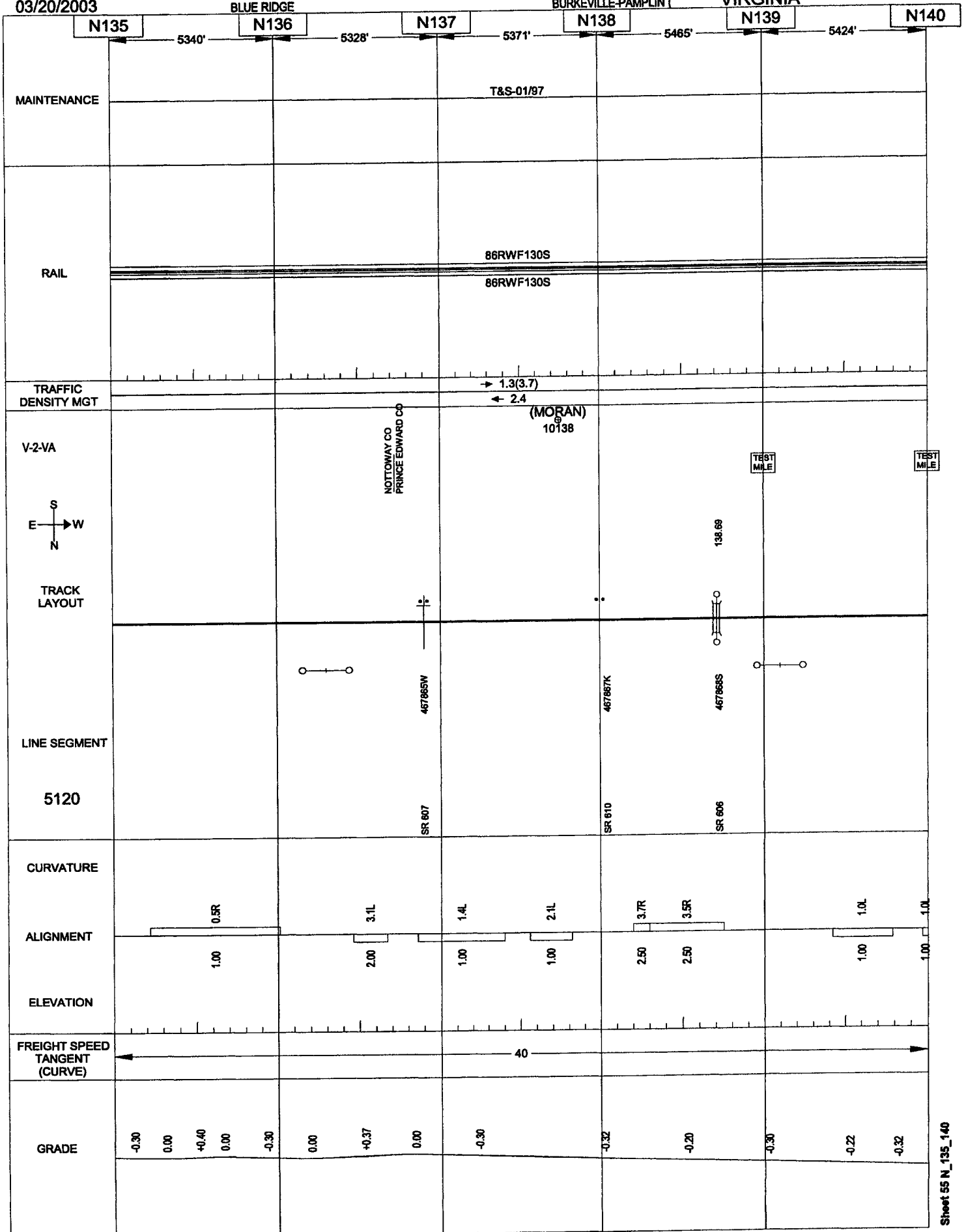
-0.30

03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

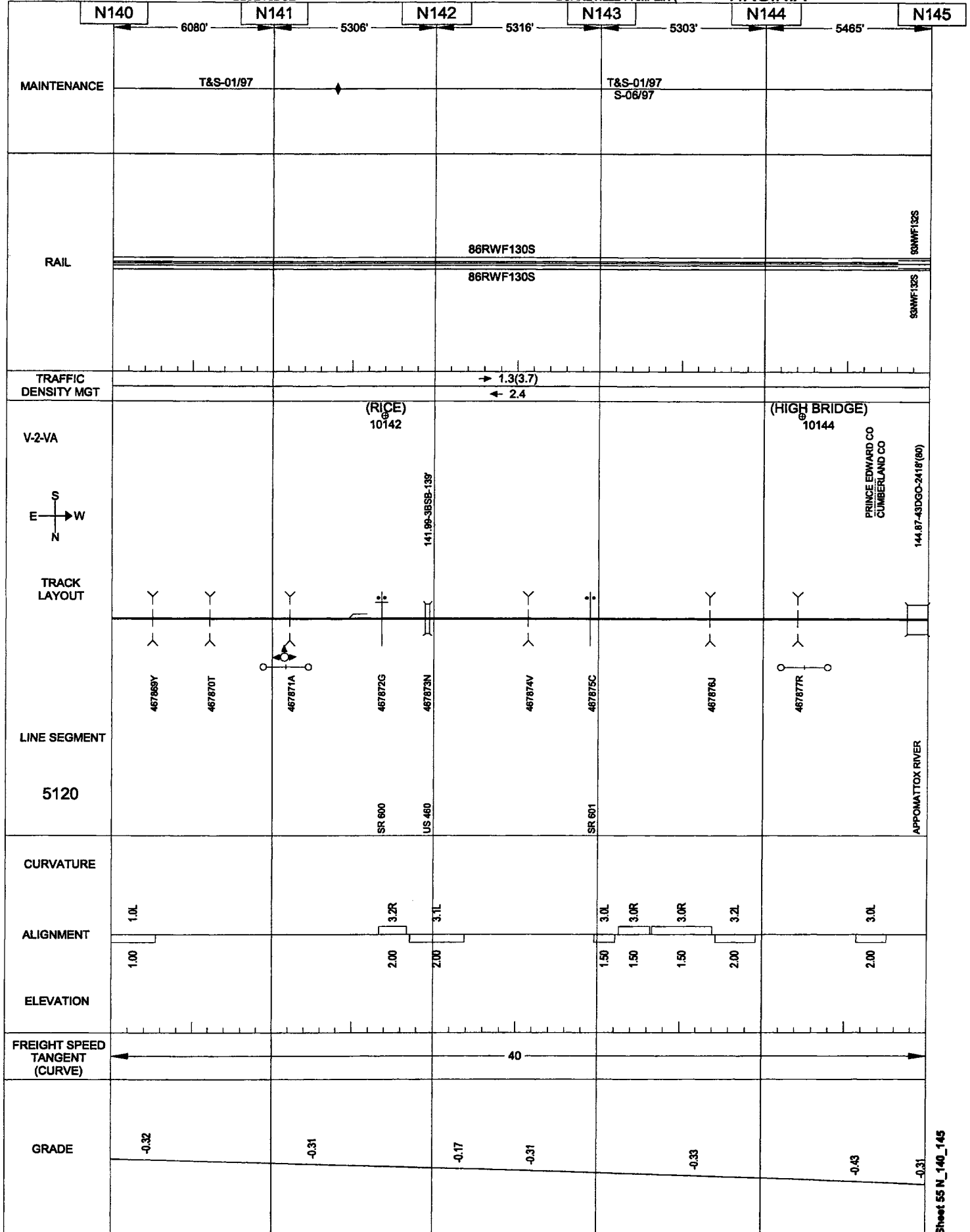


03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

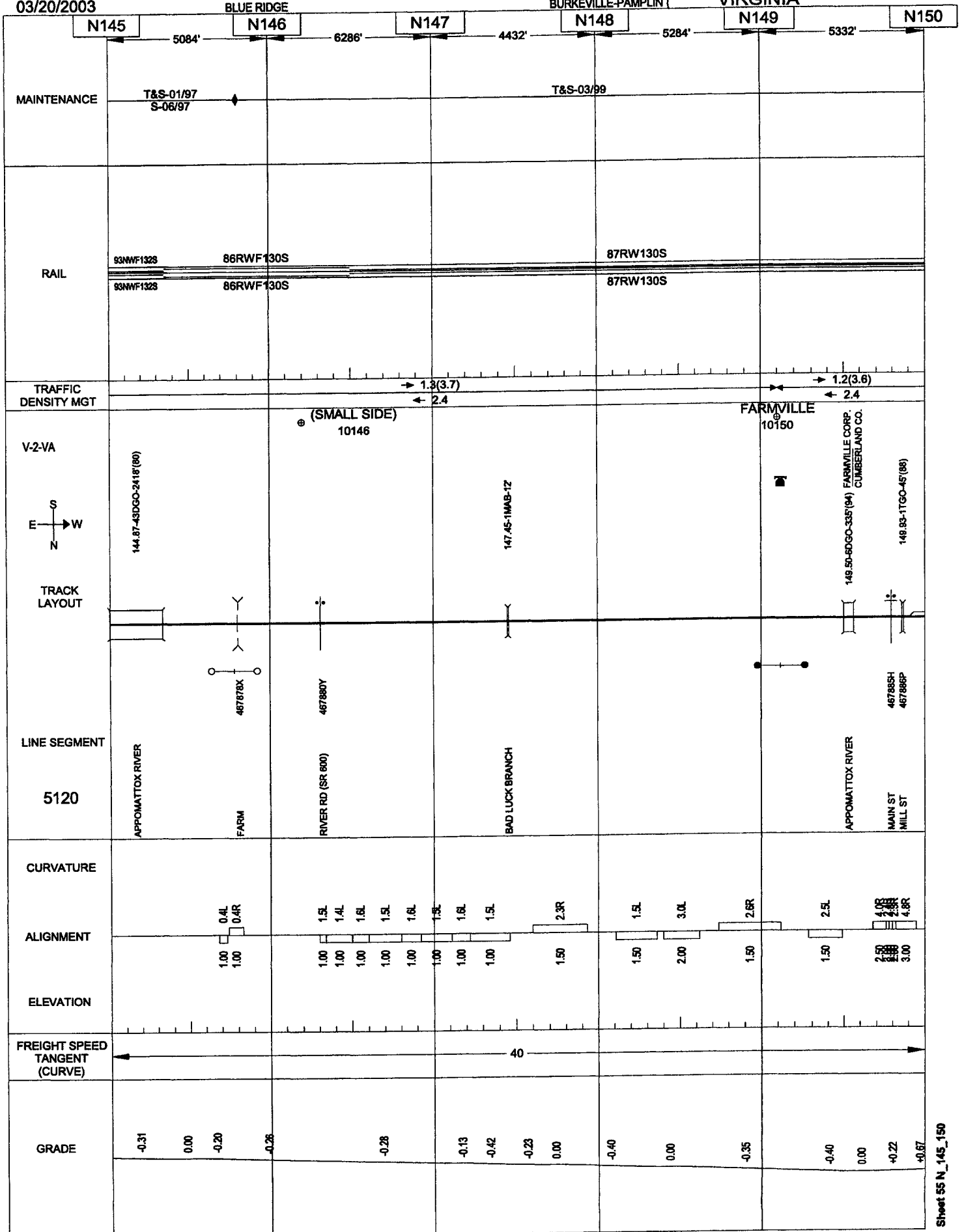


03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

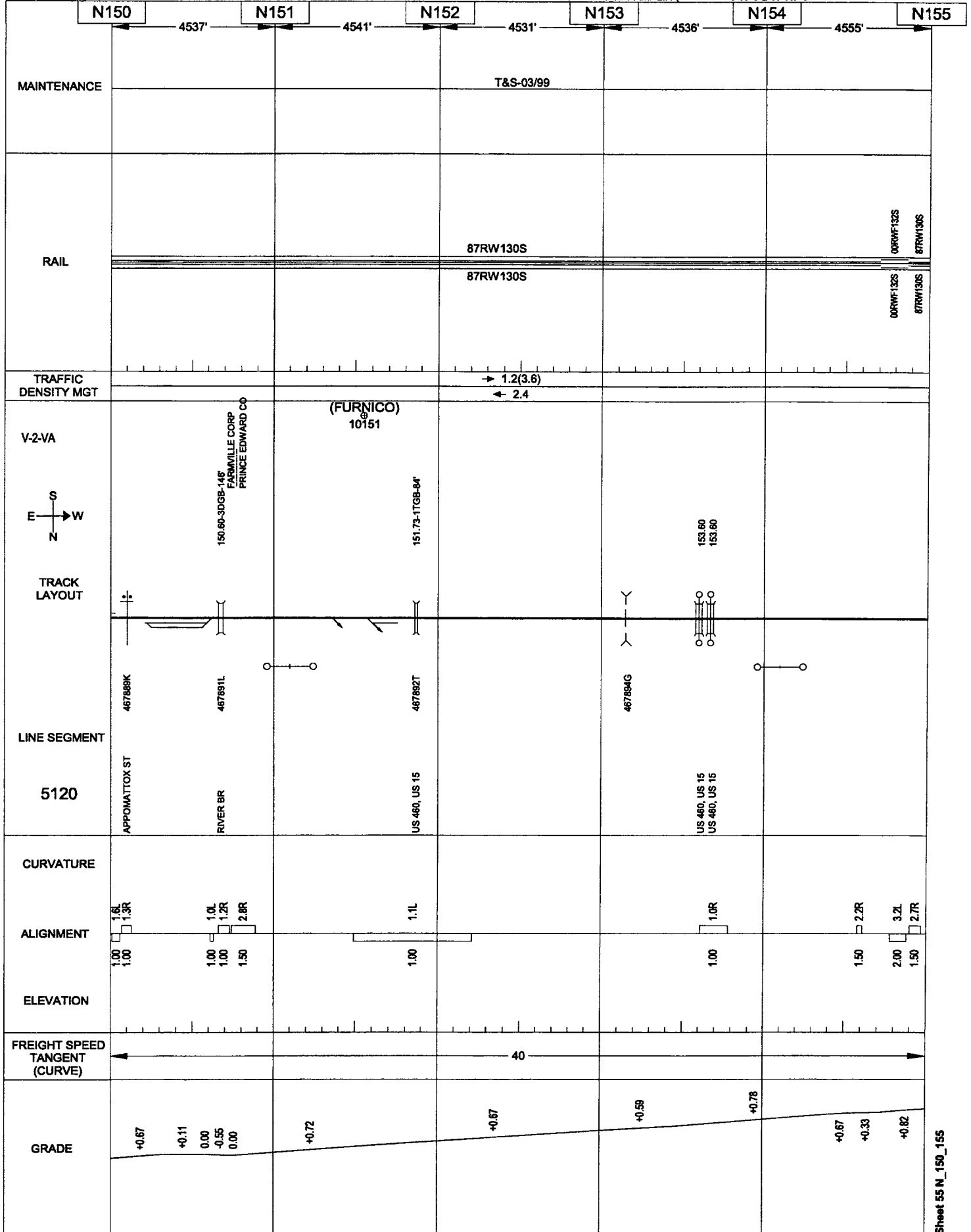


03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

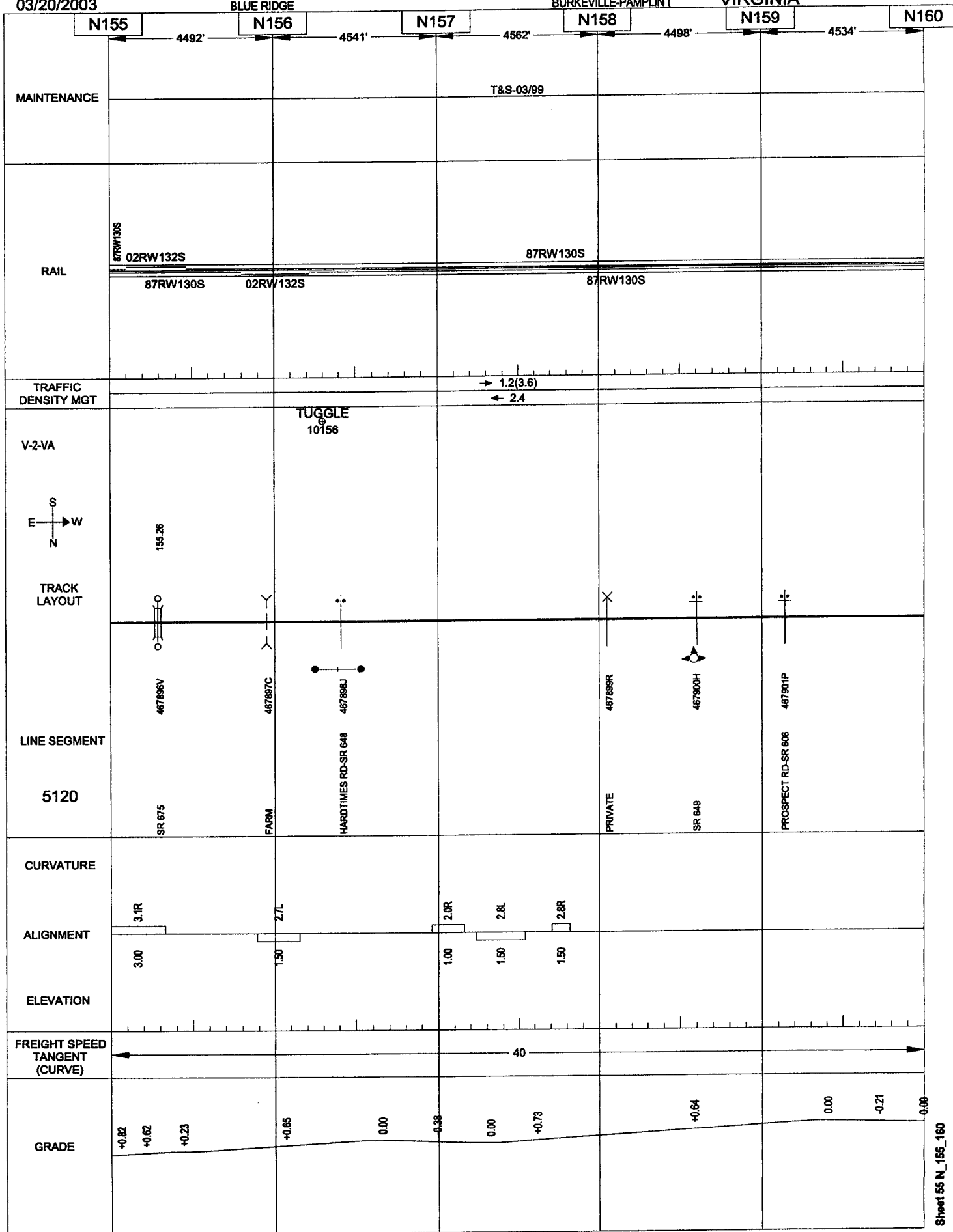


03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

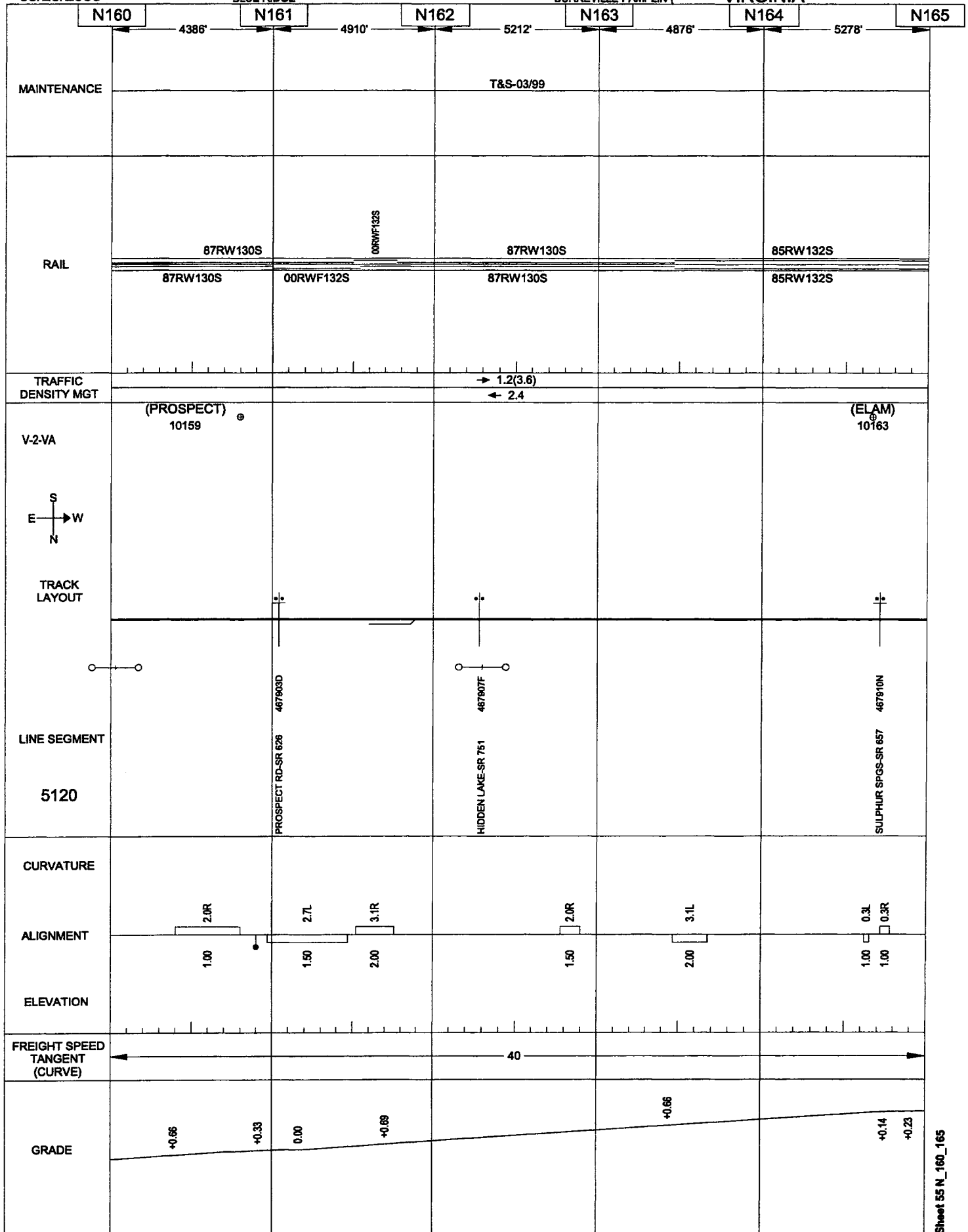


03/20/2003

BLUE RIDGE

BURKEVILLE-PAMPLIN (

VIRGINIA

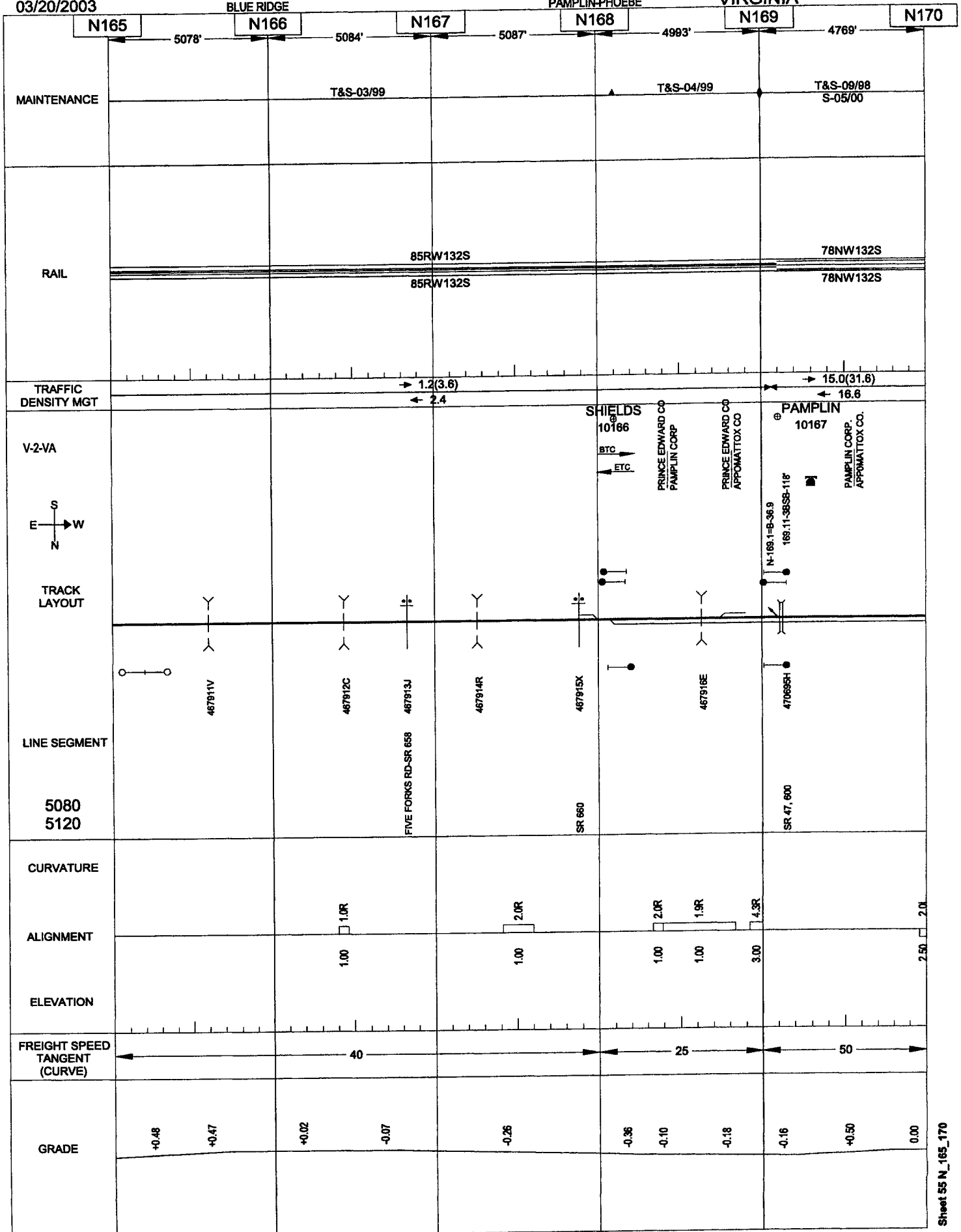


03/20/2003

BLUE RIDGE

PAMPLIN-PHOEBE

VIRGINIA

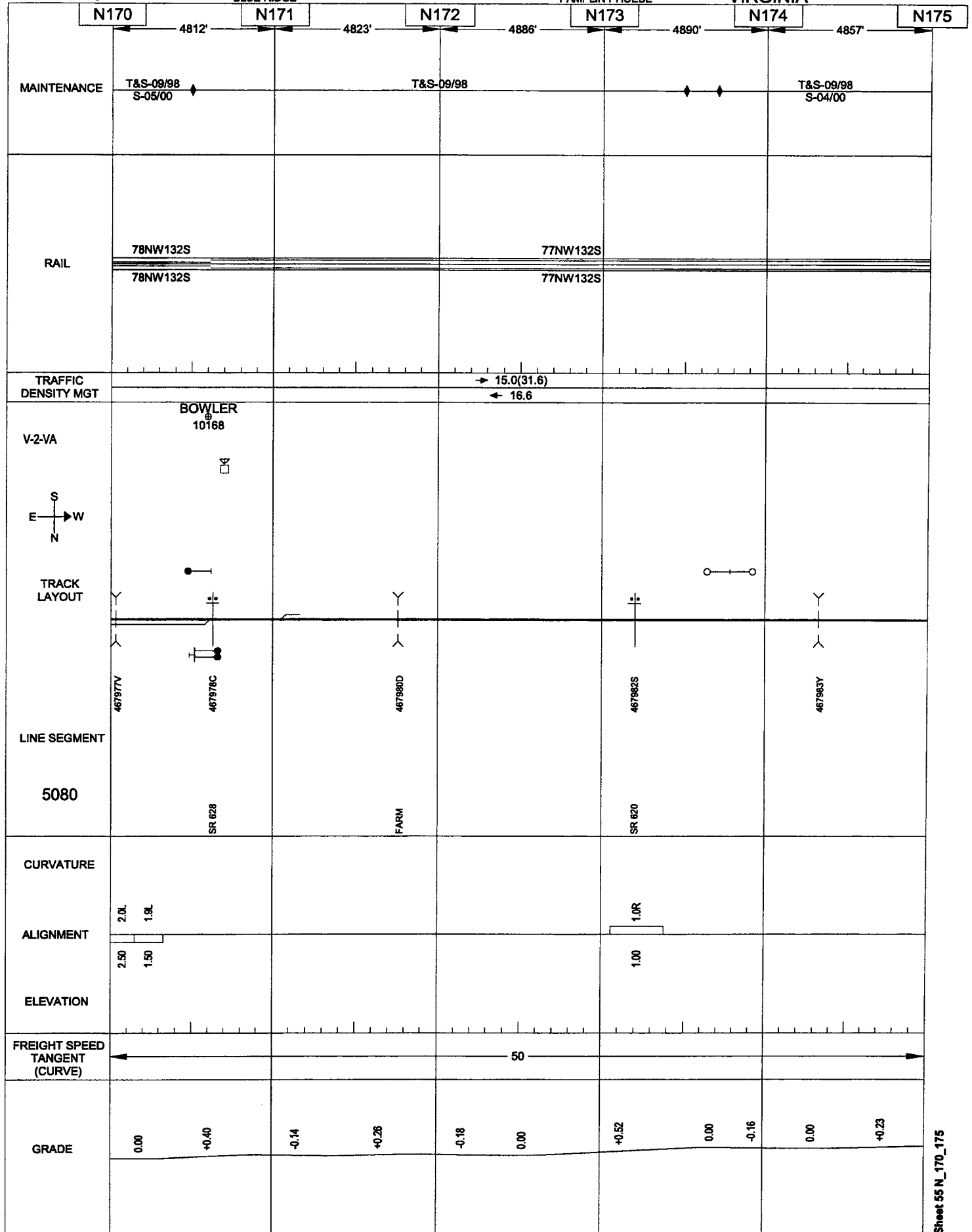


03/20/2003

BLUE RIDGE

PAMPLIN-PHOEBE

VIRGINIA

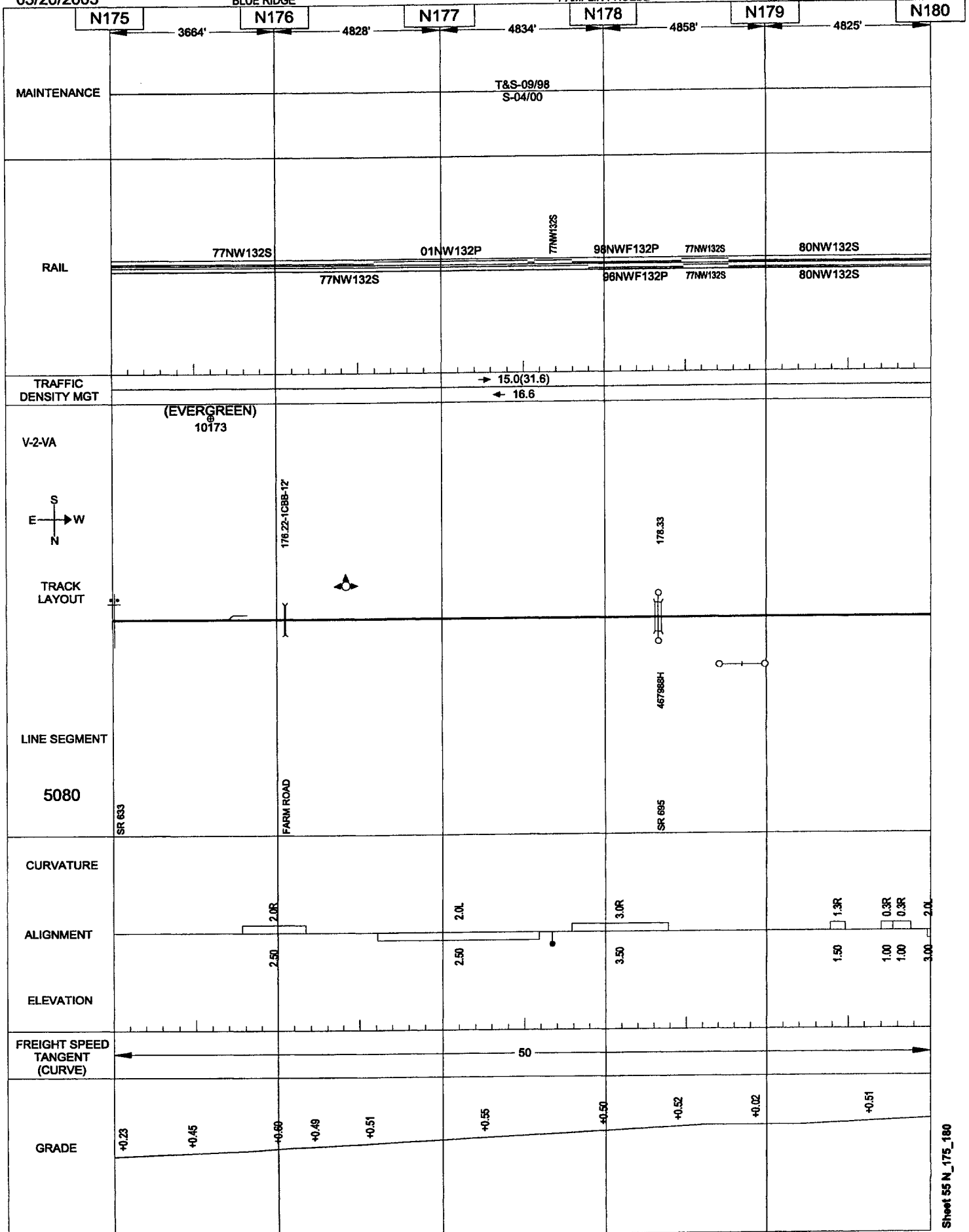


03/20/2003

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PAMPLIN-PHOEBE

VIRGINIA

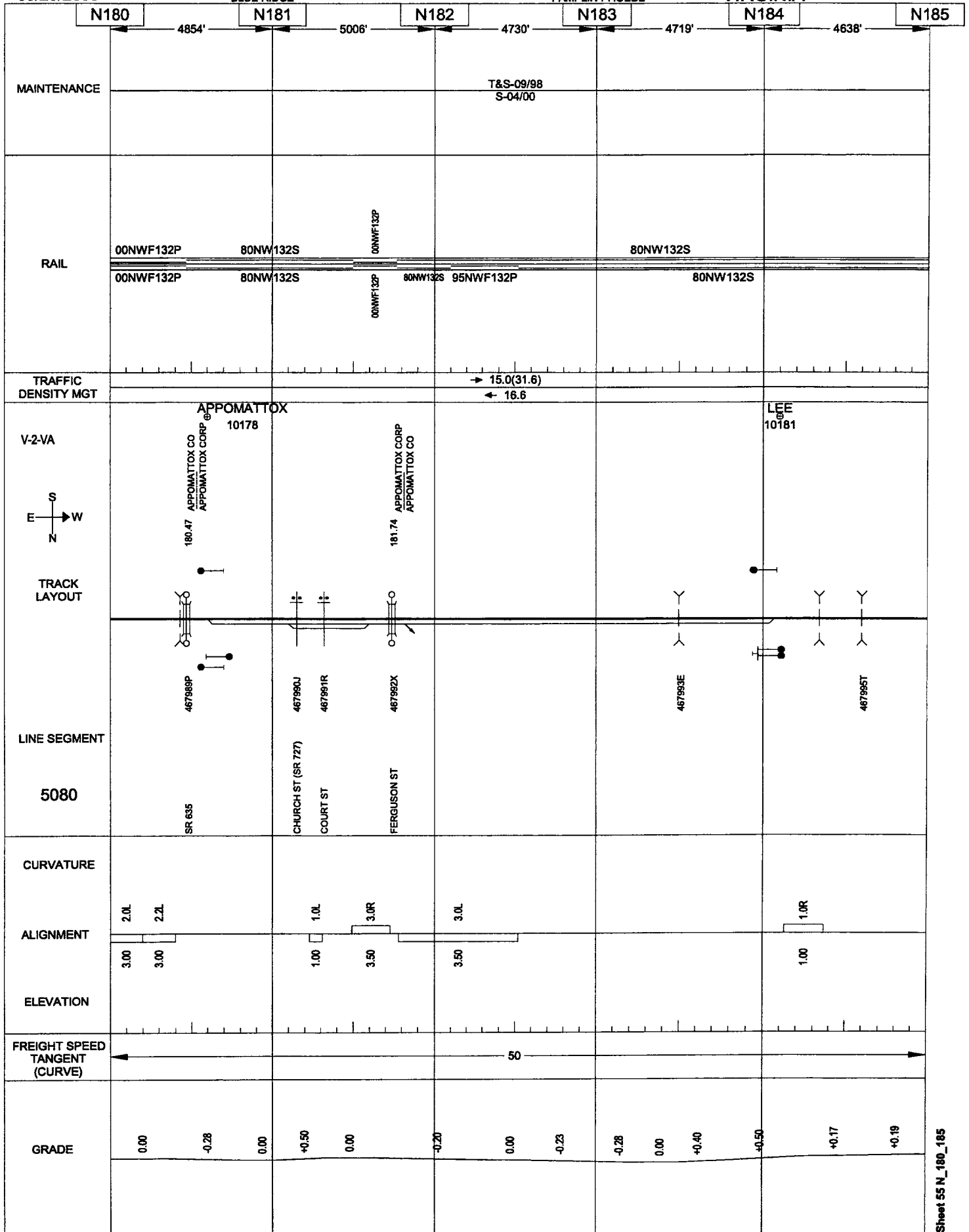


03/20/2003

BLUE RIDGE

PAMPLIN-PHOEBE

VIRGINIA

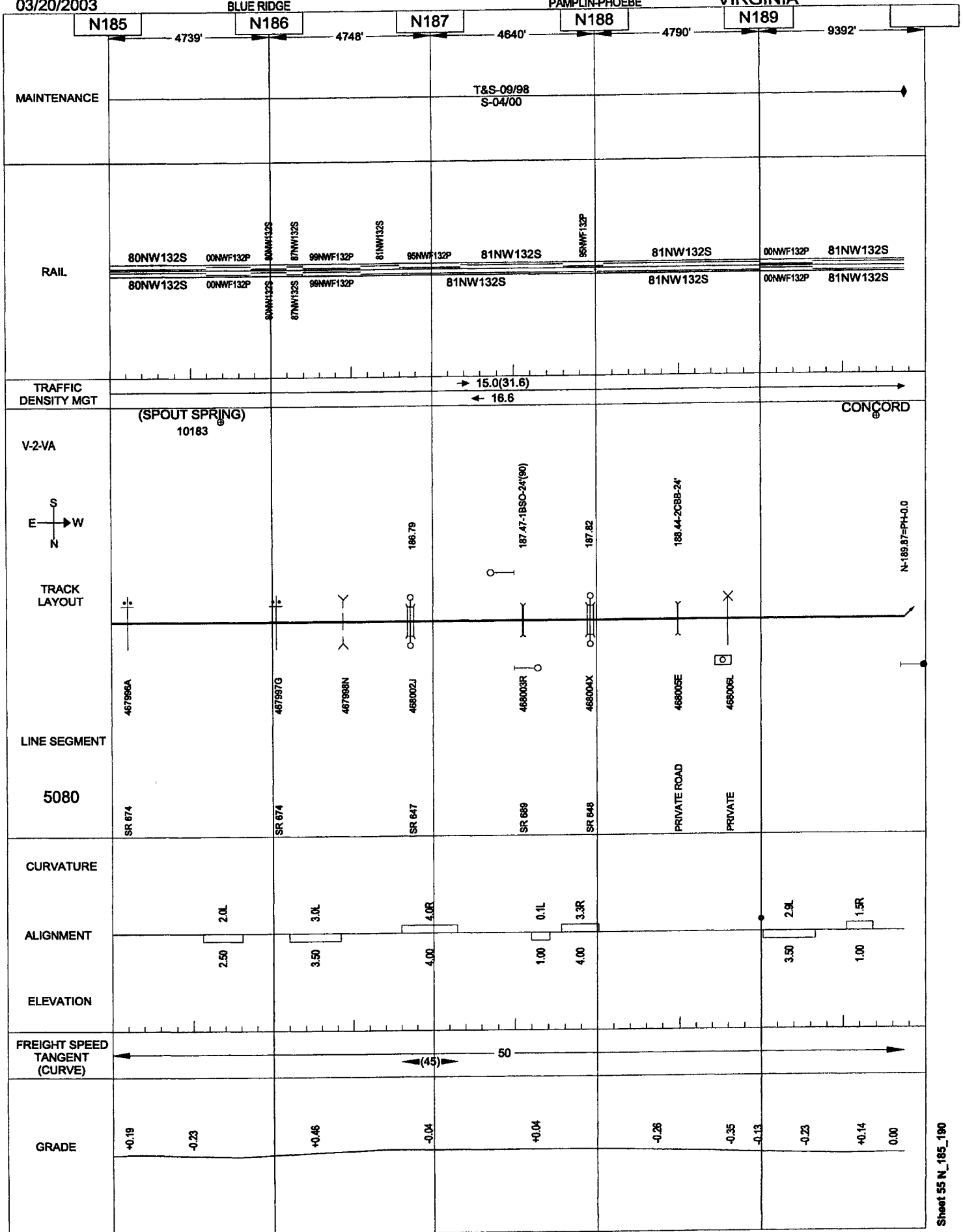


03/20/2003

BLUE RIDGE

PAMPLIN-PHOEBE

VIRGINIA



03/20/2003

BLUE RIDGE

056
HALSEY SPUR(OML)

HALSEY-FOREST

VIRGINIA

N208

N209

N210

5280'

5270'

5250'

MAINTENANCE

T&S-10/89

RAIL

27RJ130S

28RJ130S

27RJ130S

28RJ130S

TRAFFIC
DENSITY MGT

→ 0.0(0.0)

← 0.0

V-2-VA

ITO
10202

(HALSEY)
10203

(SIMS)
10204

S
E → W
N

TRACK
LAYOUT

208.20

THOMAS CEMENT

209.88

LINE SEGMENT

5260

468344J

SIMS WHOLESALE CO
FOREST BROOK RD 468343C

468342V

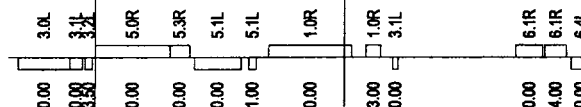
SR 281

SR 281

CURVATURE

ALIGNMENT

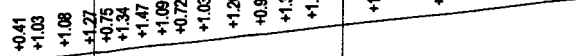
ELEVATION



FREIGHT SPEED
TANGENT
(CURVE)

10

GRADE



057

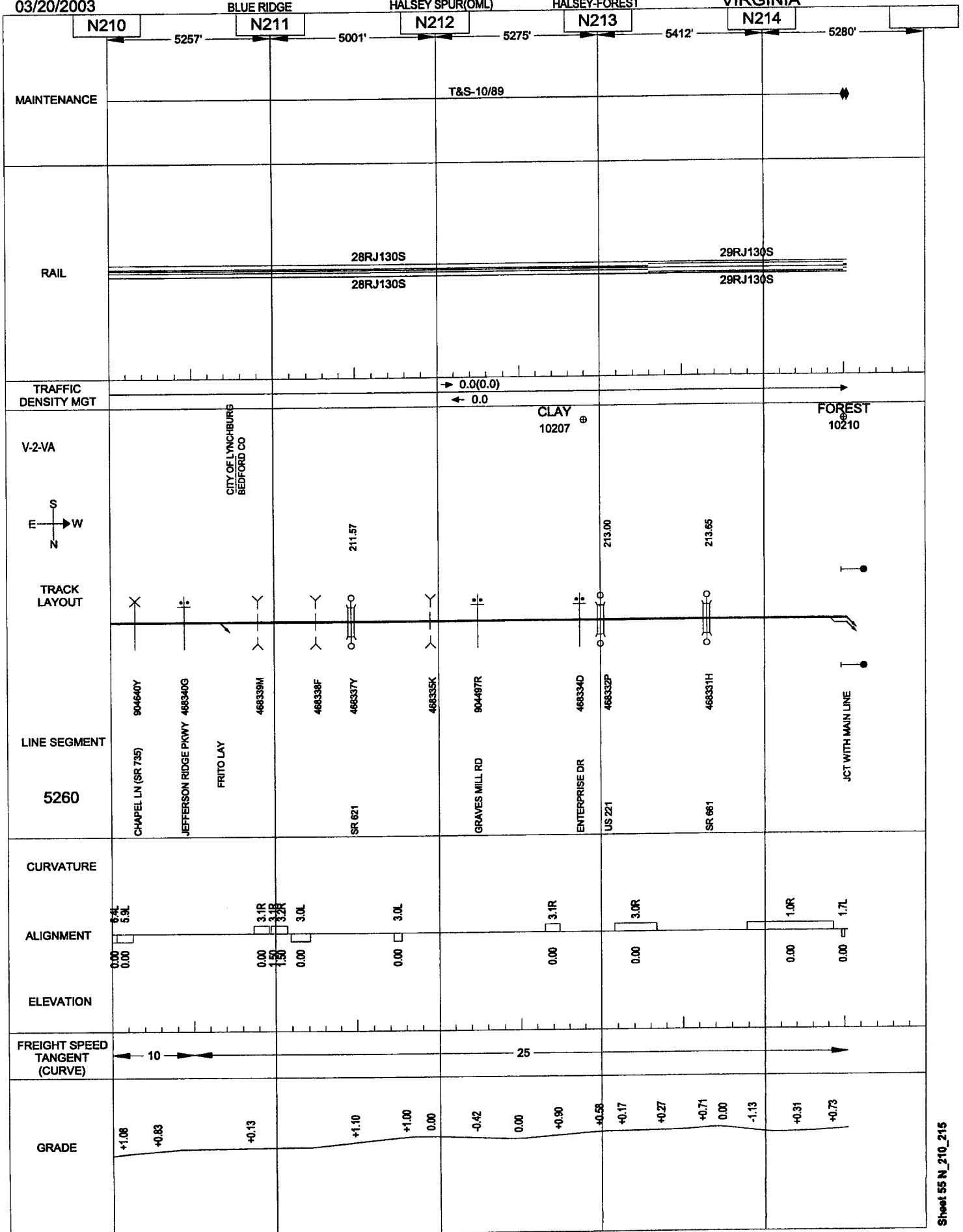
03/20/2003

BLUE RIDGE

HALSEY SPUR(OML)

HALSEY-FOREST

VIRGINIA



058

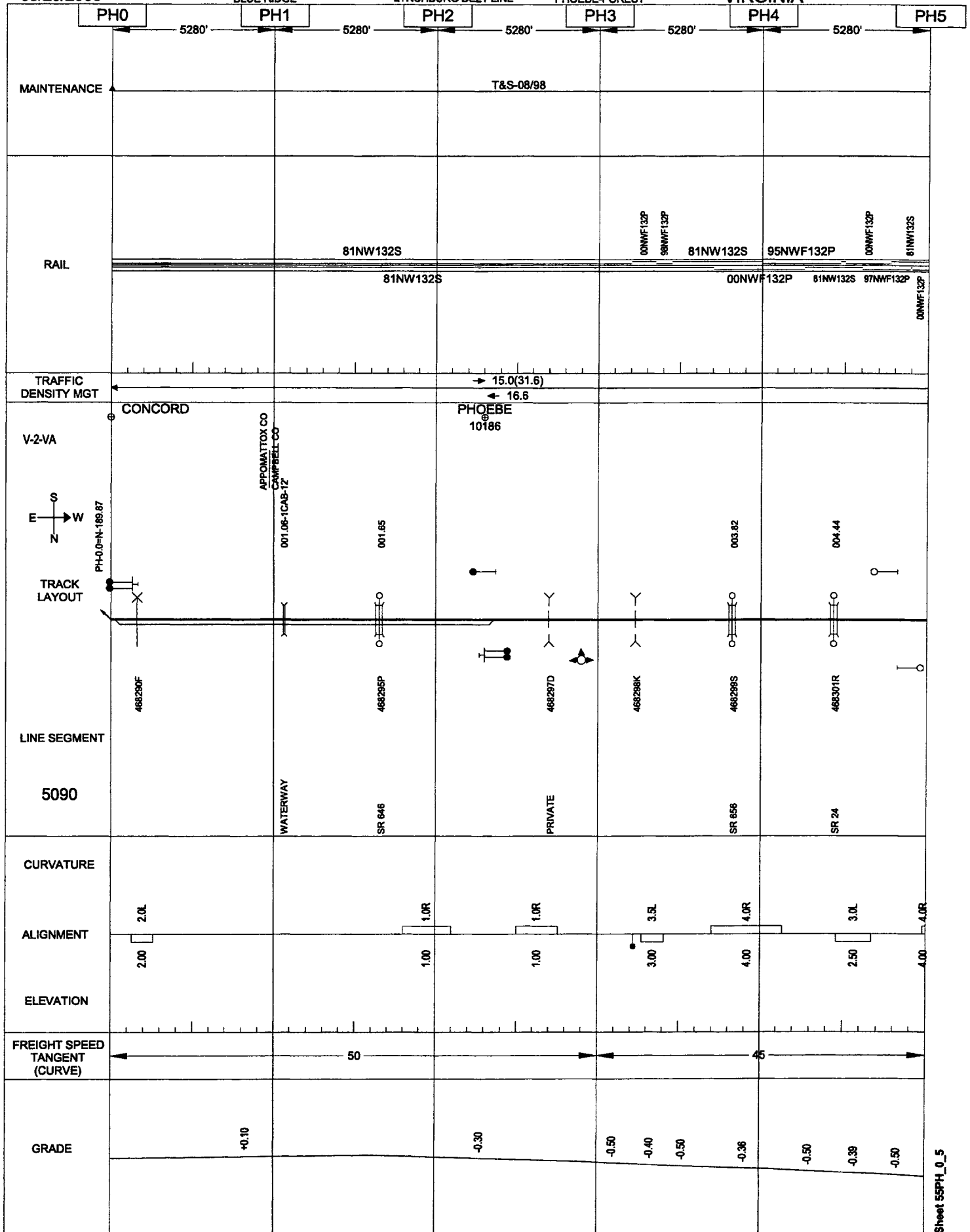
03/20/2003

BLUE RIDGE

LYNCHBURG BELT LINE

PHOEBE-FOREST

VIRGINIA



059

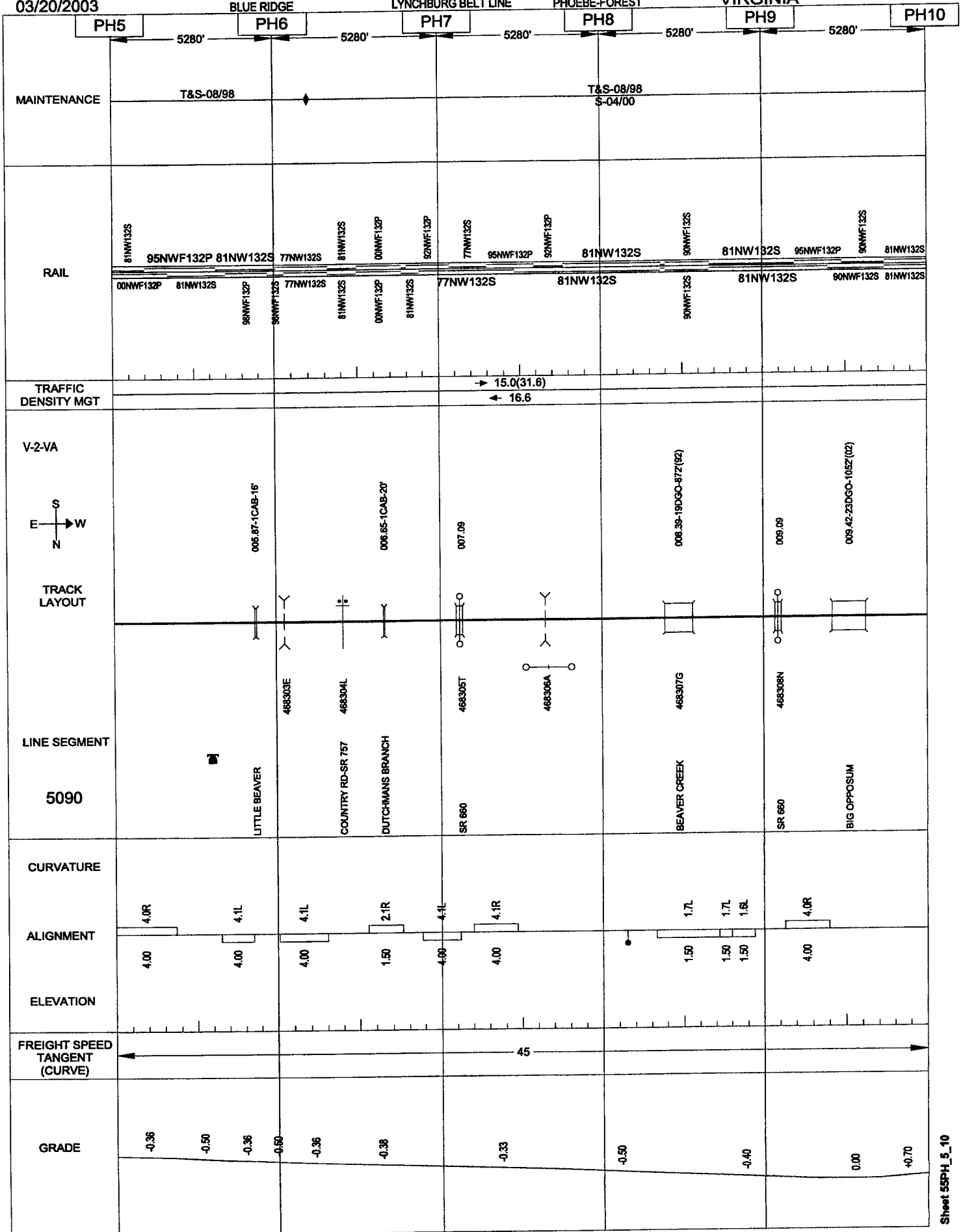
03/20/2003

BLUE RIDGE

LYNCHBURG BELT LINE

PHOEBE-FOREST

VIRGINIA



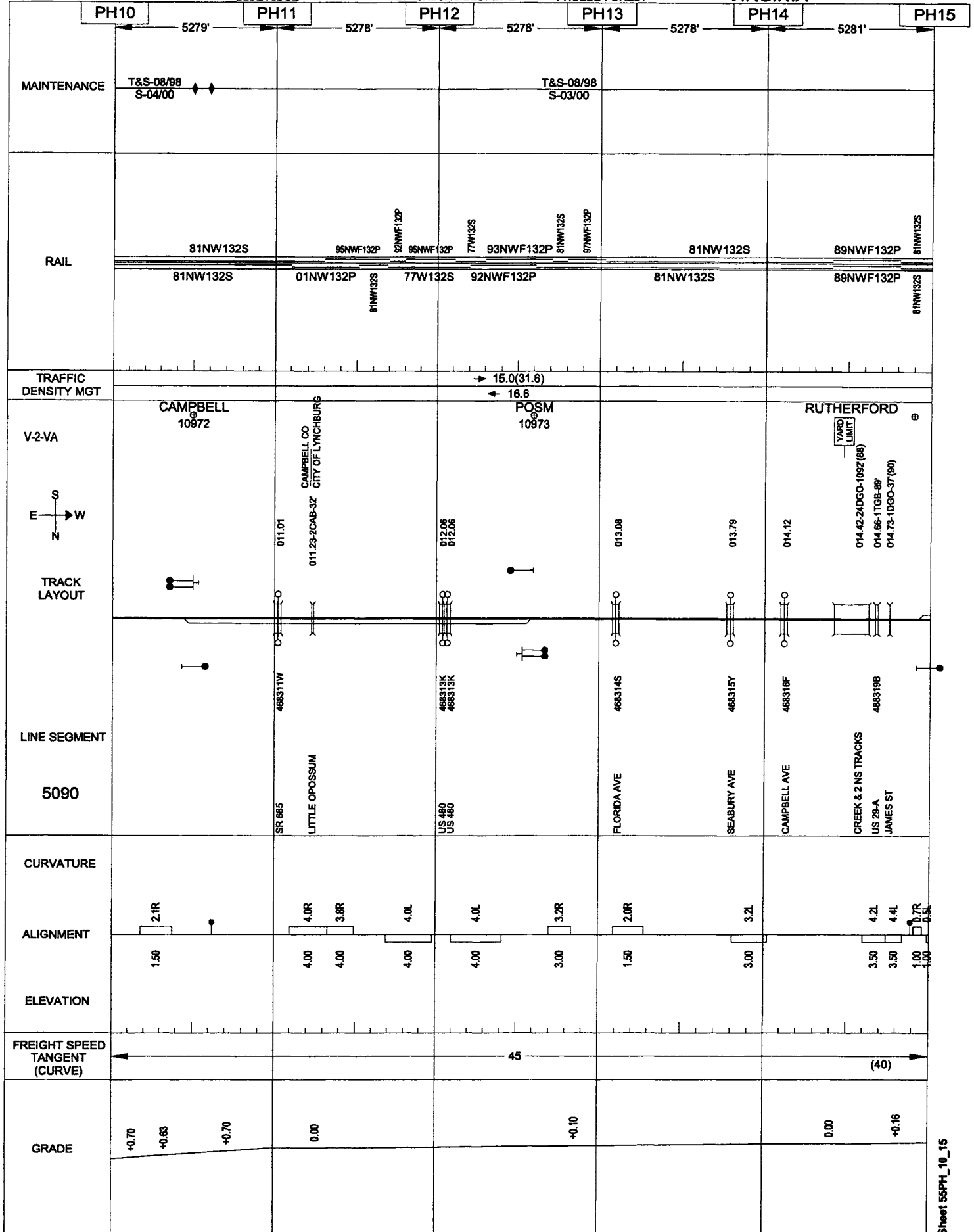
03/20/2003

BLUE RIDGE

LYNCHBURG BELT LINE

PHOEBE-FOREST

VIRGINIA



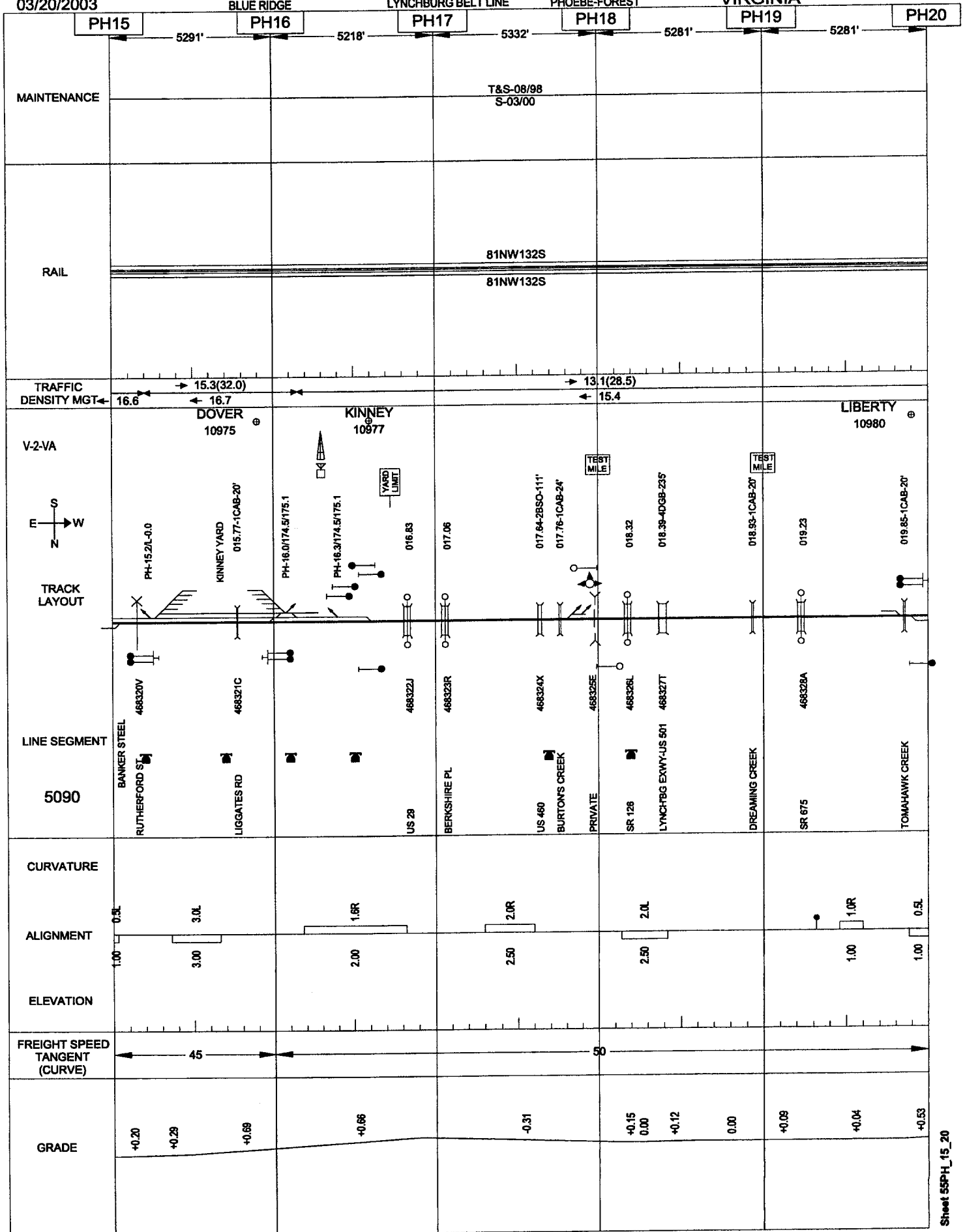
03/20/2003

BLUE RIDGE

LYNCHBURG BELT LINE

PHOEBE-FOREST

VIRGINIA



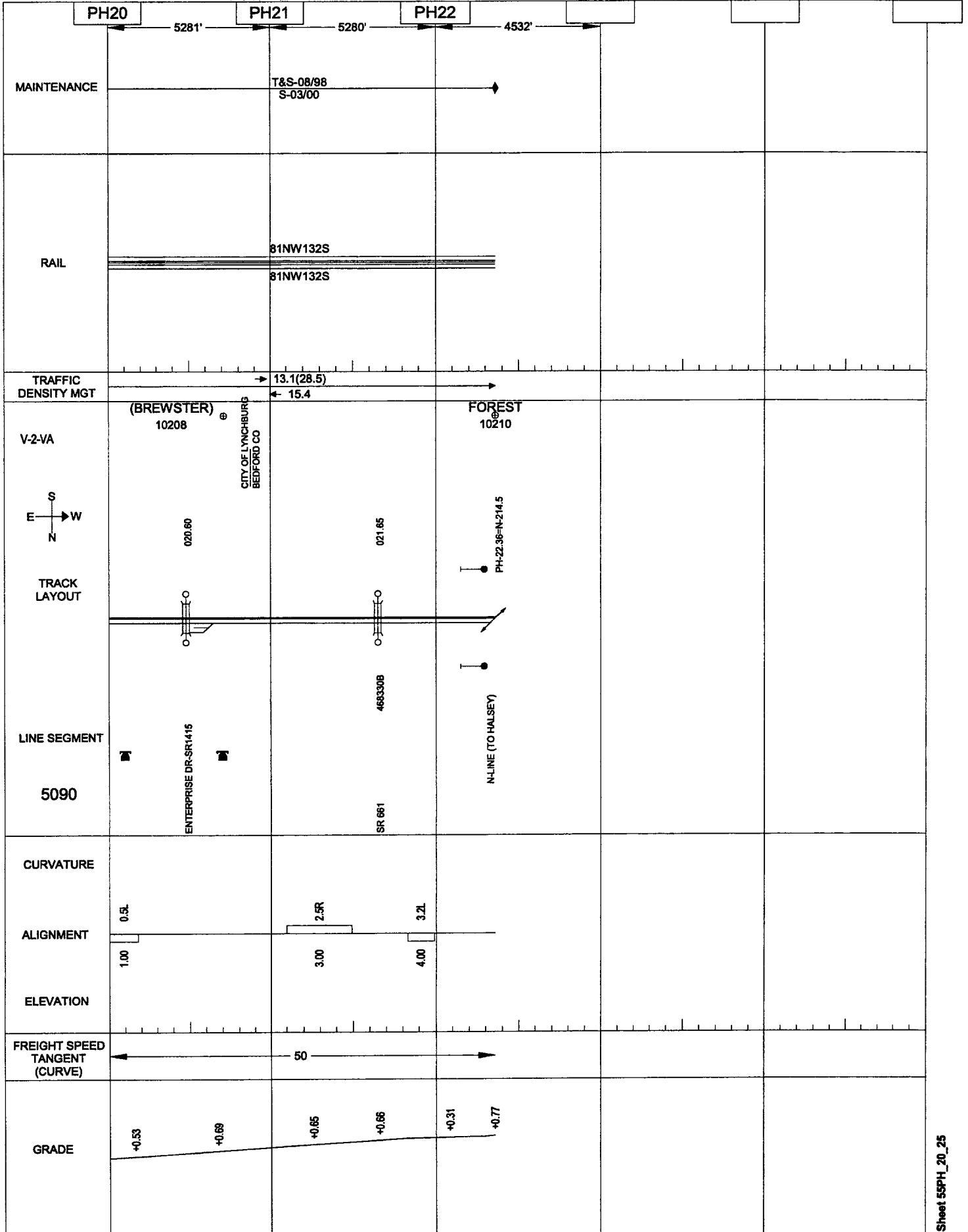
03/20/2003

BLUE RIDGE

LYNCHBURG BELT LINE

PHOEBE-FOREST

VIRGINIA



063

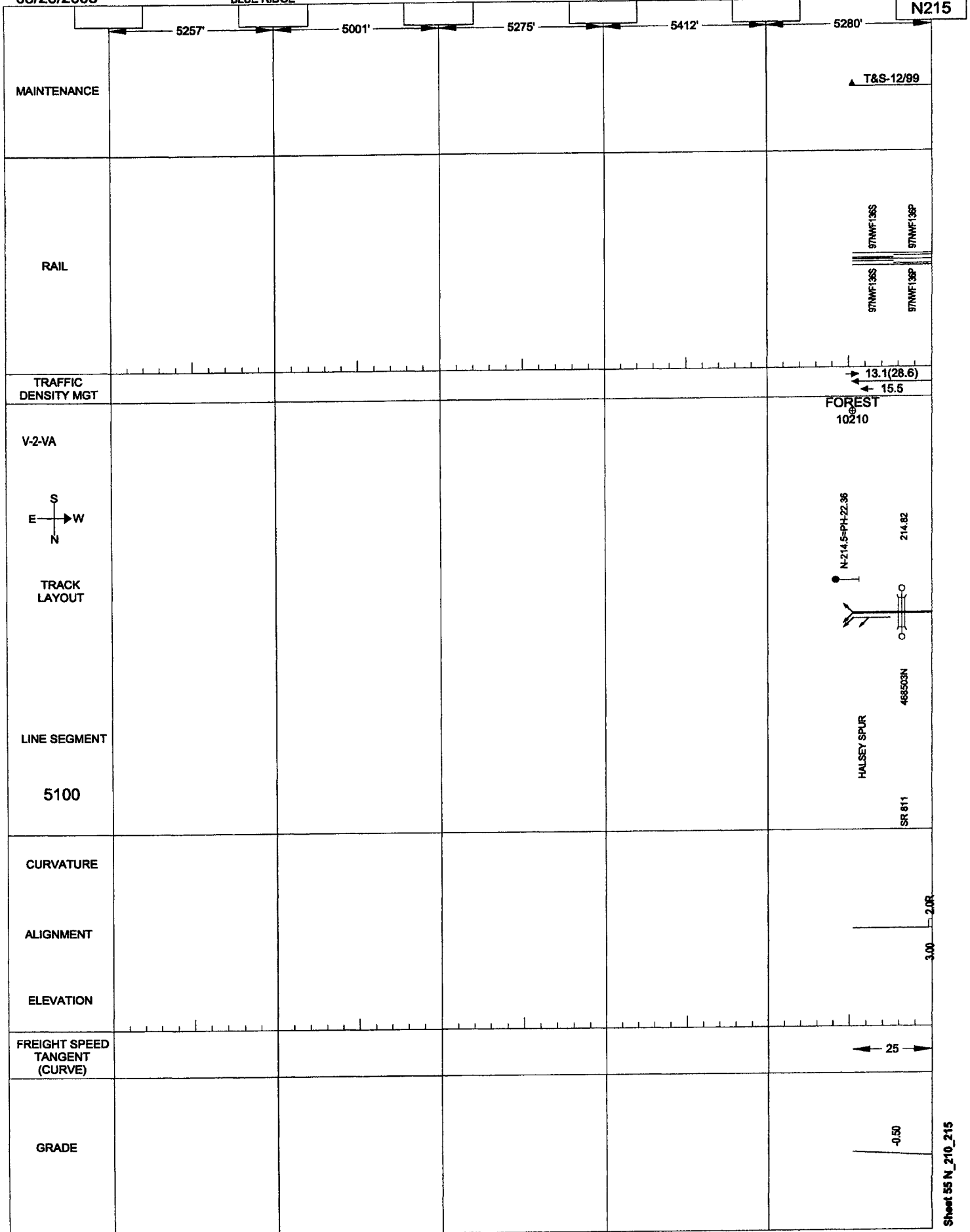
03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA

N215

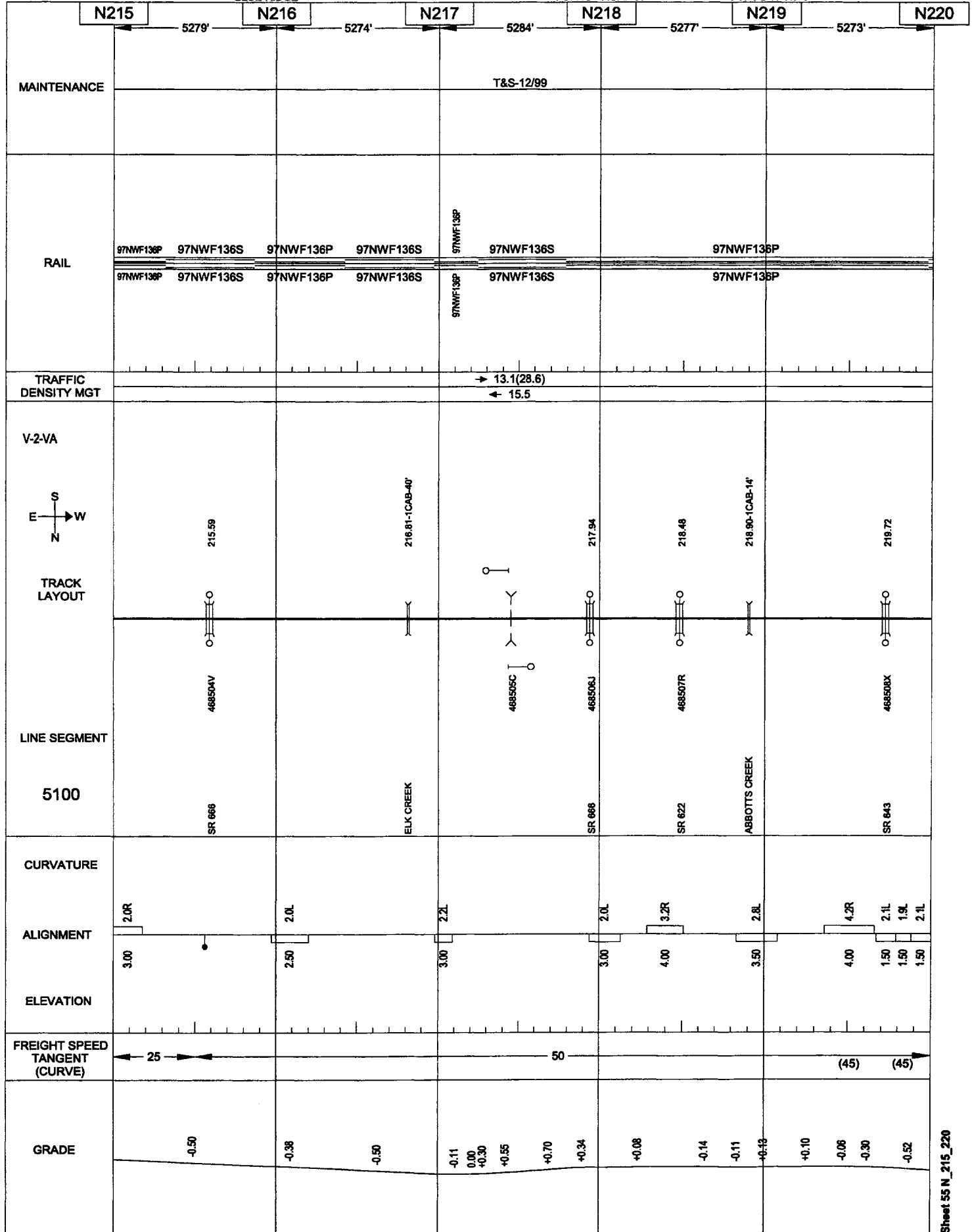


03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA

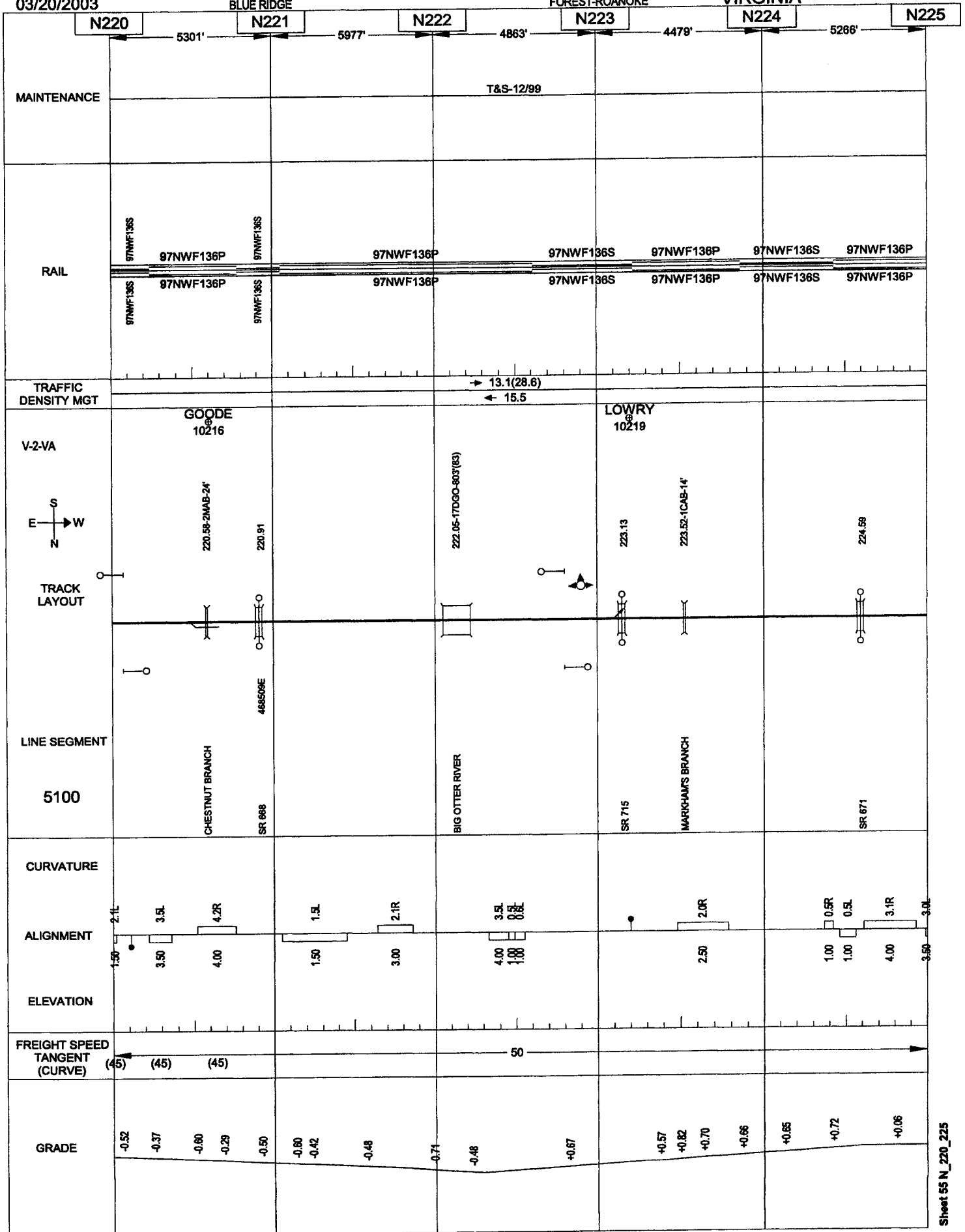


03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA

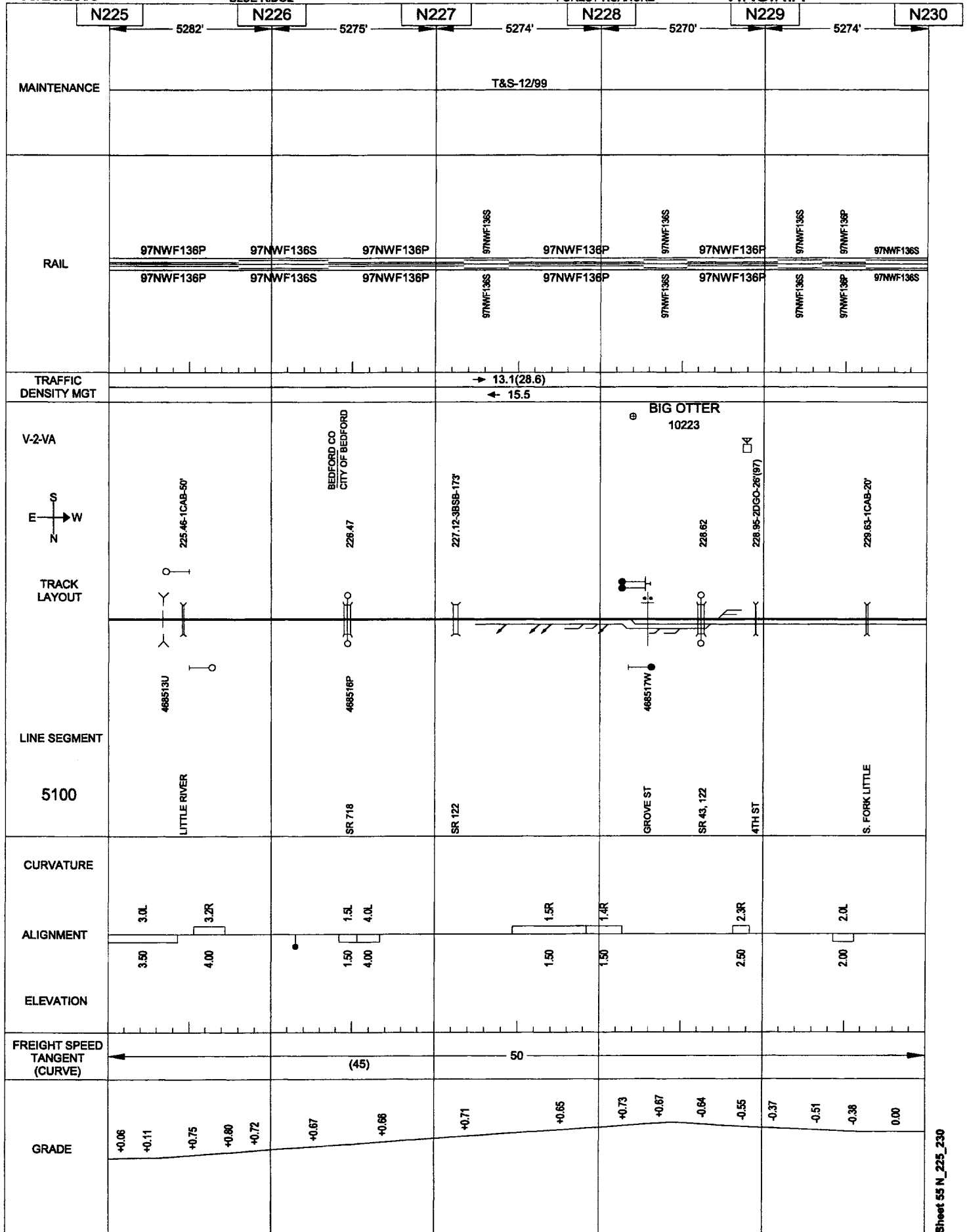


03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA



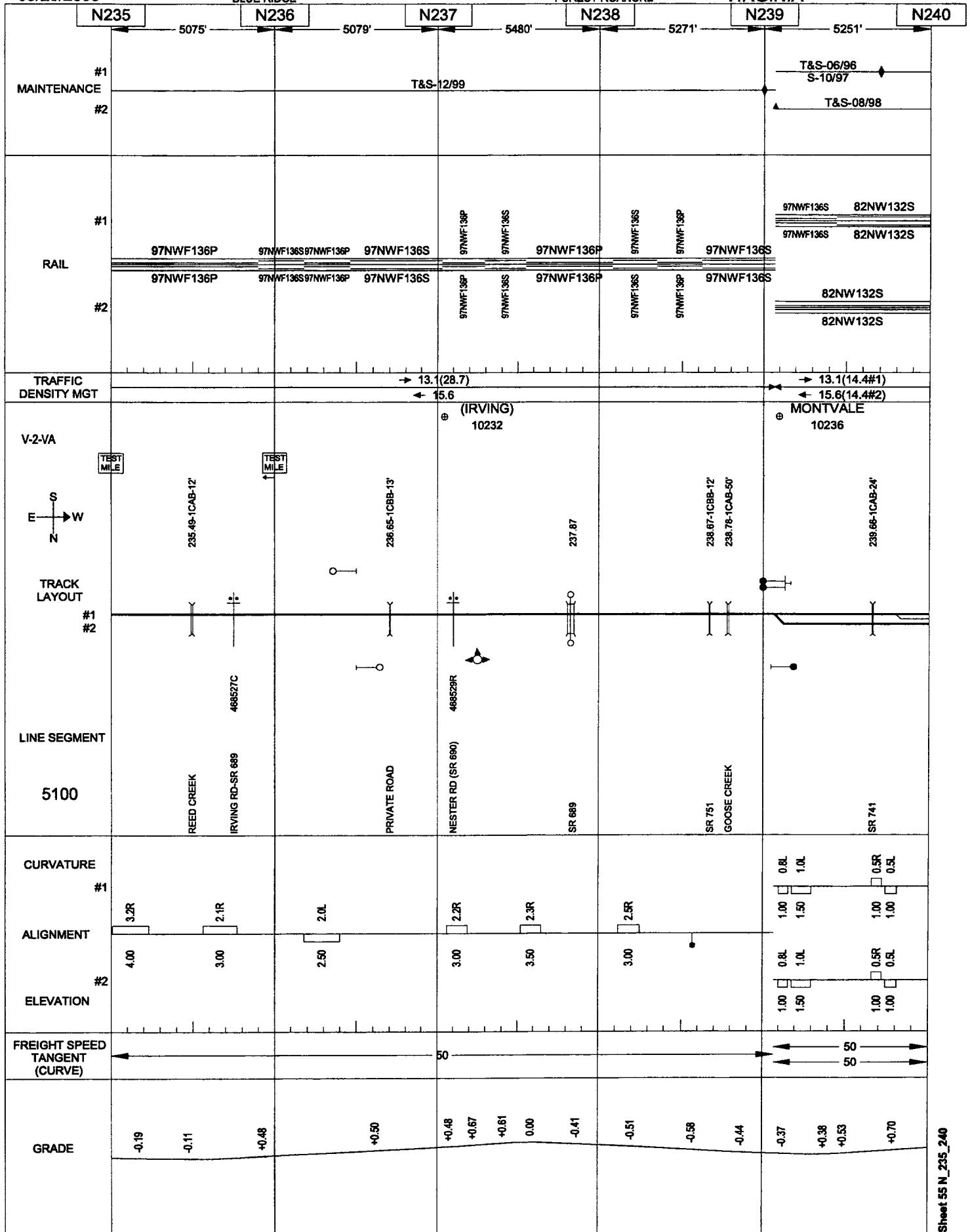
Sheet 55 N_230_235

03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA



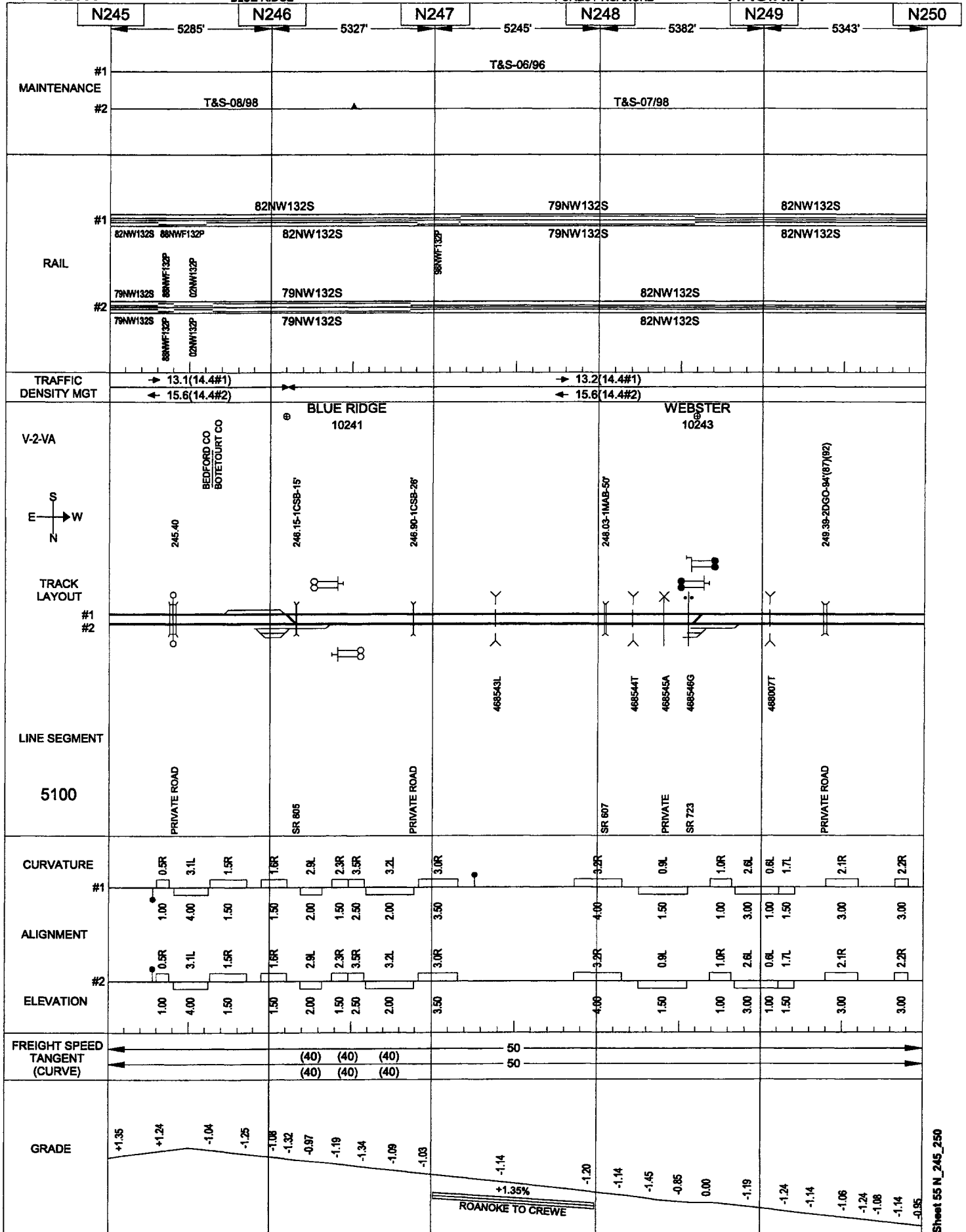
Sheet 55 N_240_245

03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA

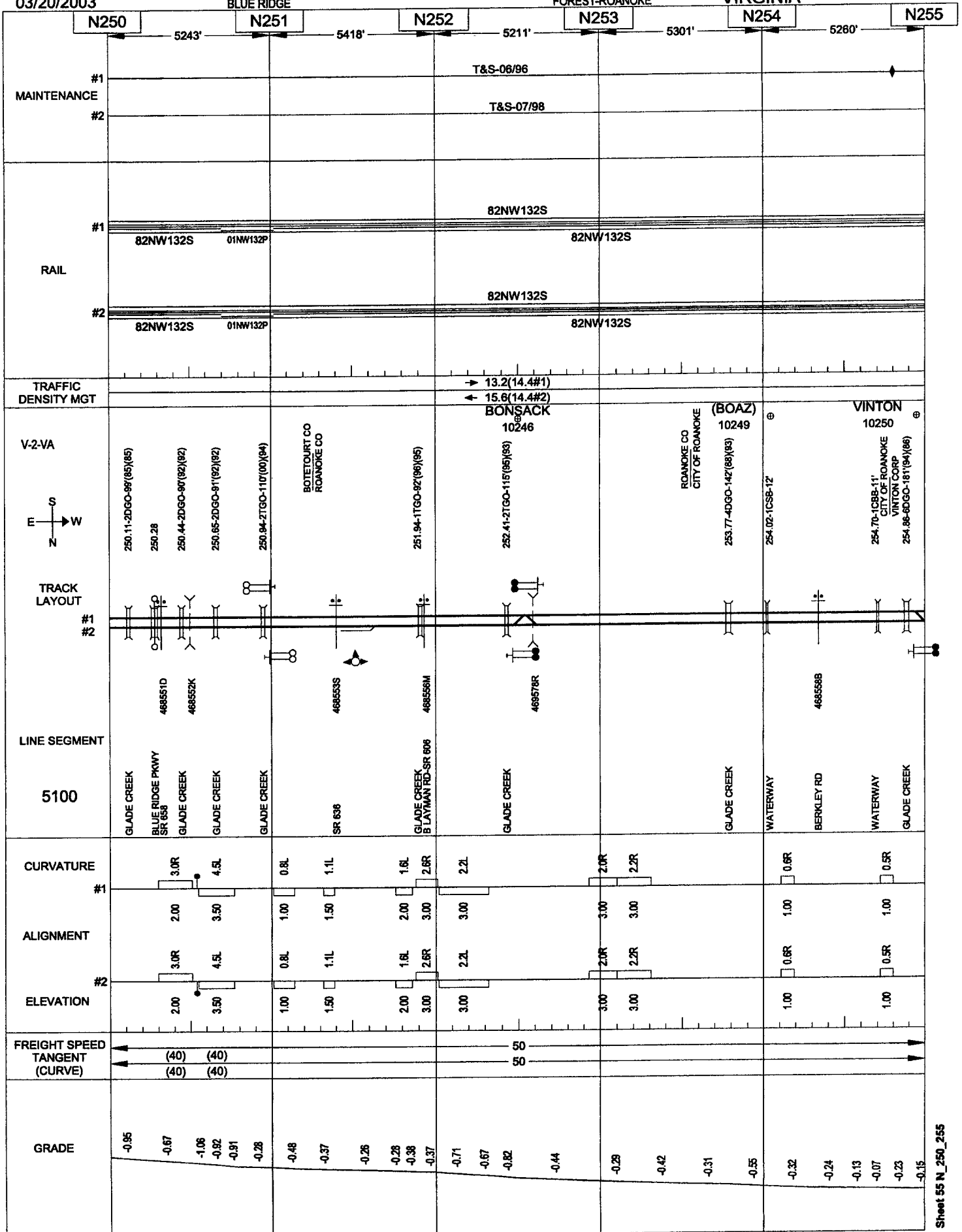


03/20/2003

BLUE RIDGE

FOREST-ROANOKE

VIRGINIA

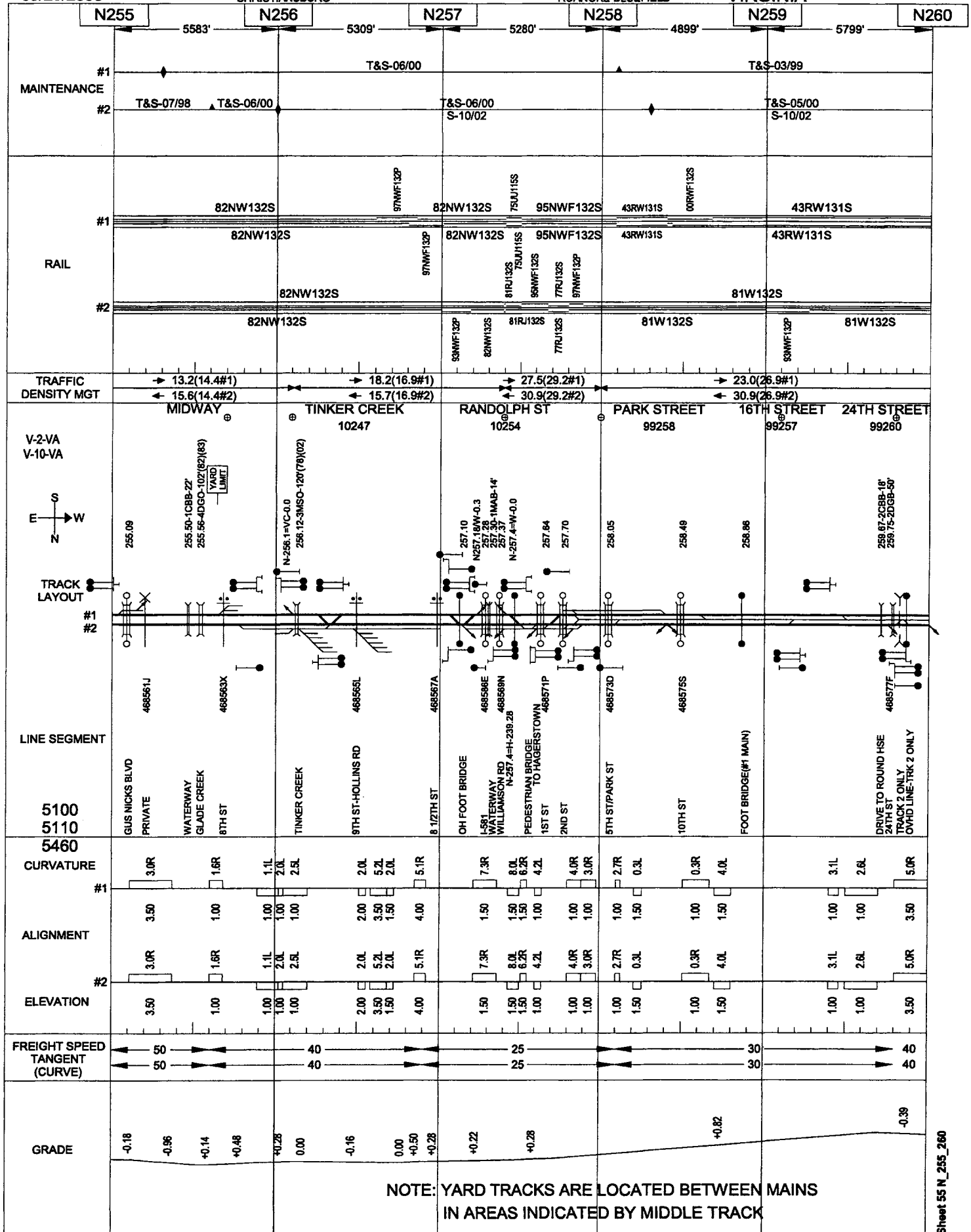


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

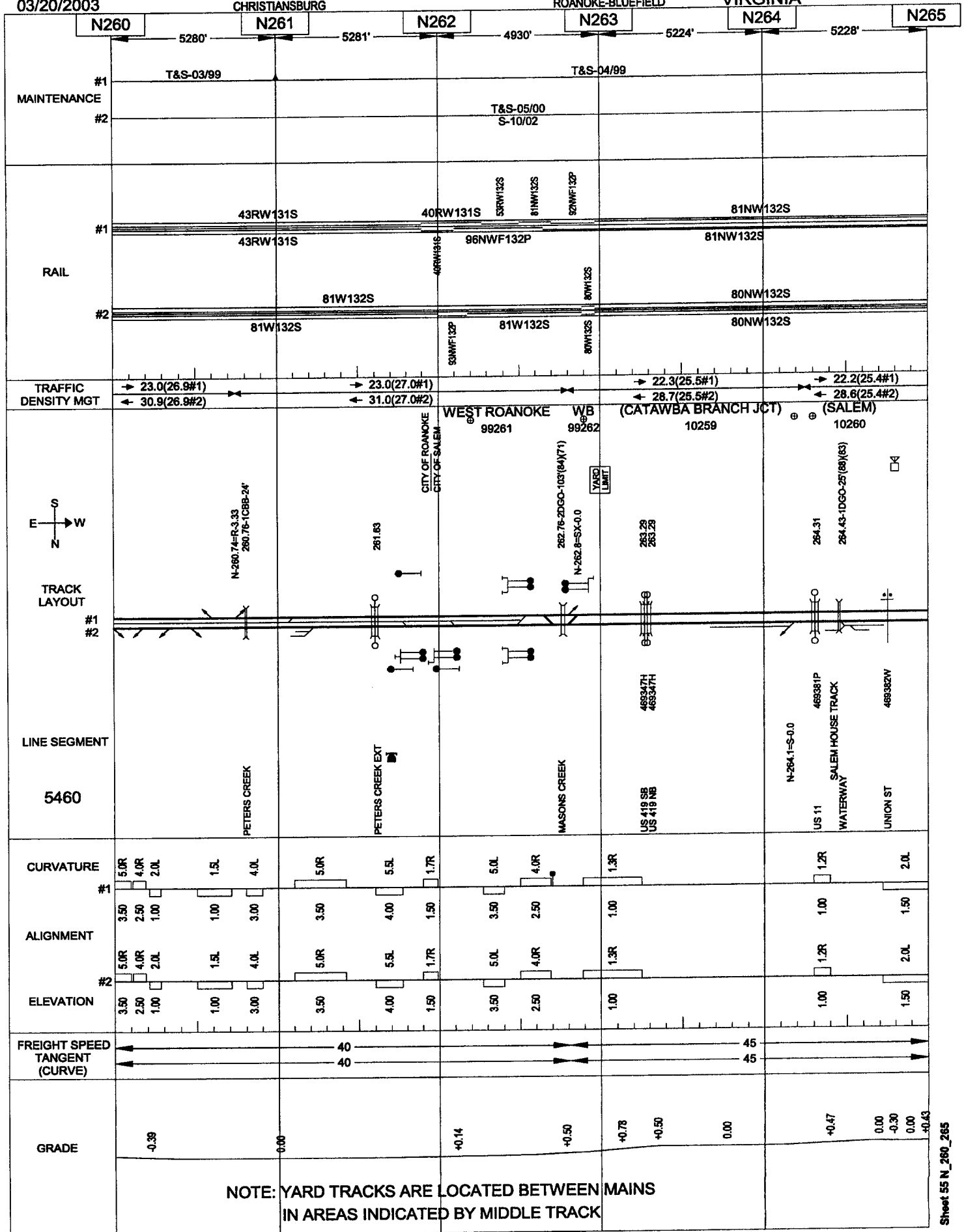


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

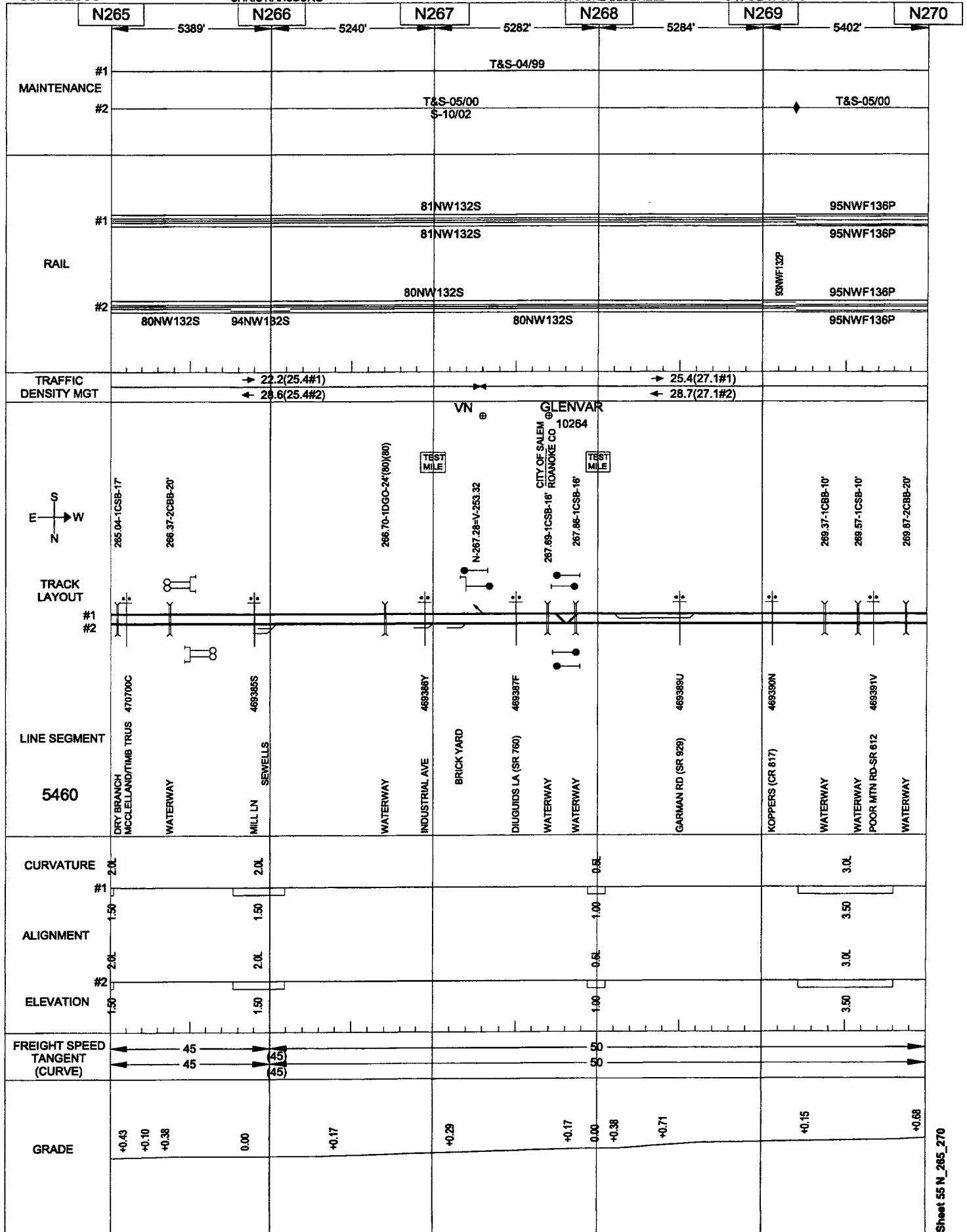


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

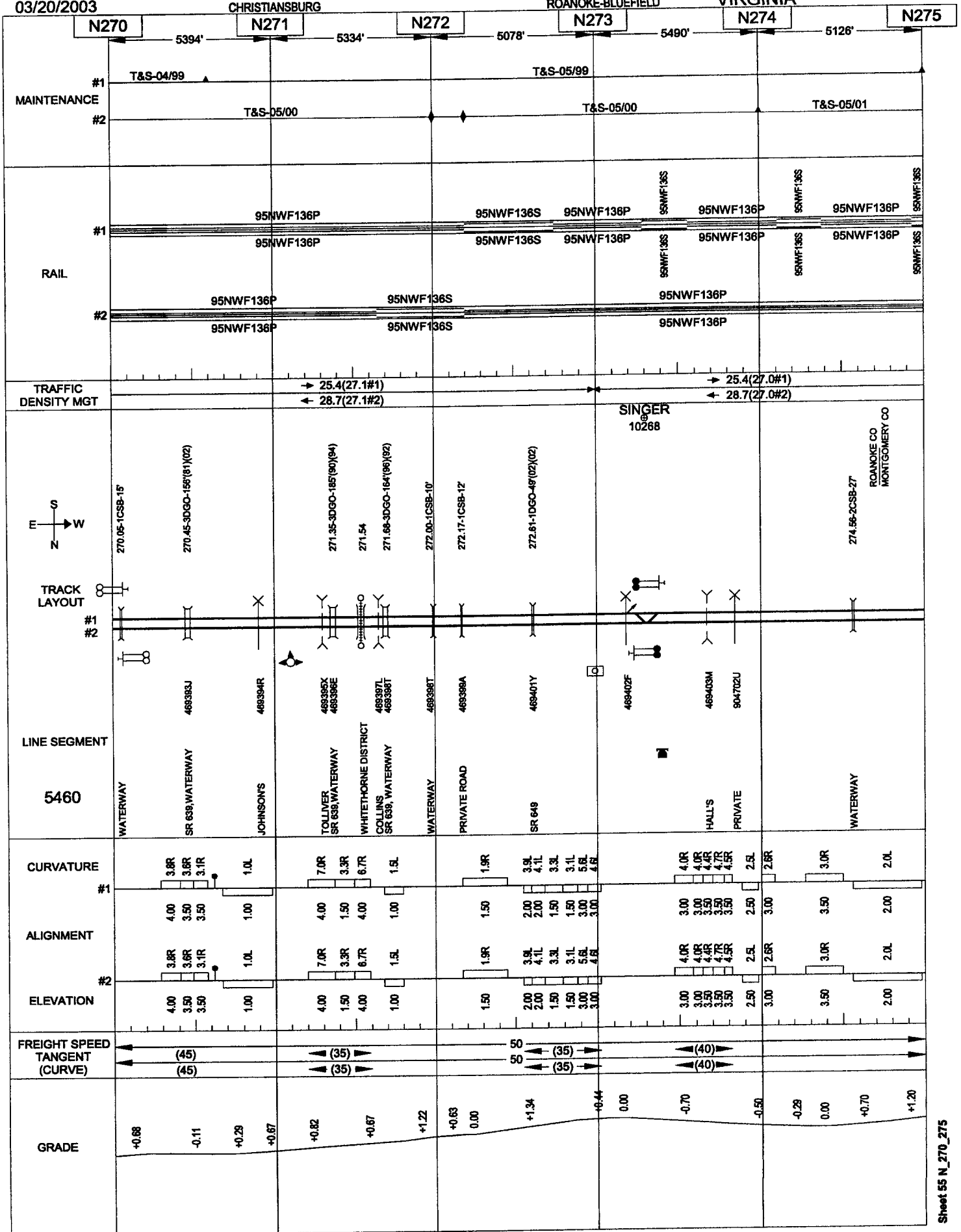


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

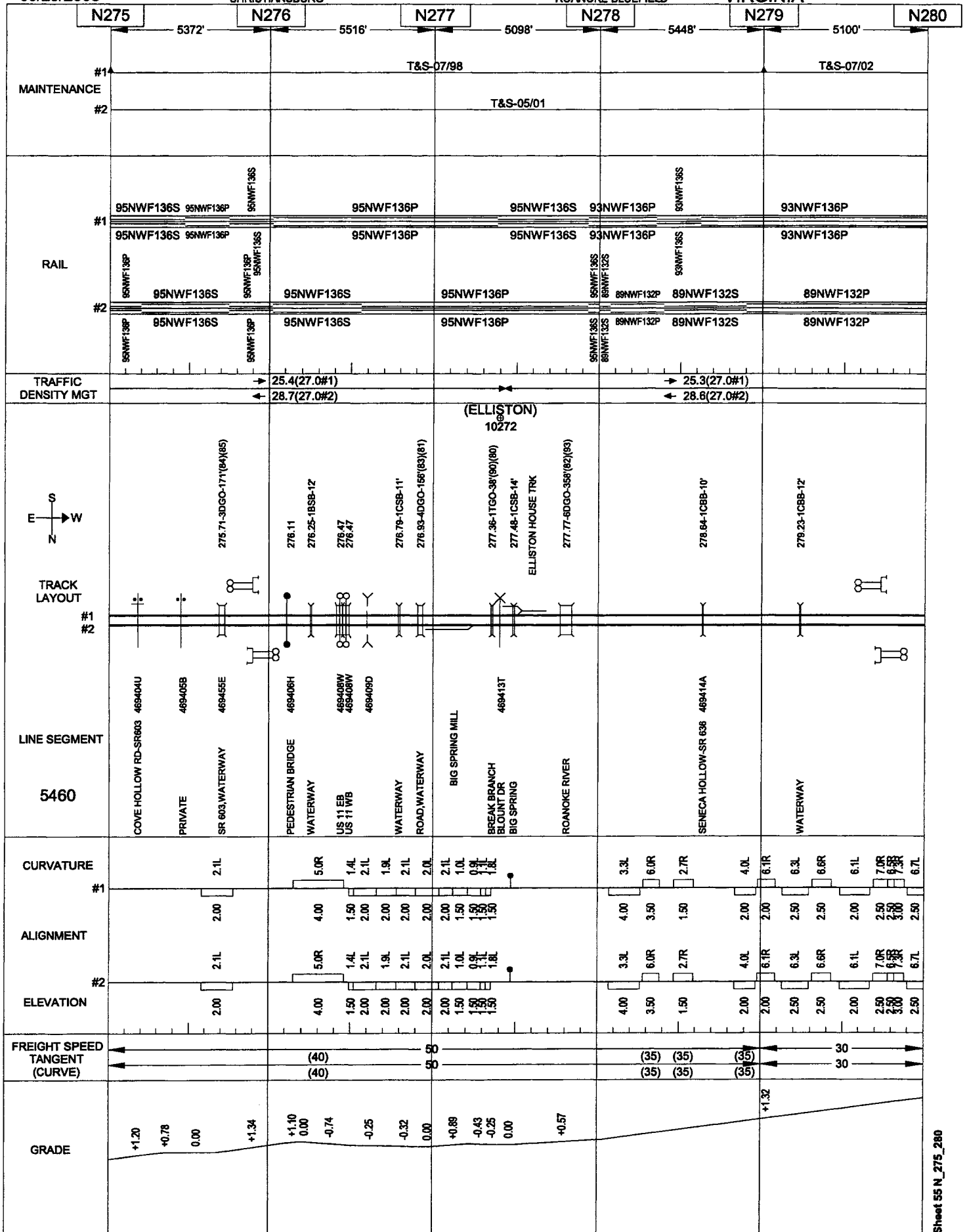


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

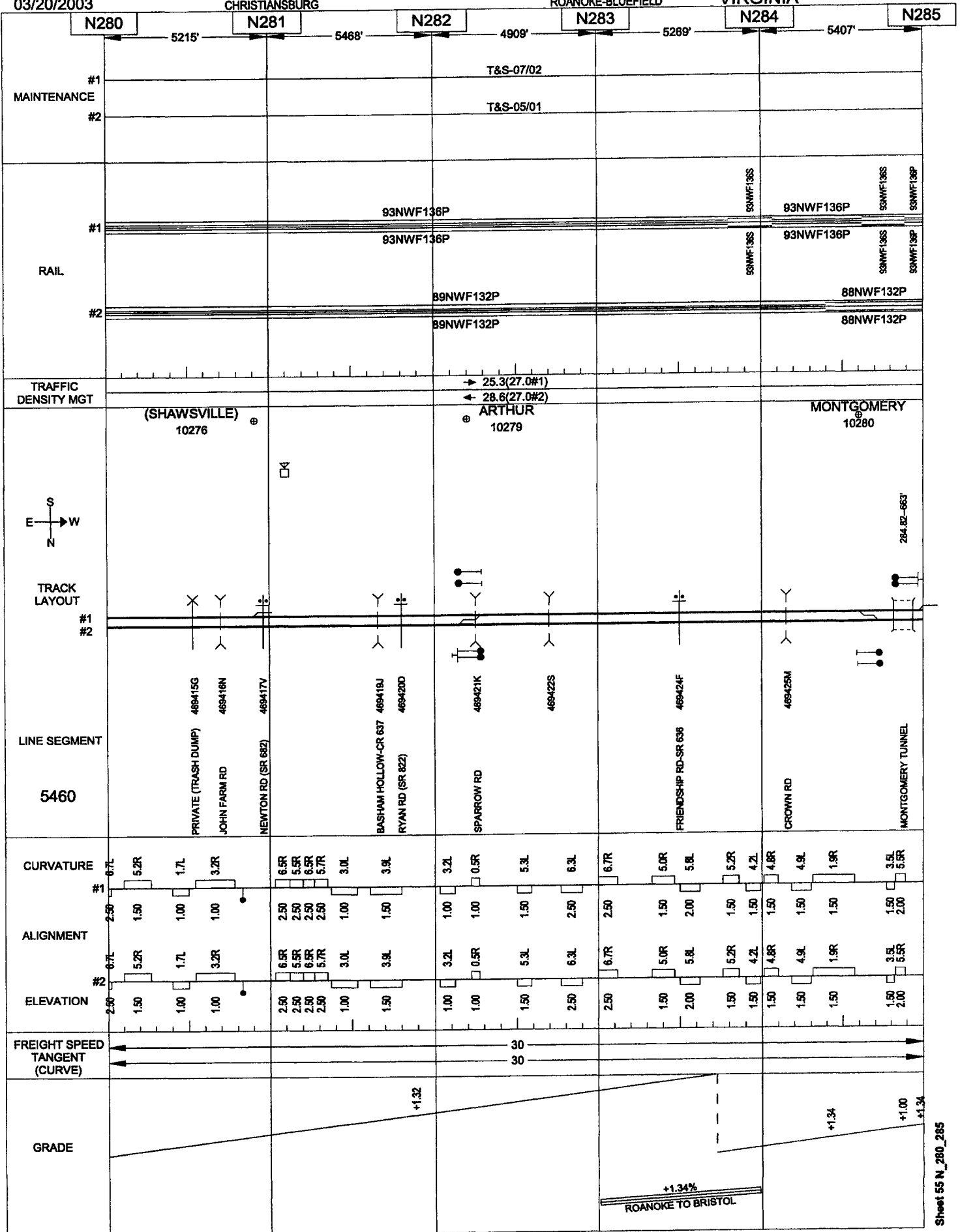


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

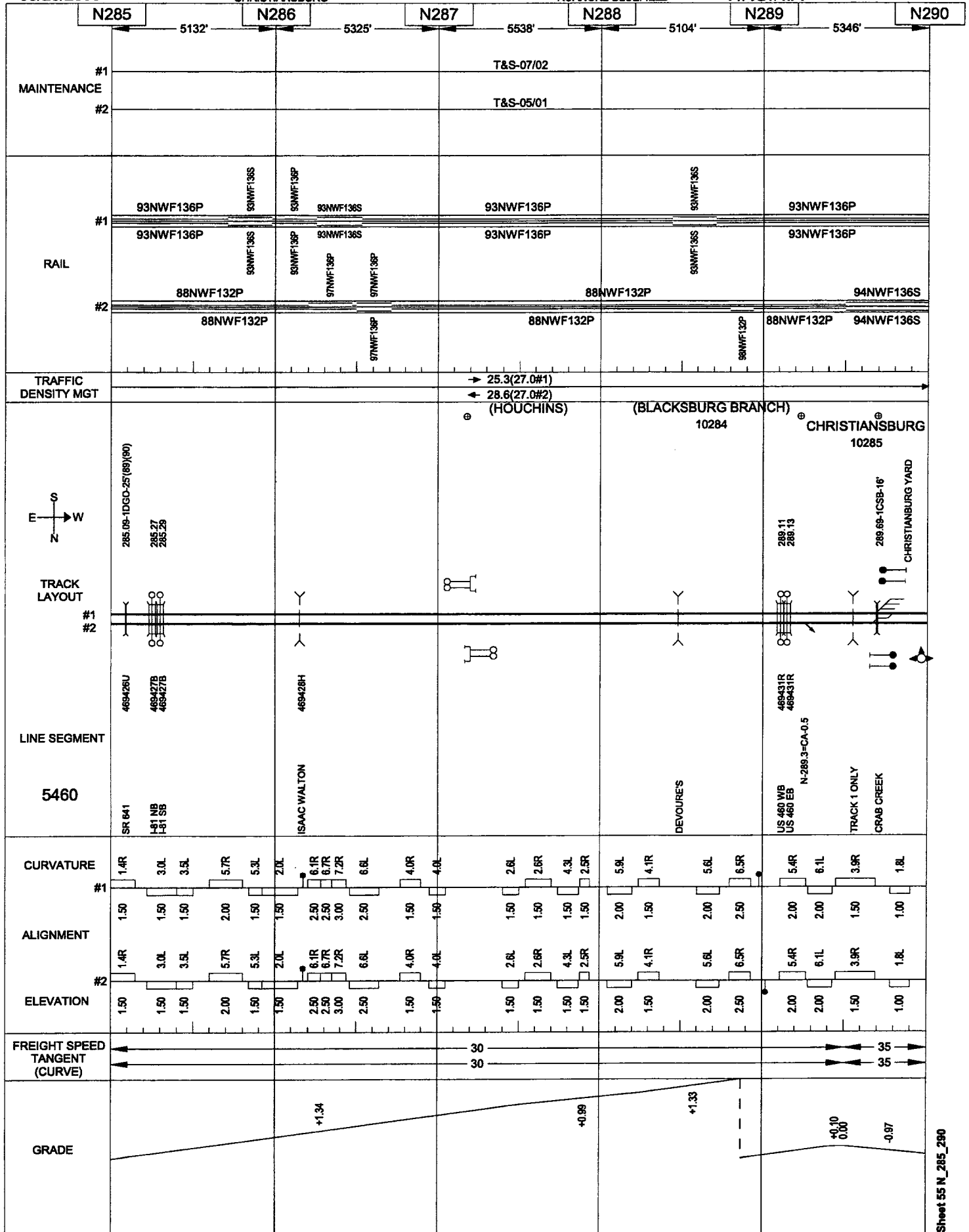


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

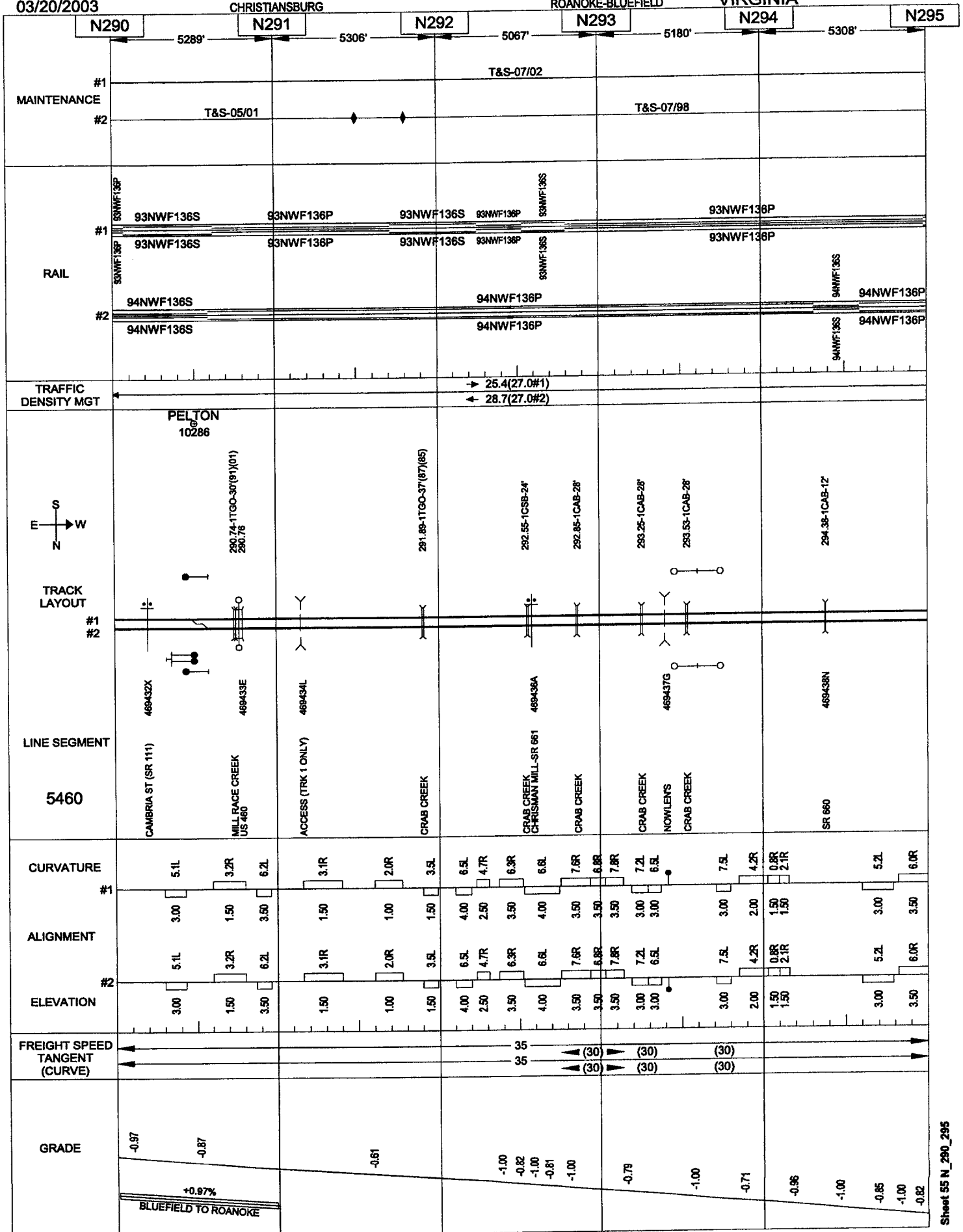


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

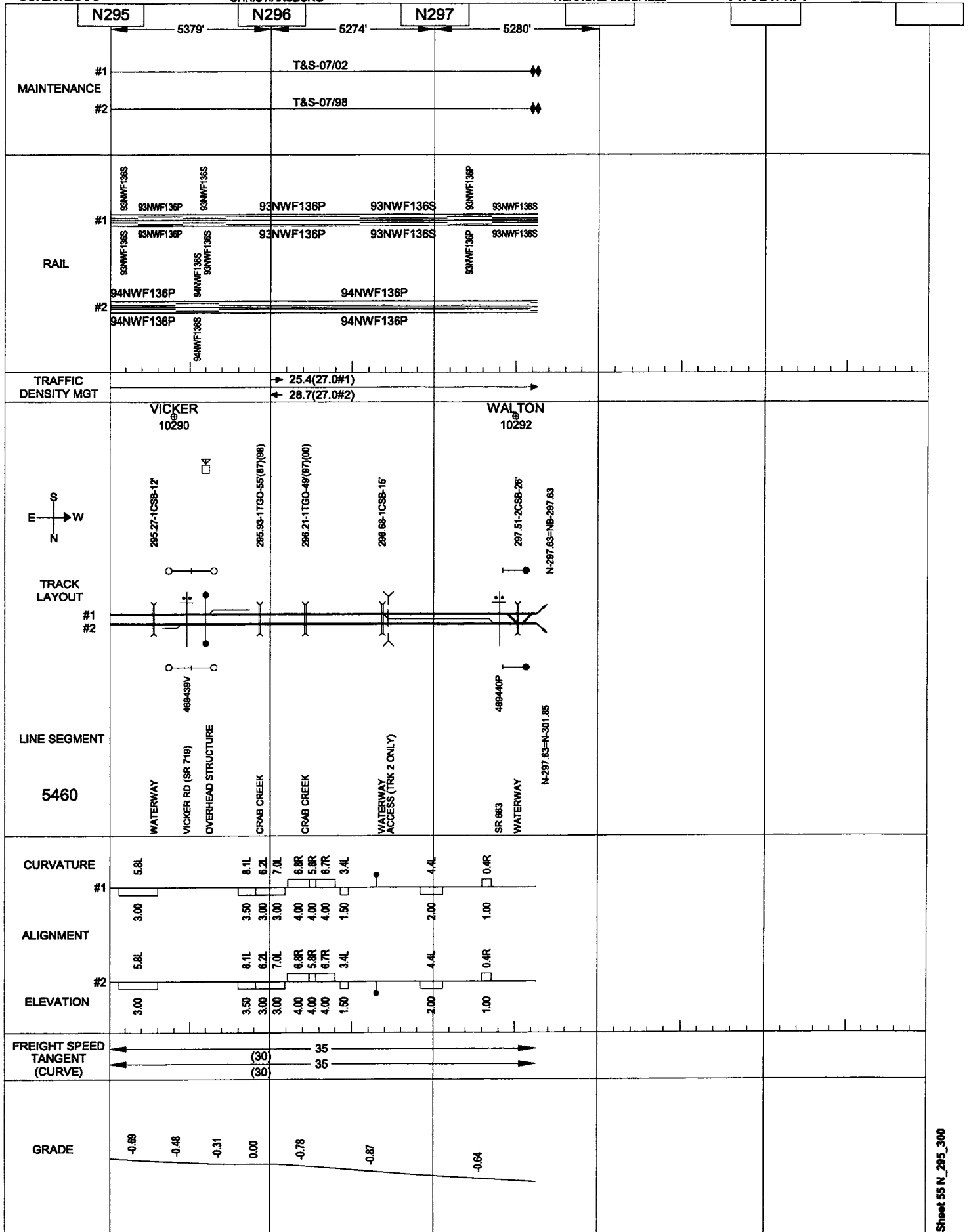


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA



081

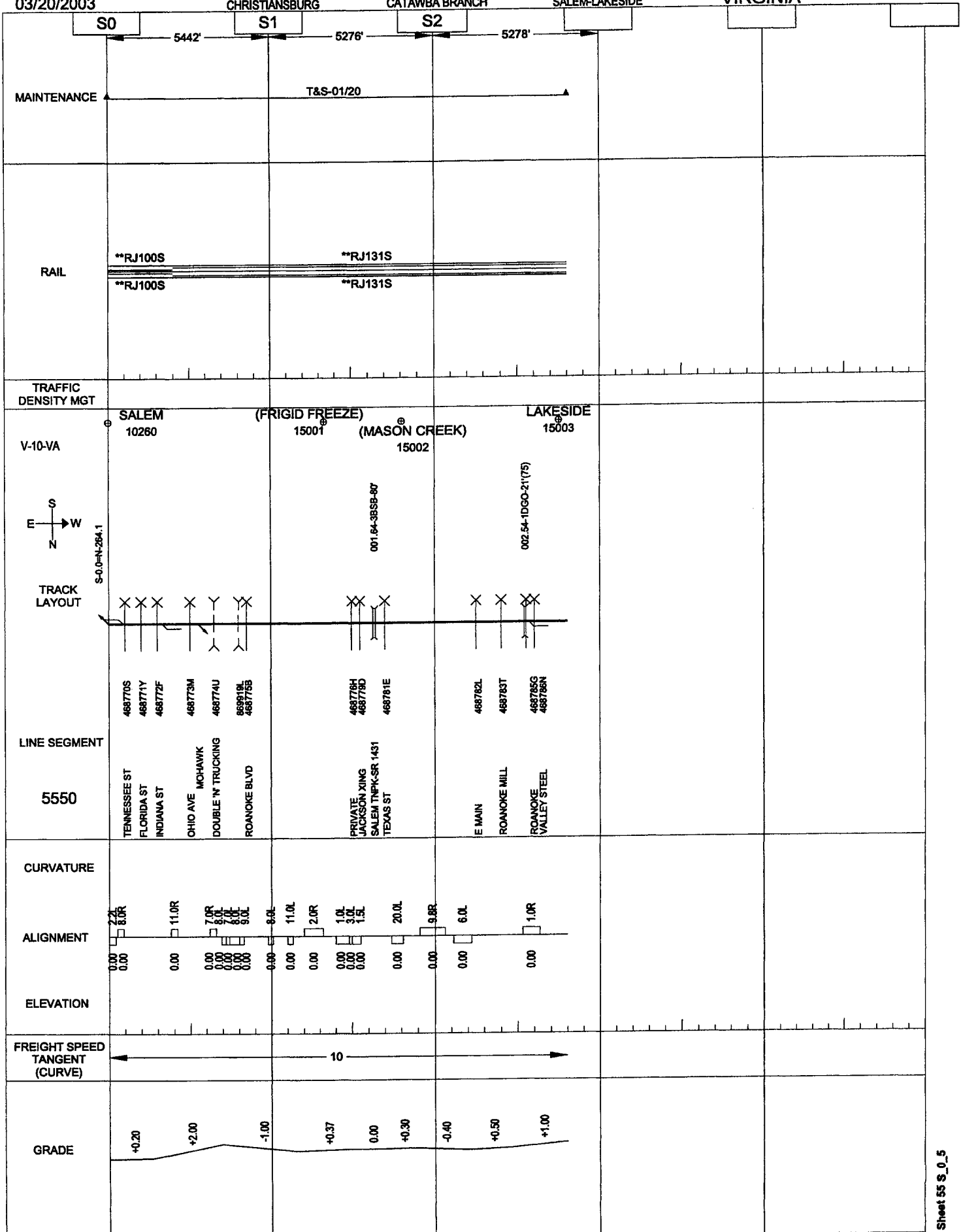
03/20/2003

CHRISTIANSBURG

CATAWBA BRANCH

SALEM-LAKESIDE

VIRGINIA



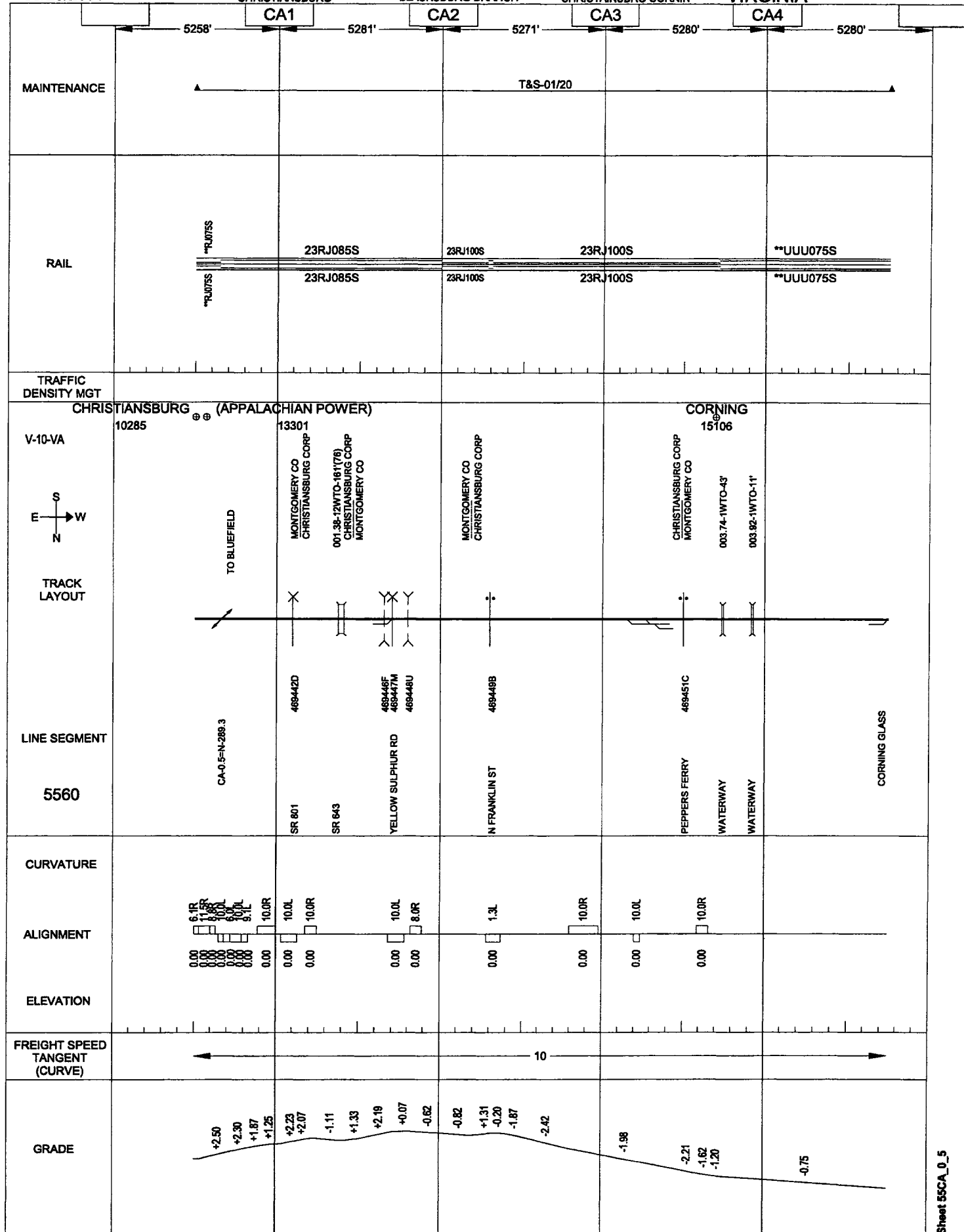
03/20/2003

CHRISTIANSBURG

BLACKSBURG BRANCH

CHRISTIANSBURG-CORNIN

VIRGINIA



03/20/2003

083

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

N302

N303

N304

N305

5280'

5280'

5280'

5280'

MAINTENANCE

T&S-07/98
S-09/00

RAIL

91NWF132S

91NWF132S

TRAFFIC
DENSITY MGT20.5(40.2)
← 19.720.4(40.2)
← 19.8

WALTON

MEADOW

(PEPPER)
10295

(BLUFF)

TRACK
LAYOUT

LINE SEGMENT

5470

N-301.85=N-297.63

N-302.3/NB-298.0

302.71-1CAB-50'

BRAMBLE BUSH

WATERWAY

NS CROSSING

SR 114 EB
SR 114 WB

NS CROSSING

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

35

GRADE

0.00

-0.13

0.00

-0.17

0.00

-0.05

0.00

-0.09

0.00

-0.17

-0.15

0.00

-0.07

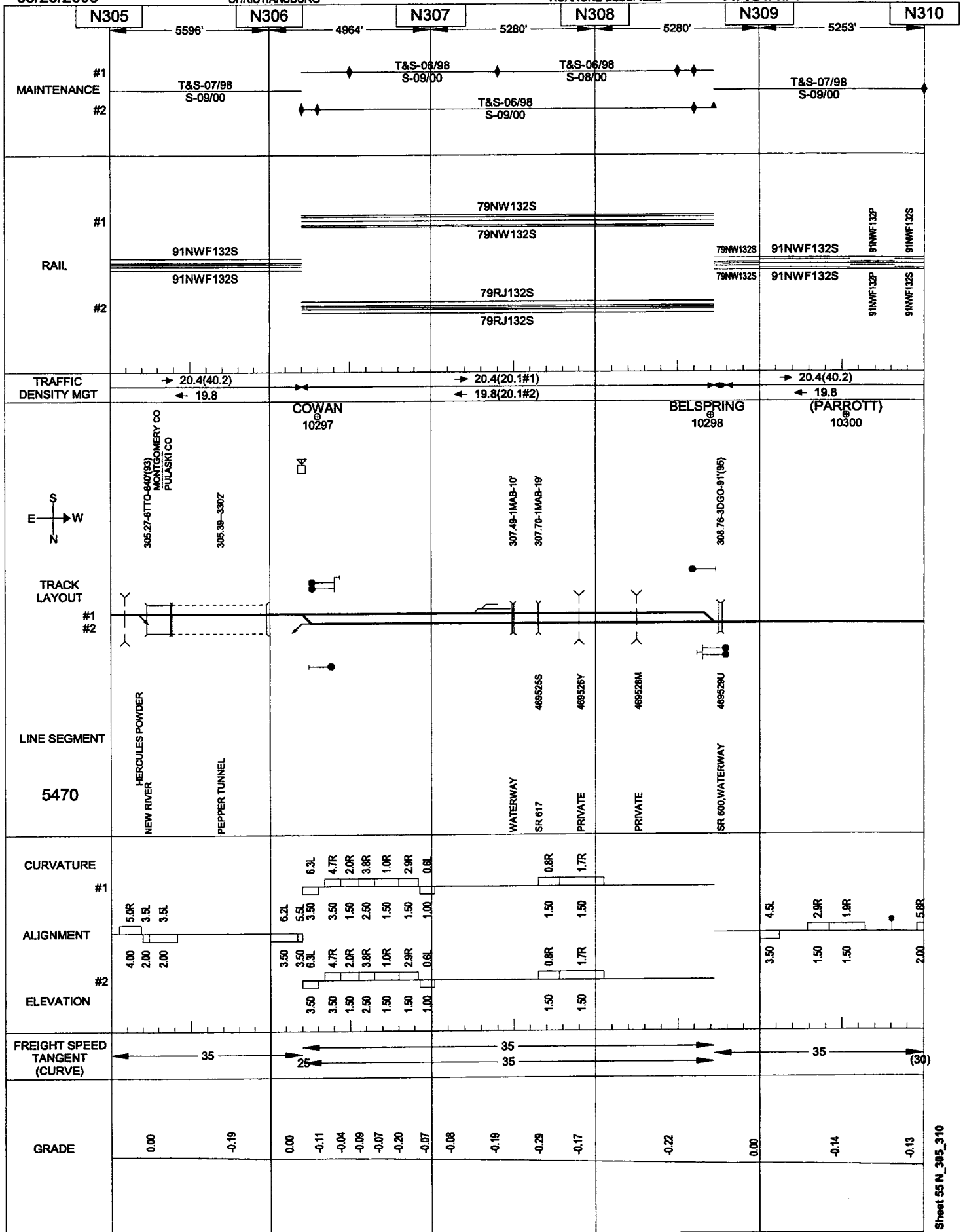
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03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

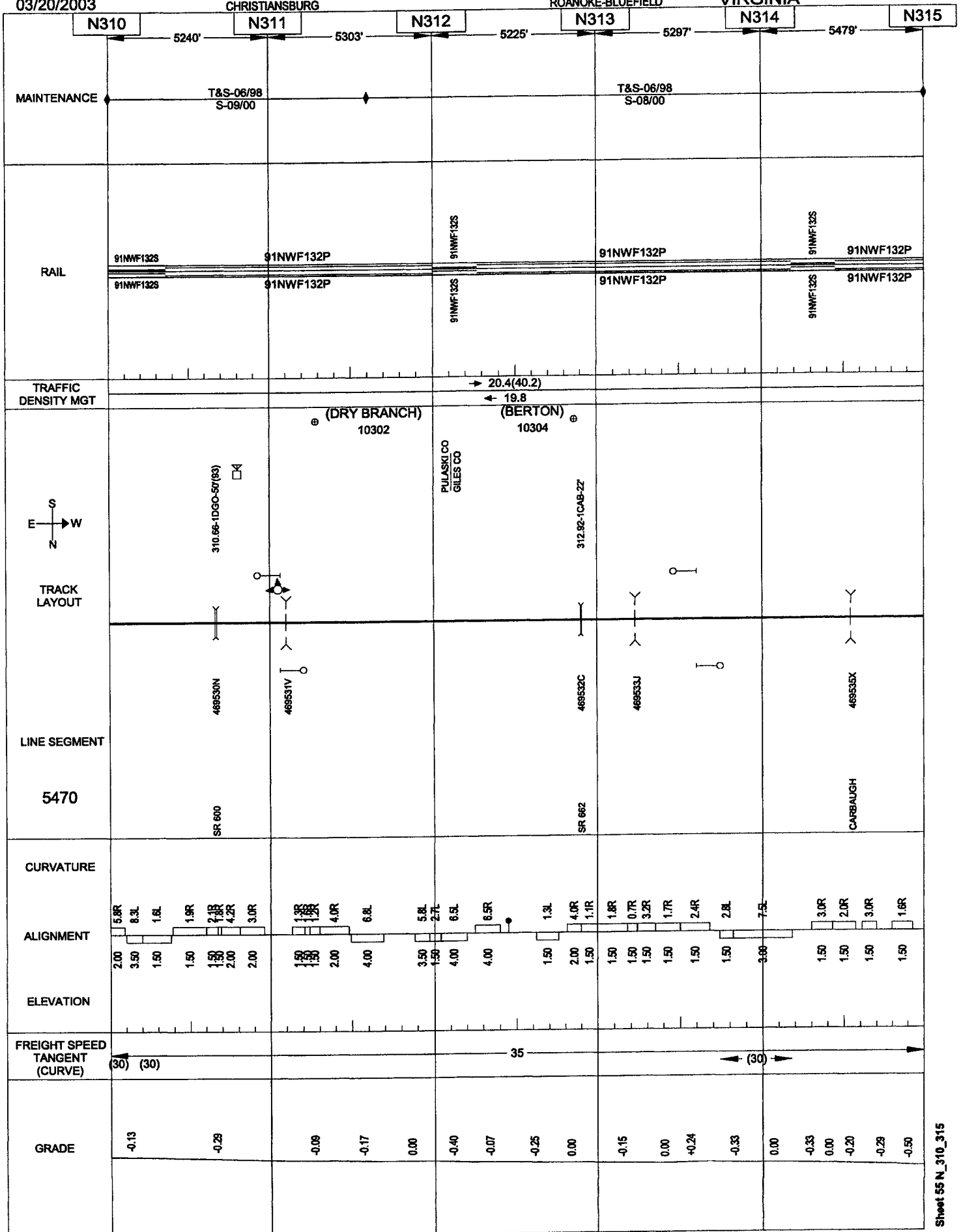


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

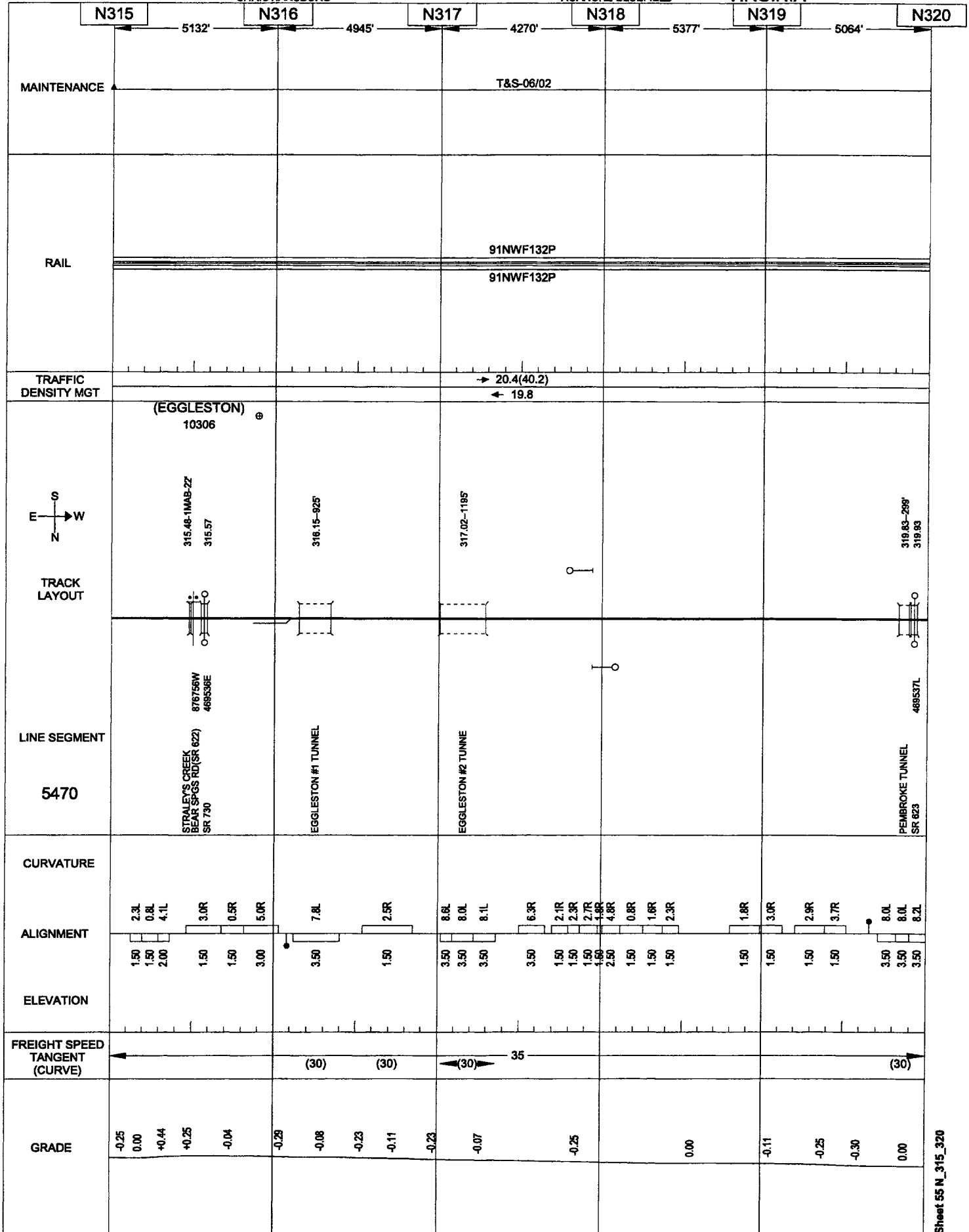


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA



Sheet 55 N_320_325

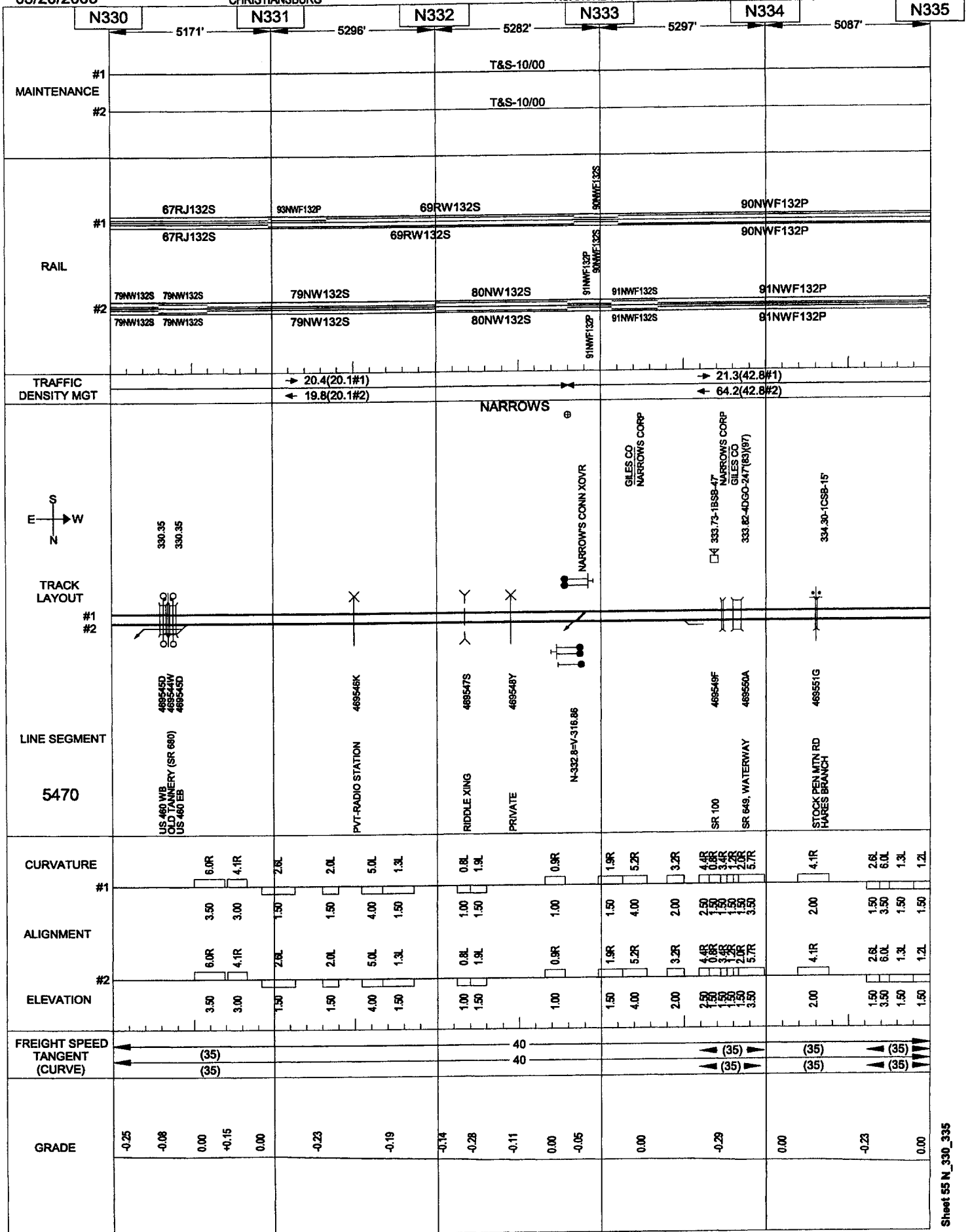


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

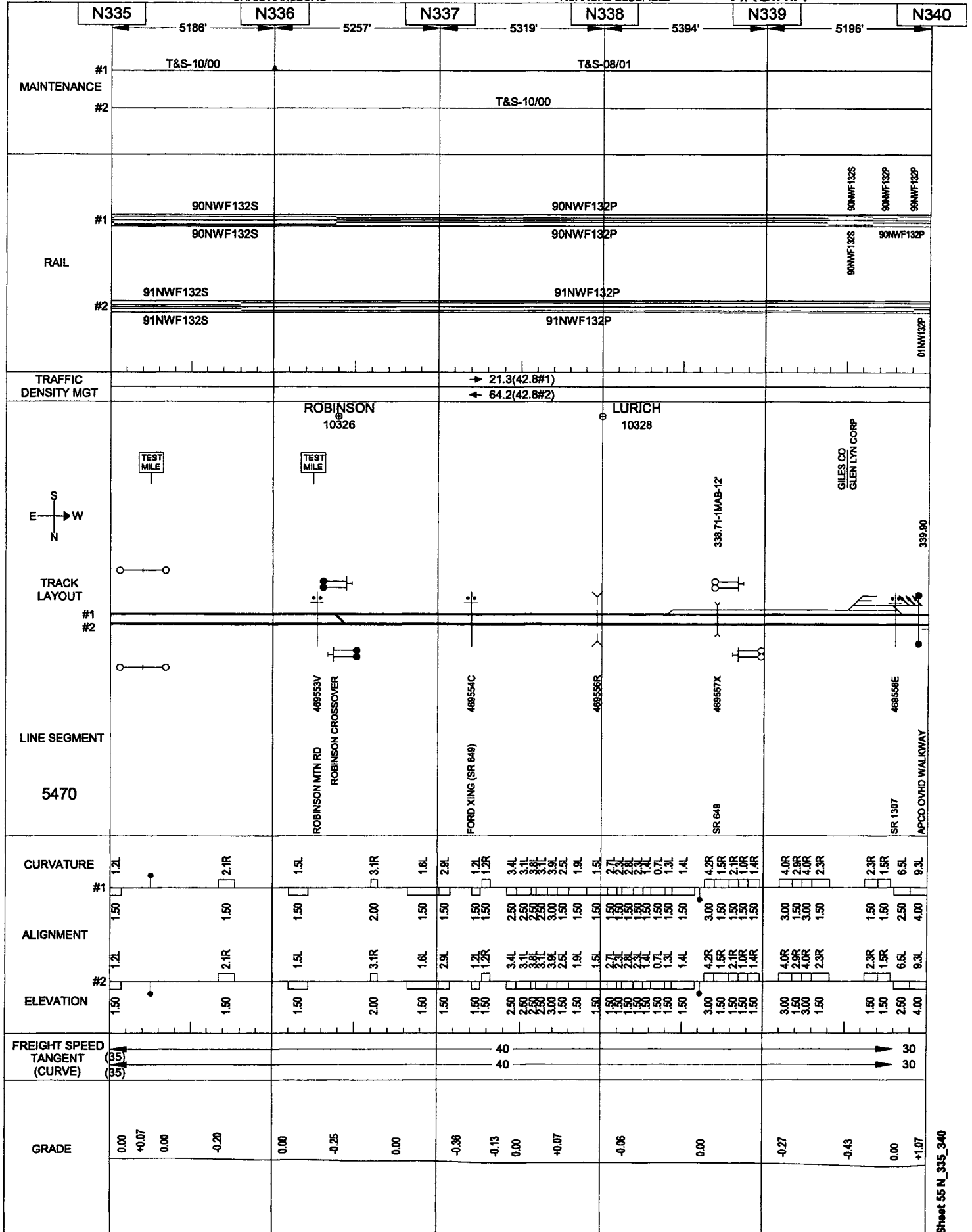


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA



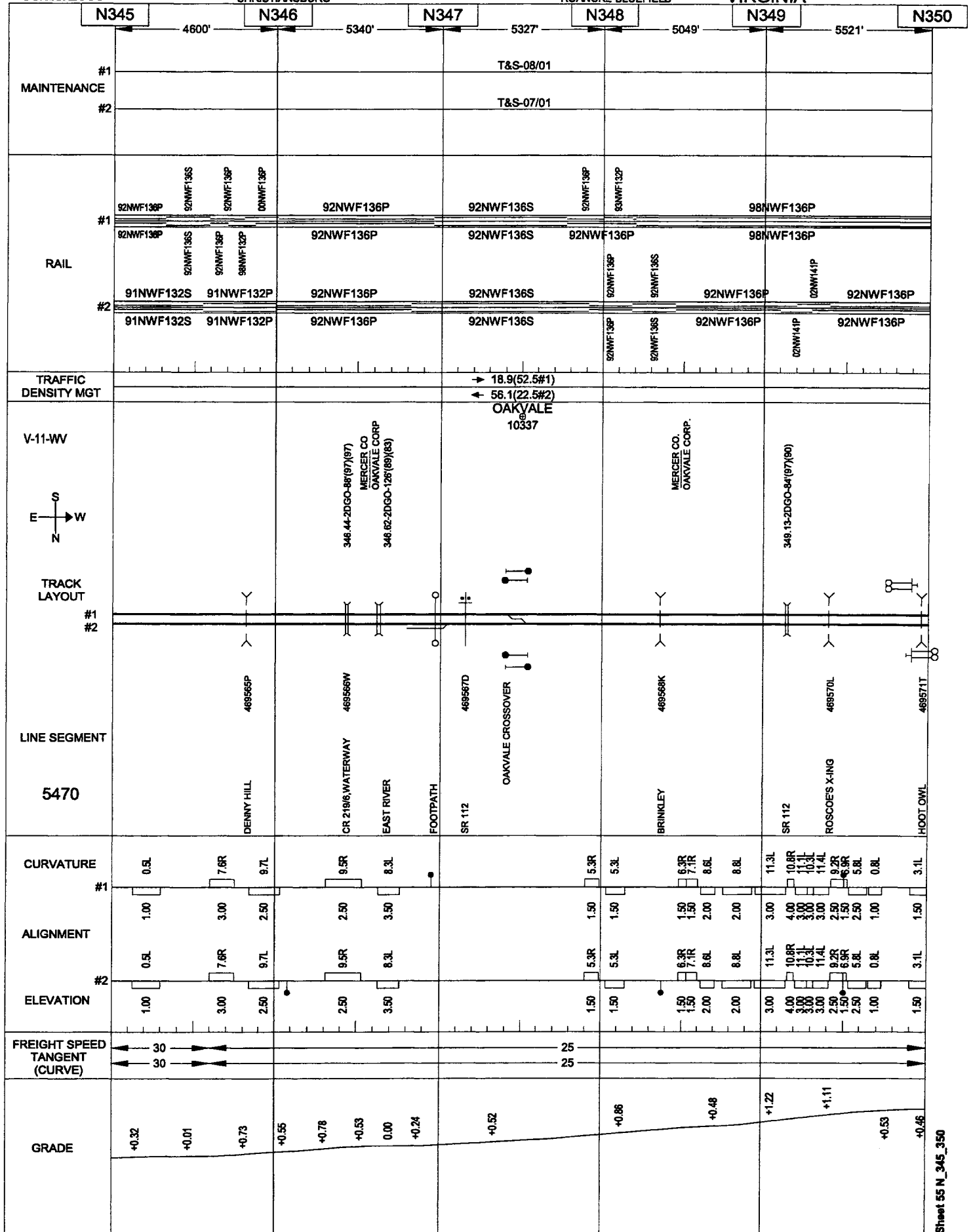
Sheet 55 N_340_345

03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

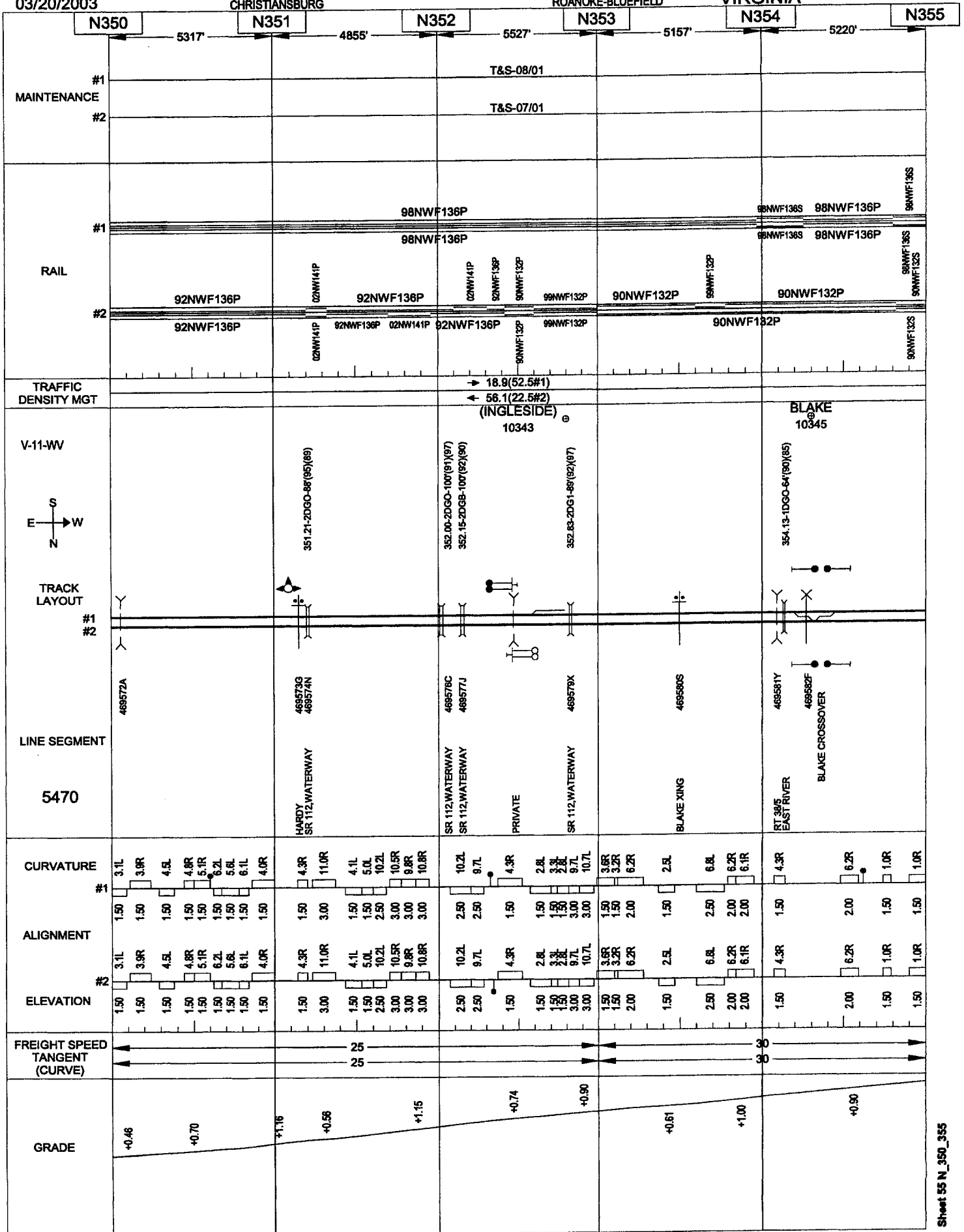


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA

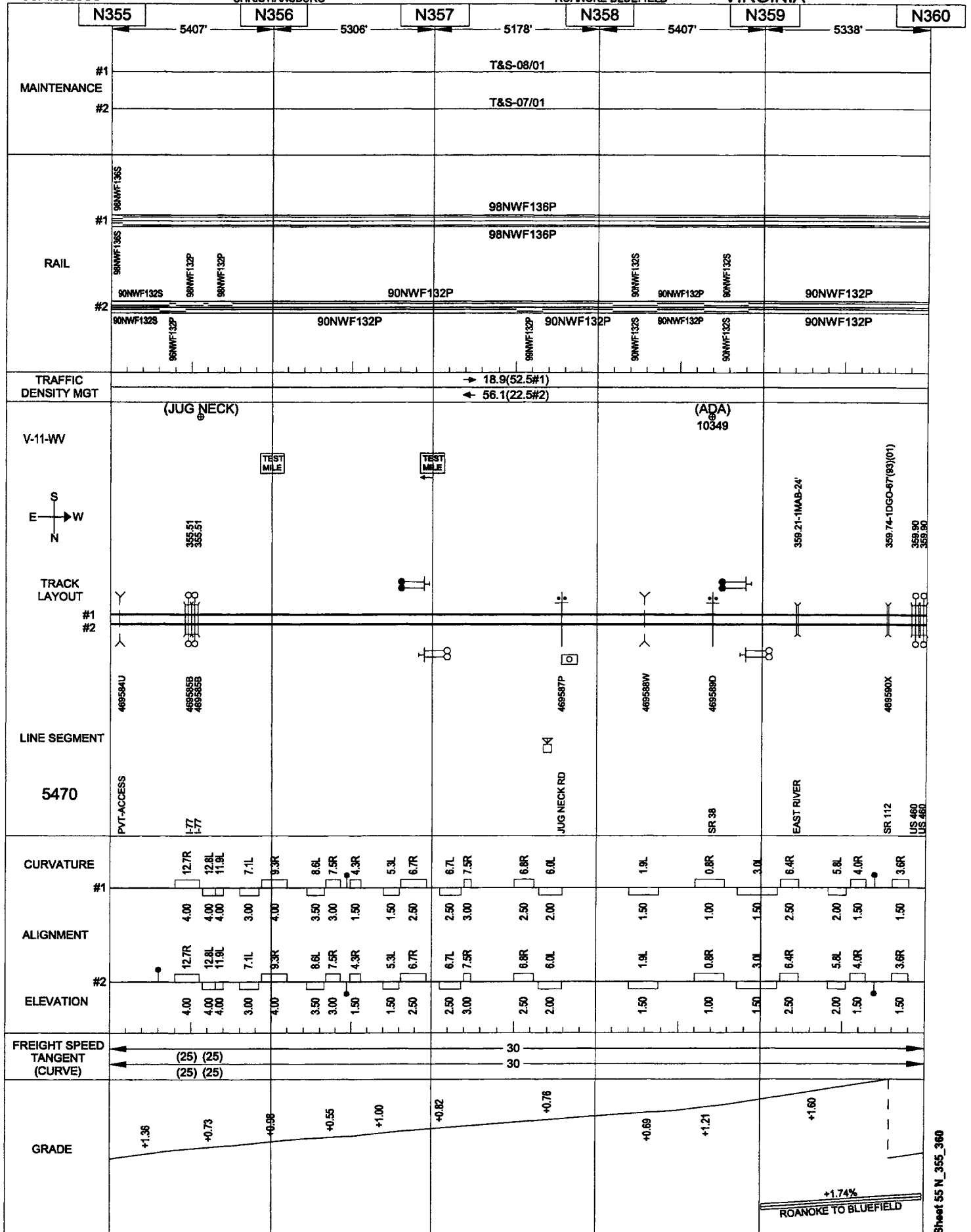


03/20/2003

CHRISTIANSBURG

ROANOKE-BLUEFIELD

VIRGINIA



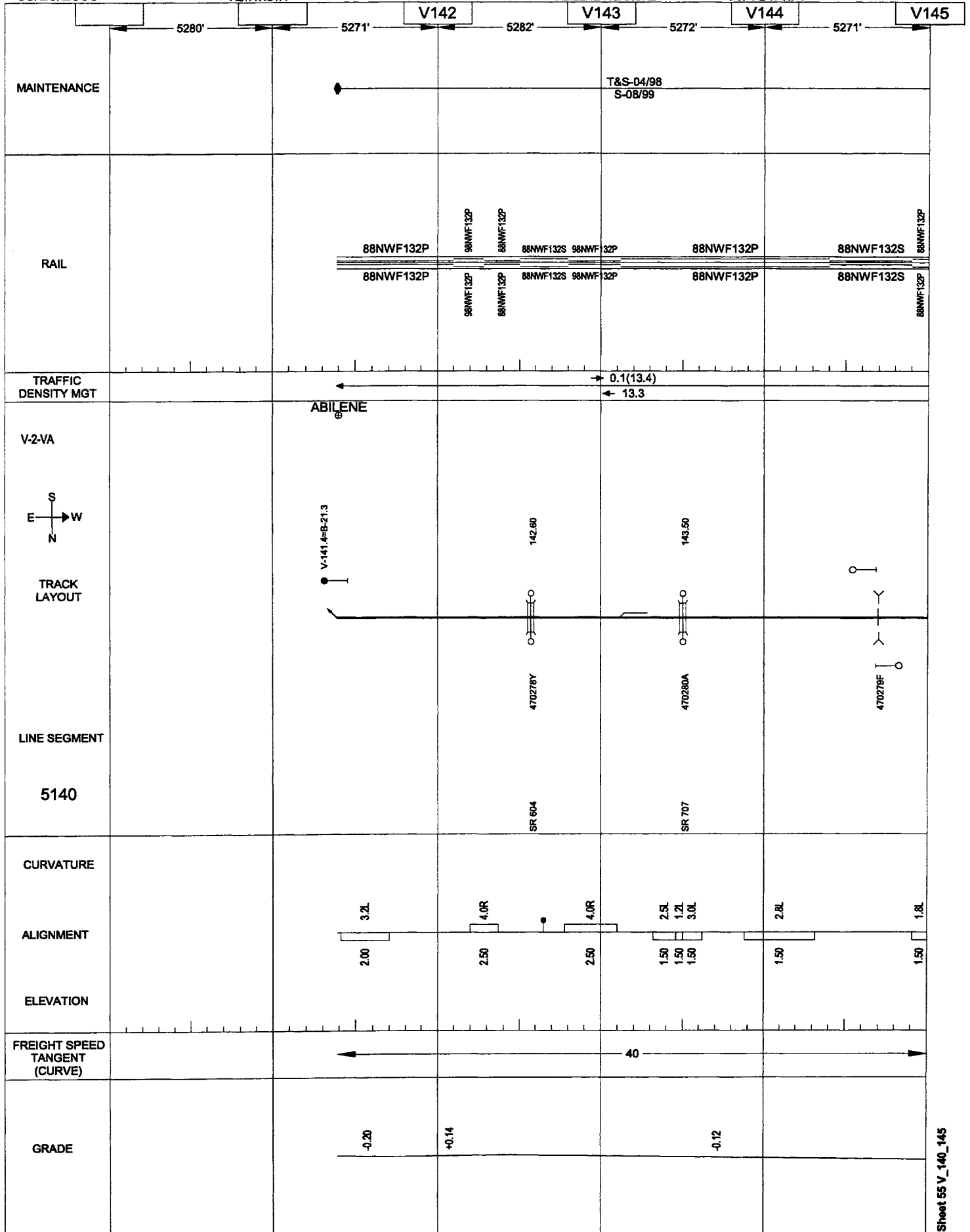


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA



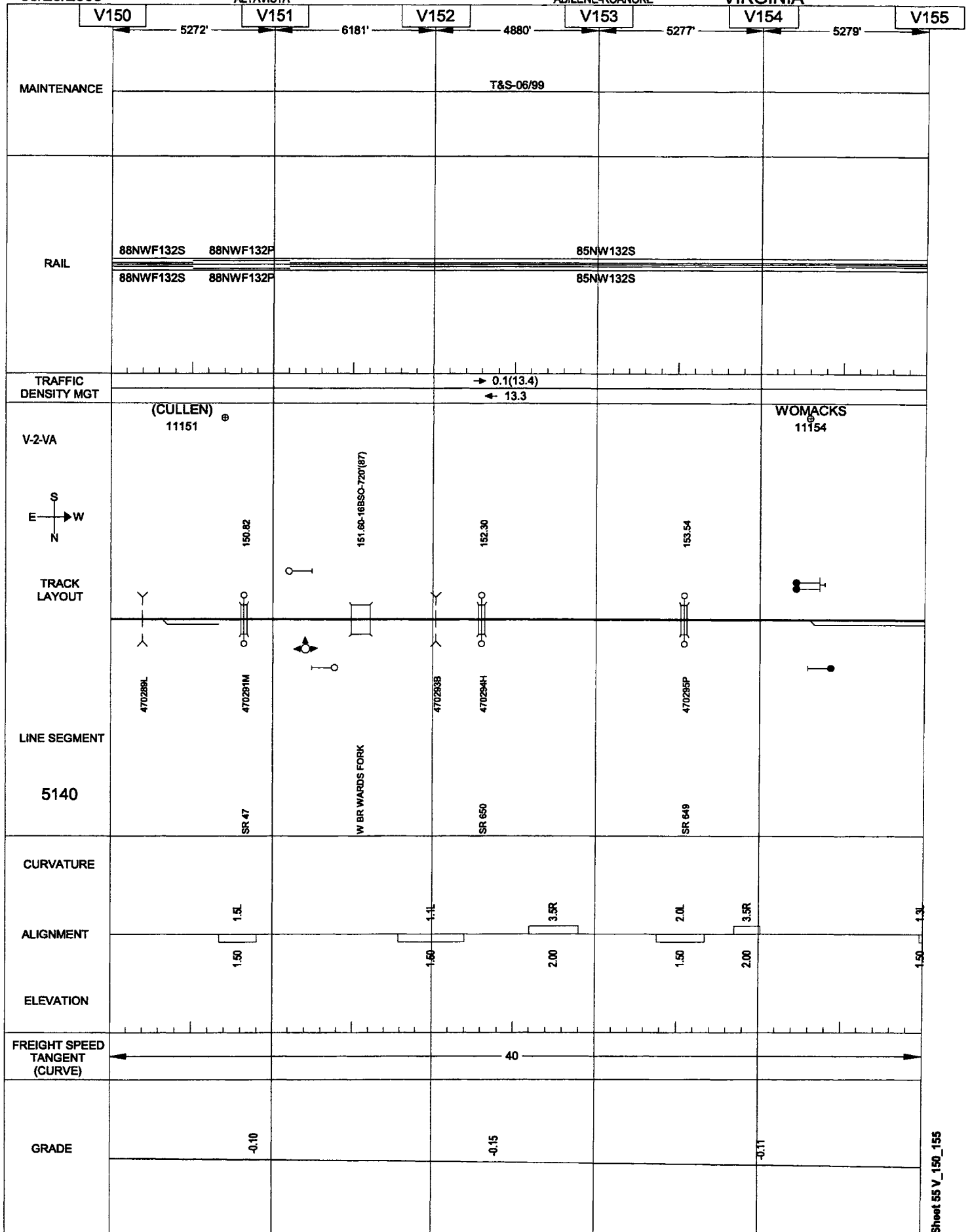
Sheet 55 V_145_150

03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

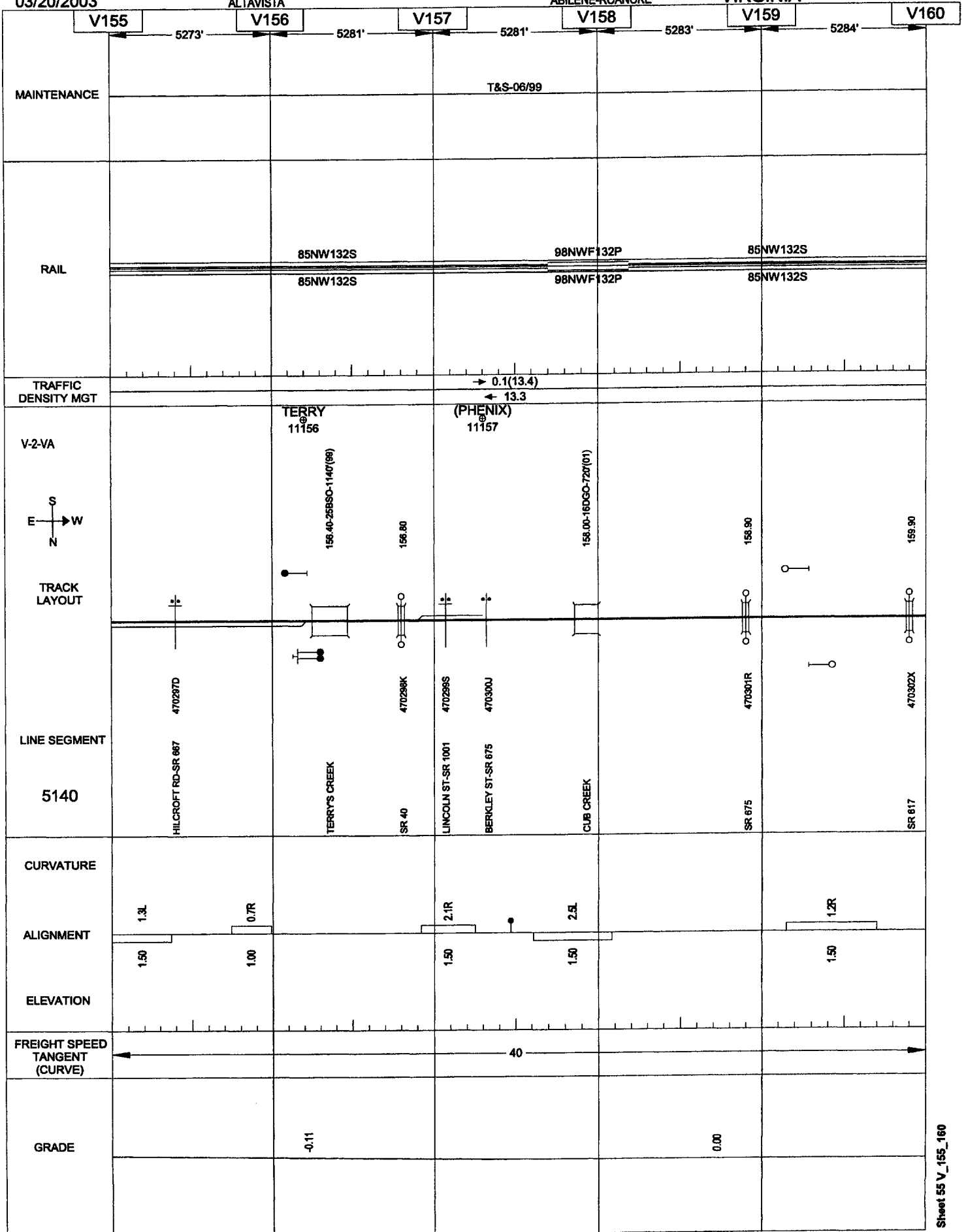


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

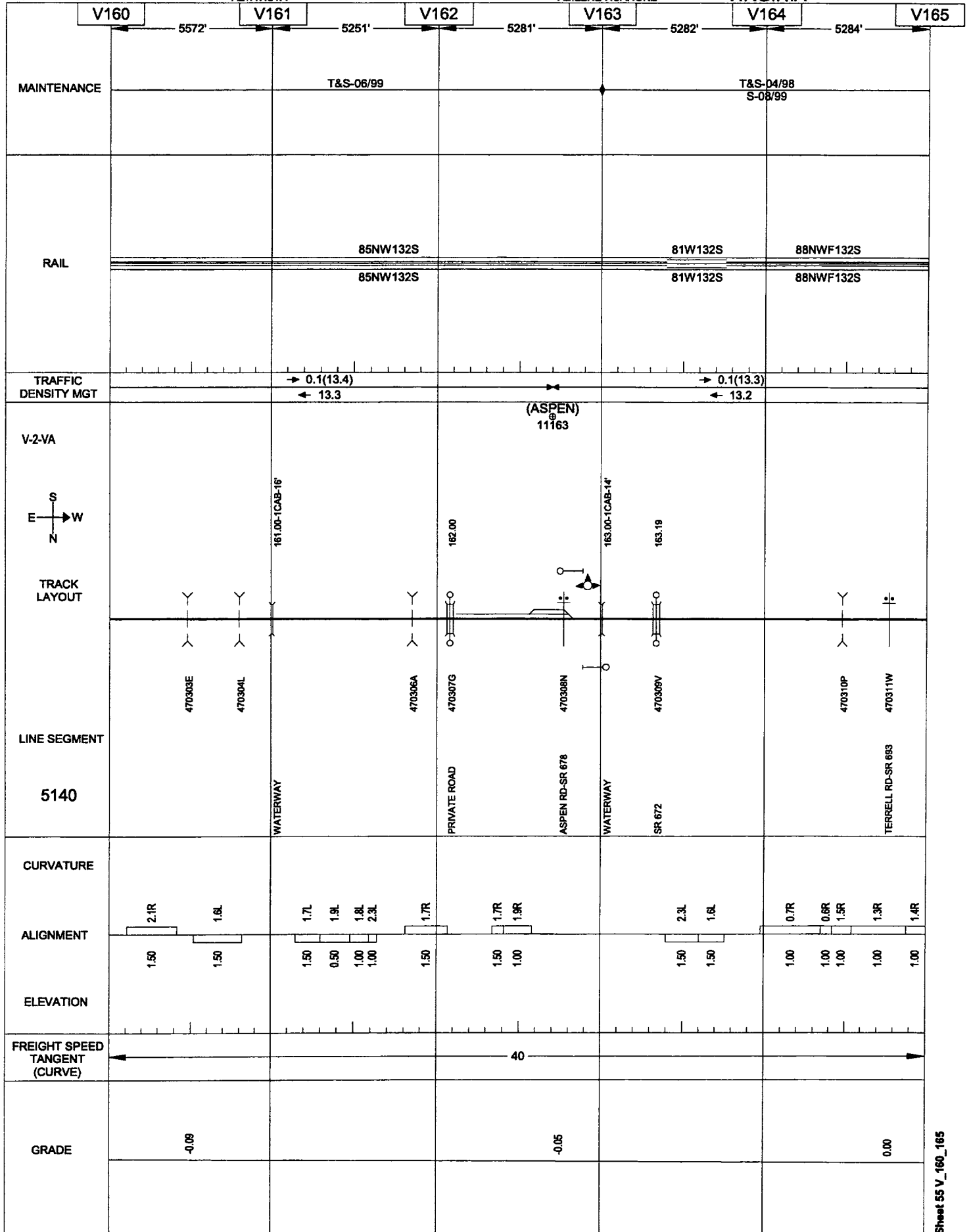


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

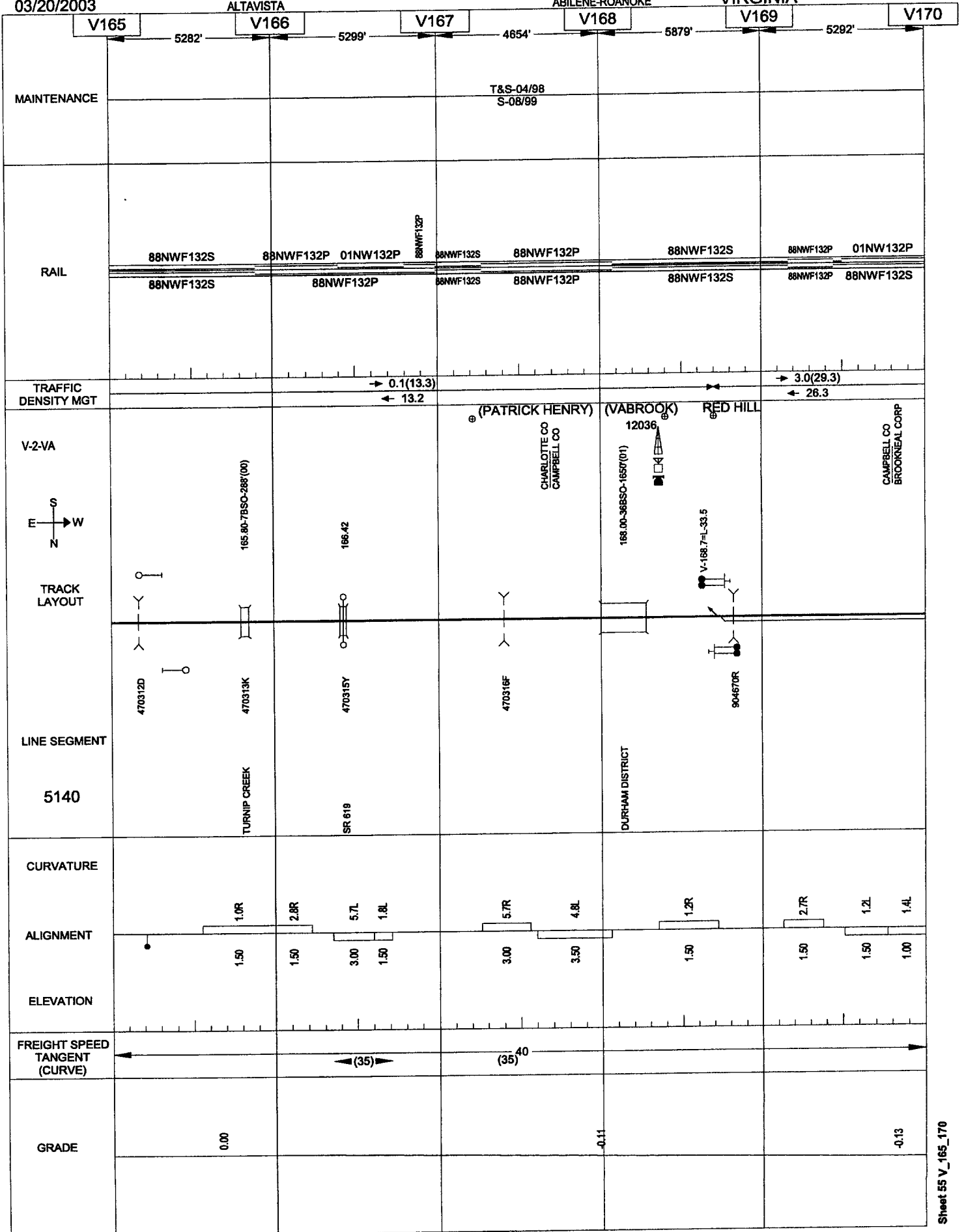


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

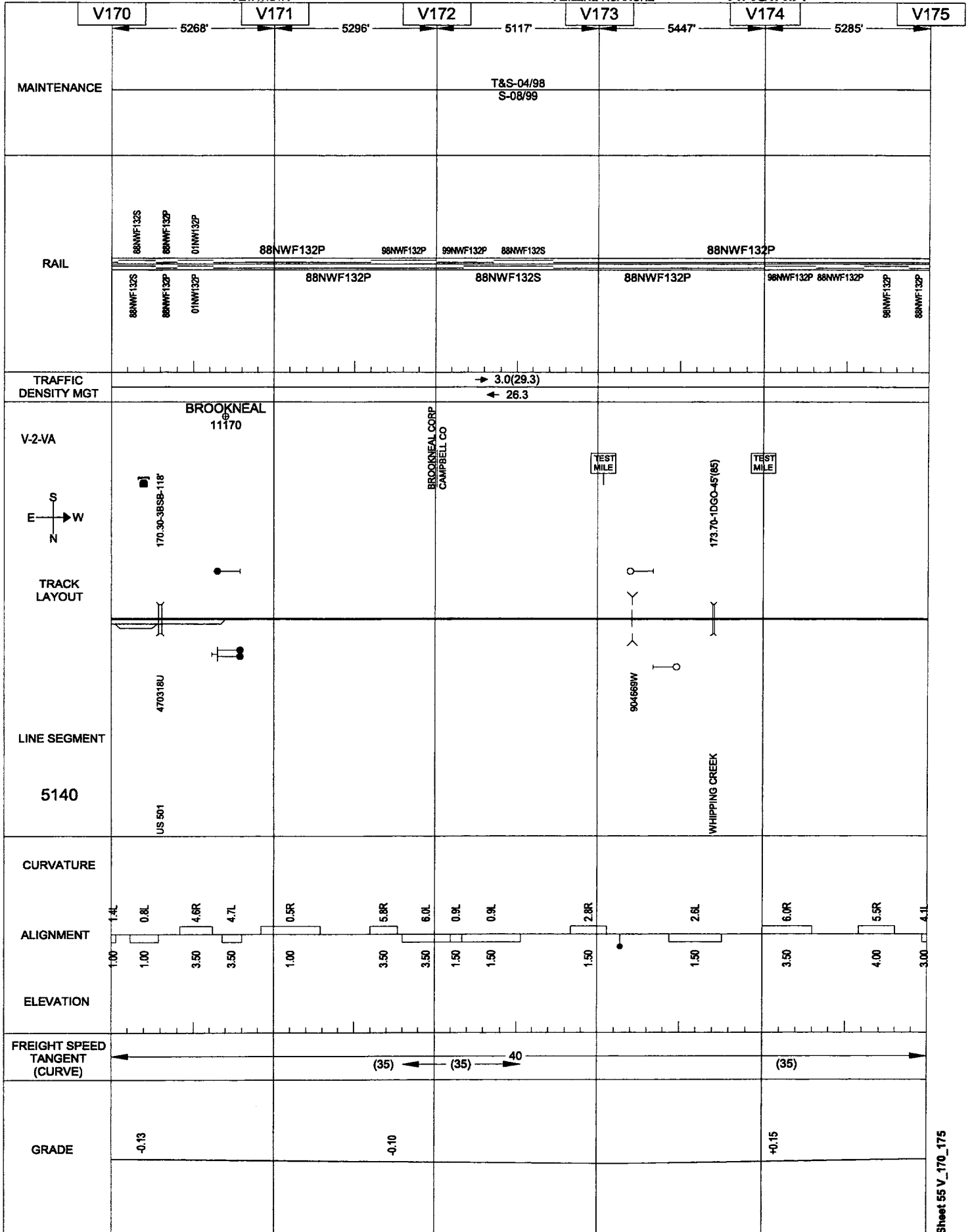


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

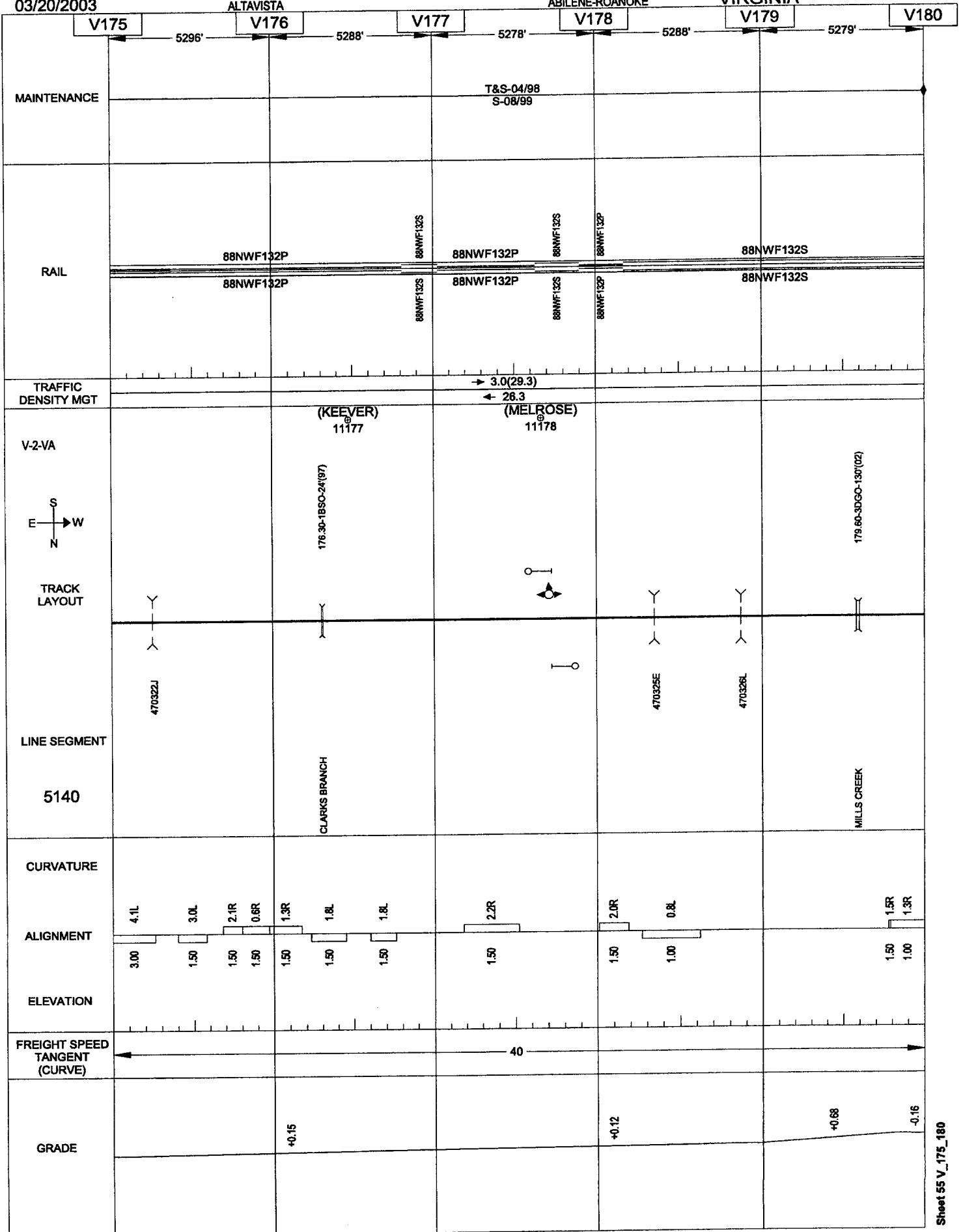


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

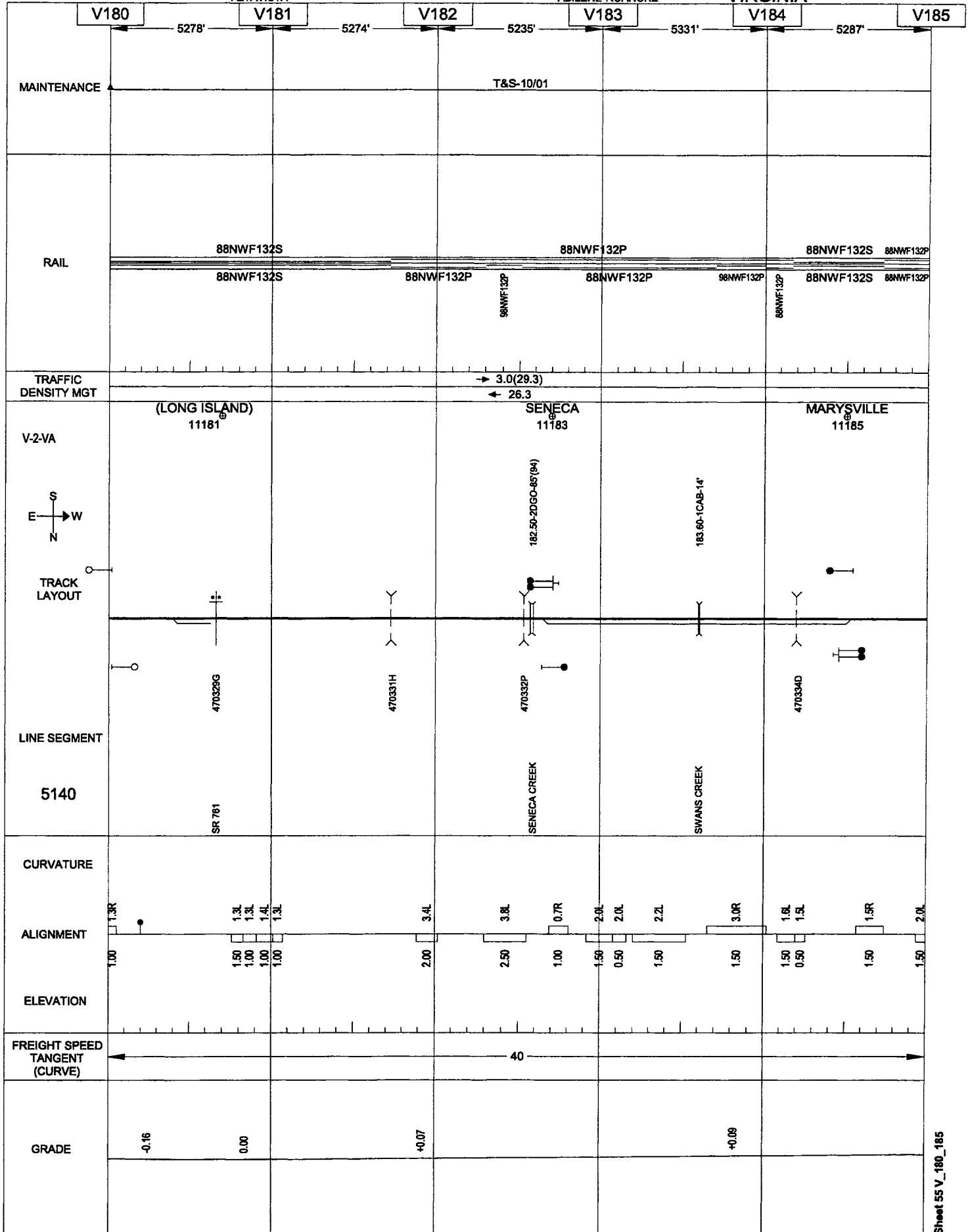


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

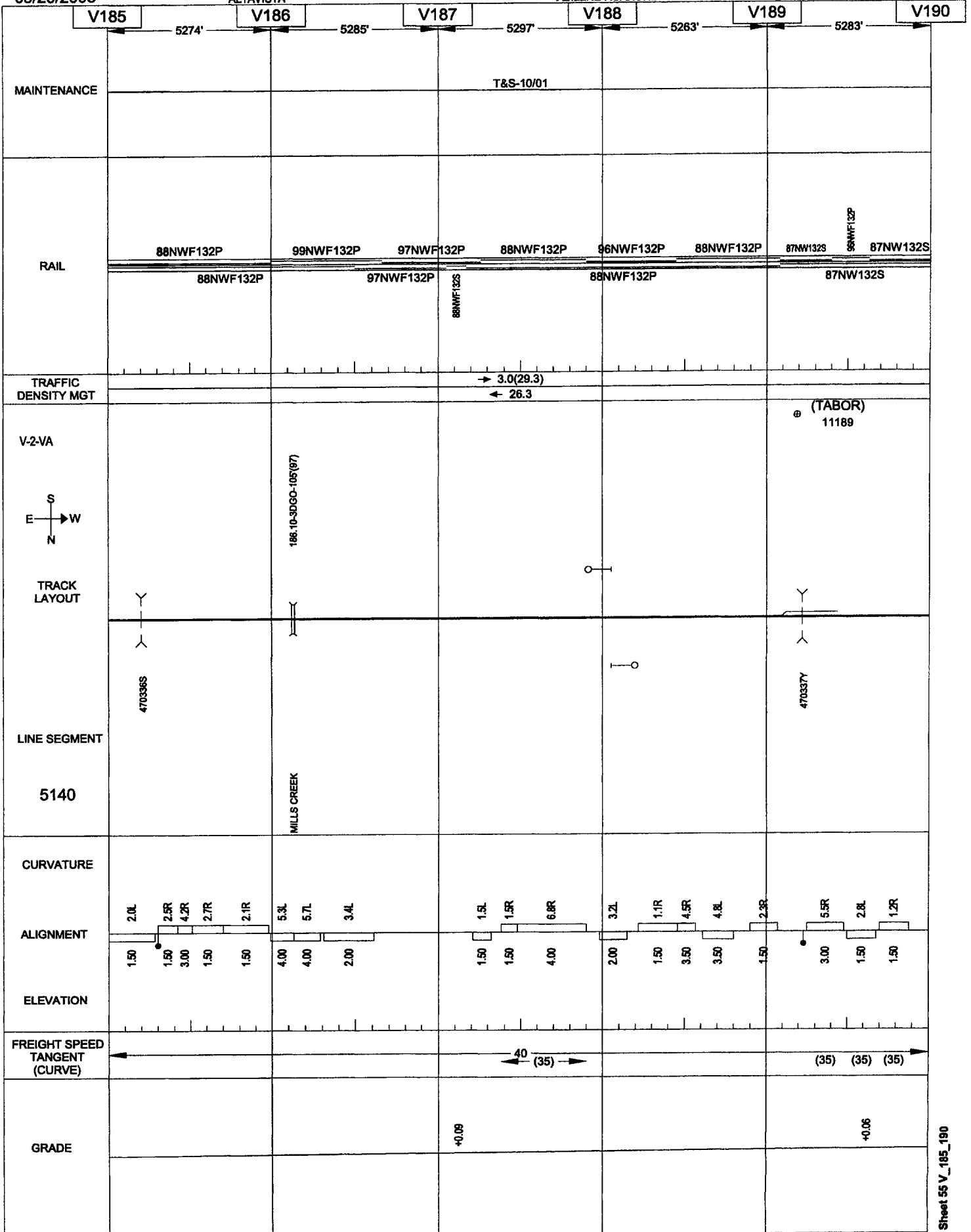


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

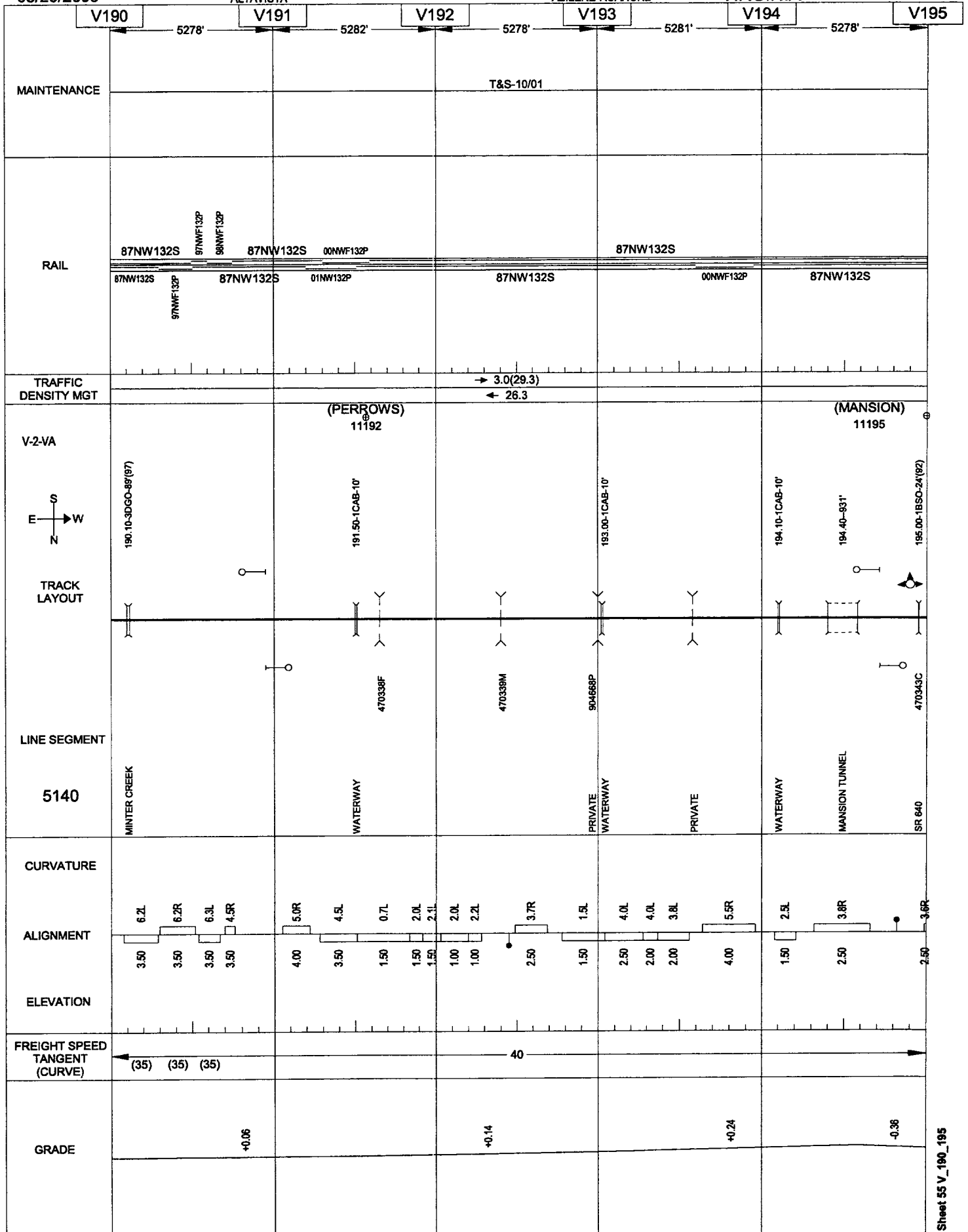


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

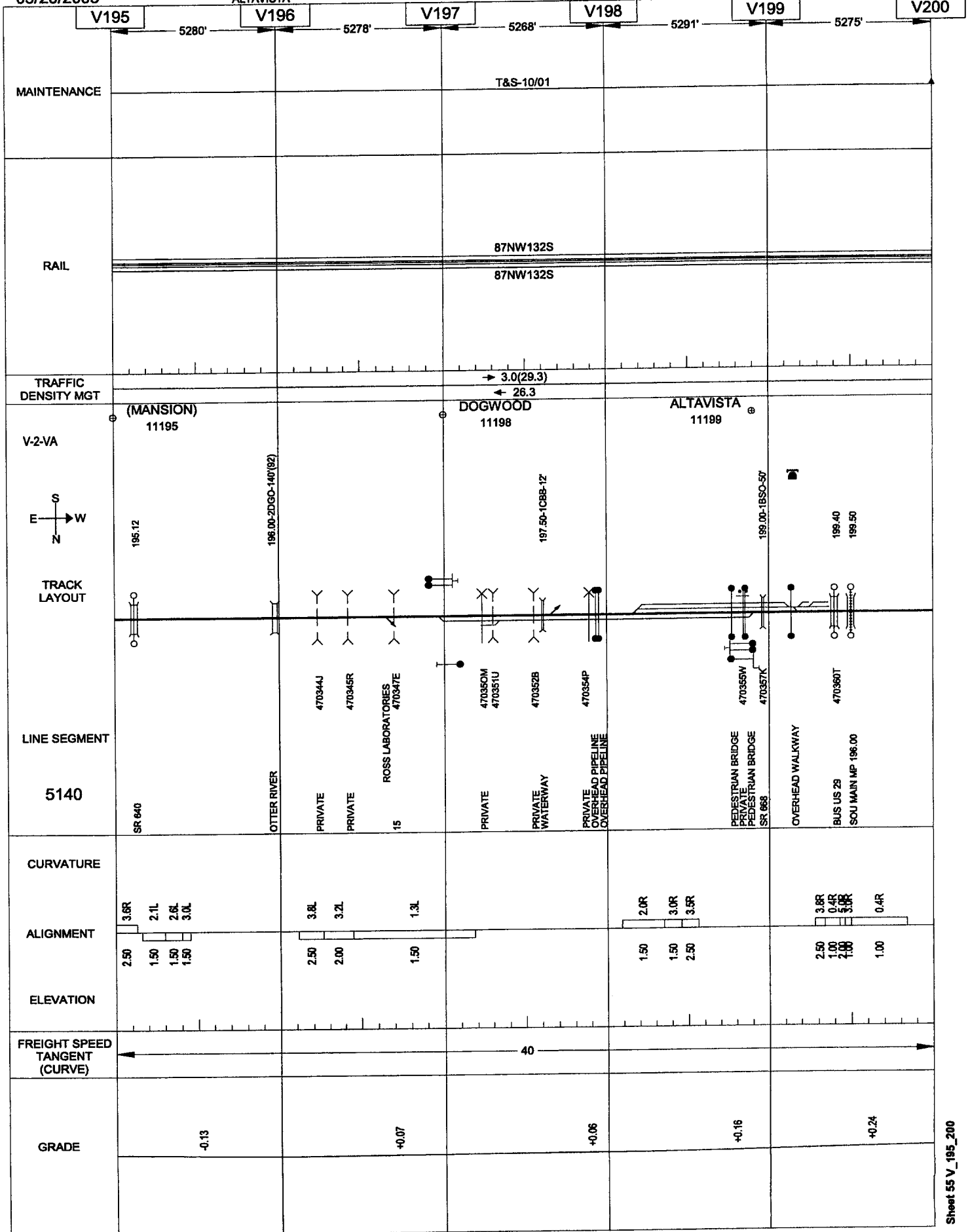


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

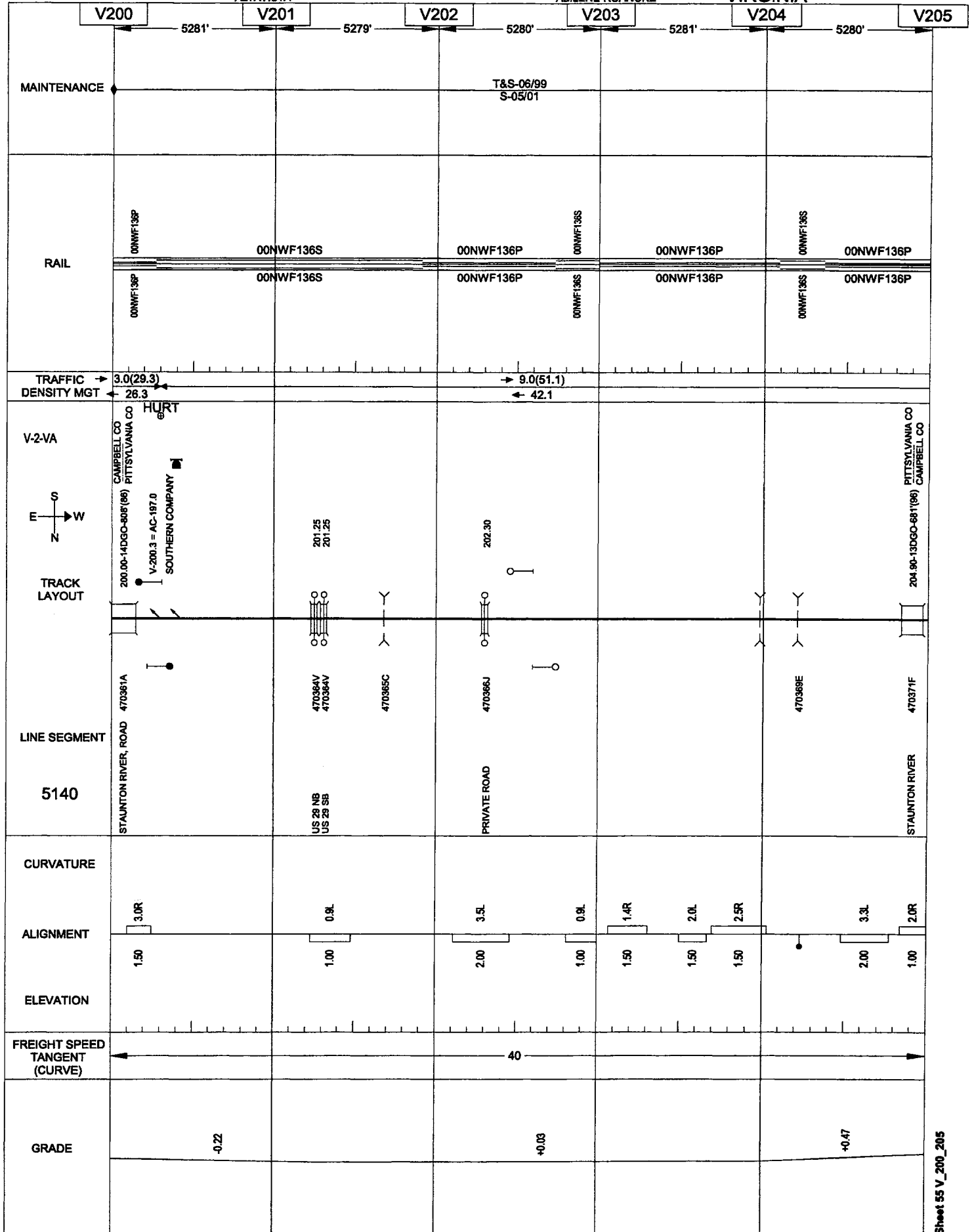


03/20/2003

ALTAVISTA

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VIRGINIA

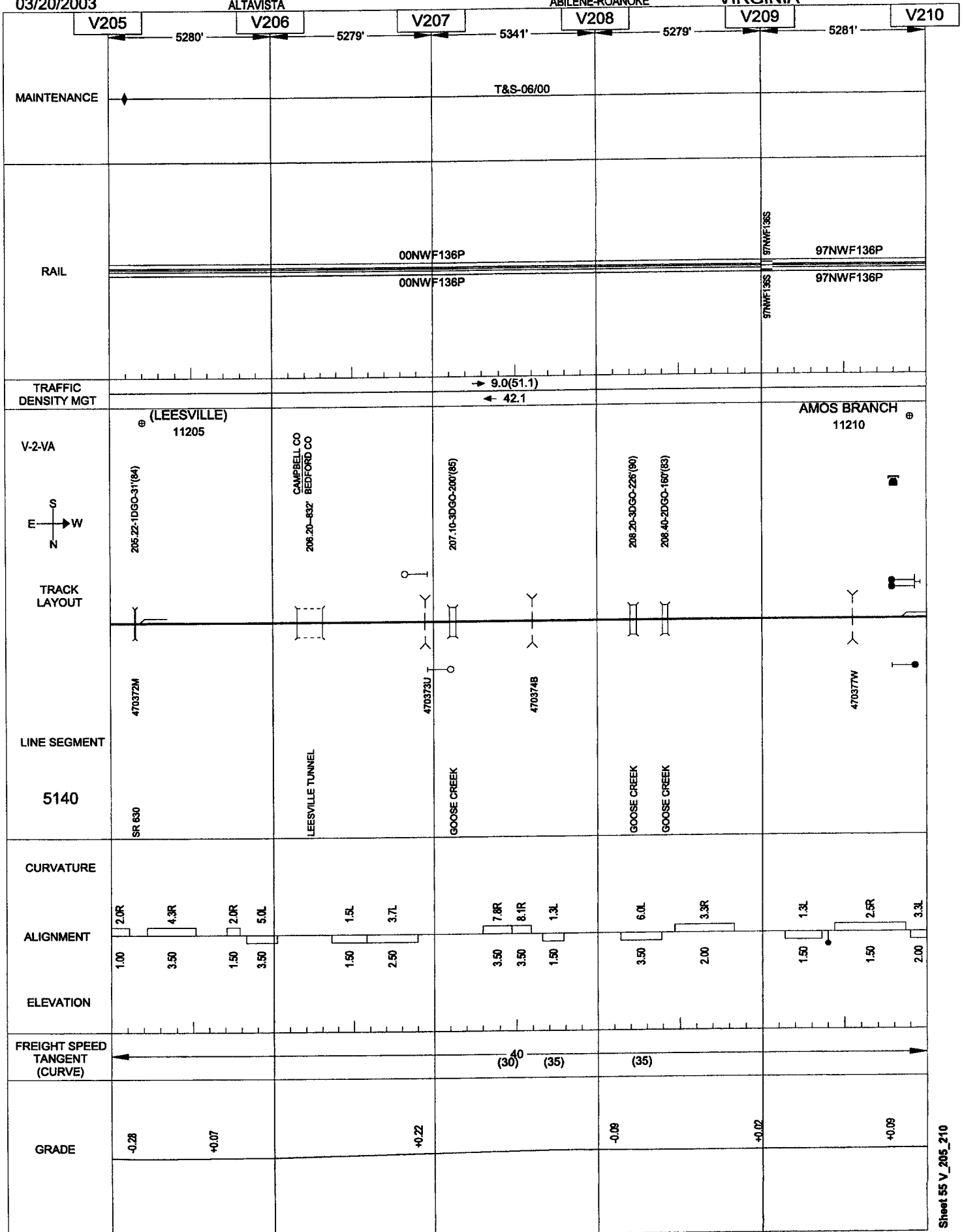


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

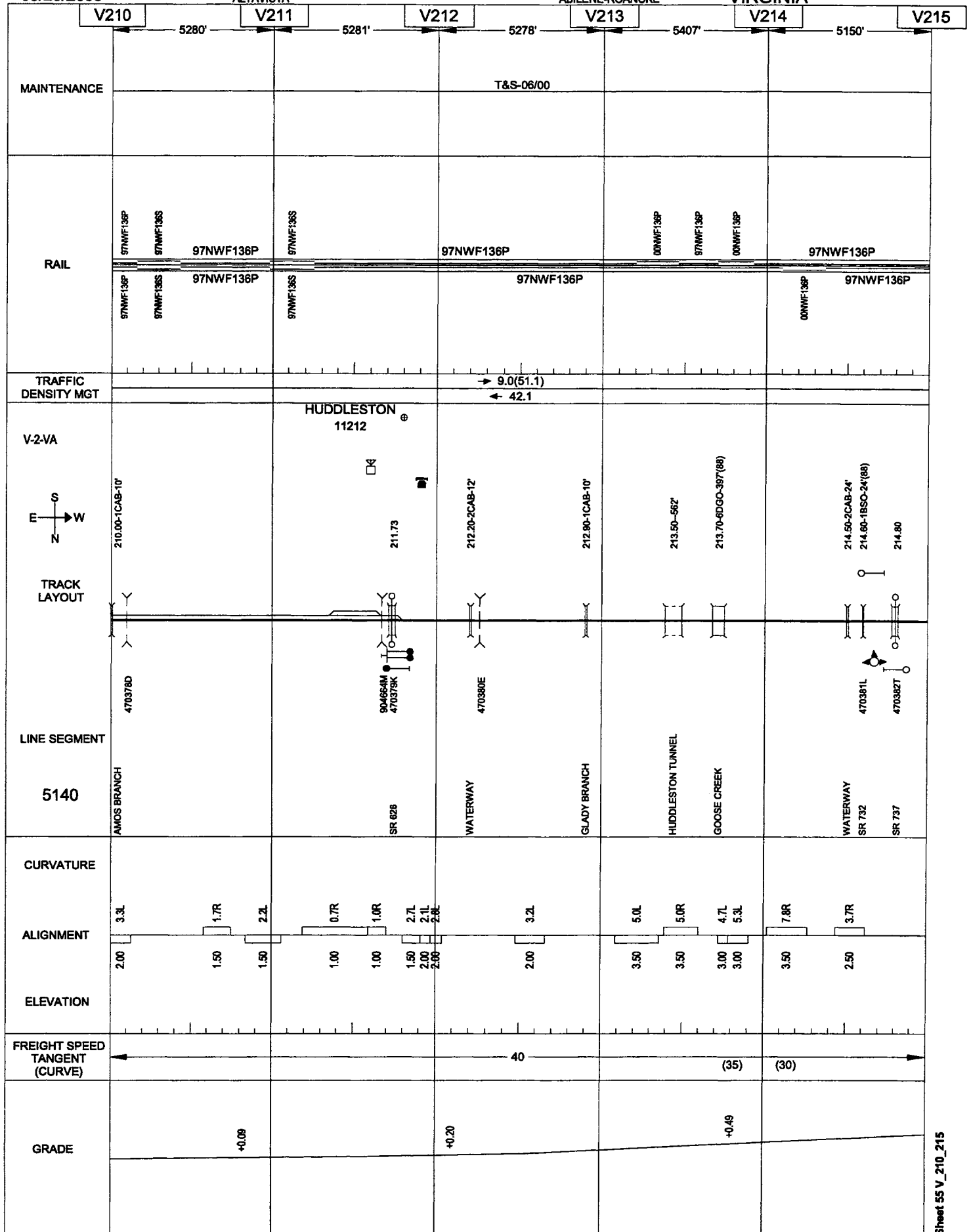


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

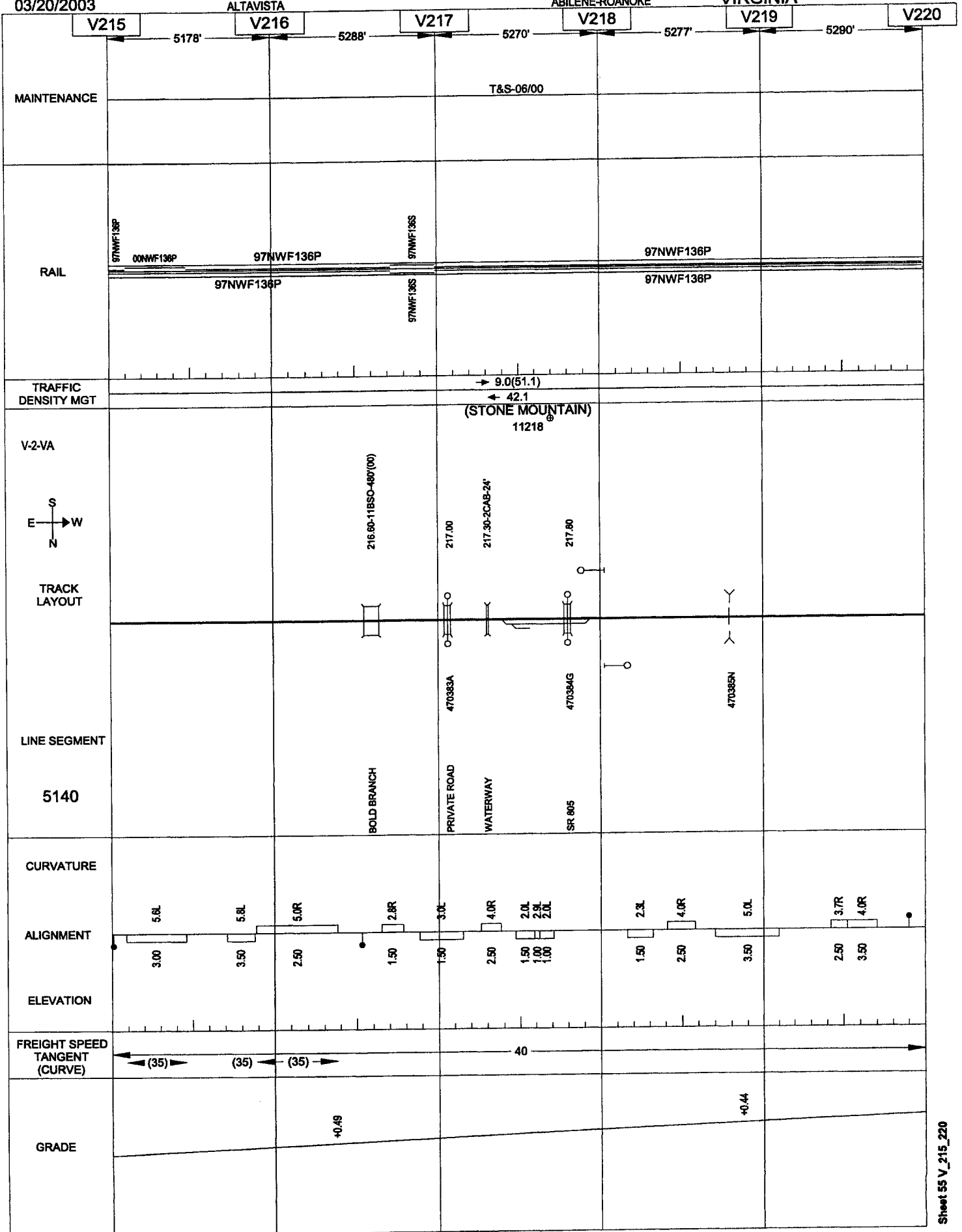


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

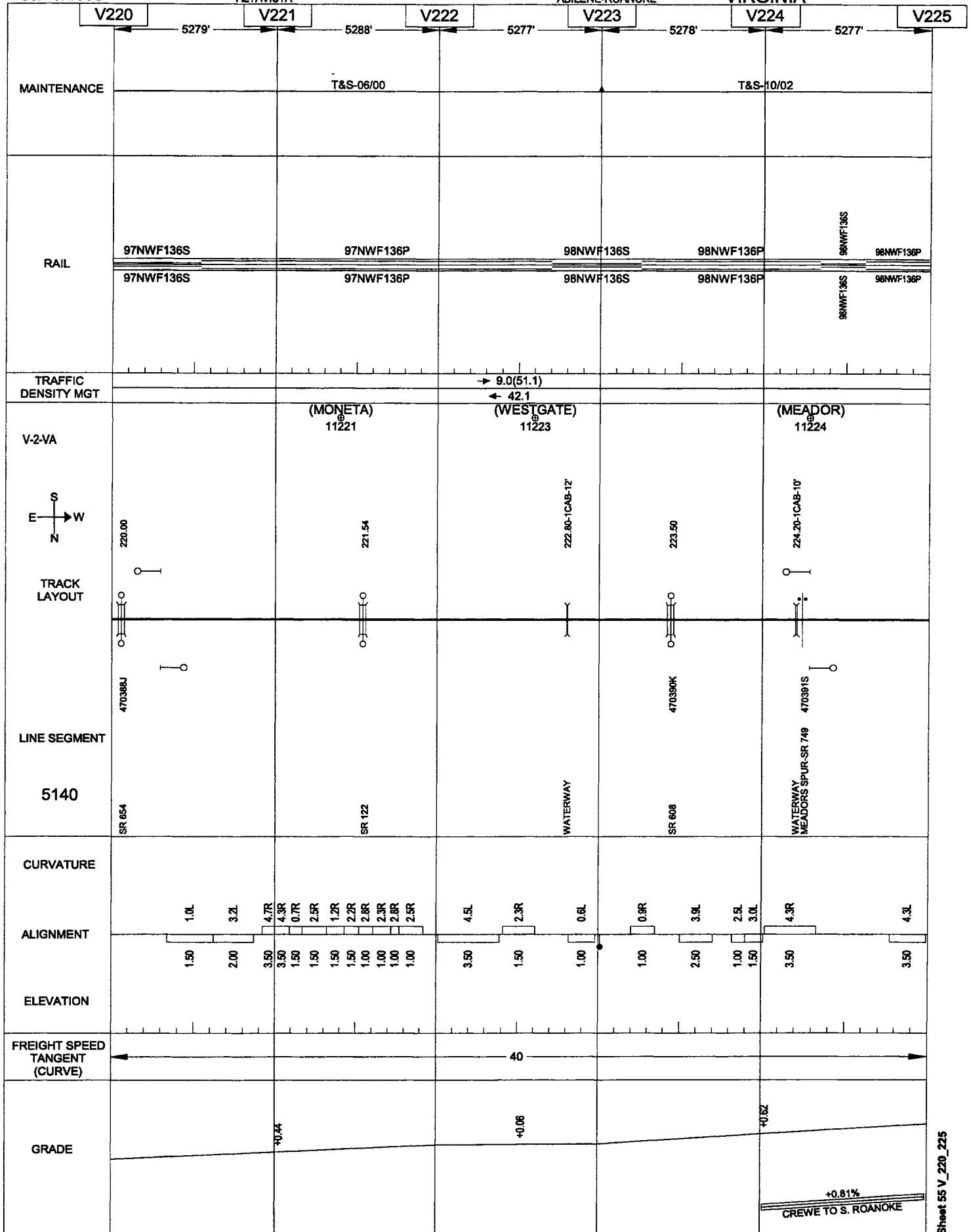


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

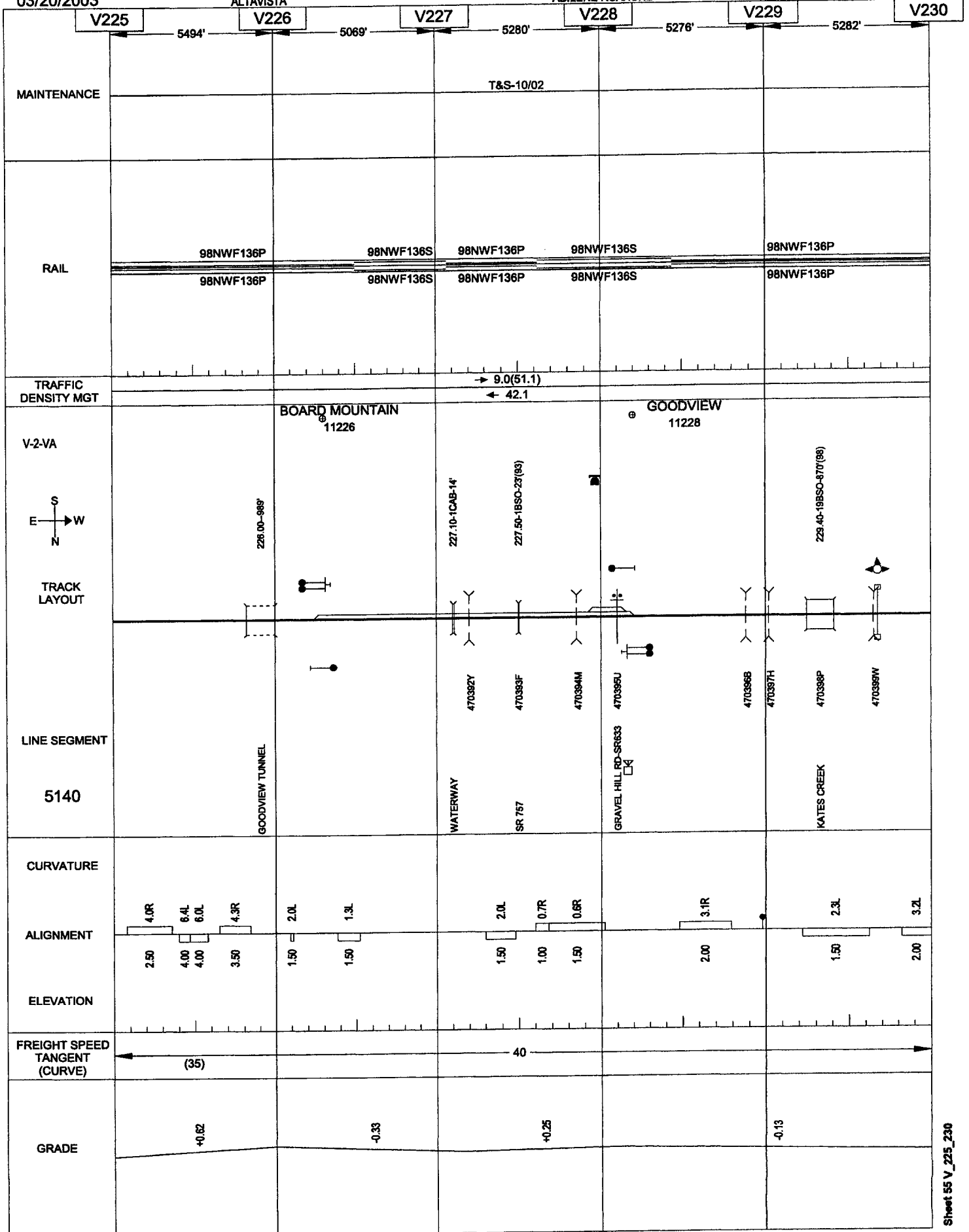


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

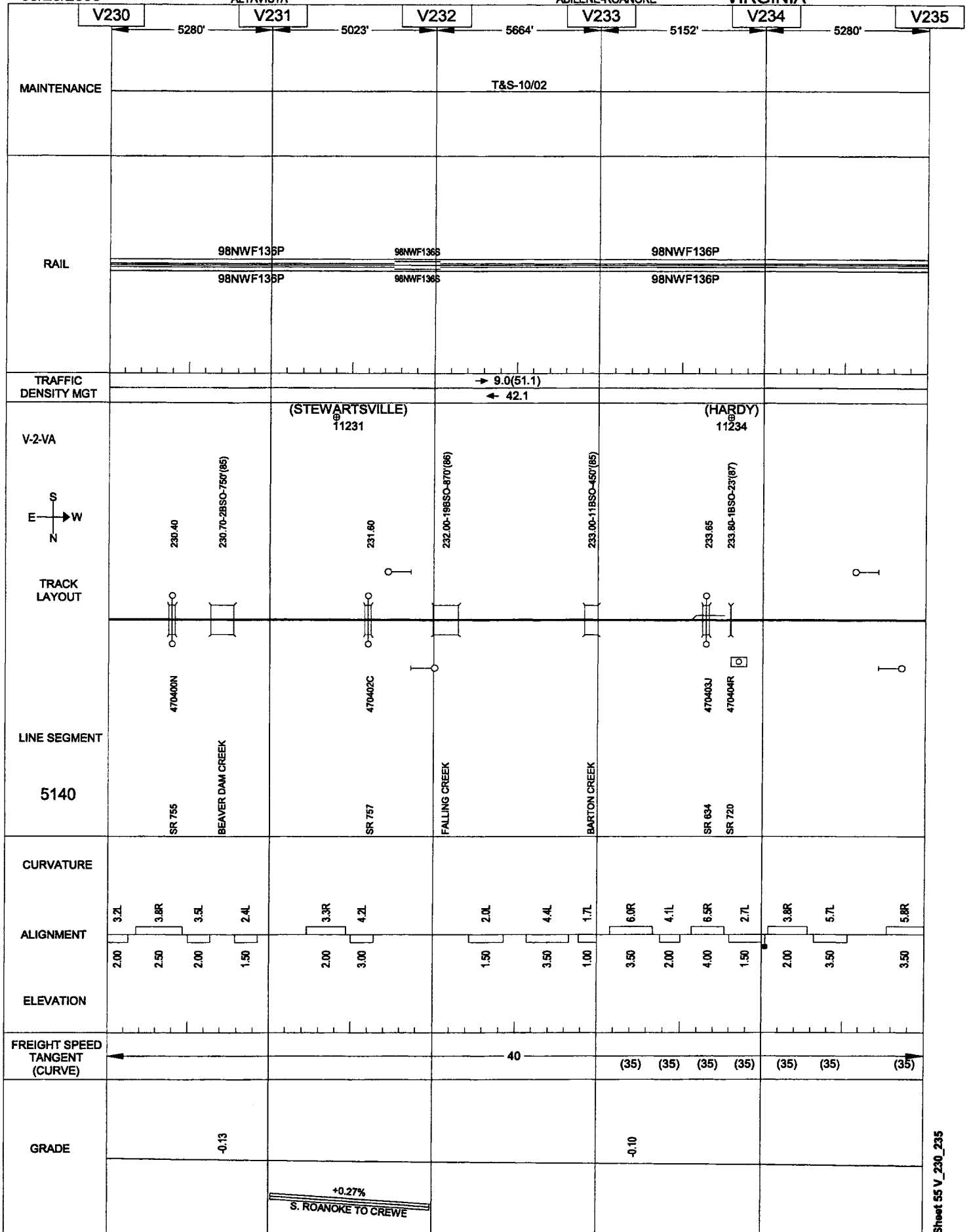


03/20/2003

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ABILENE-ROANOKE

VIRGINIA

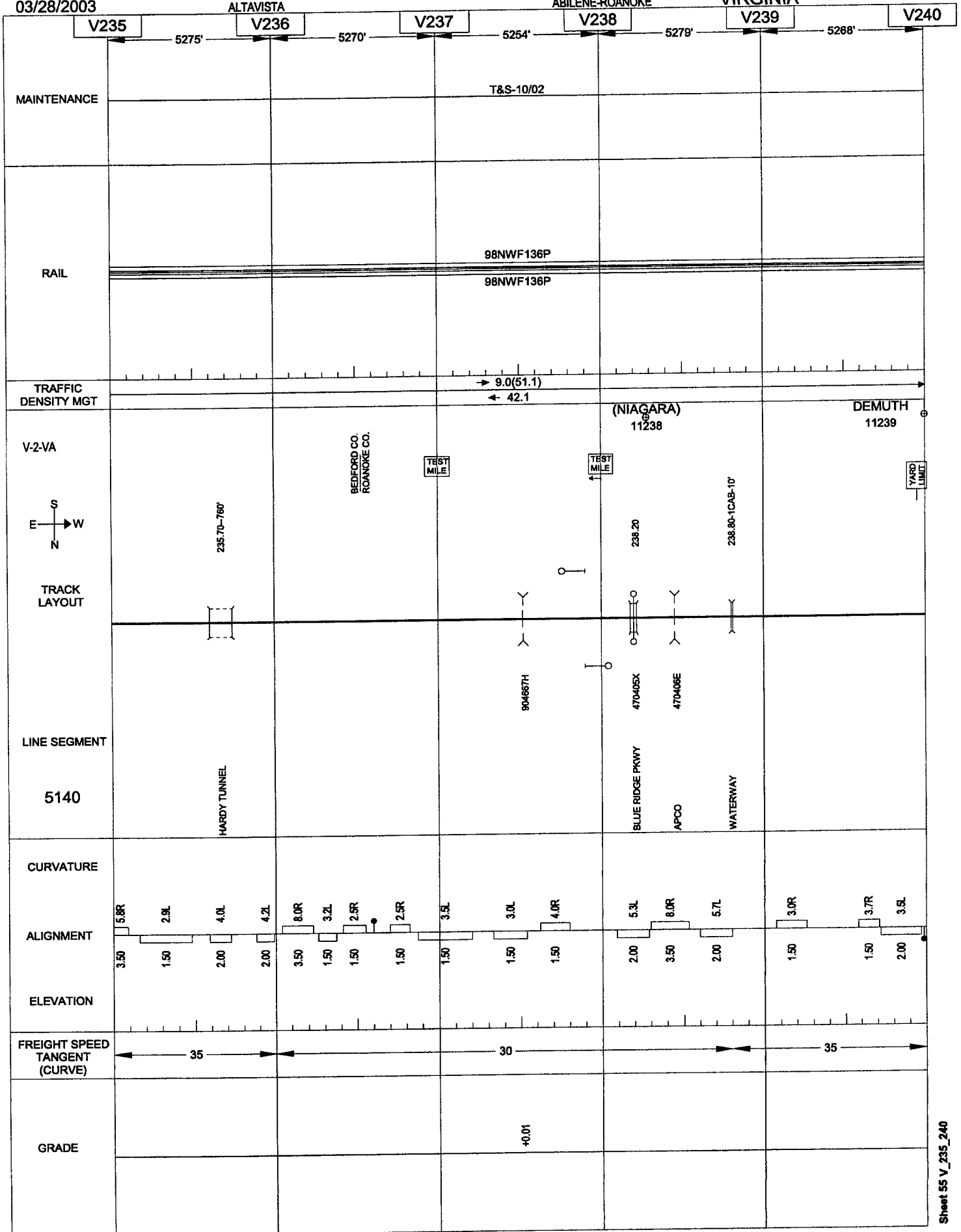


03/28/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

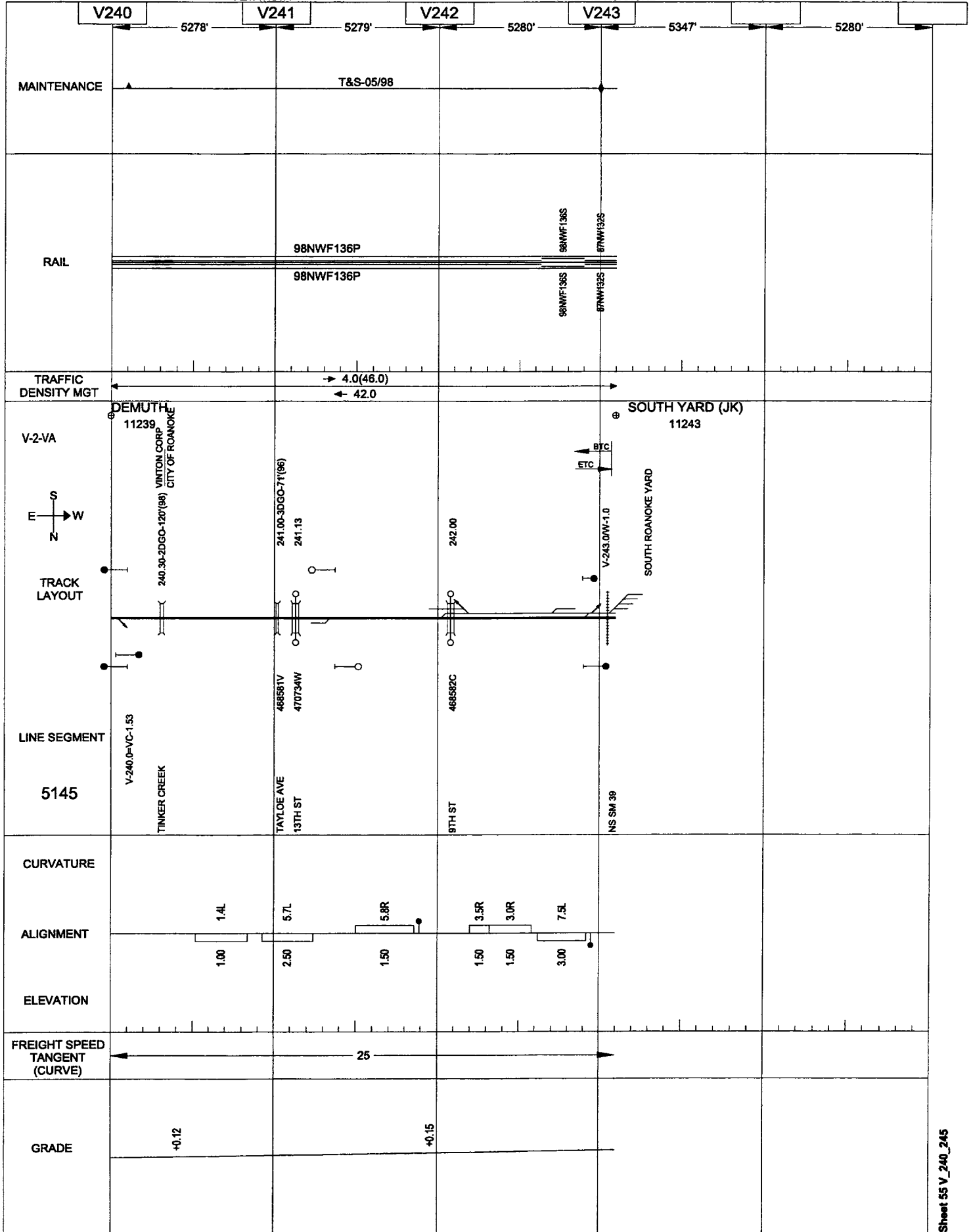


03/20/2003

ALTAVISTA

ABILENE-ROANOKE

VIRGINIA

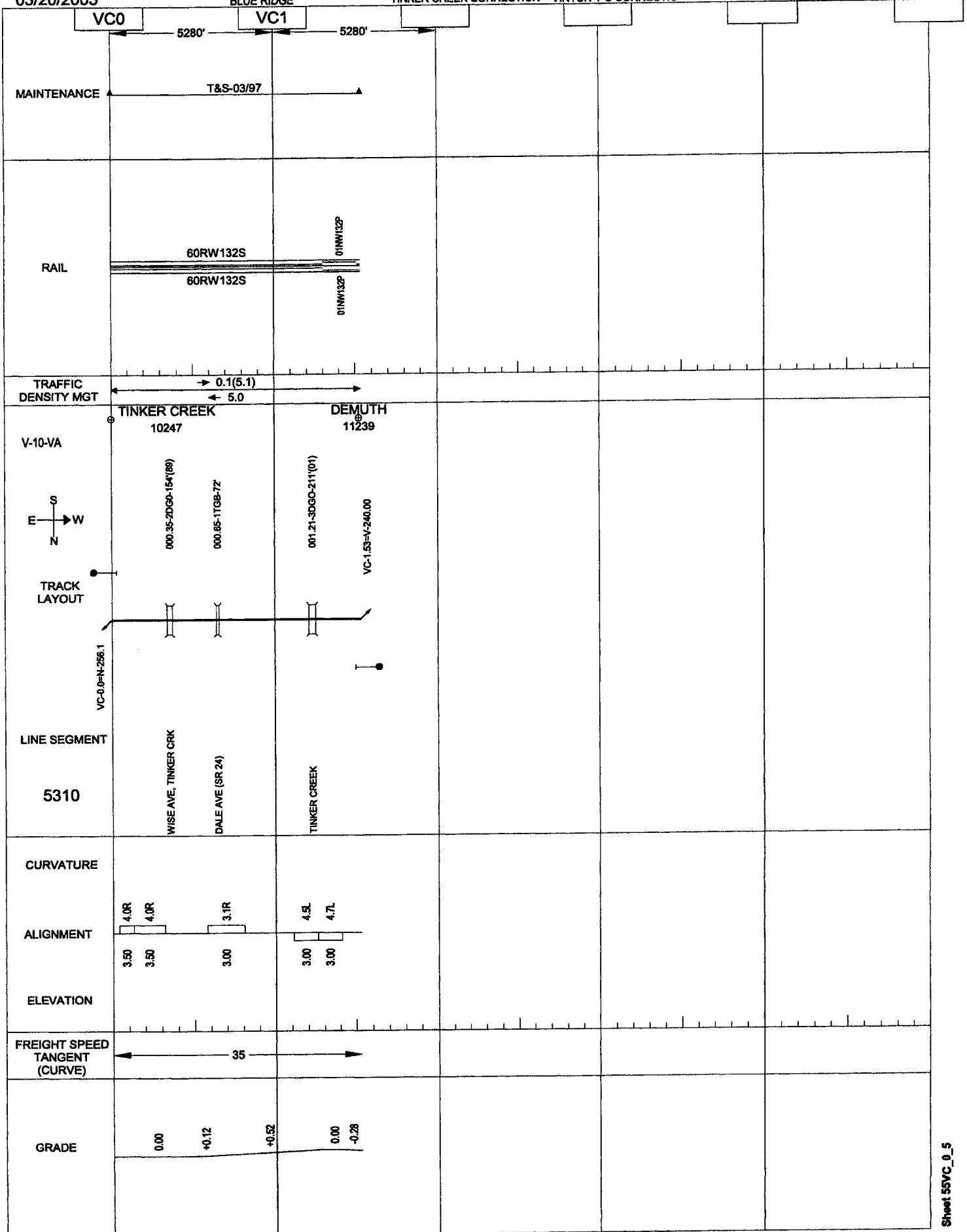


03/20/2003

BLUE RIDGE

TINKER CREEK CONNECTION VINTON-T C CONNECTIO

VIRGINIA



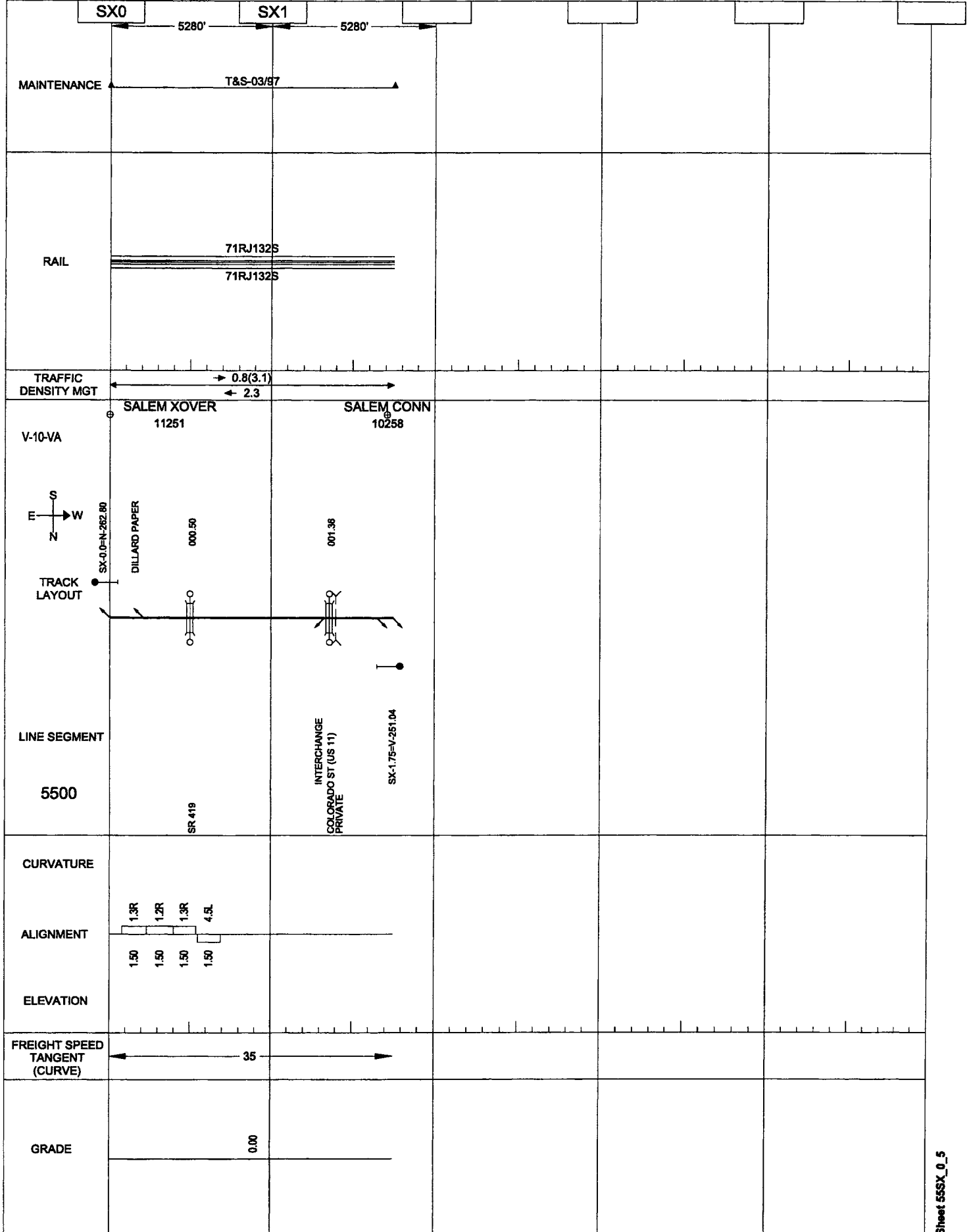
03/20/2003

CHRISTIANSBURG

SALEM CONNECTION

SALEM

VIRGINIA



03/20/2003

CHRISTIANSBURG

GLENVAR CROSSOVER

GLENVAR

VIRGINIA

GX0

5280'

MAINTENANCE

RAIL

**RW132S
**RW132S

TRAFFIC DENSITY MGT

3.3(3.4)
0.1

VN
11252

V-10-VA

S
E → W
N

TRACK
LAYOUT

GX-0.28=V-253.32

GX-0.0=N-287.28

LINE SEGMENT

5510

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

35

GRADE

0.00

03/20/2003

WHITETHORNE

ROANOKE-NARROWS

VIRGINIA

V244

V245

5278'

5279'

5280'

5347'

5280'

MAINTENANCE

T&S-05/96
S-01/98

RAIL

80NW100S

80NW100S

80W100S

88RWF132S

88RWF132S

80W100S

TRAFFIC
DENSITY MGT

→ 4.0 (46.1)

← 42.1

V-2-VA

SOUTH YARD (JK)

11243

WE S ROANOKE YD

11244

S
E → W
NTRACK
LAYOUTBTC
ETC

243.11

243.19

243.55

243.75

244.35

LINE SEGMENT

5480

V-243 1/W-0.9

468587L

468588T

468589A

WALNUT ST
JEFFERSON STFRANKLIN RD
I-581

MAIN ST

CURVATURE

ALIGNMENT

ELEVATION

4.5R

0.00

3.6R

0.00

3.7R

0.00

3.6R

0.00

3.7R

0.00

5.2R

0.00

6.0L

0.00

1.9L

0.00

5.7L

0.00

FREIGHT SPEED
TANGENT
(CURVE)

15

GRADE

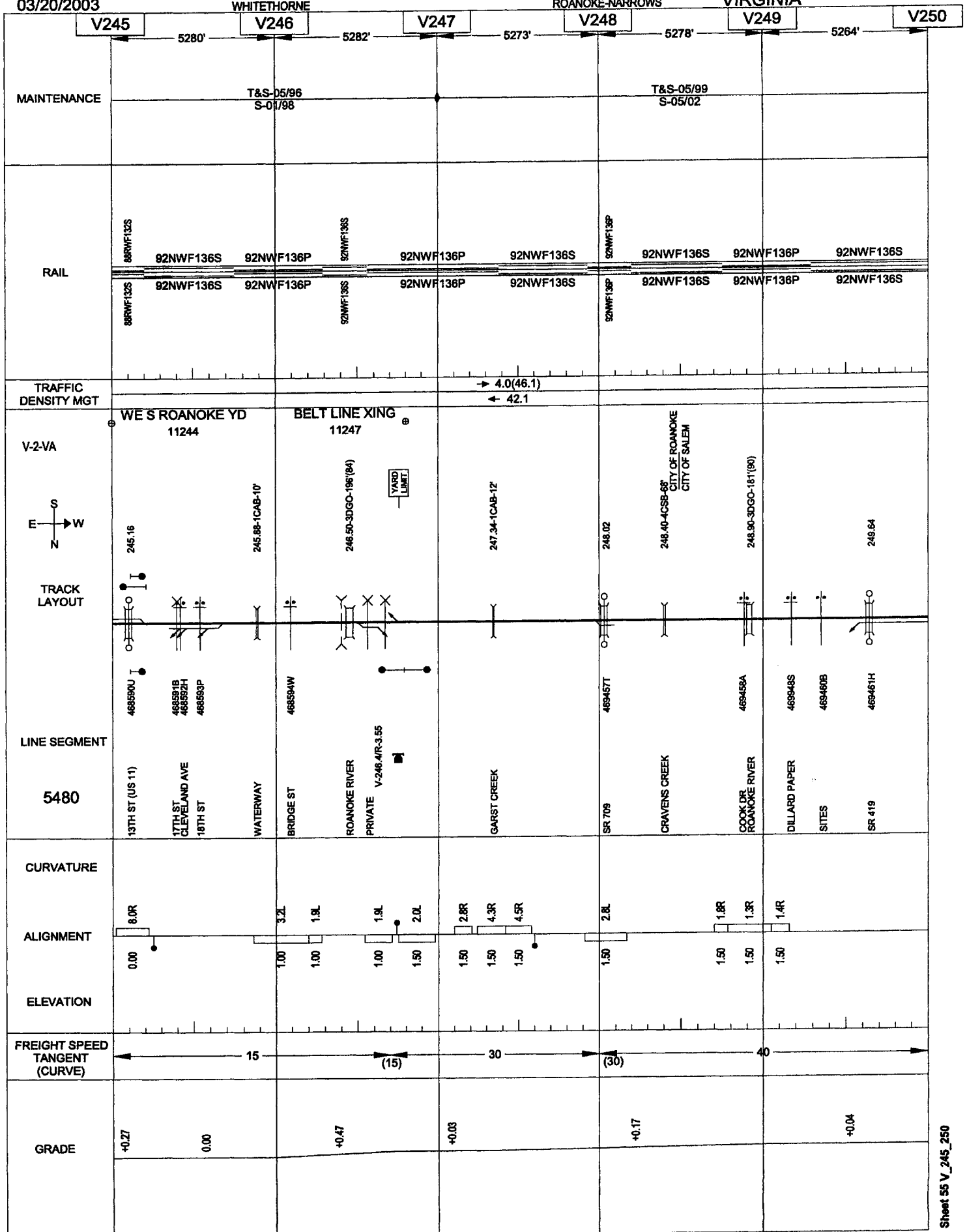
+0.27

03/20/2003

WHITETHORNE

ROANOKE-NARROWS

VIRGINIA

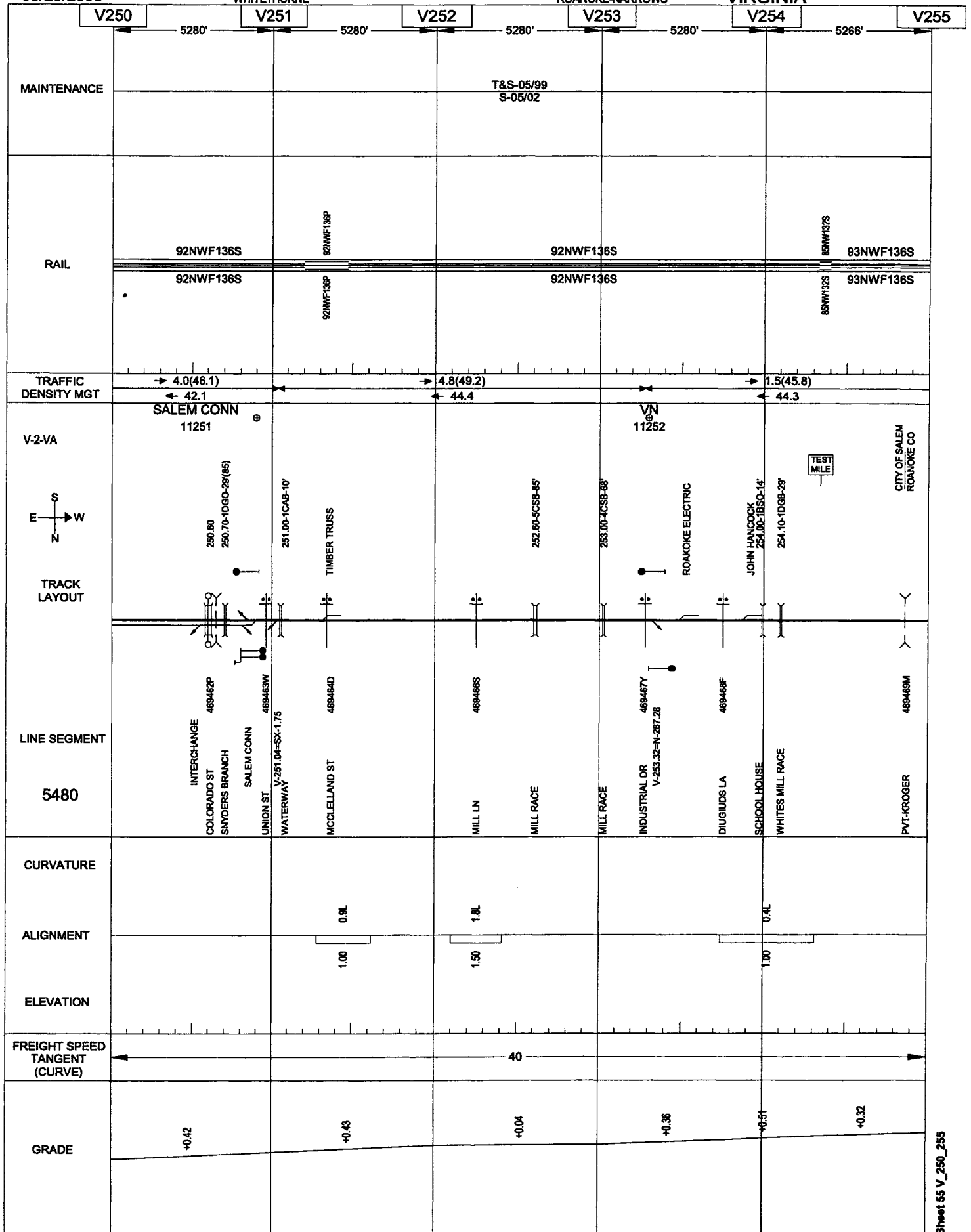


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ROANOKE-NARROWS

VIRGINIA

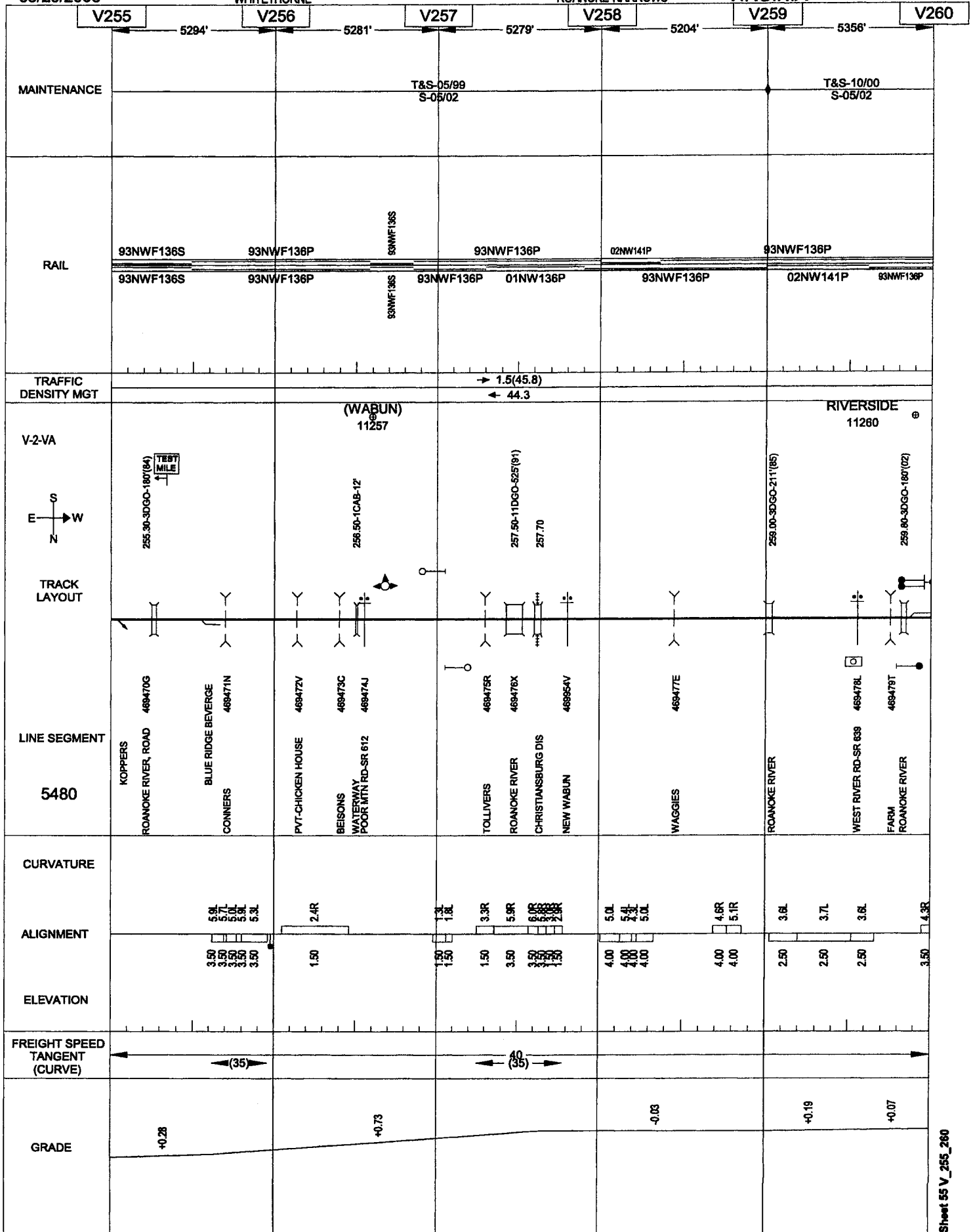


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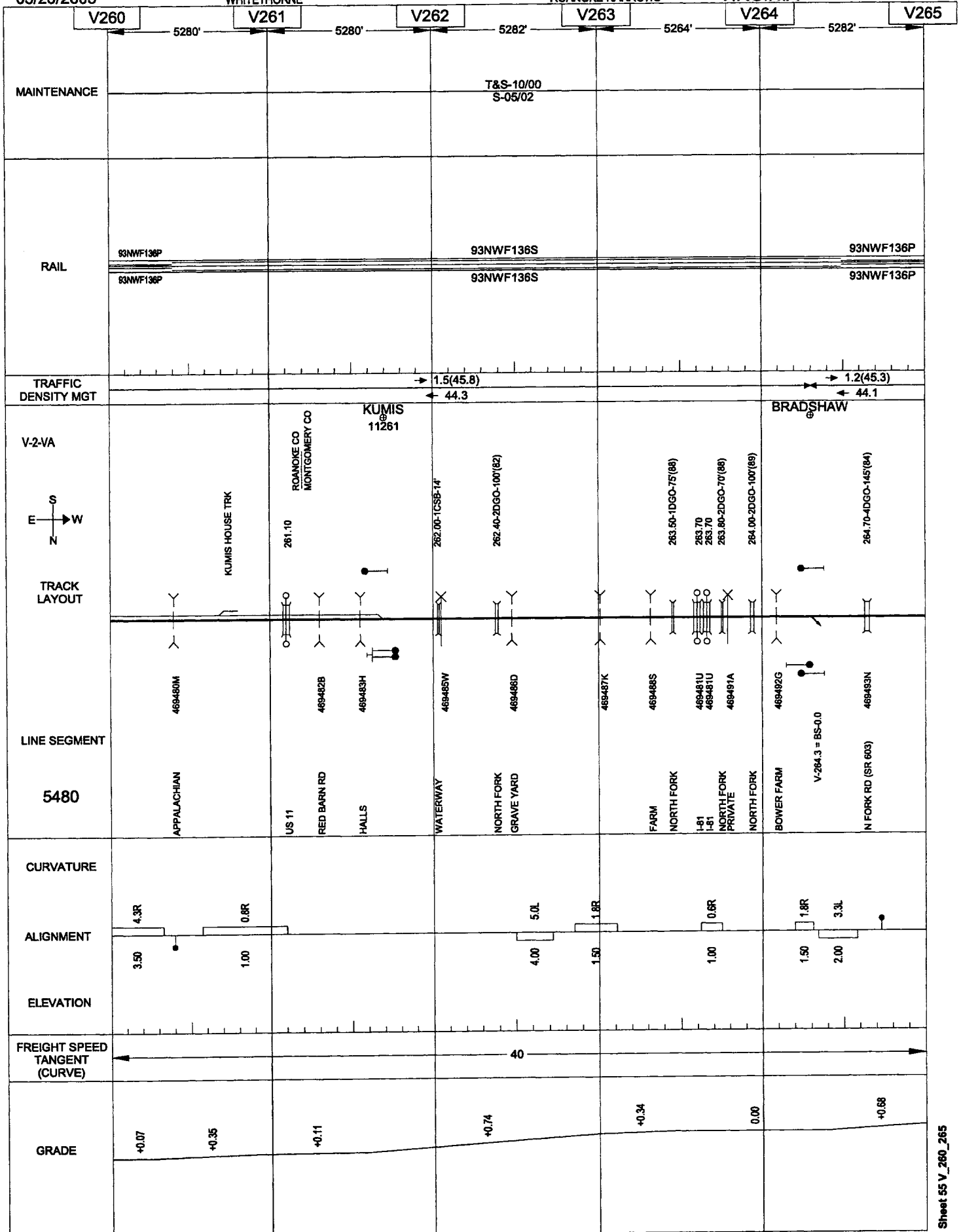


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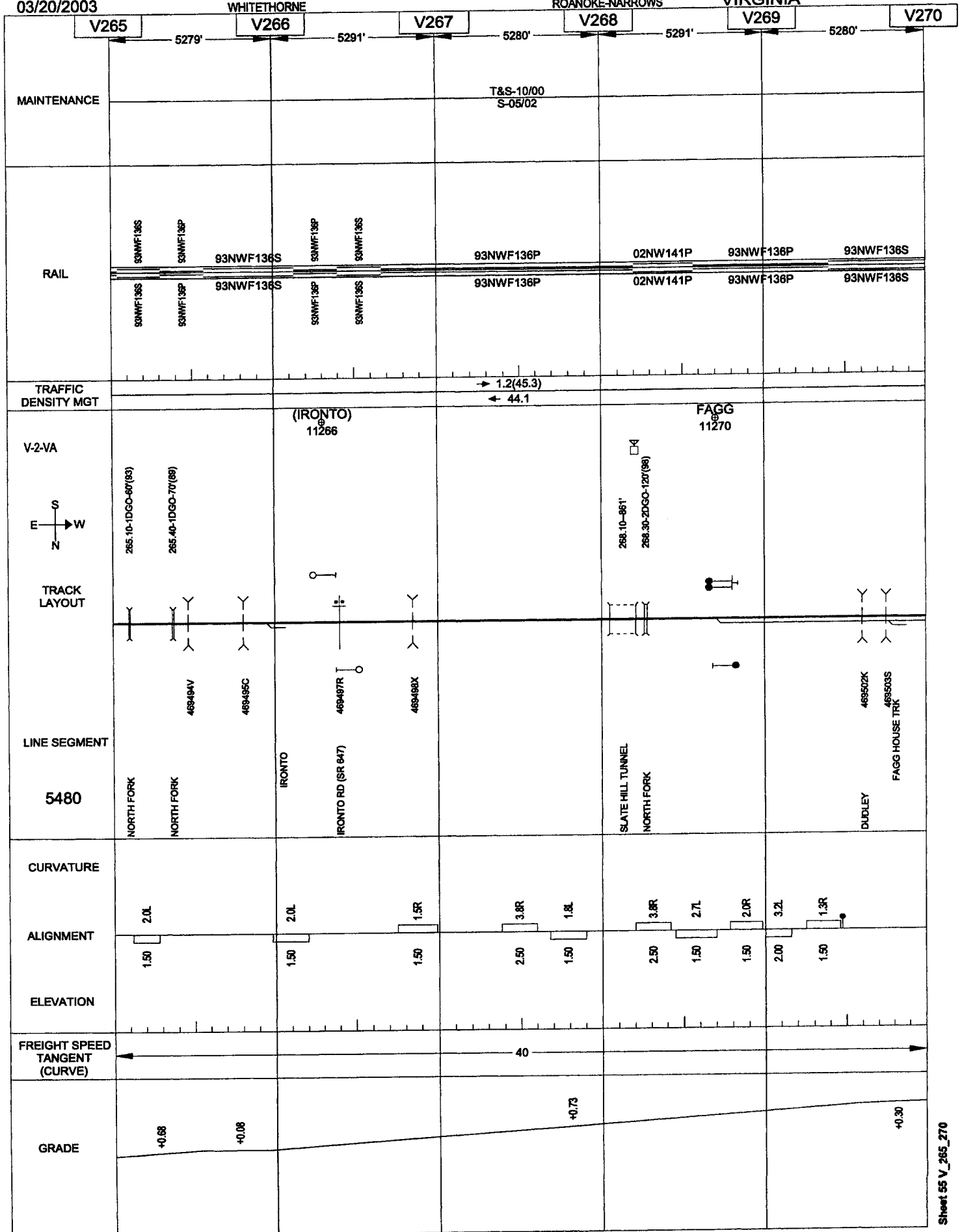


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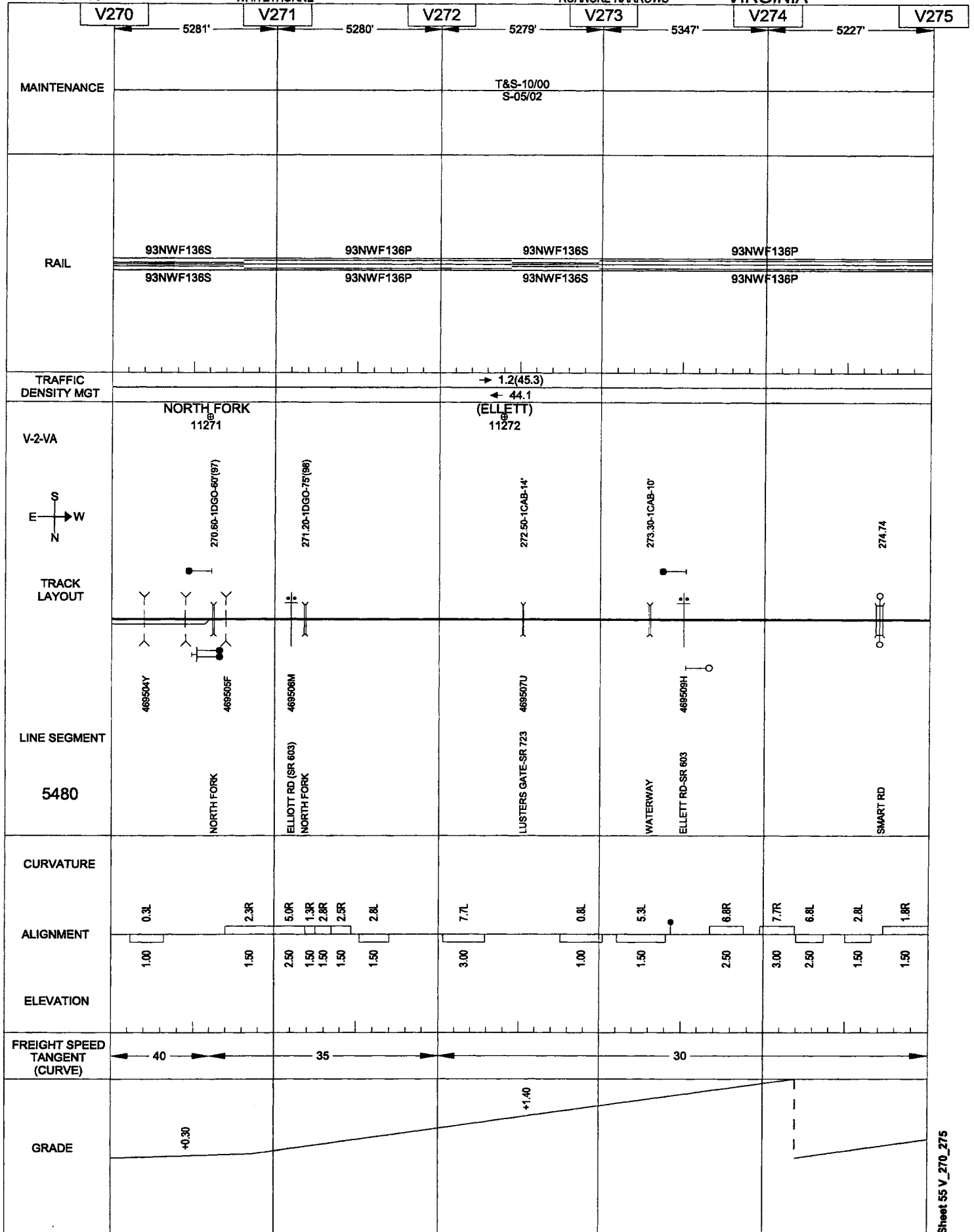


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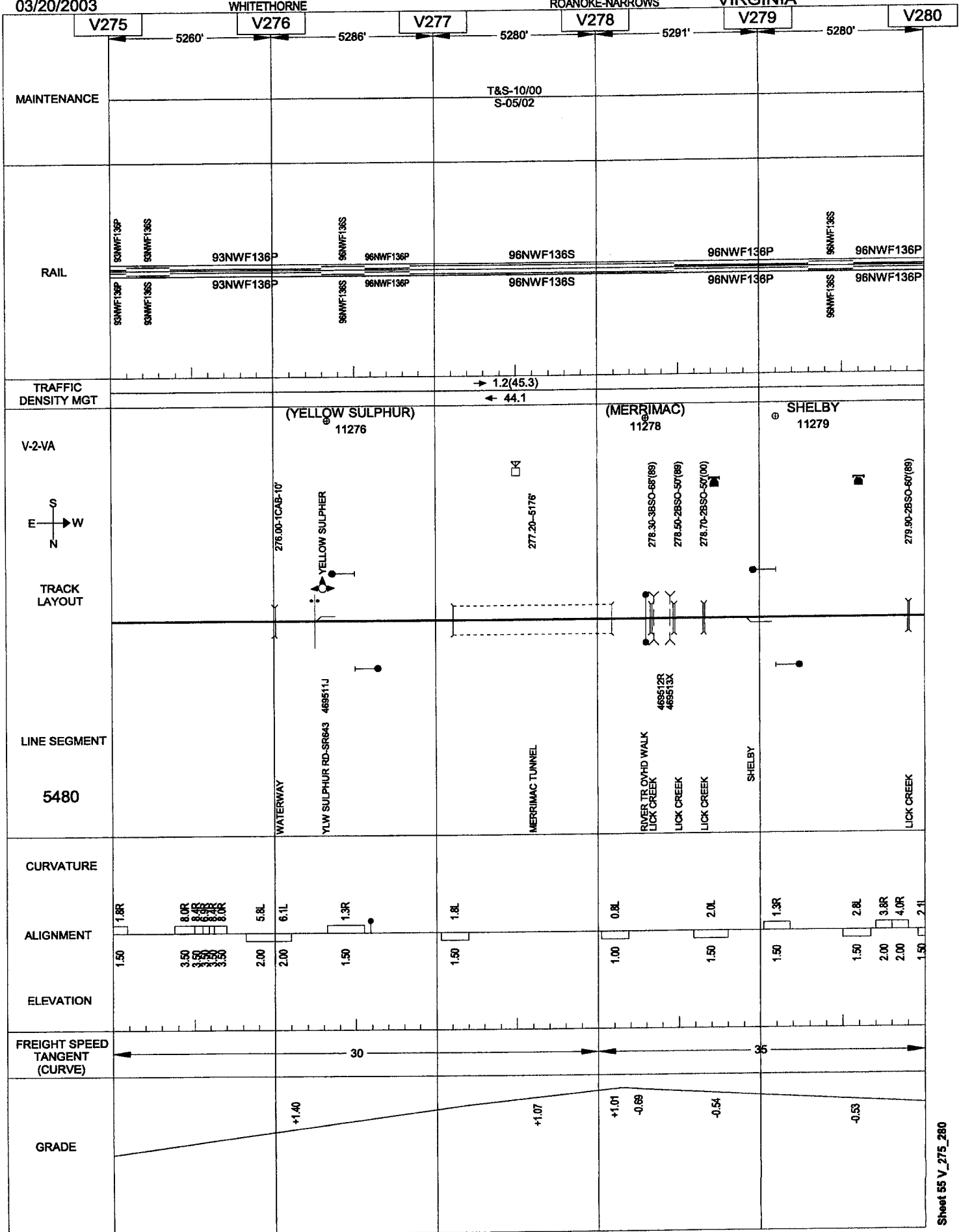


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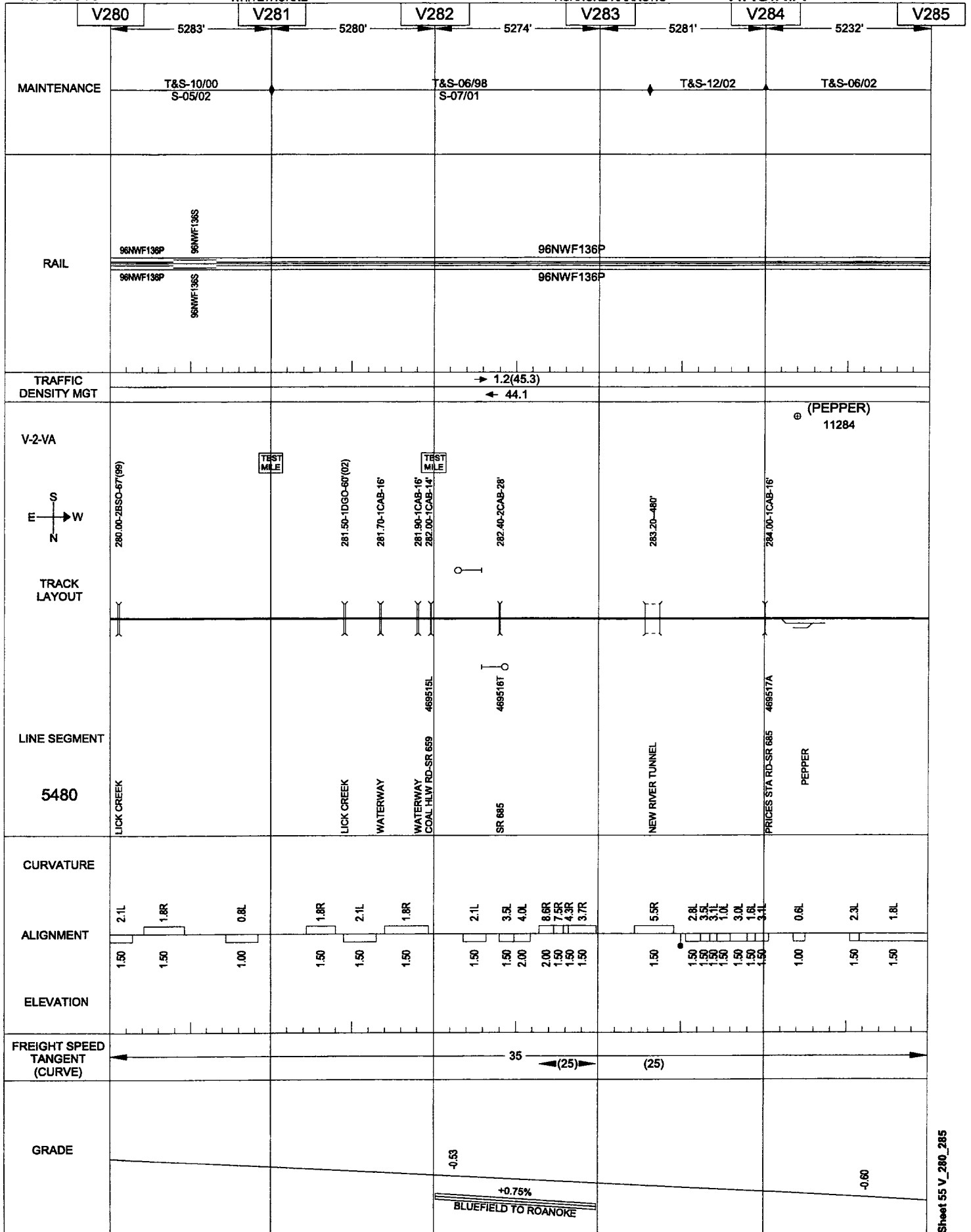


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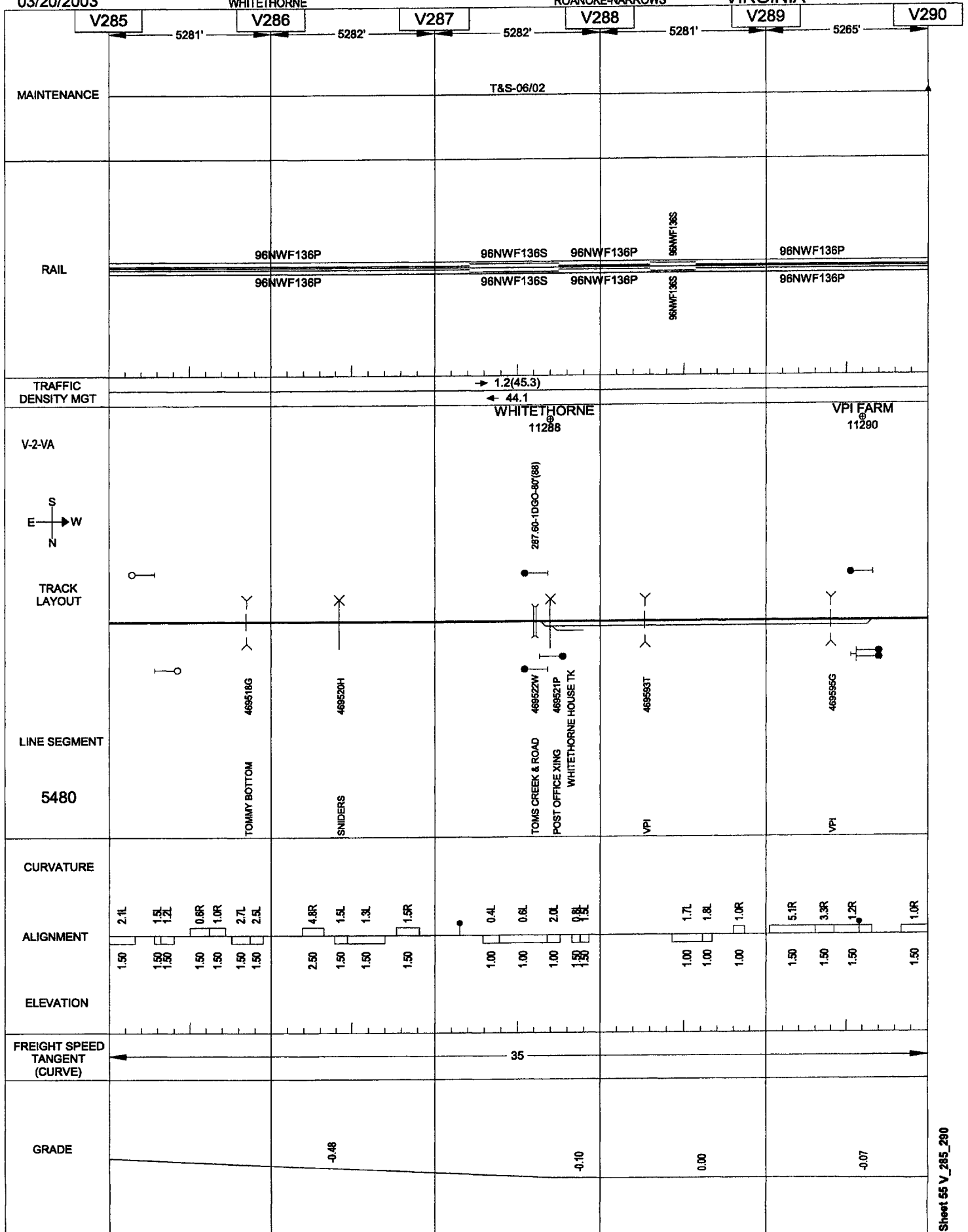


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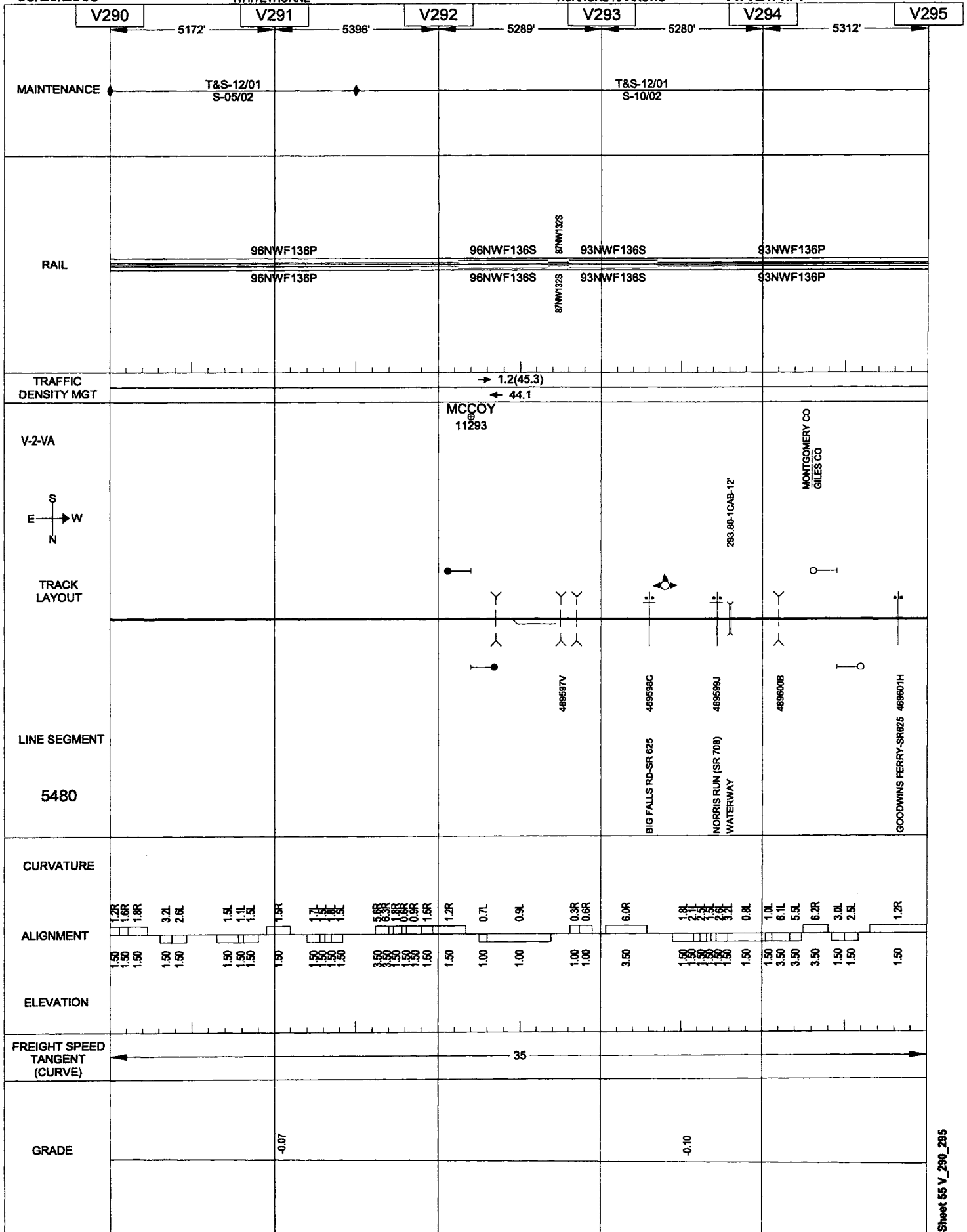


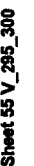
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ROANOKE-NARROWS

VIRGINIA



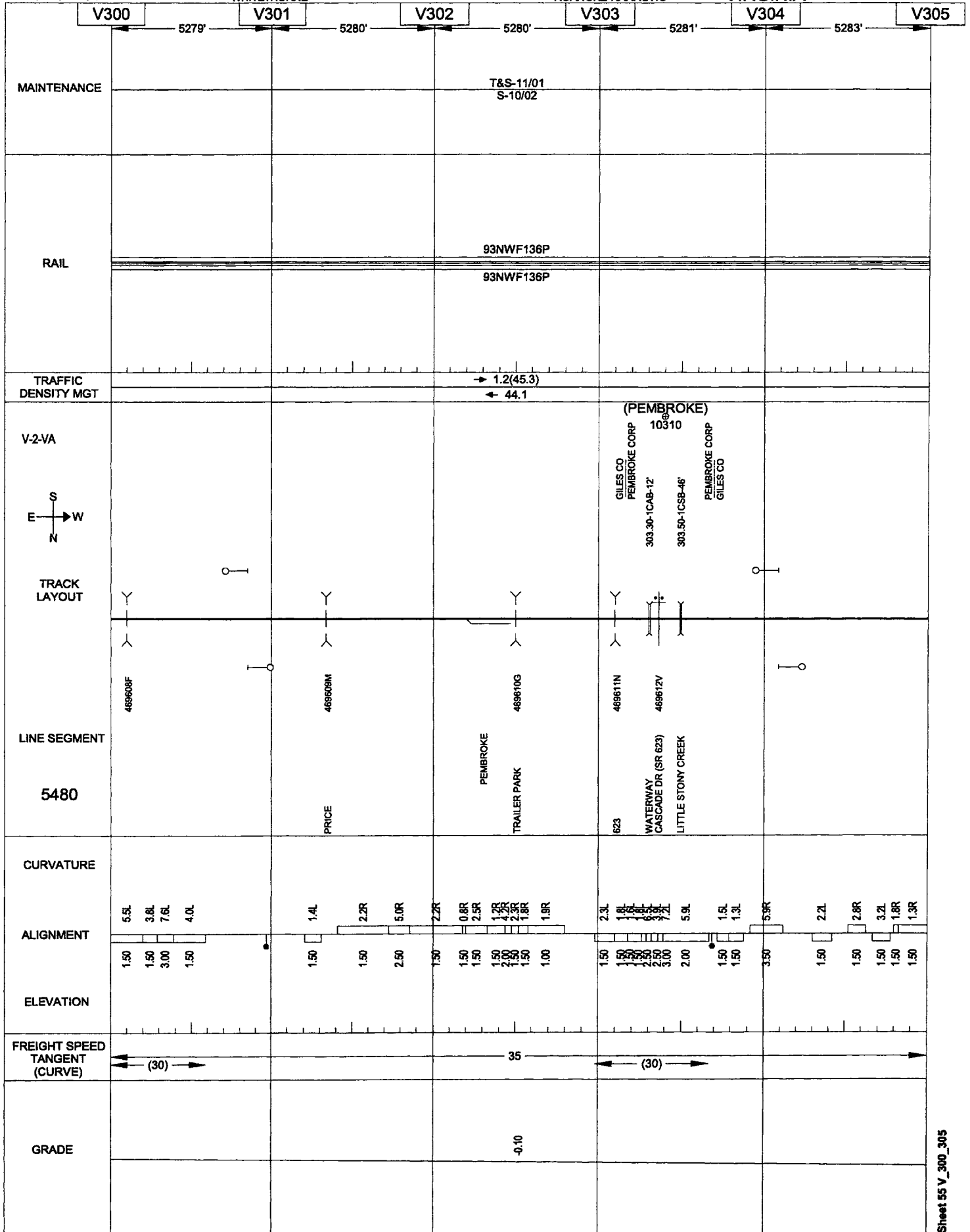


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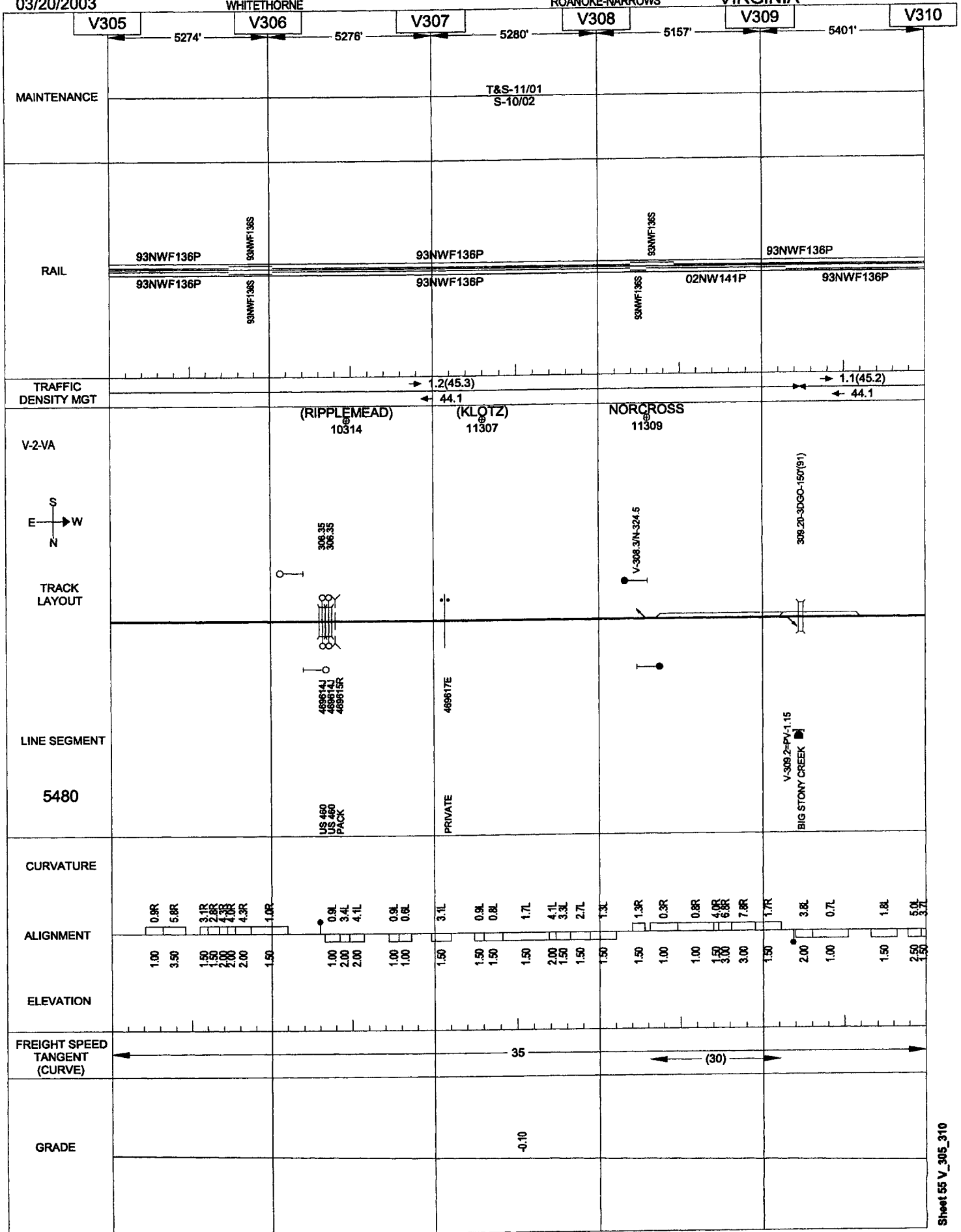


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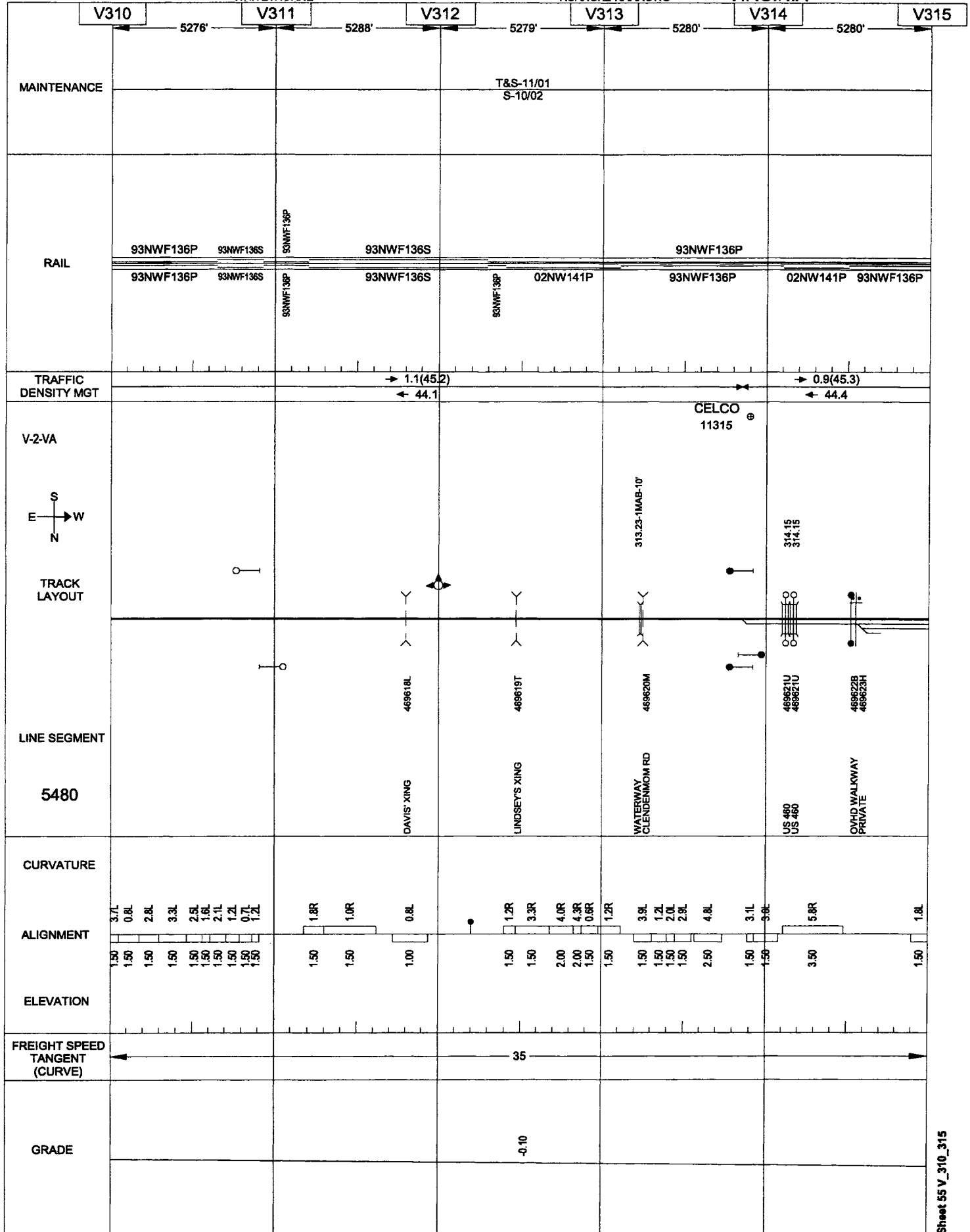


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ROANOKE-NARROWS

VIRGINIA

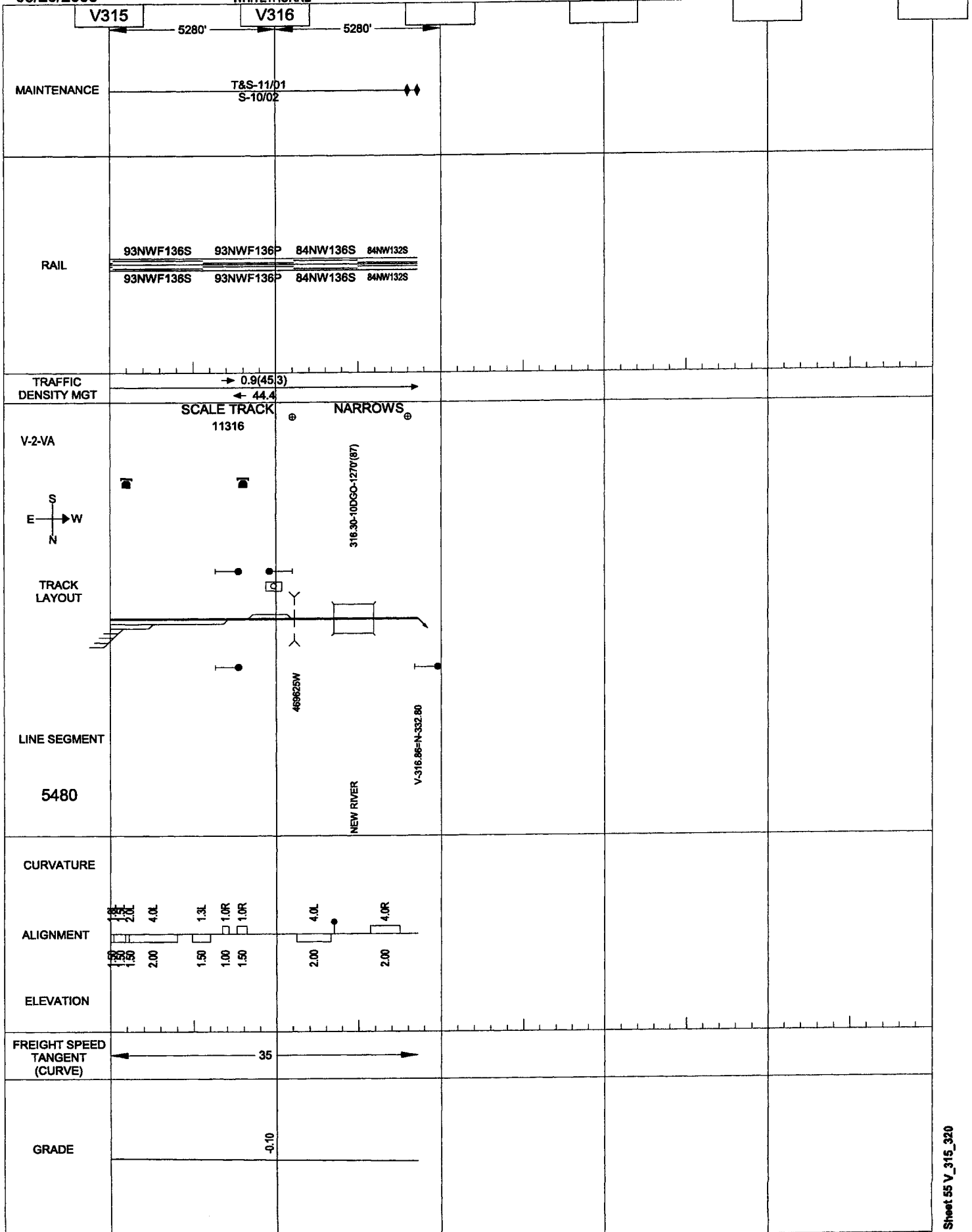


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ROANOKE-NARROWS

VIRGINIA



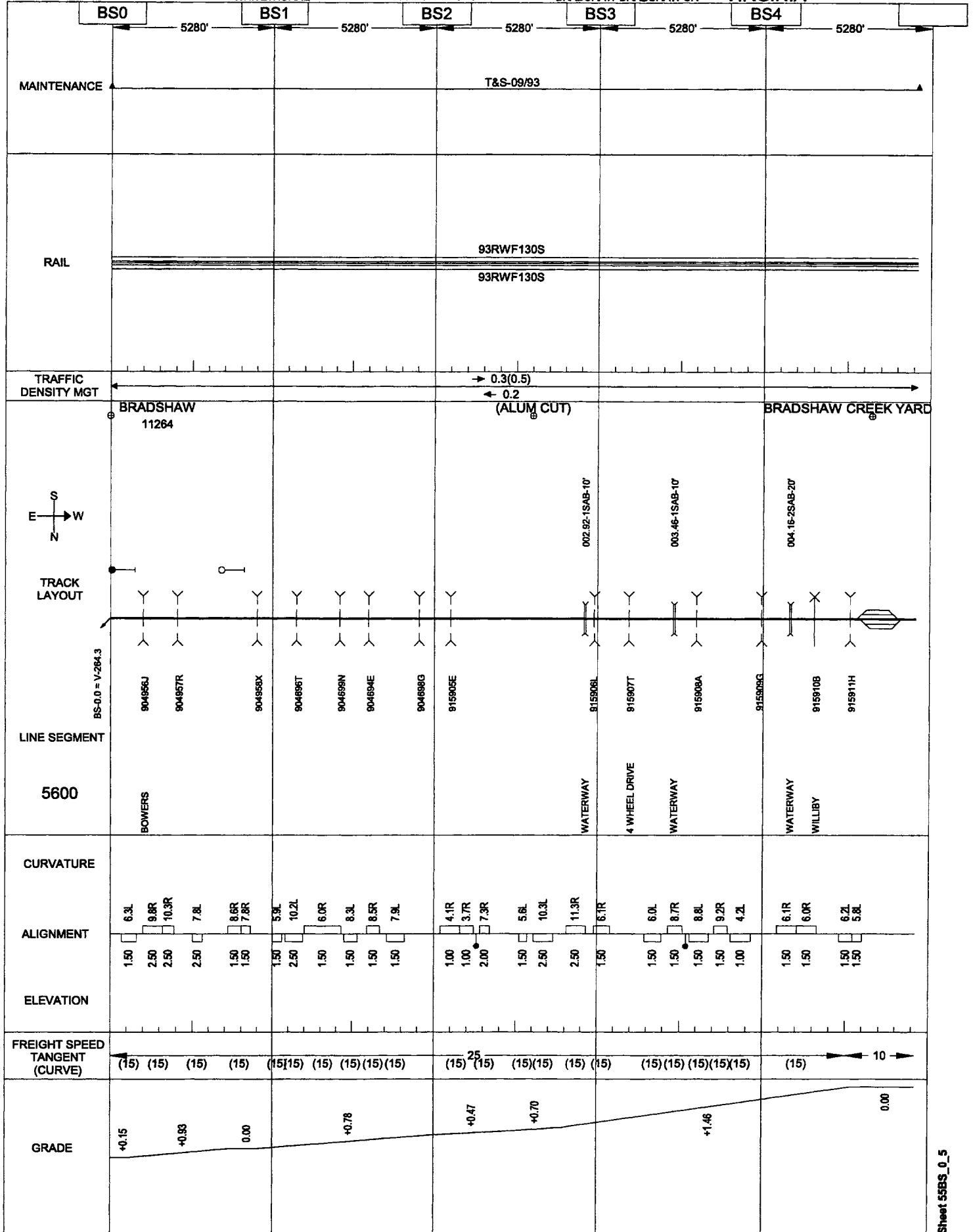
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WHITETHORNE

BRADSHAW SPUR

BRADSHAW-BRADSHAW CR

VIRGINIA



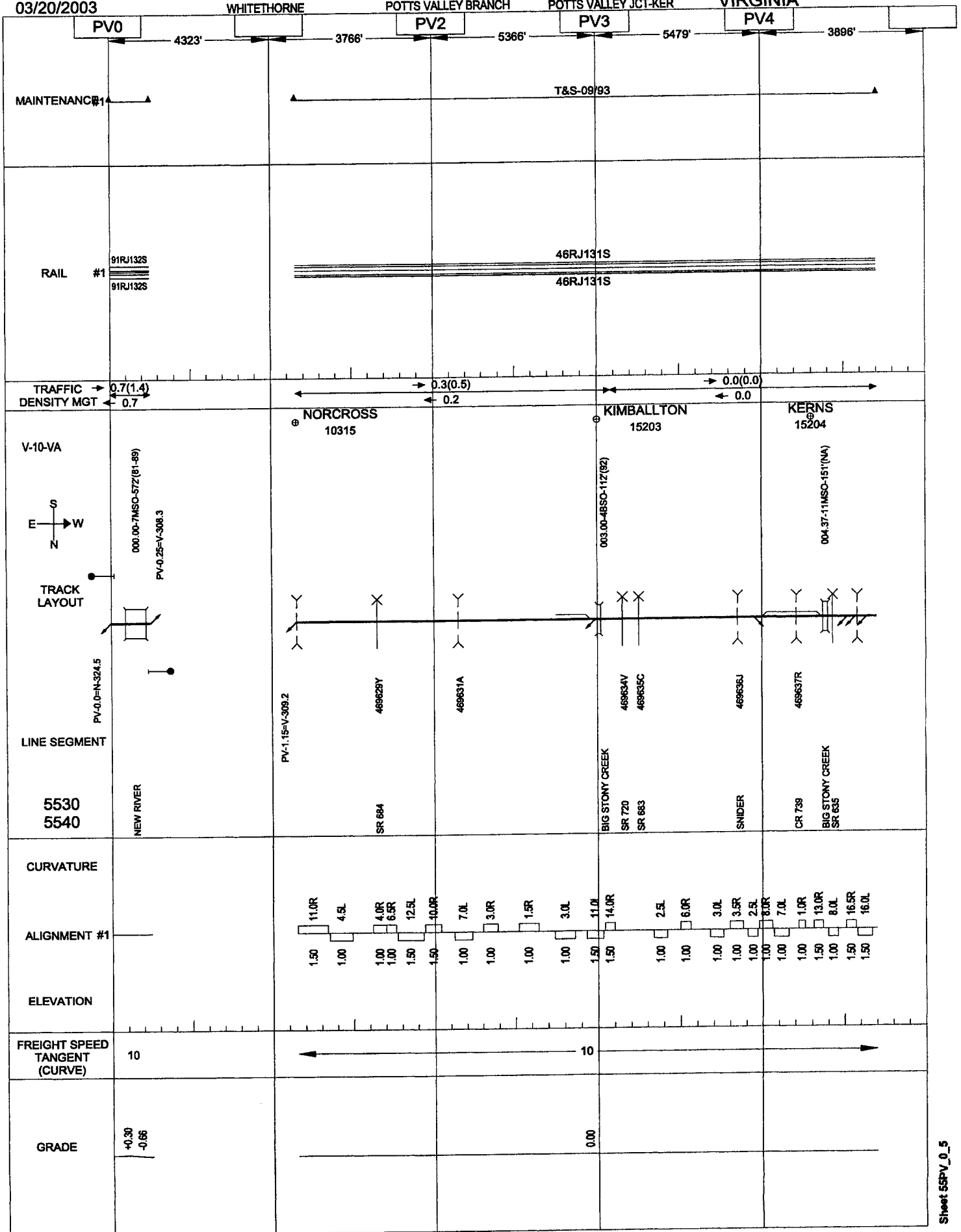
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WHITETHORNE

POTTS VALLEY BRANCH

POTTS VALLEY JCT-KER

VIRGINIA



Sheet 55NB_295_300

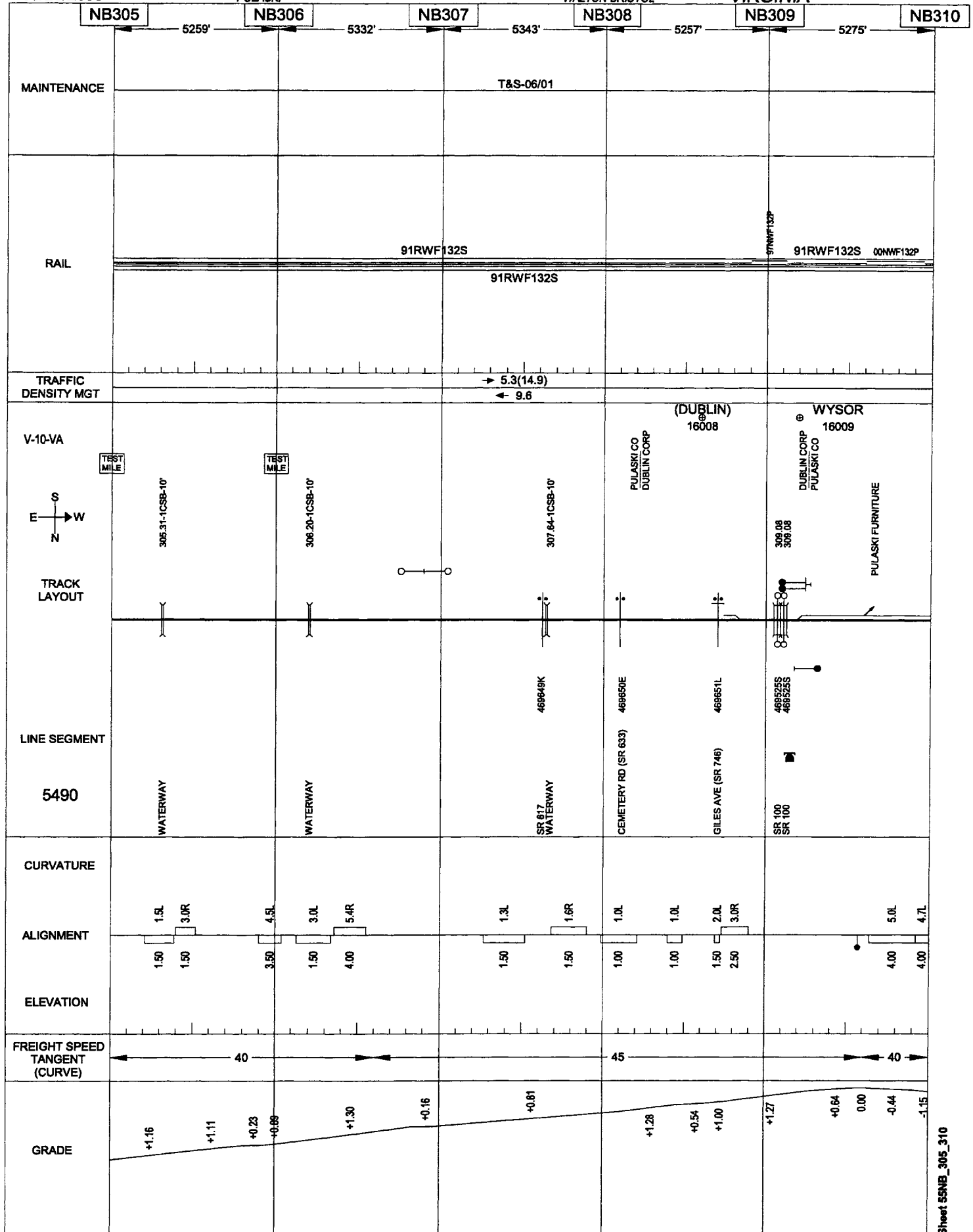
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03/20/2003

PULASKI

WALTON-BRISTOL

VIRGINIA

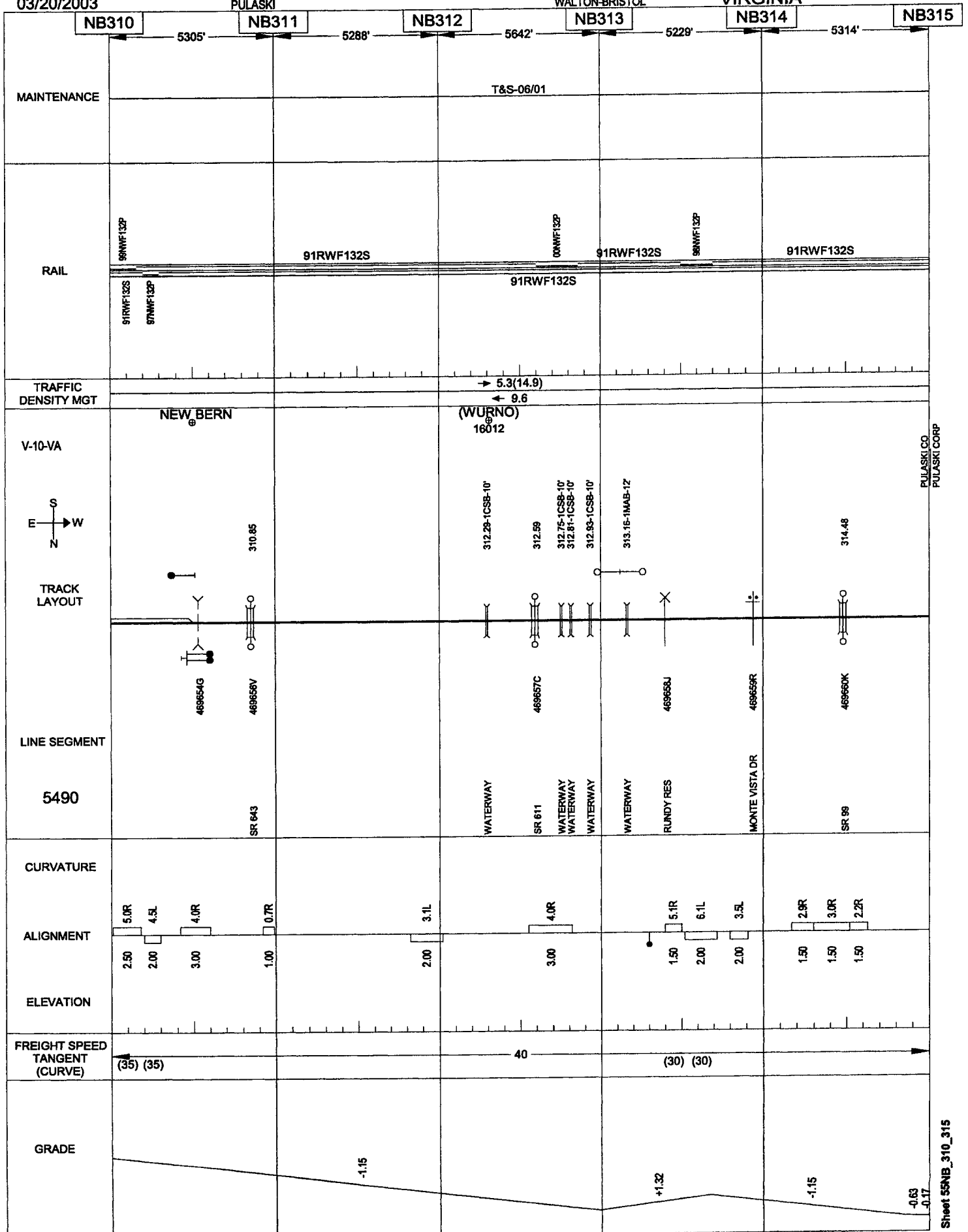


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PULASKI

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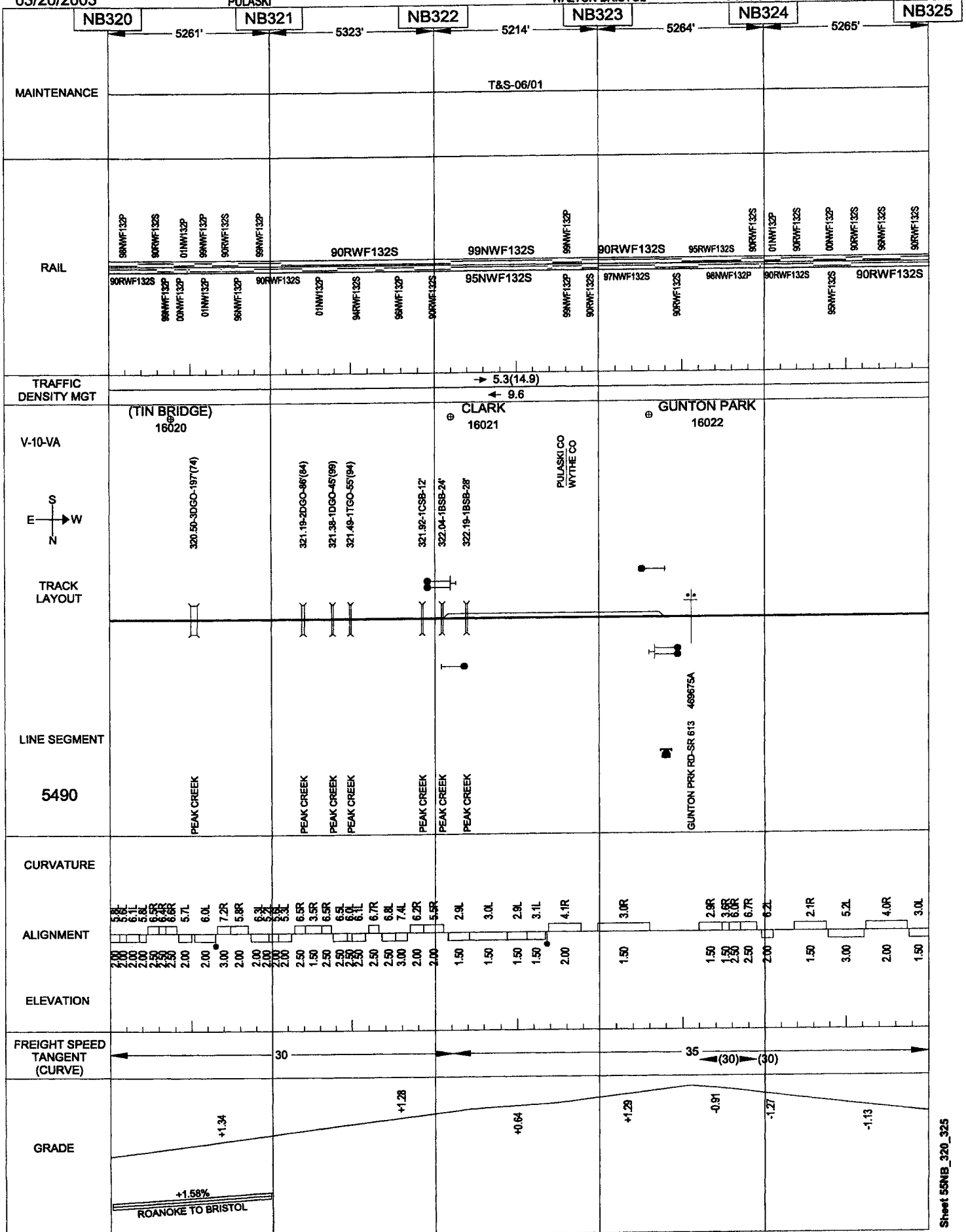
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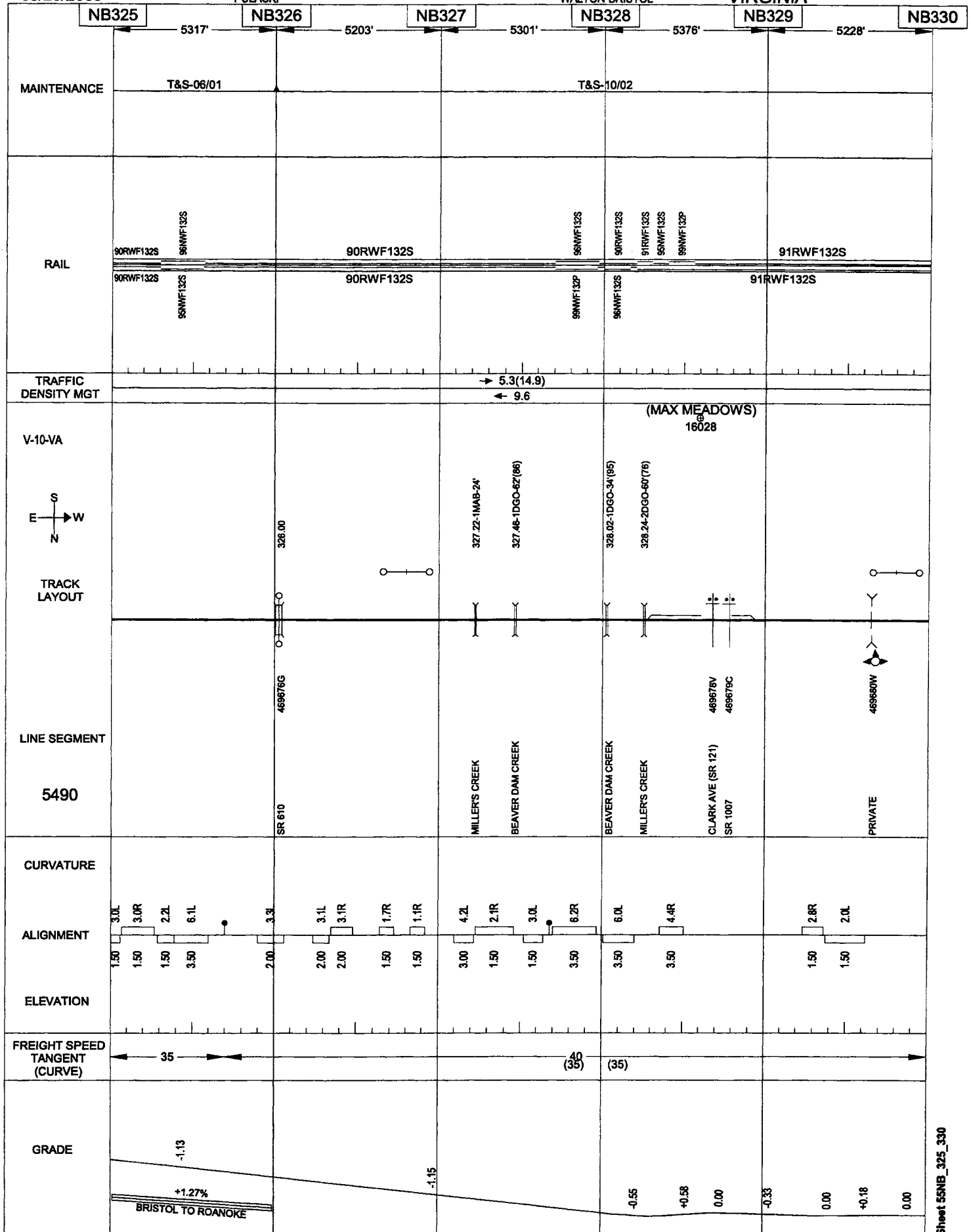


03/20/2003

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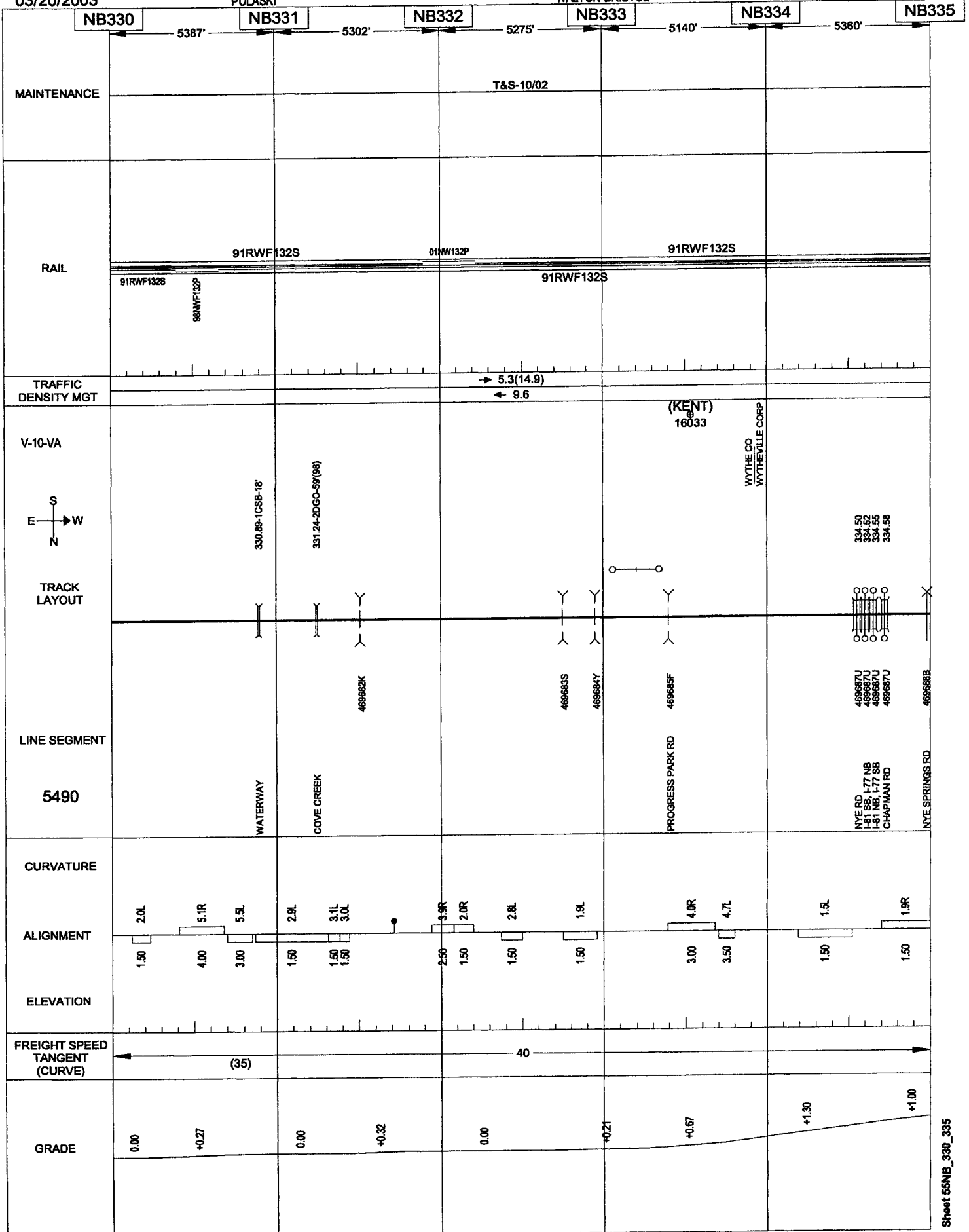


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VIRGINIA

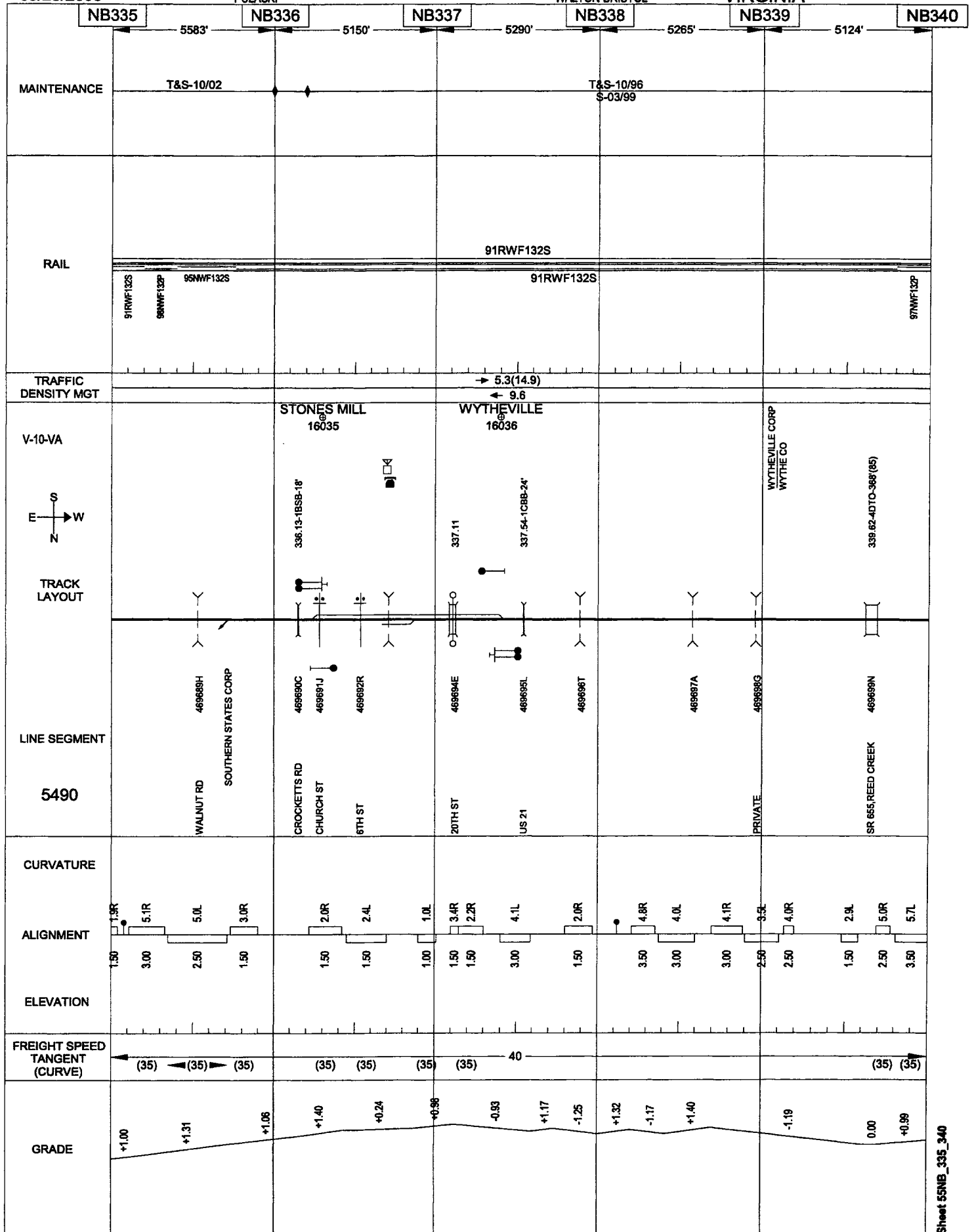


03/20/2003

PULASKI

WALTON-BRISTOL

VIRGINIA



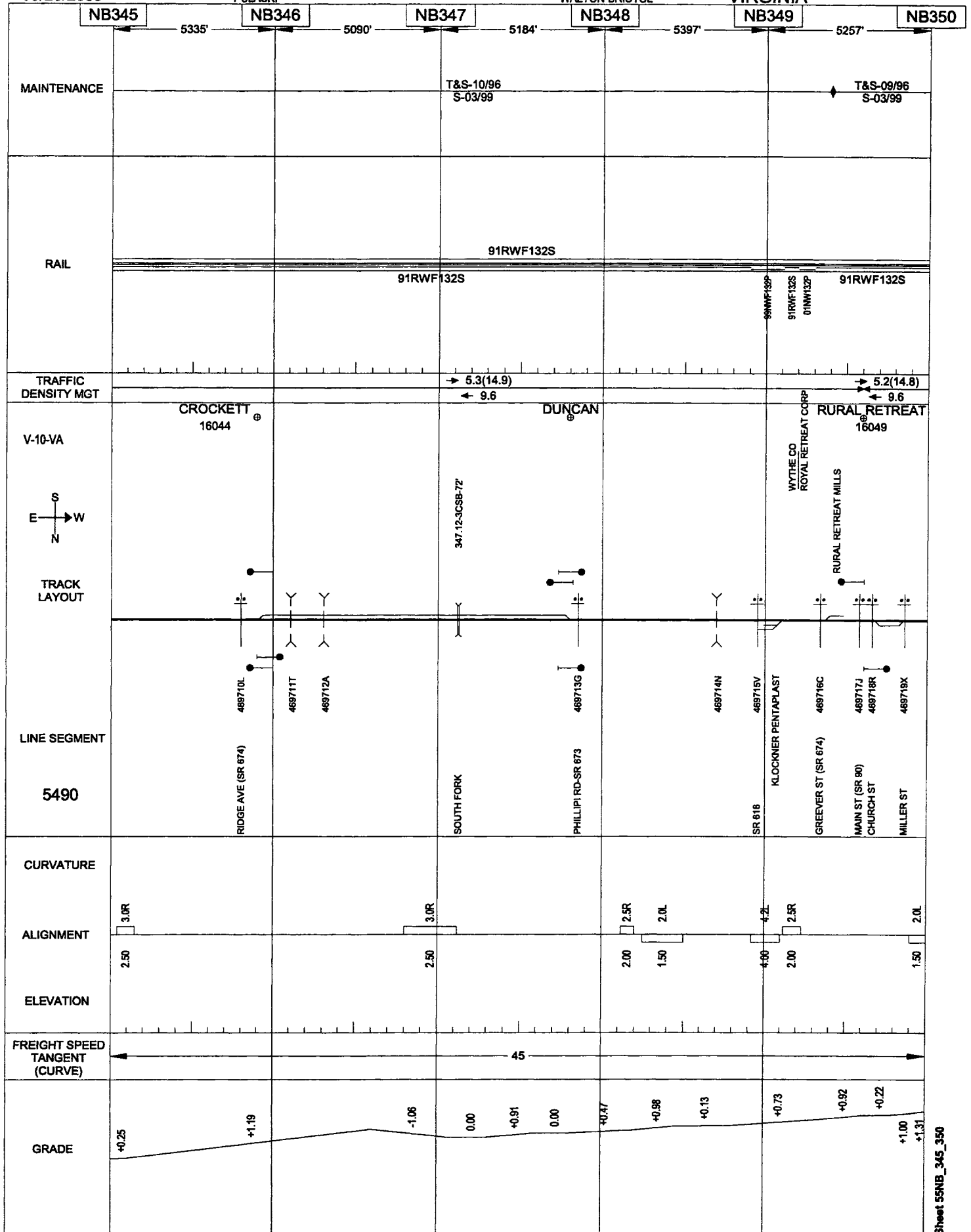
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03/20/2003

PULASKI

WALTON-BRISTOL

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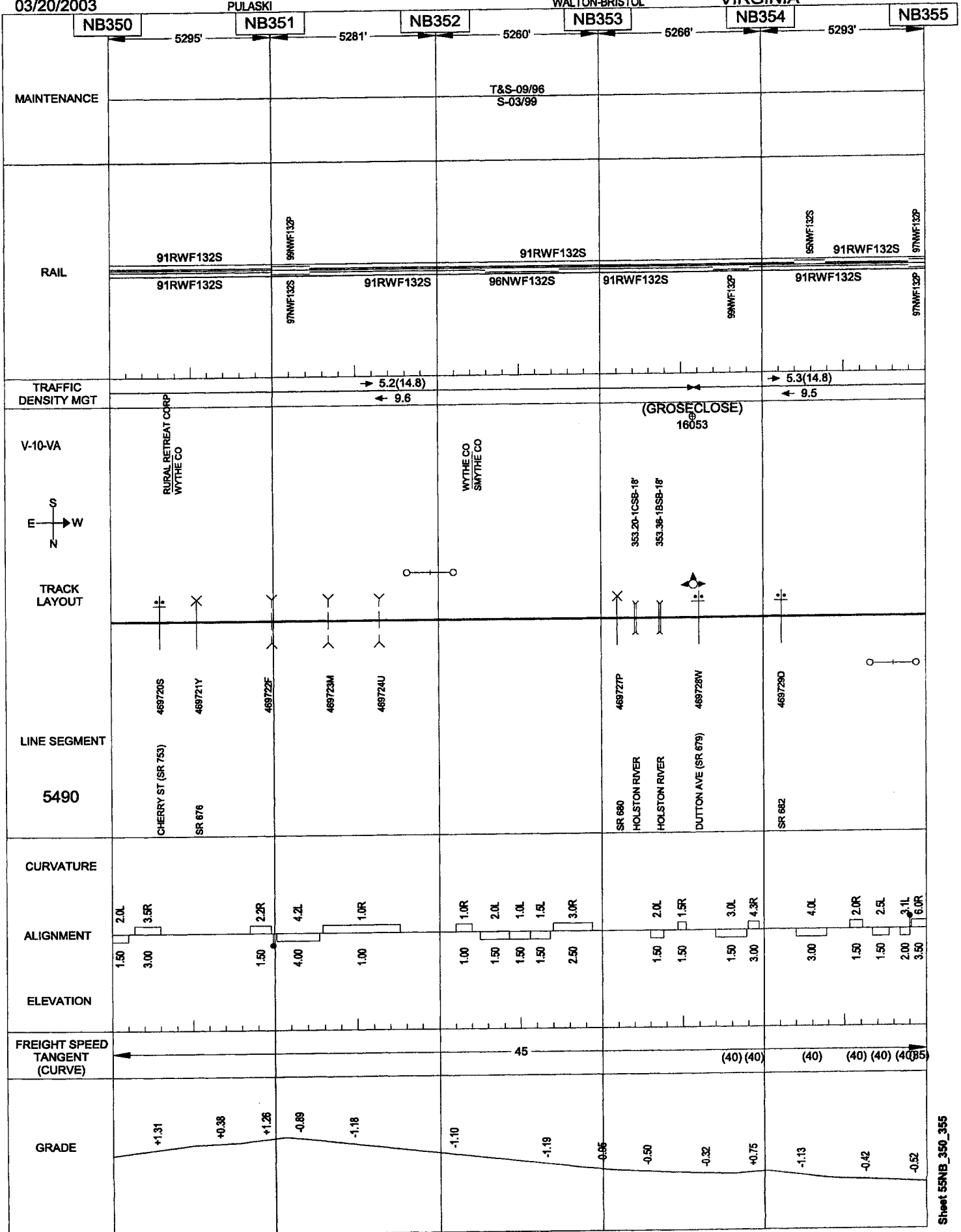


03/20/2003

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WALTON-BRISTOL

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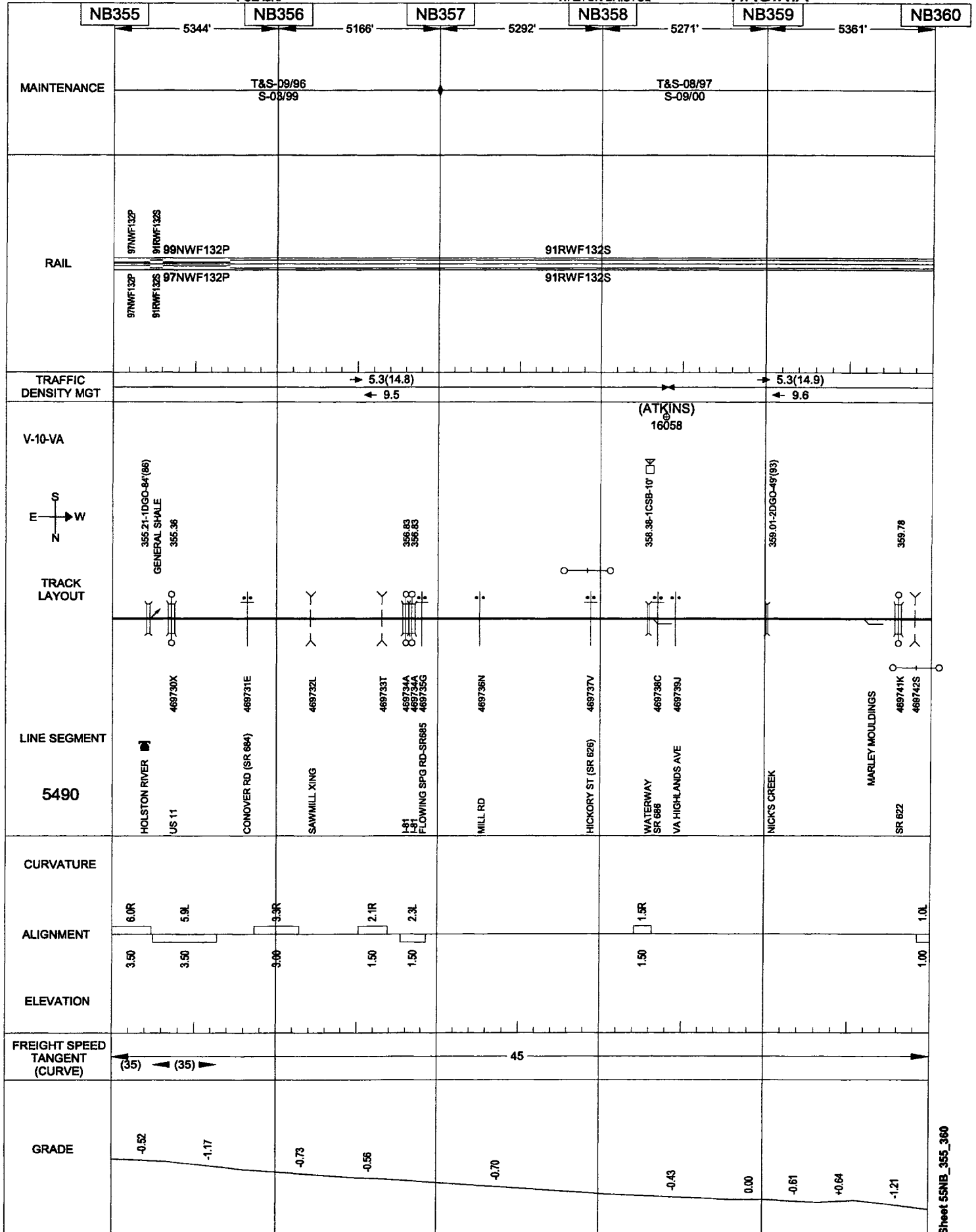


03/20/2003

PULASKI

WALTON-BRISTOL

VIRGINIA



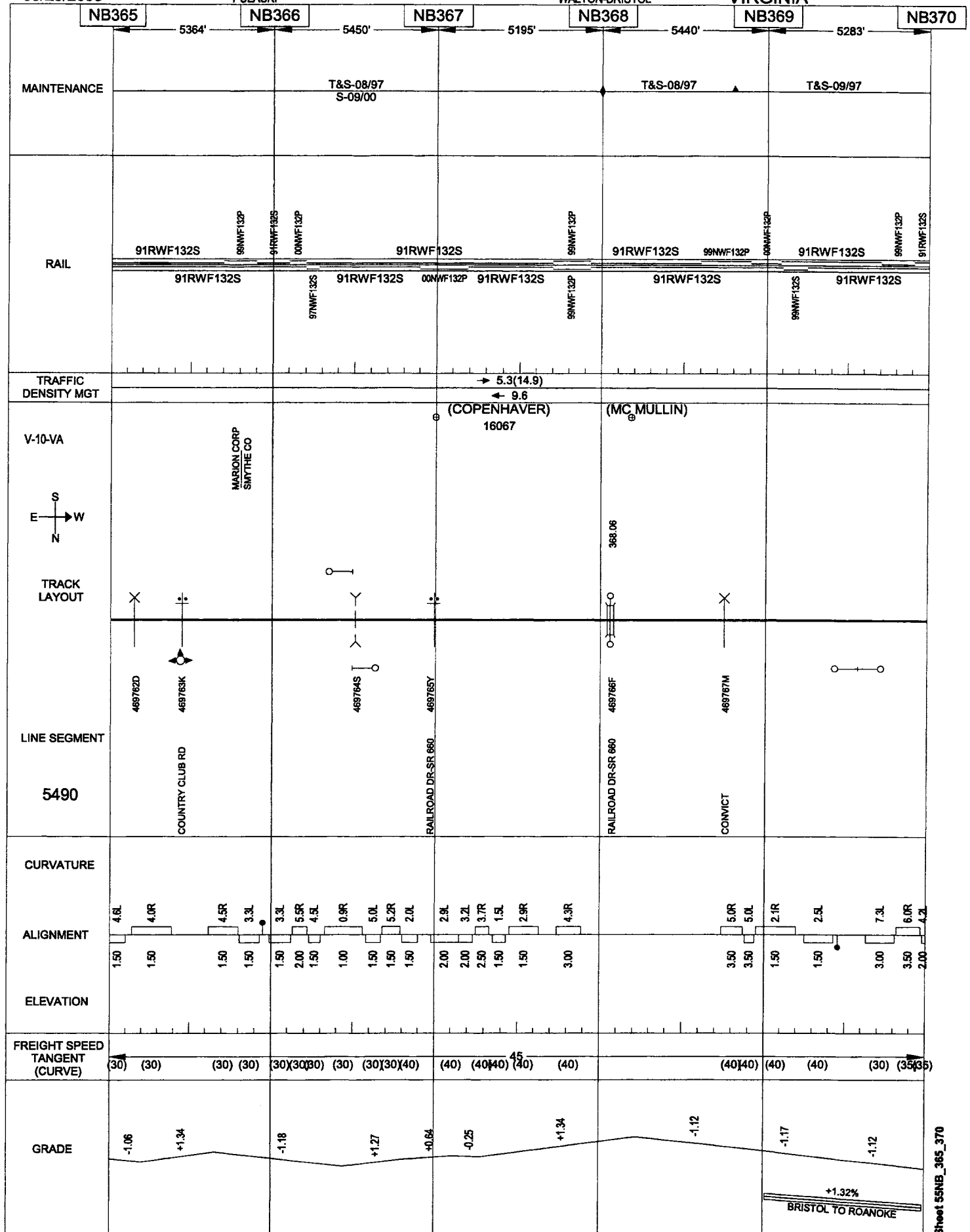
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03/20/2003

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WALTON-BRISTOL

VIRGINIA

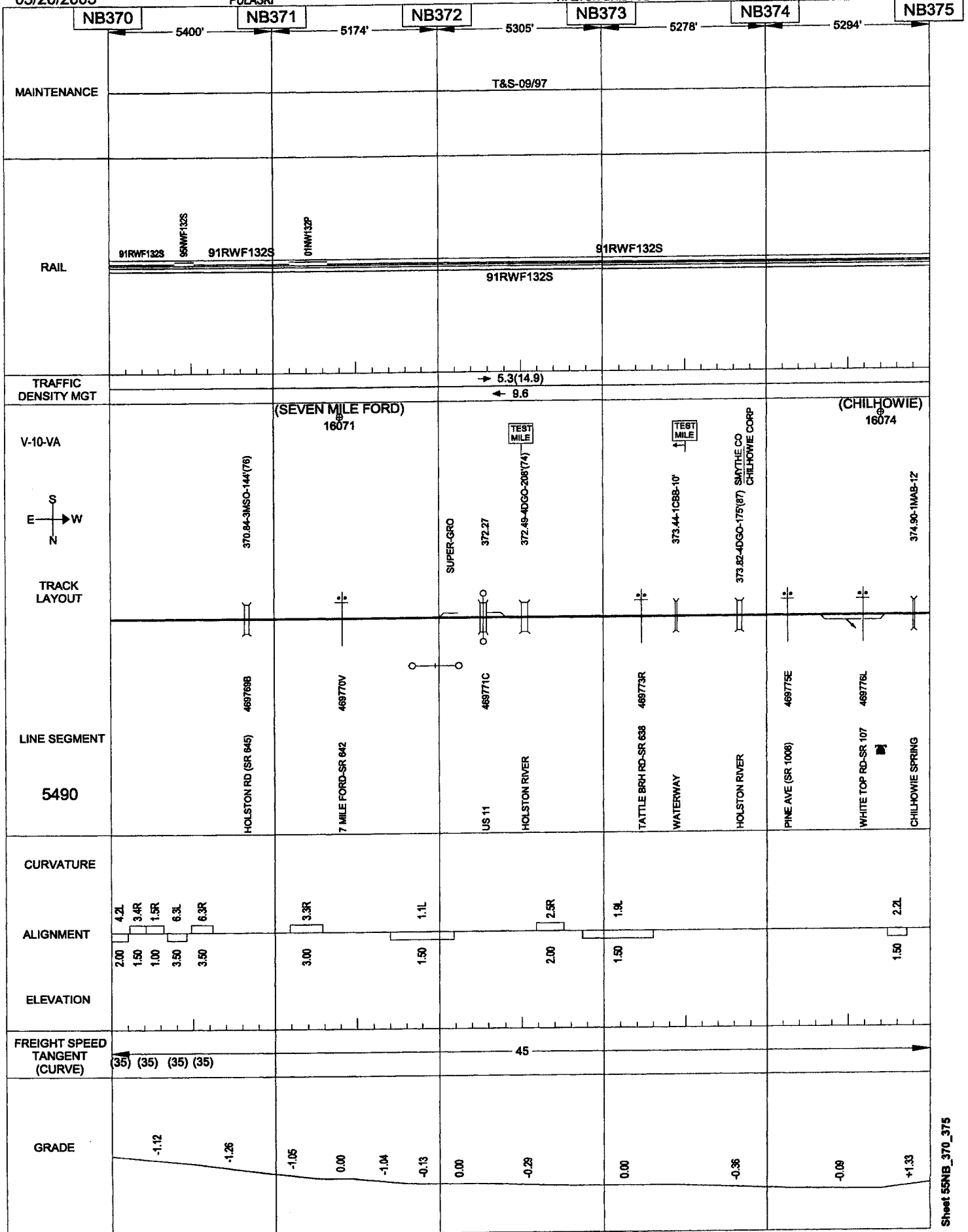


03/20/2003

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WALTON-BRISTOL

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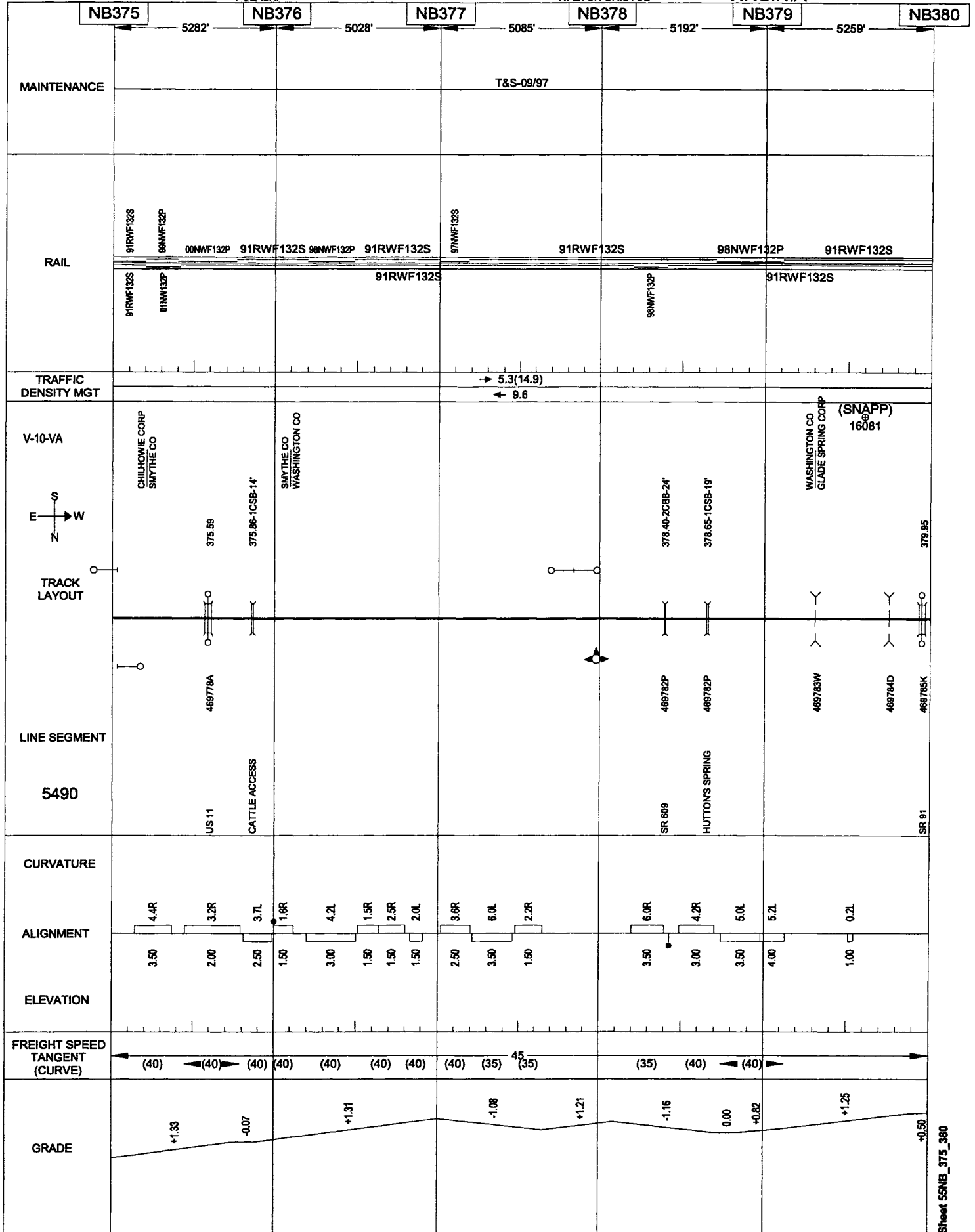


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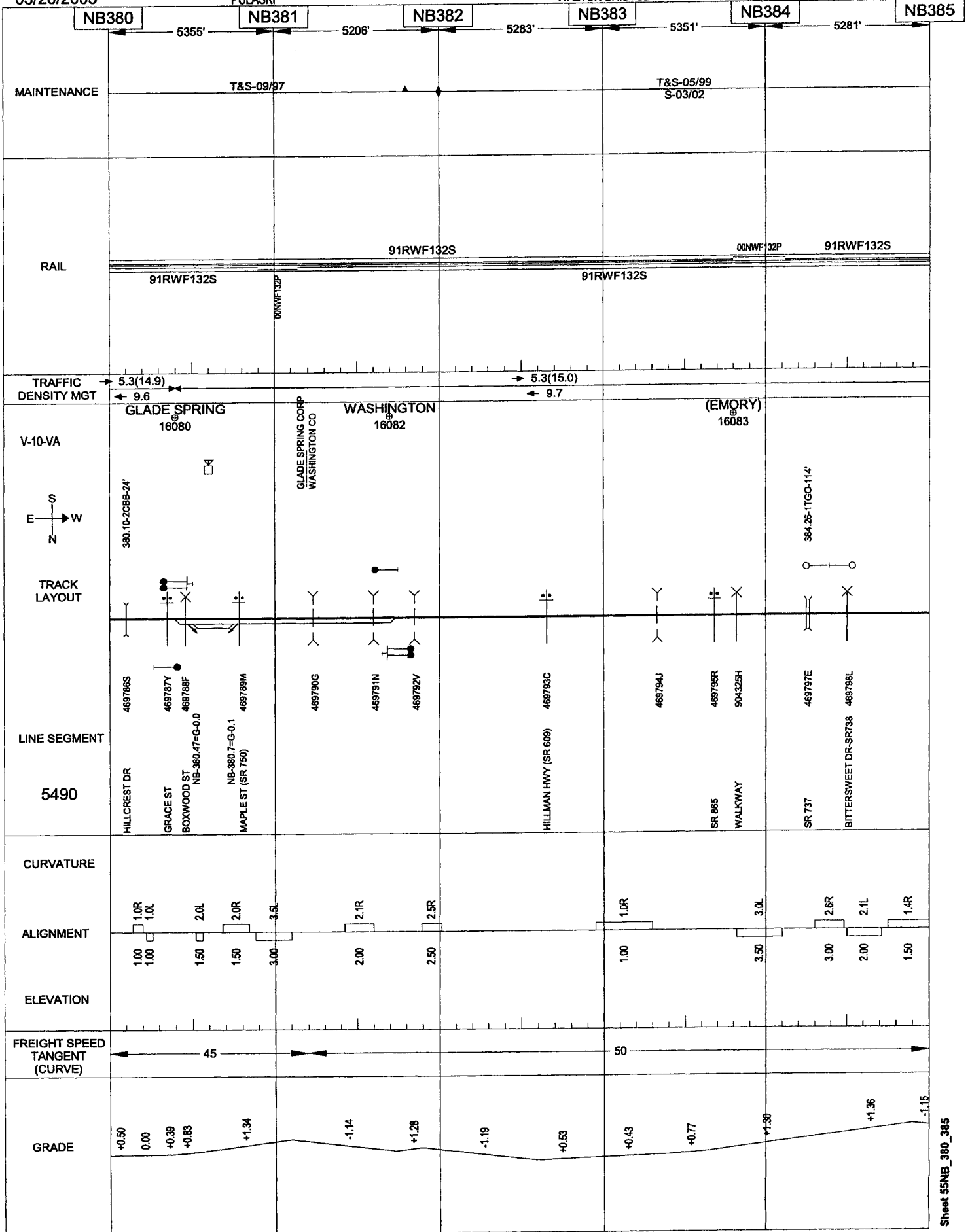


03/20/2003

PULASKI

WALTON-BRISTOL

VIRGINIA



Sheet 55NB_385_390

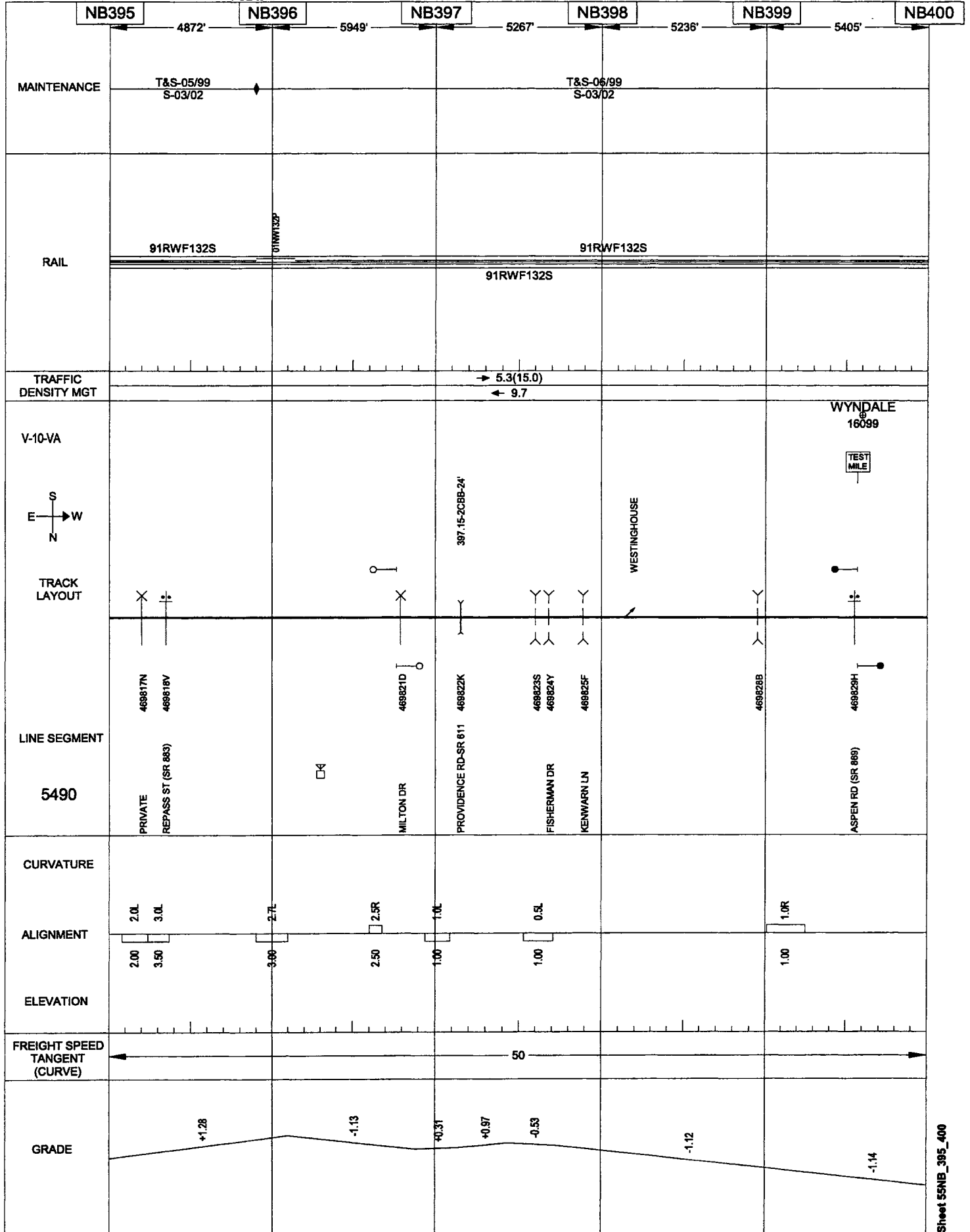
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03/20/2003

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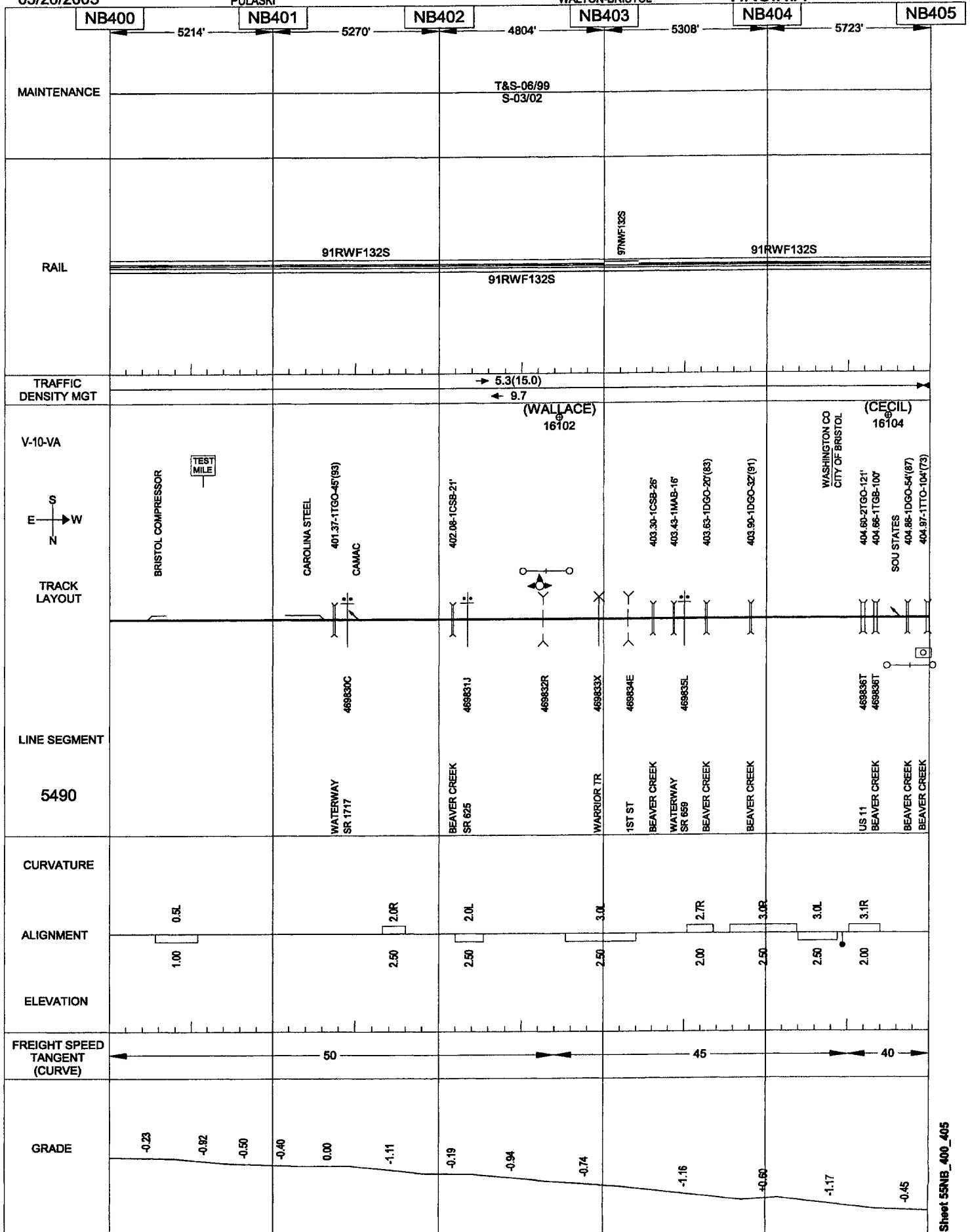


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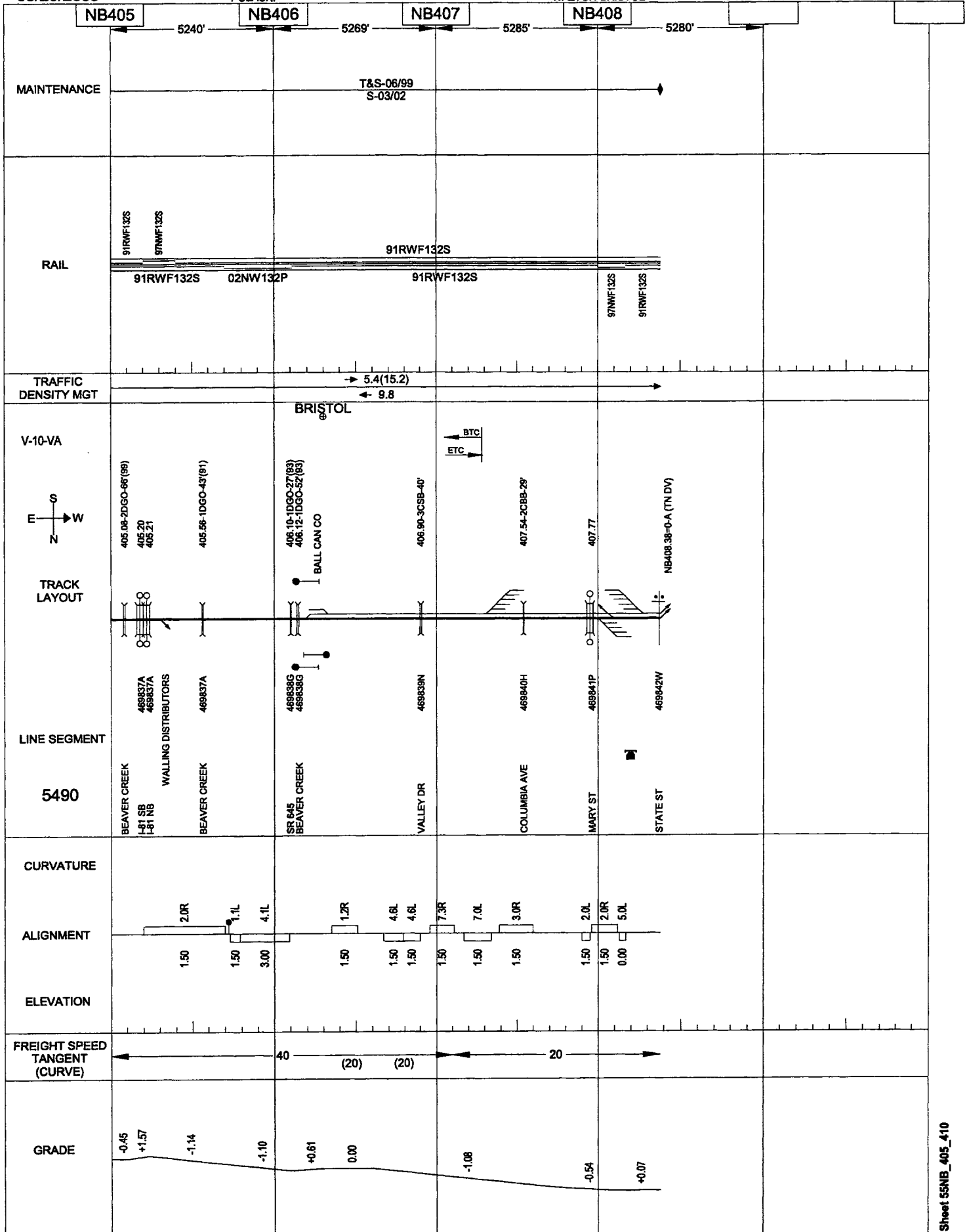


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WALTON-BRISTOL

VIRGINIA



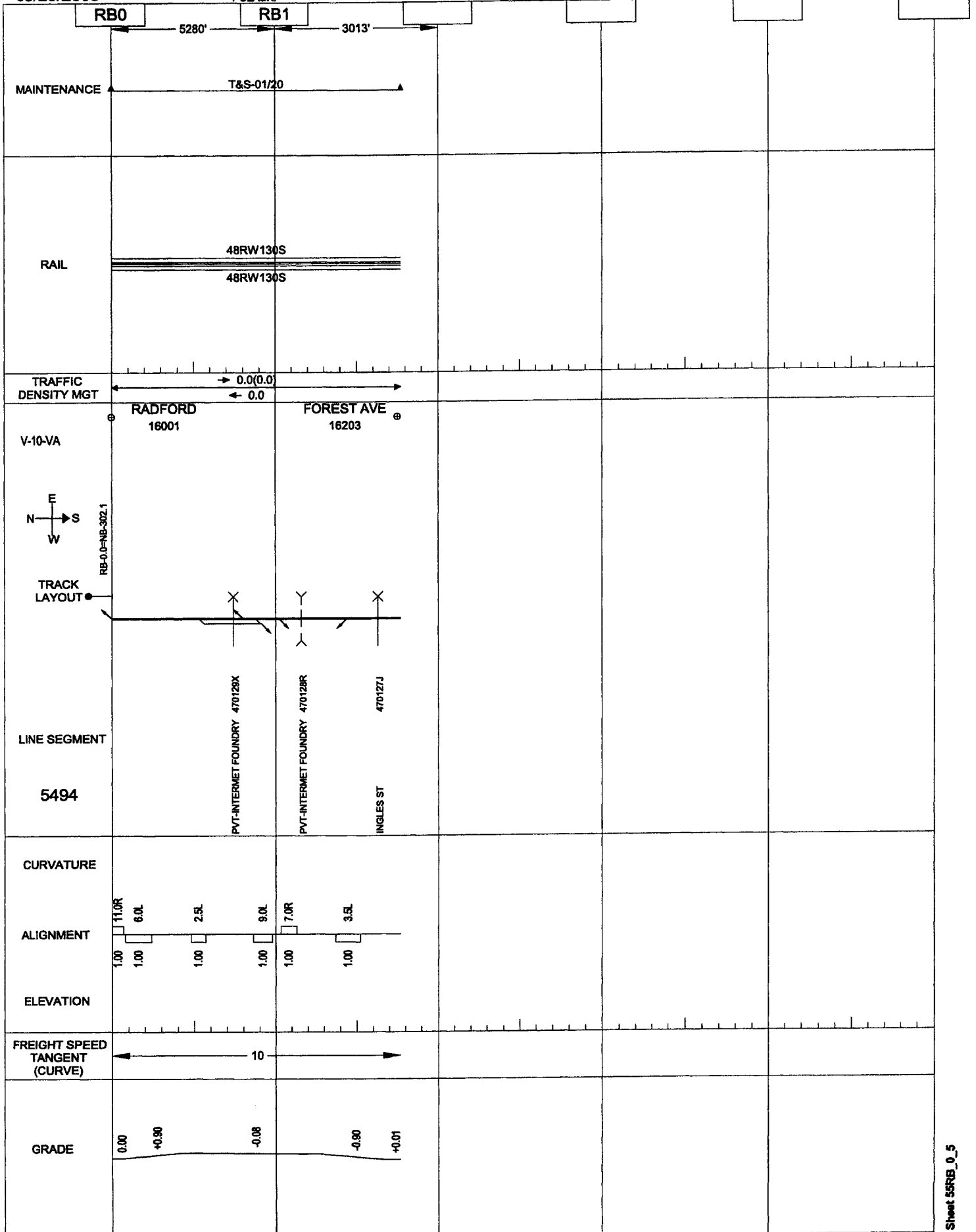
03/20/2003

PULASKI

RADFORD BRANCH

RADFORD-FOREST AVENUE

VIRGINIA



03/20/2003

HAGERSTOWN

HAGERSTOWN SEC.

SHIPPENSBURG-HAGERST

VIRGINIA

HW74

1594'

5370'

T&S-08/00

MAINTENANCE

RAIL

88RW127S

88RW127S

TRAFFIC
DENSITY MGT

10.3(24.4)

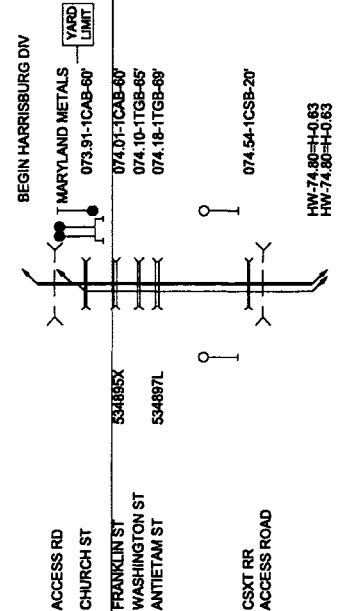
14.1

CP TOWN

HAGERSTOWN

E
N → S
W

TRACK
LAYOUT



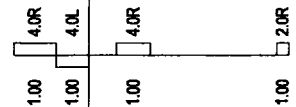
LINE SEGMENT

2442

CURVATURE

ALIGNMENT

ELEVATION



FREIGHT SPEED
TANGENT
(CURVE)

20

GRADE

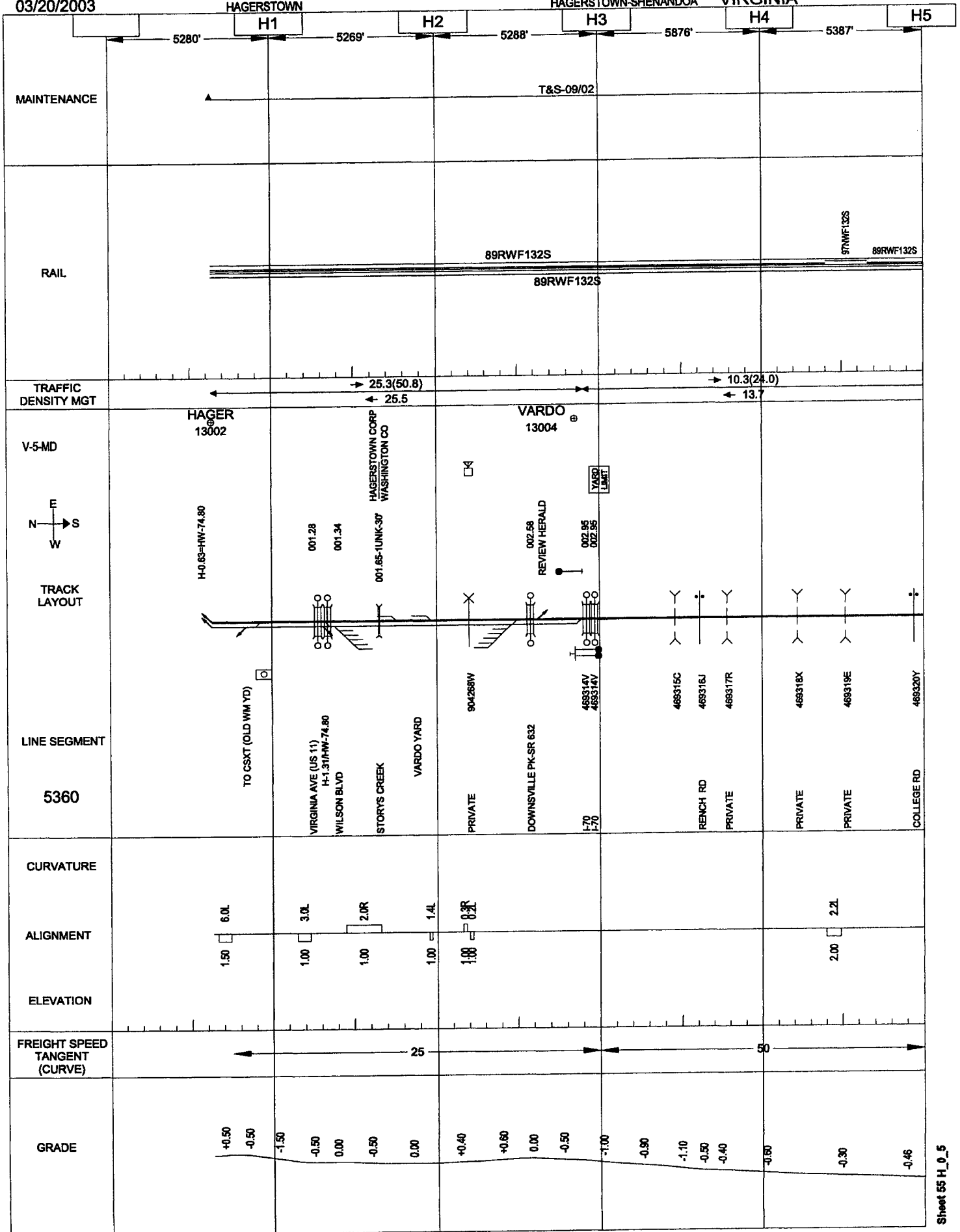


03/20/2003

HAGERSTOWN

HAGERSTOWN-SHENANDOAH

VIRGINIA

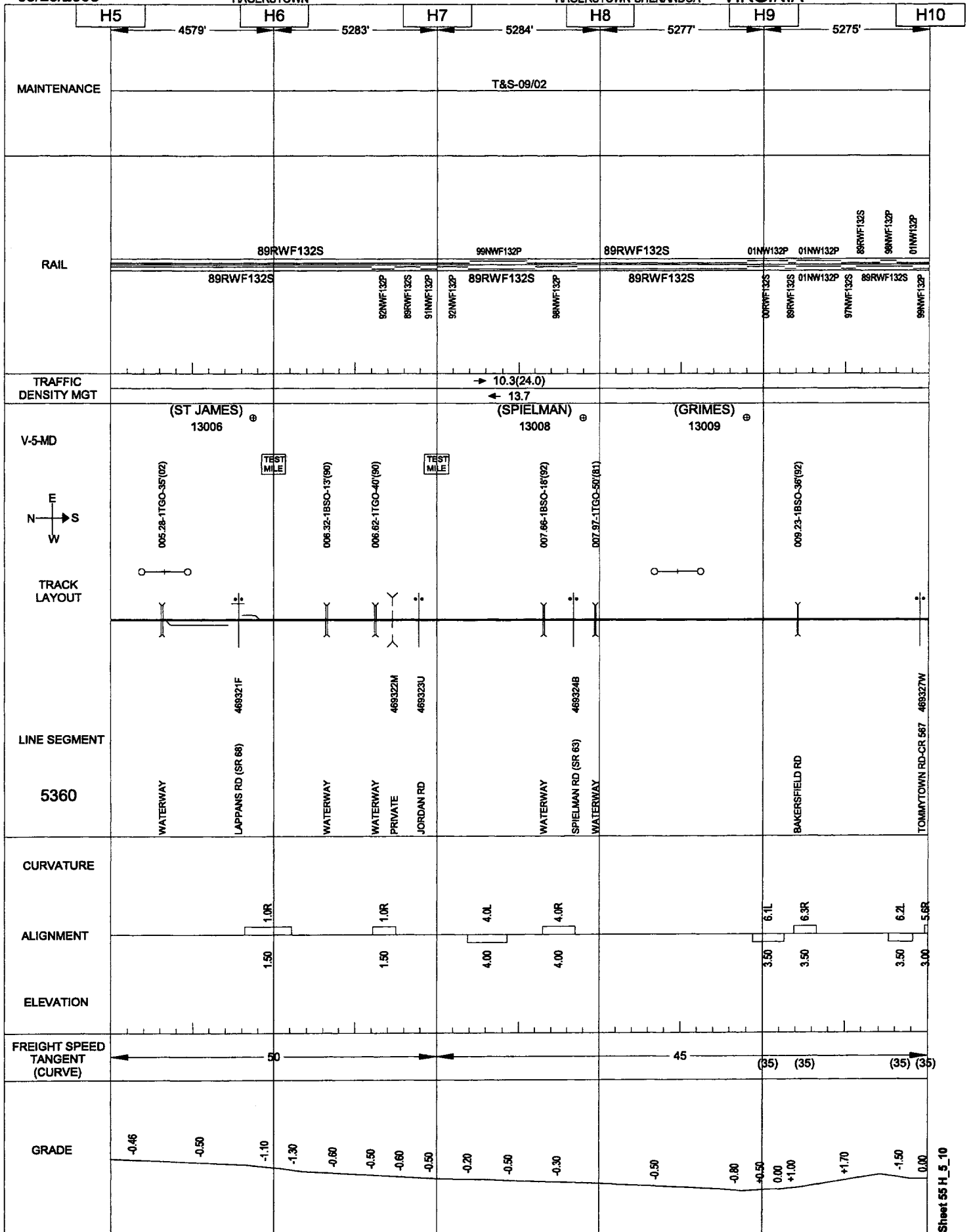


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HAGERSTOWN-SHENANDOAH

VIRGINIA

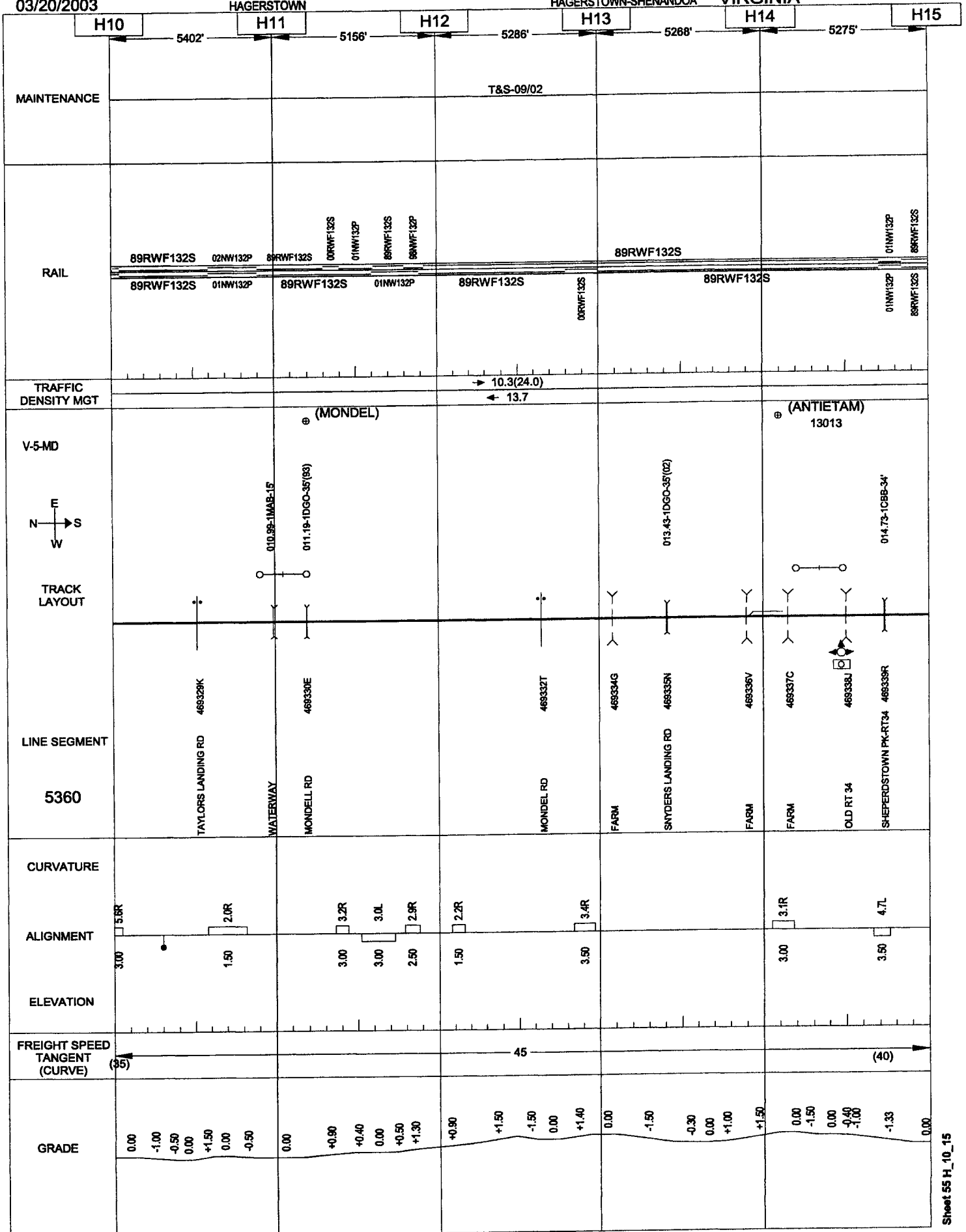


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HAGERSTOWN-SHENANDOAH

VIRGINIA

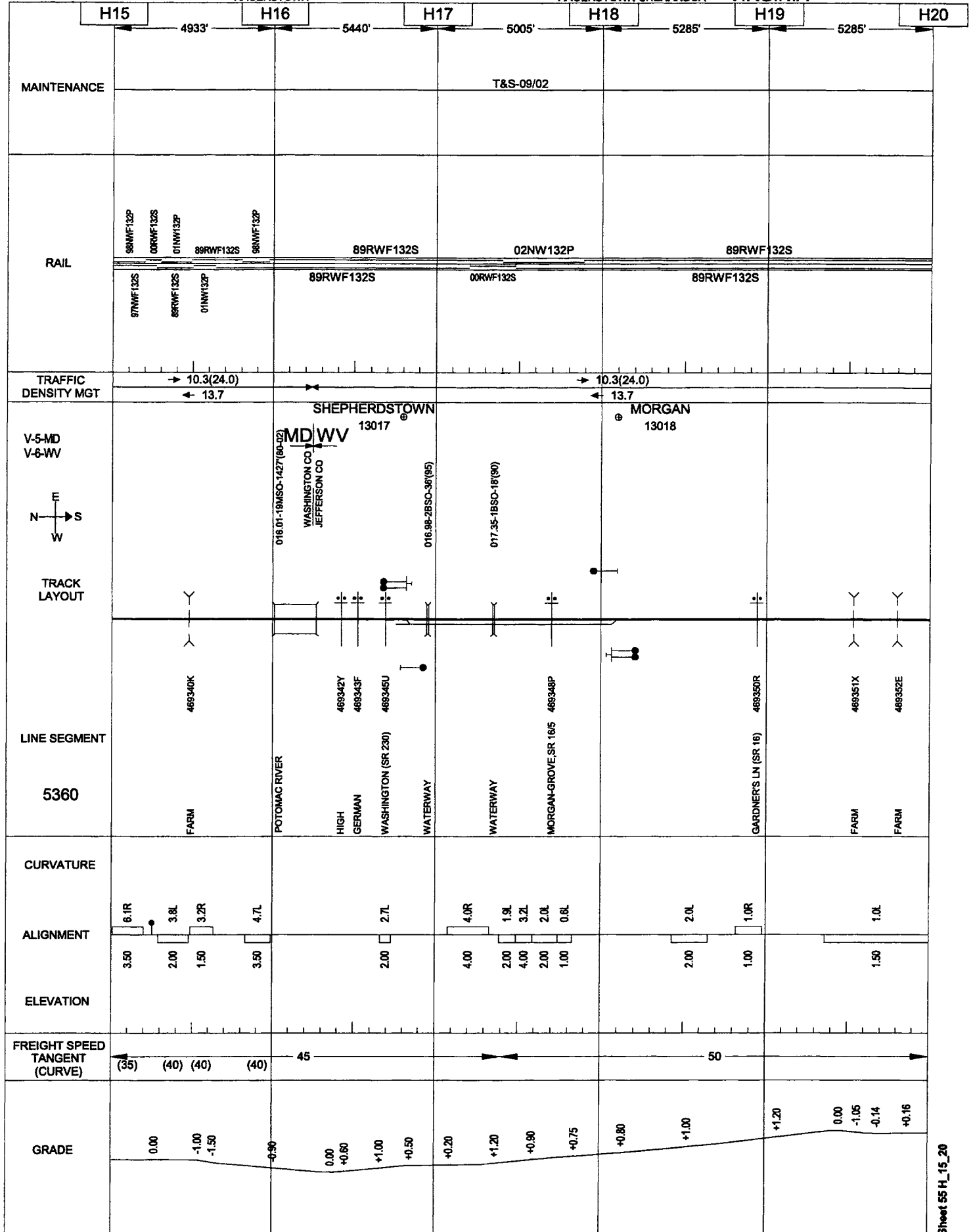


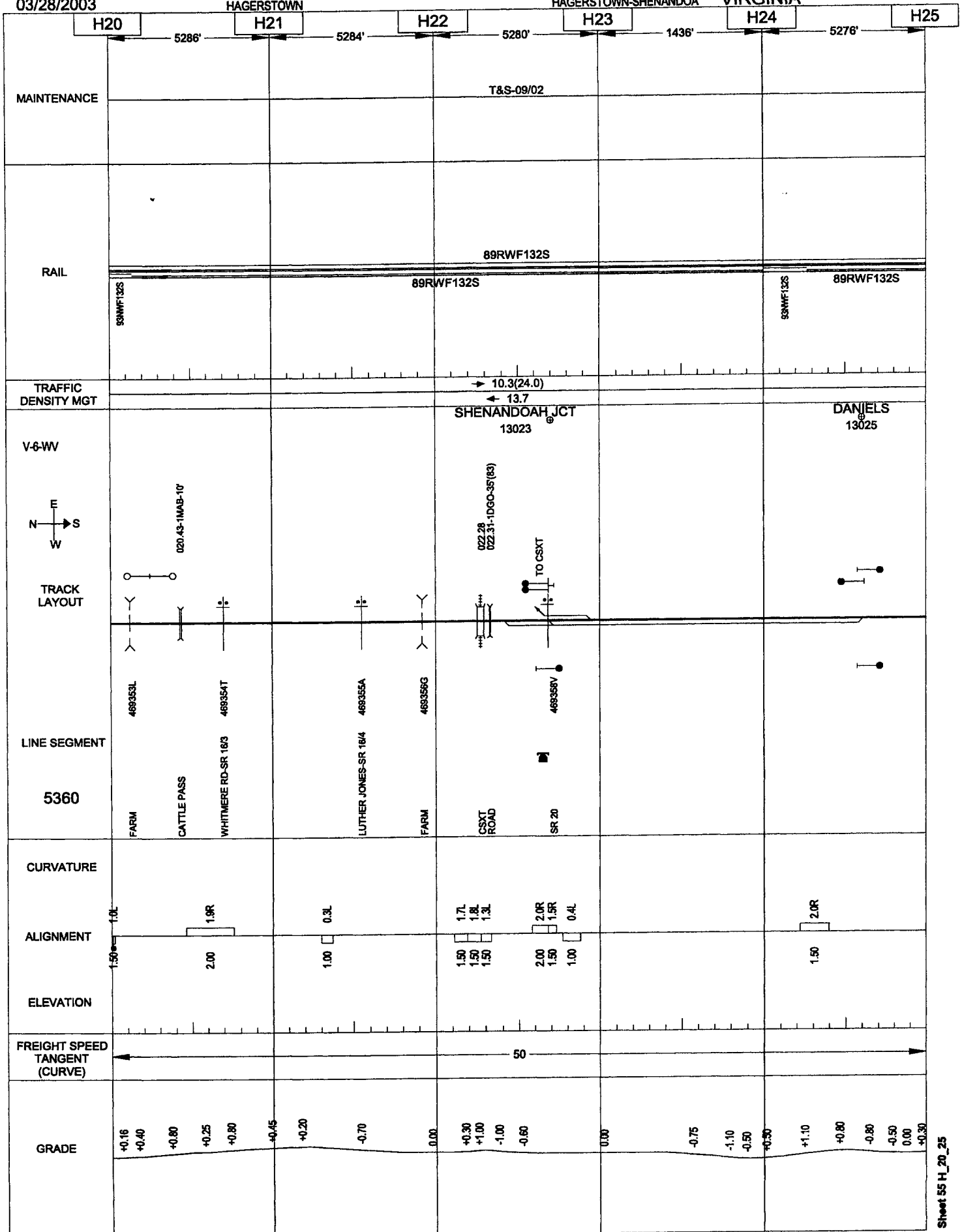
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HAGERSTOWN-SHENANDOAH

VIRGINIA



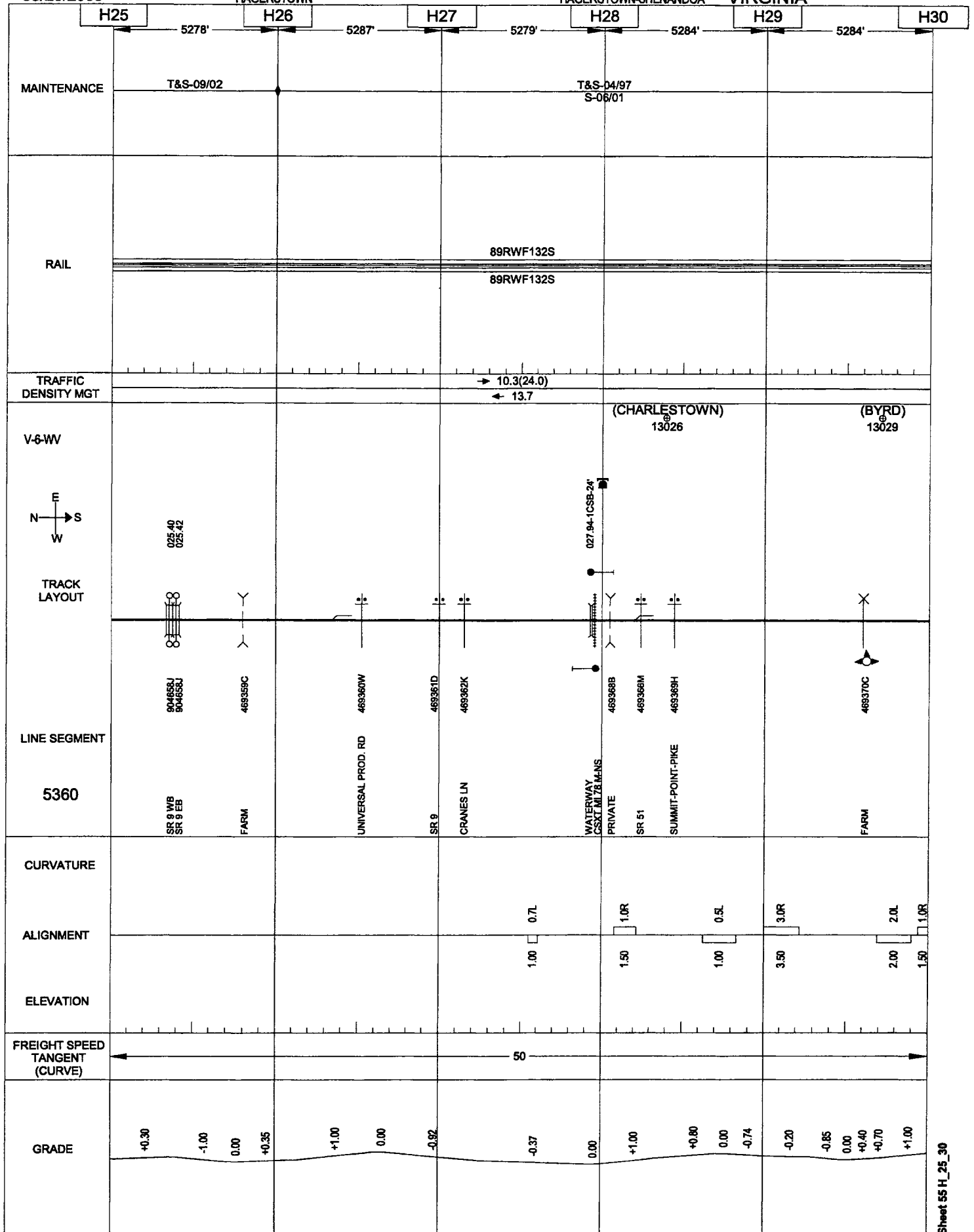


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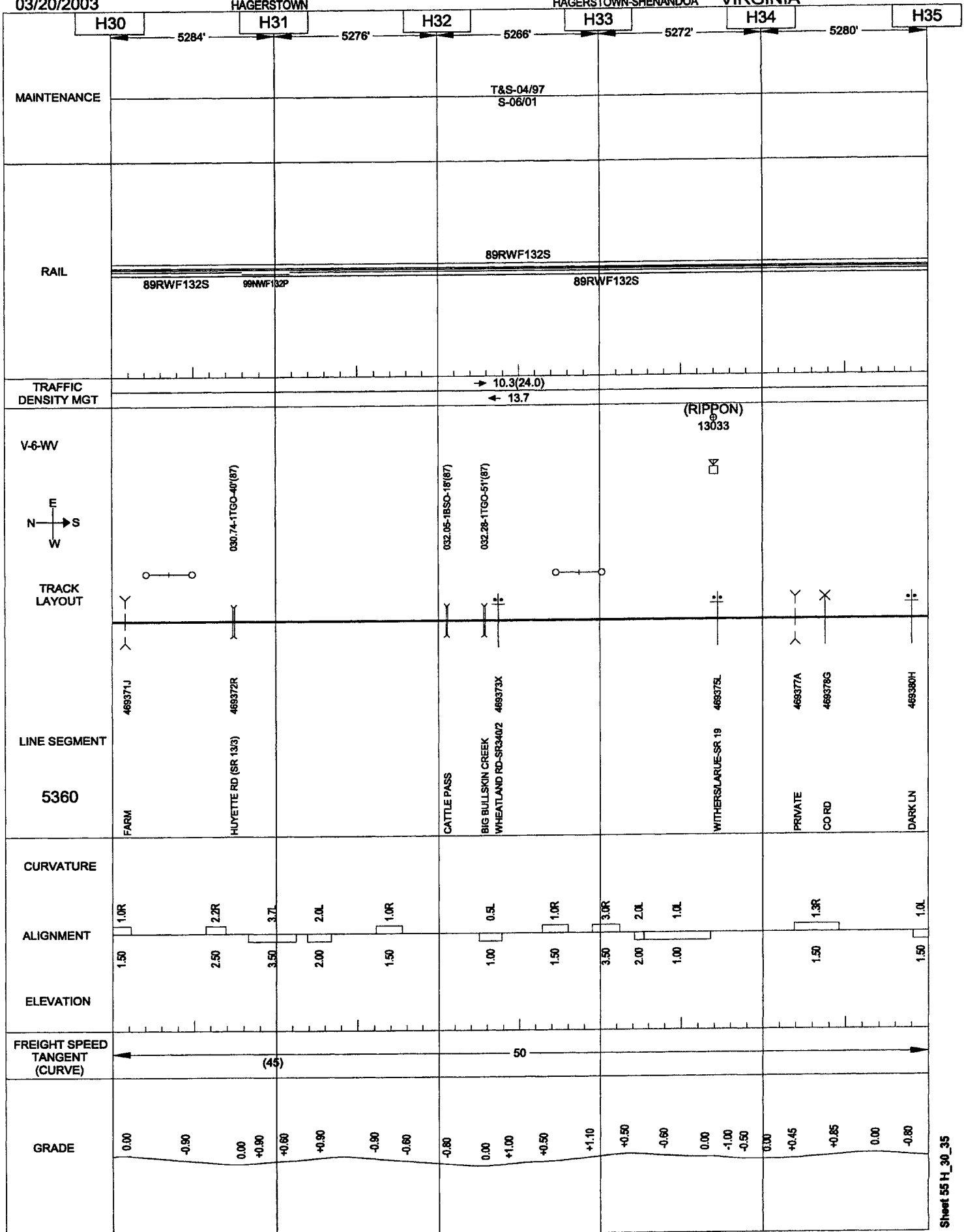


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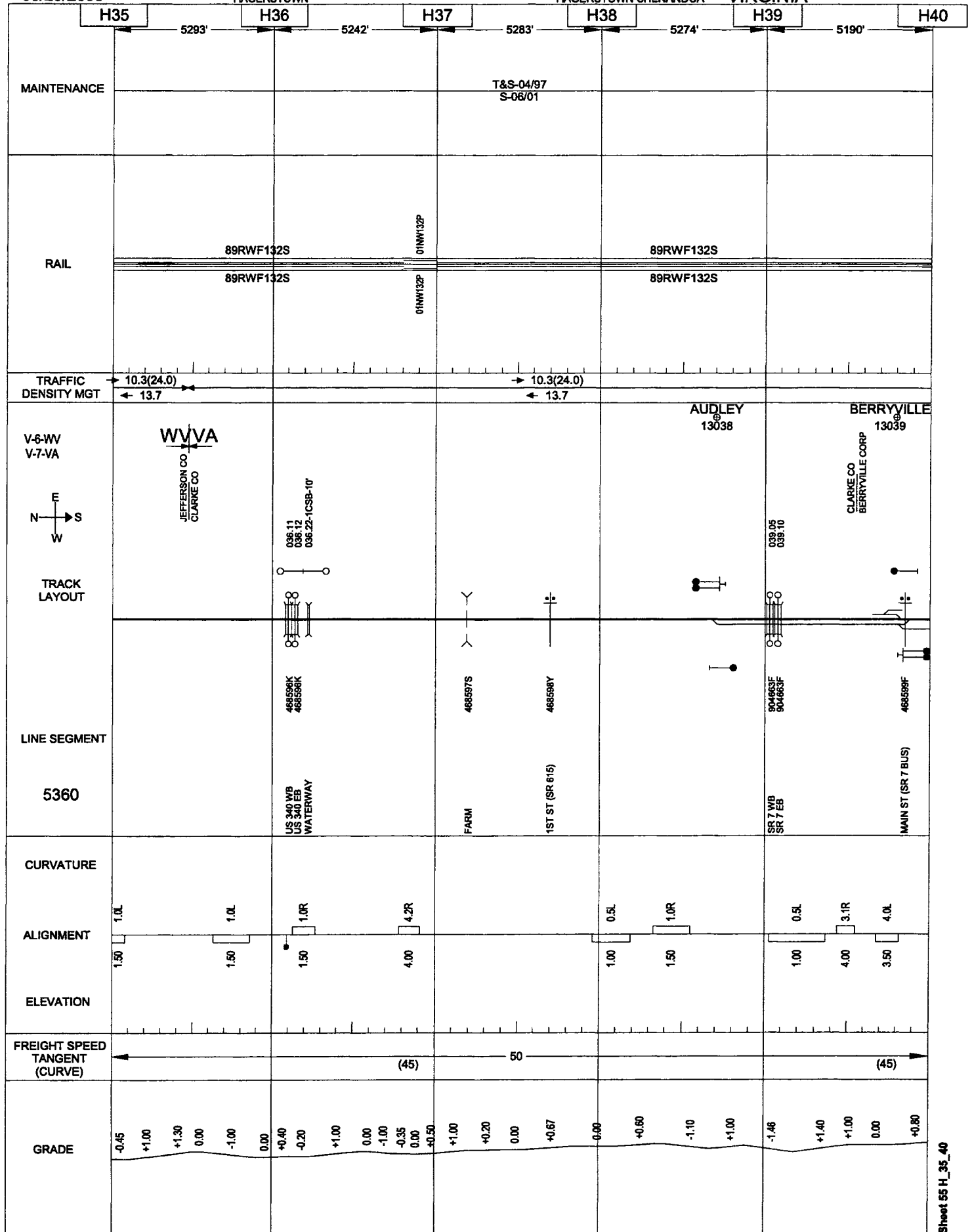


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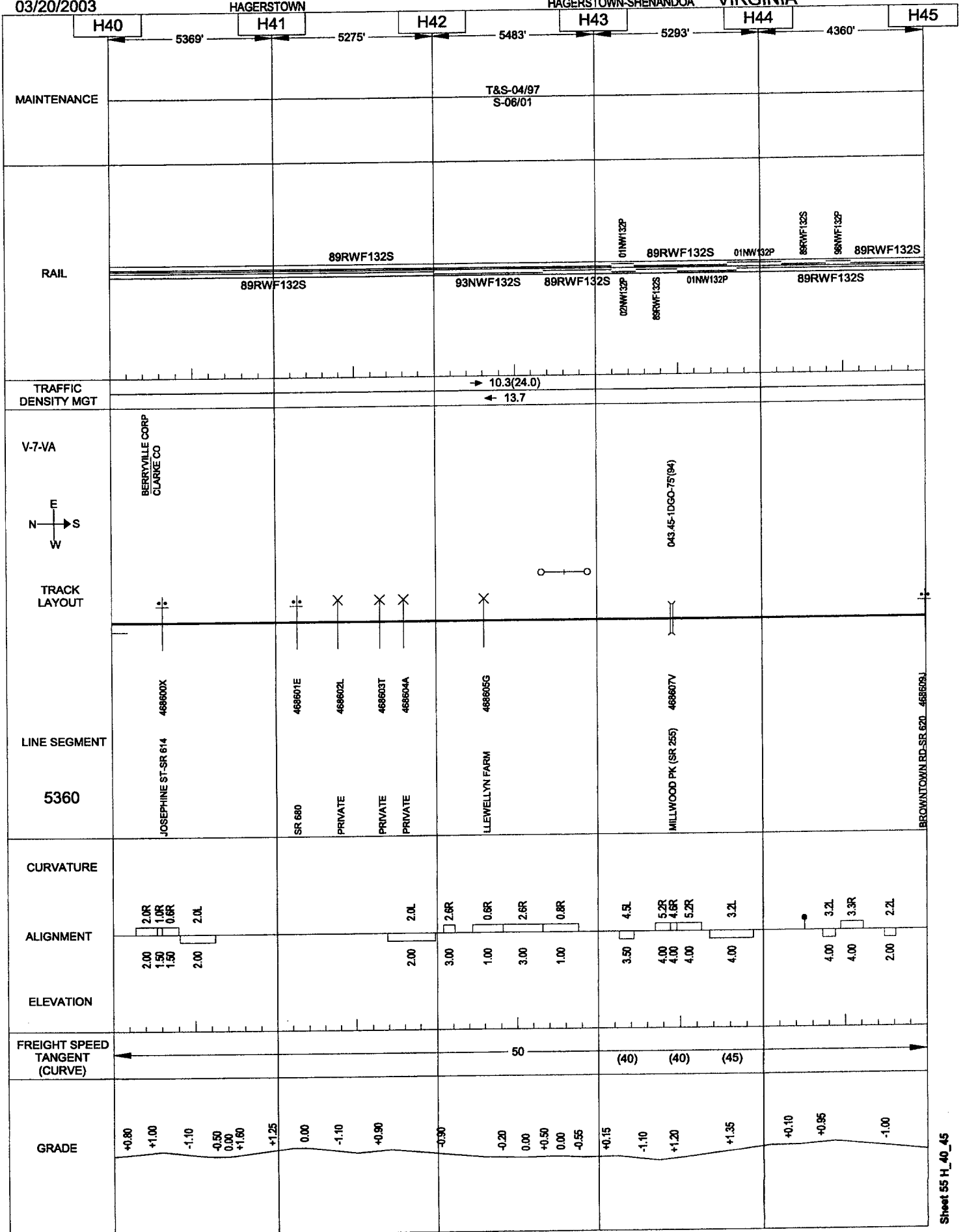


03/20/2003

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HAGERSTOWN-SHENANDOA

VIRGINIA

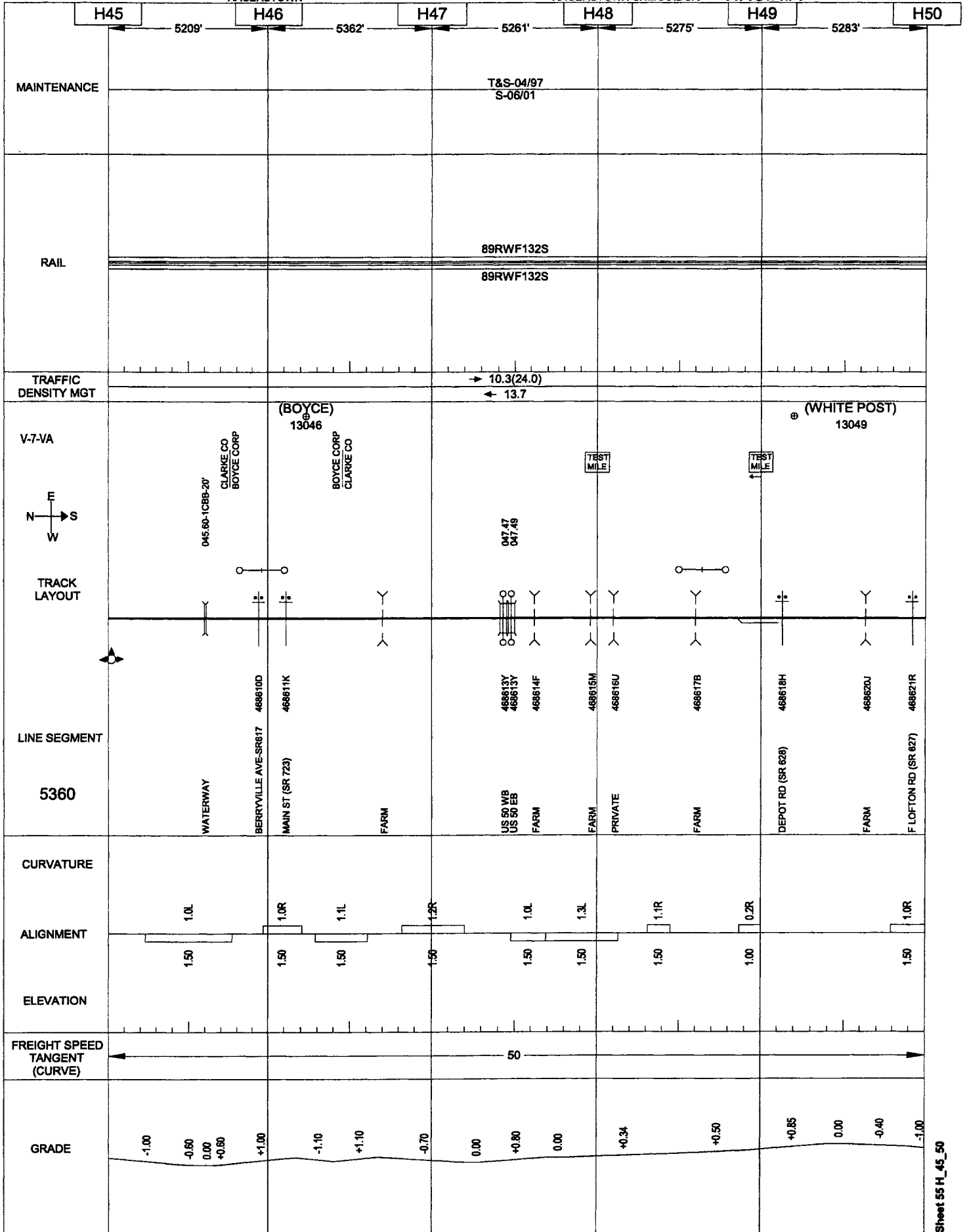


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HAGERSTOWN-SHENANDOAH

VIRGINIA

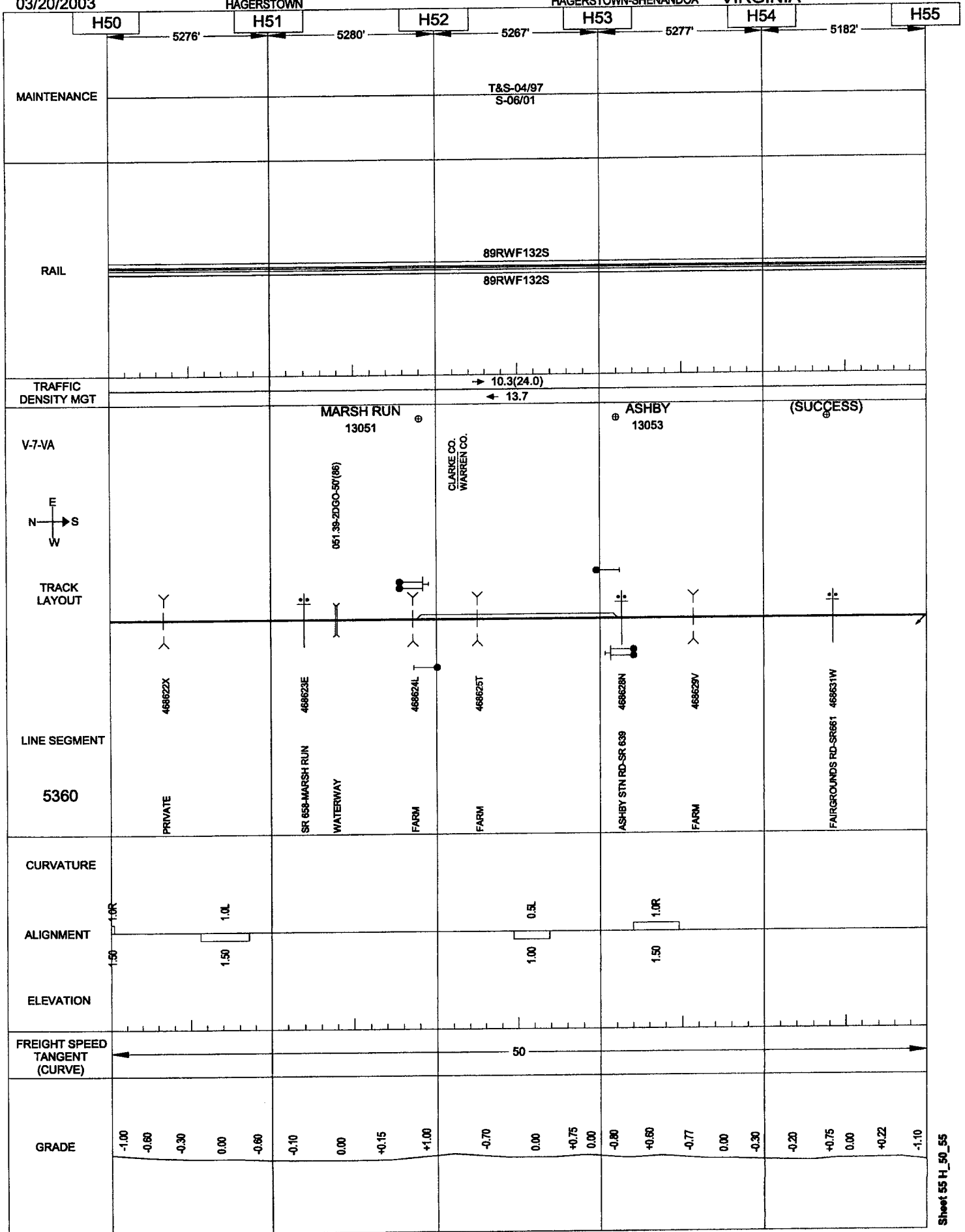


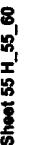
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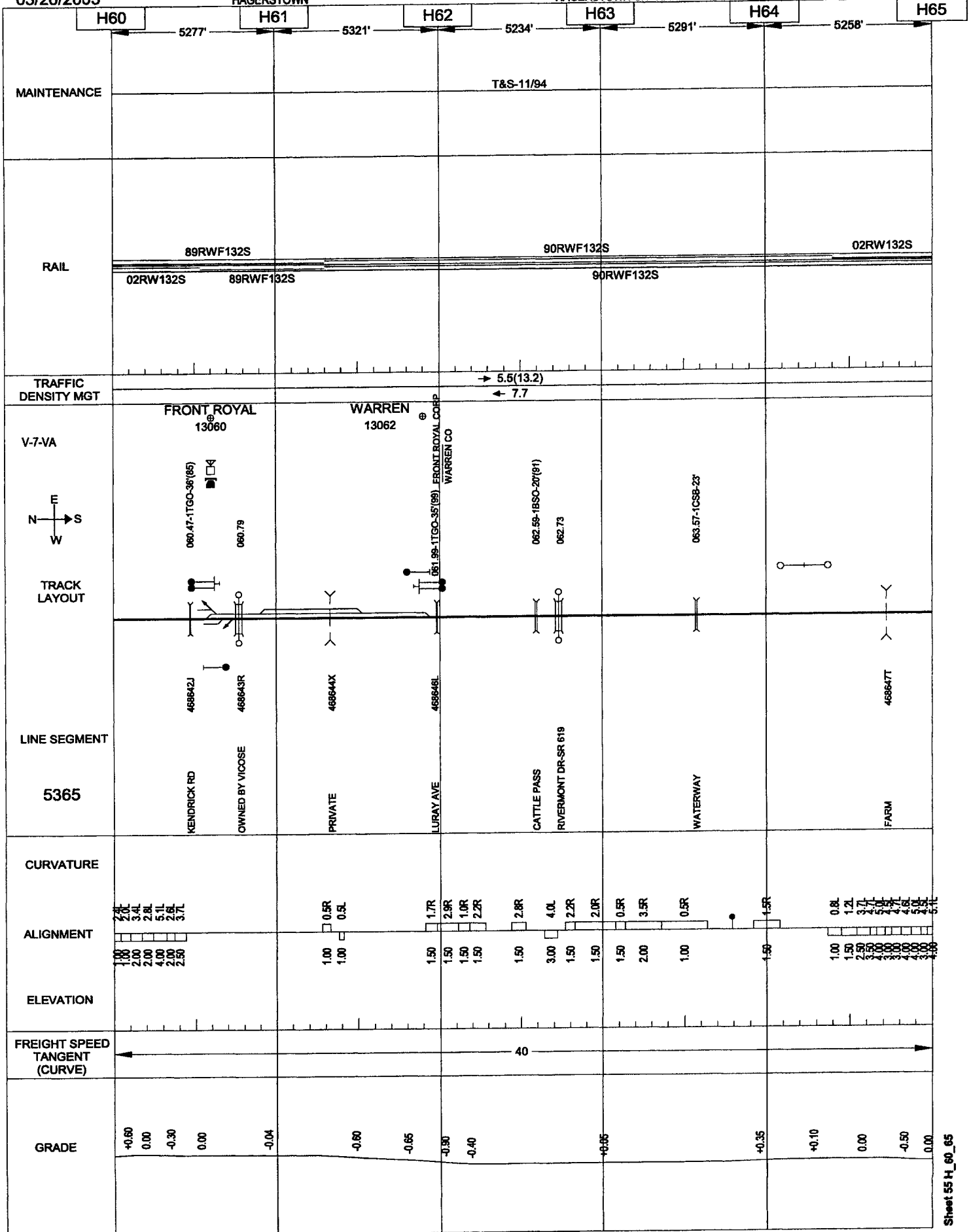


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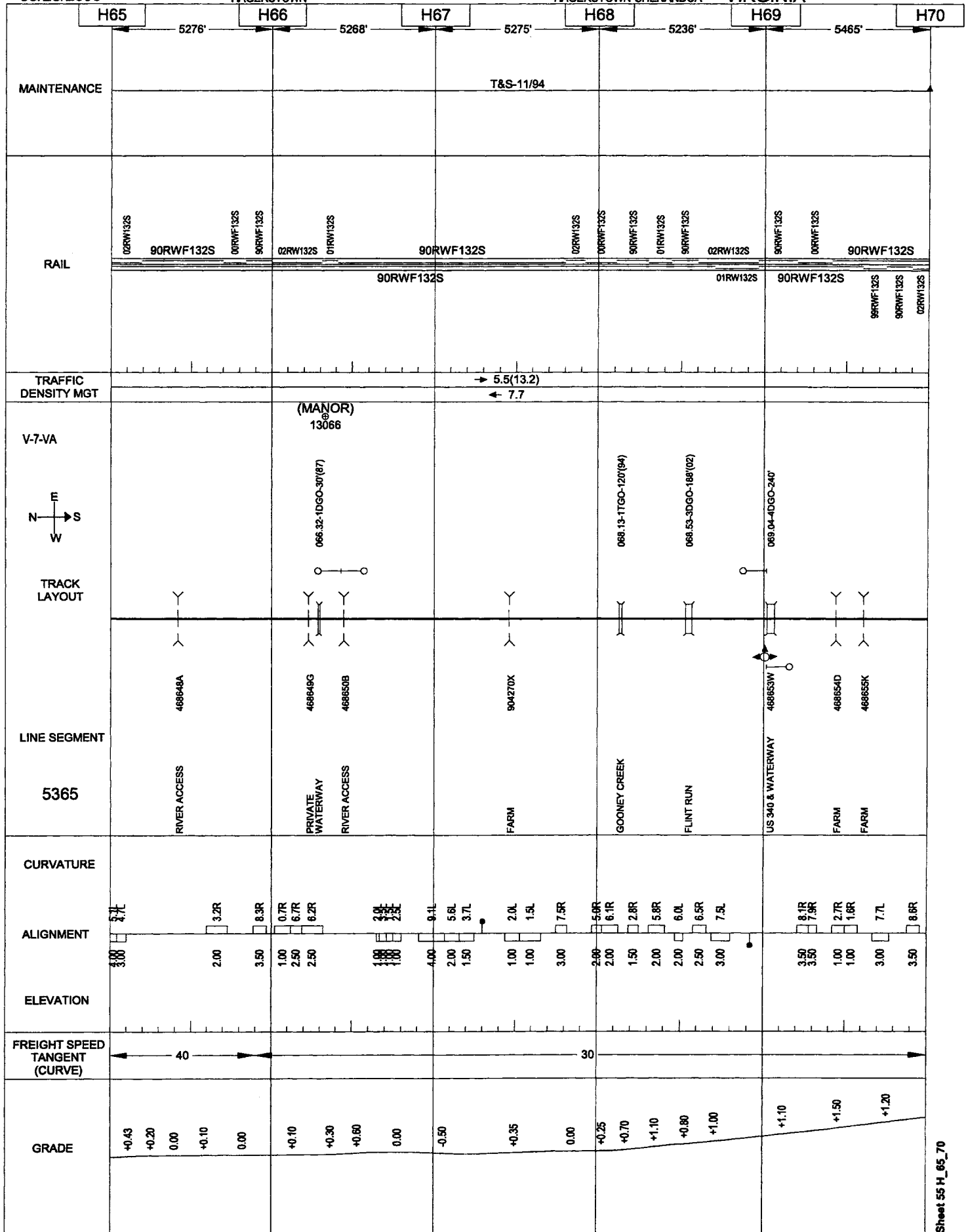


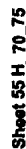
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HAGERSTOWN-SHENANDOA

VIRGINIA



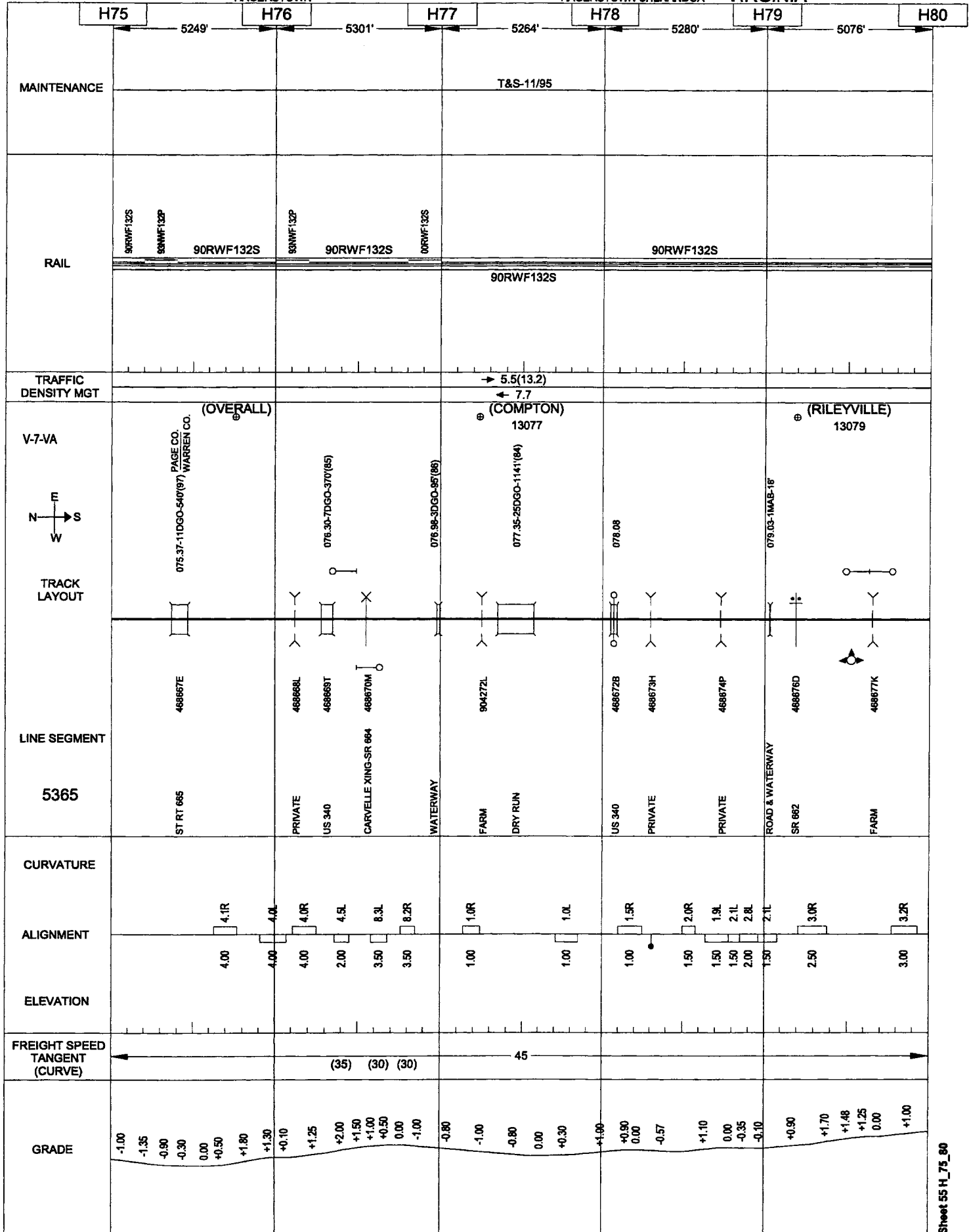


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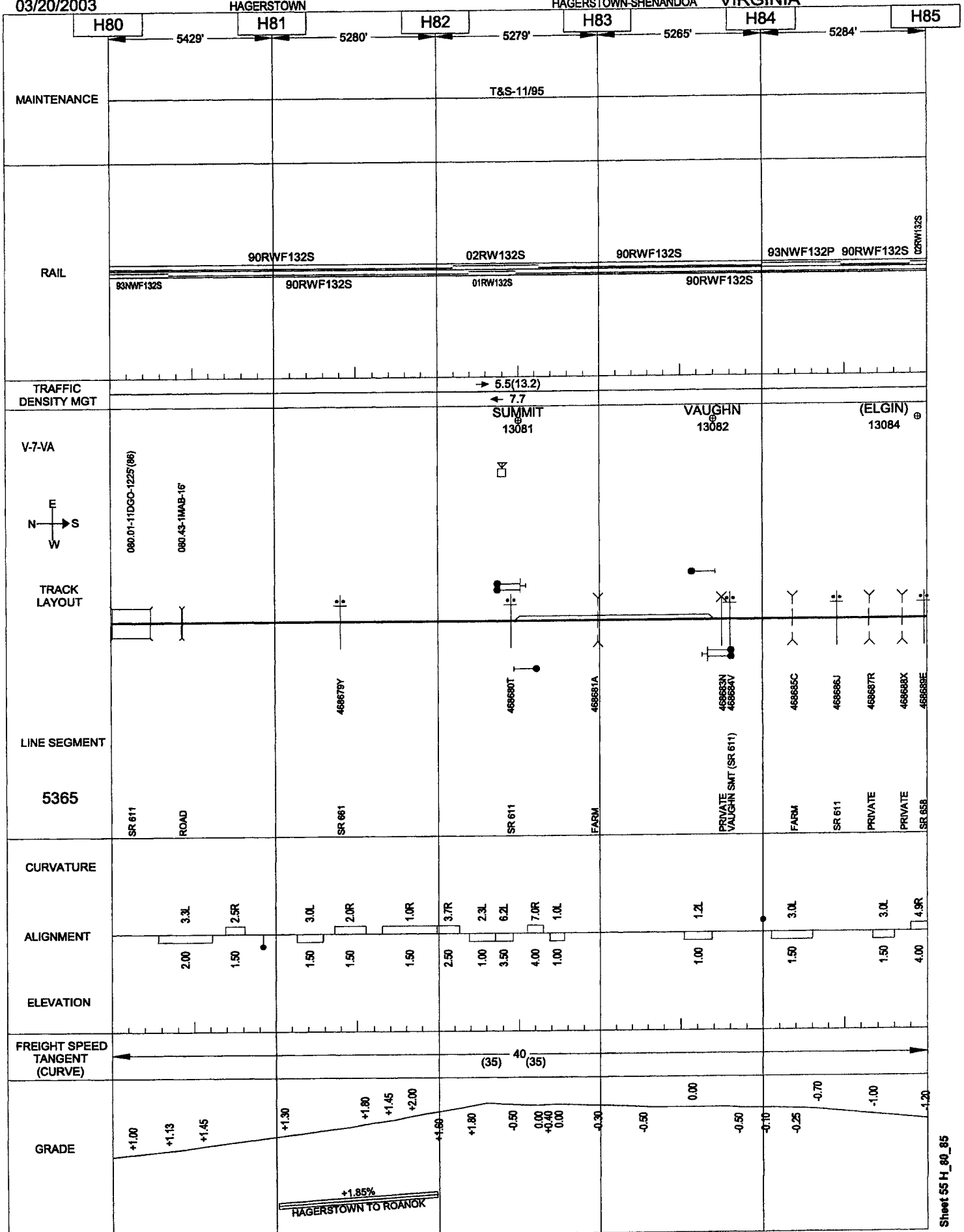


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HAGERSTOWN-SHENANDOA

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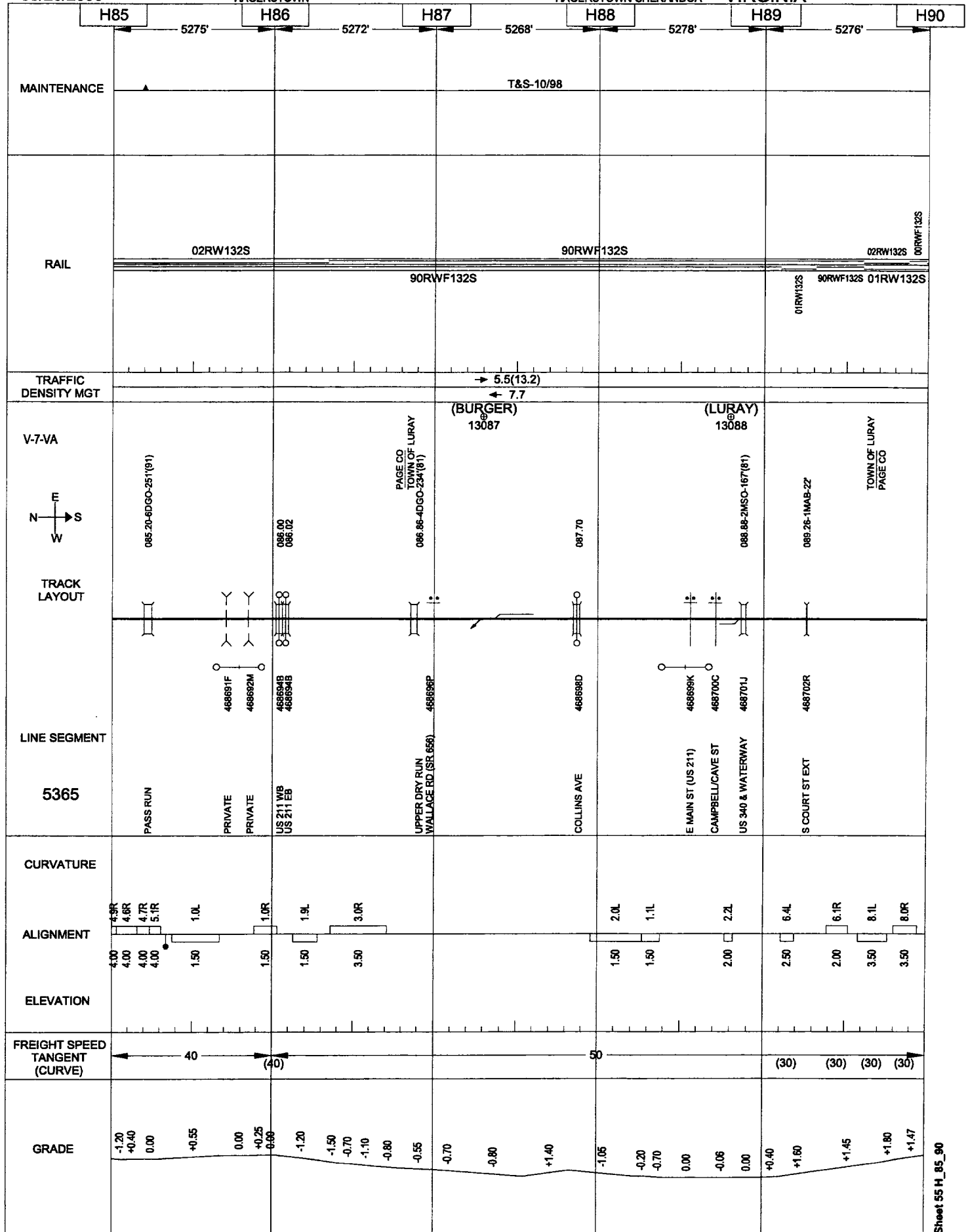


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HAGERSTOWN-SHENANDOAH

VIRGINIA

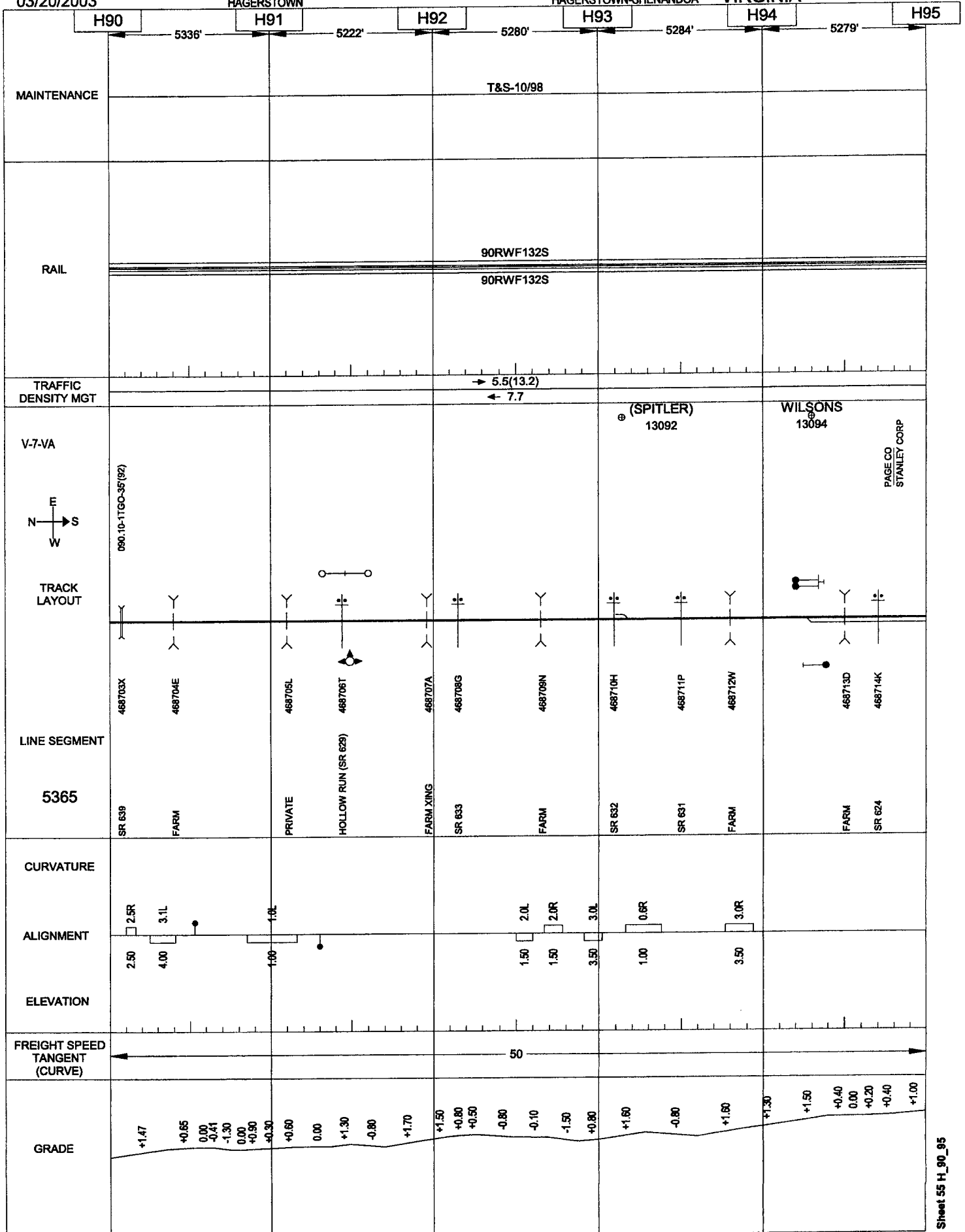


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HAGERSTOWN-SHENANDOAH

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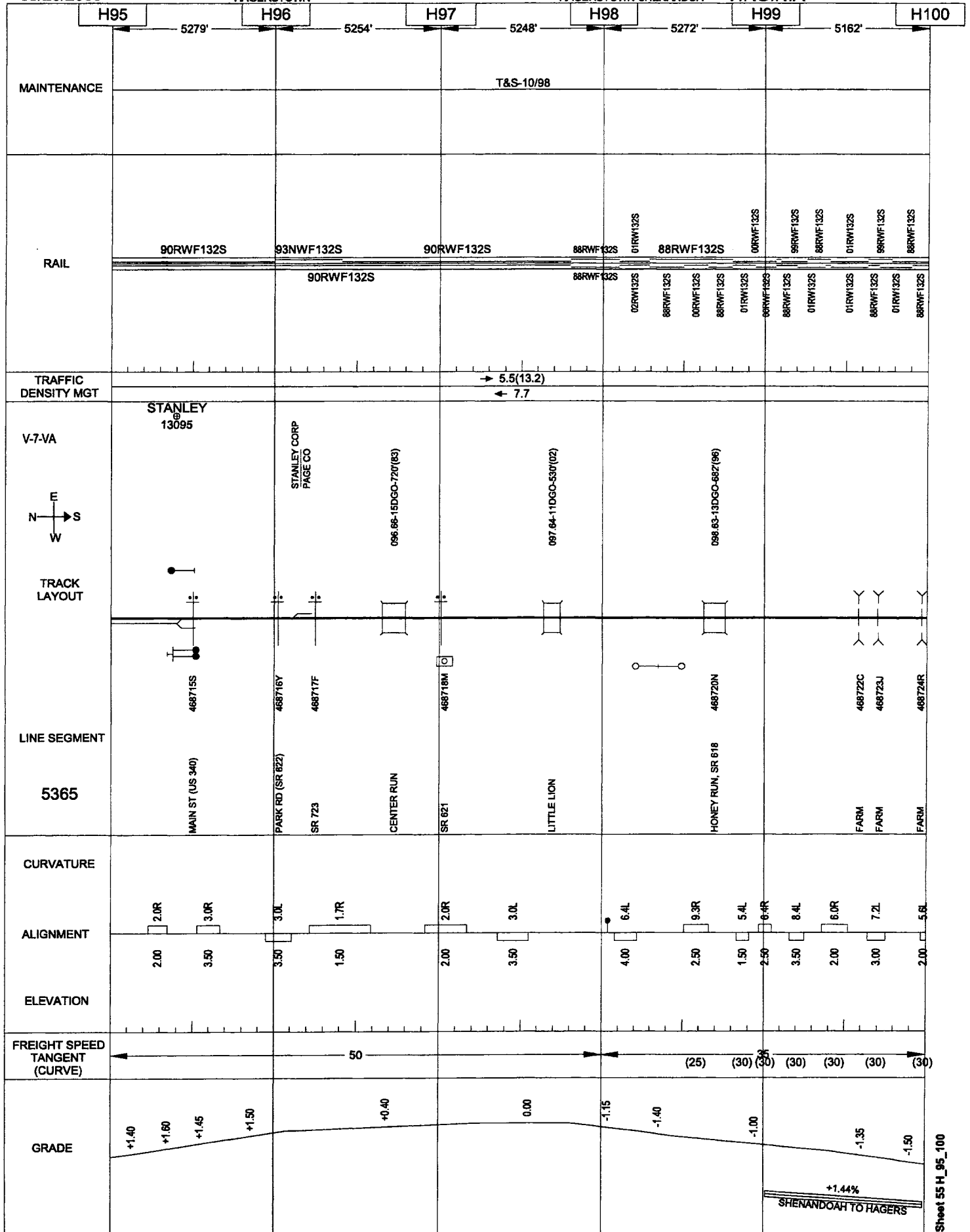


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HAGERSTOWN-SHENANDOAH

VIRGINIA

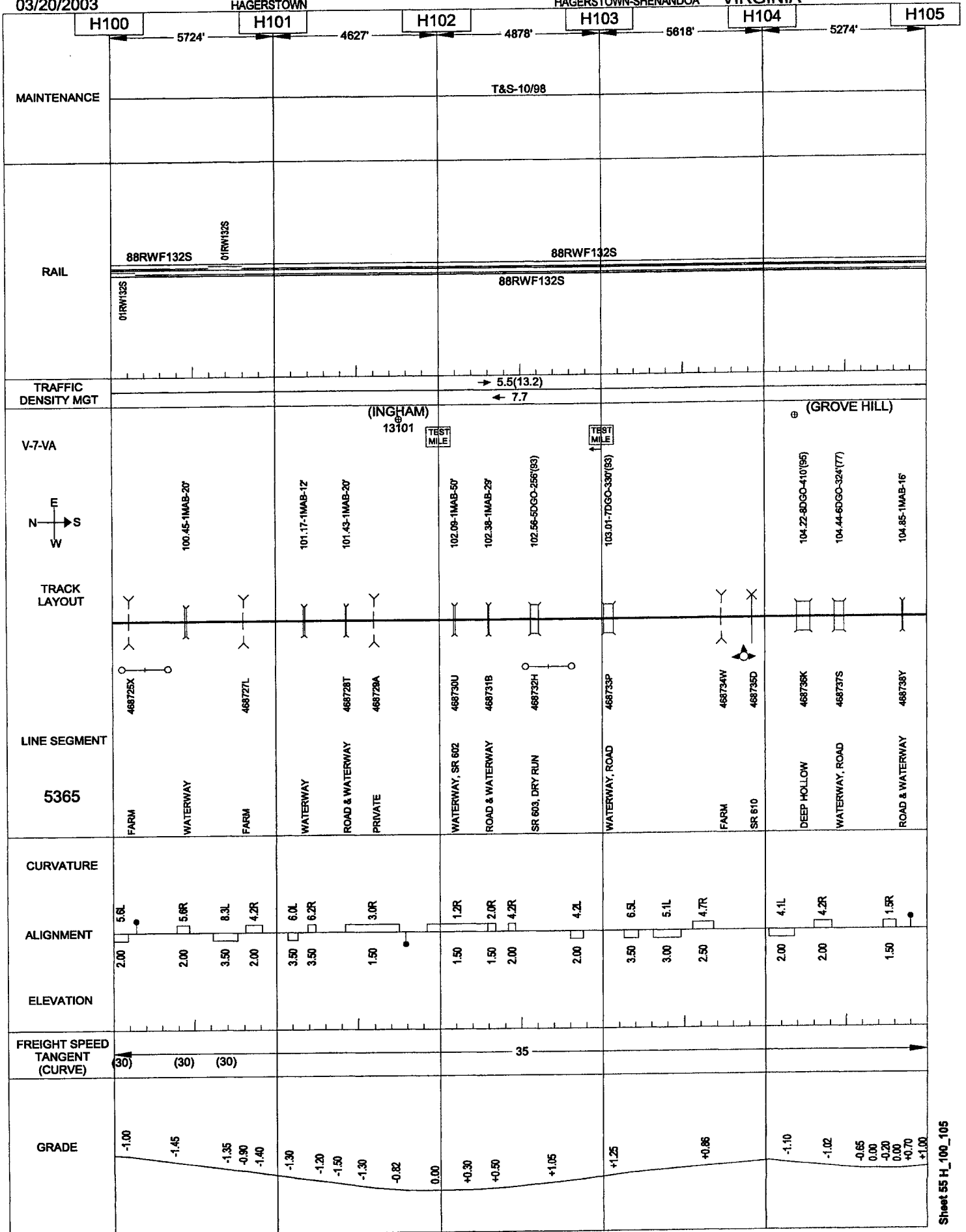


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HAGERSTOWN

HAGERSTOWN-SHENANDOAH

VIRGINIA

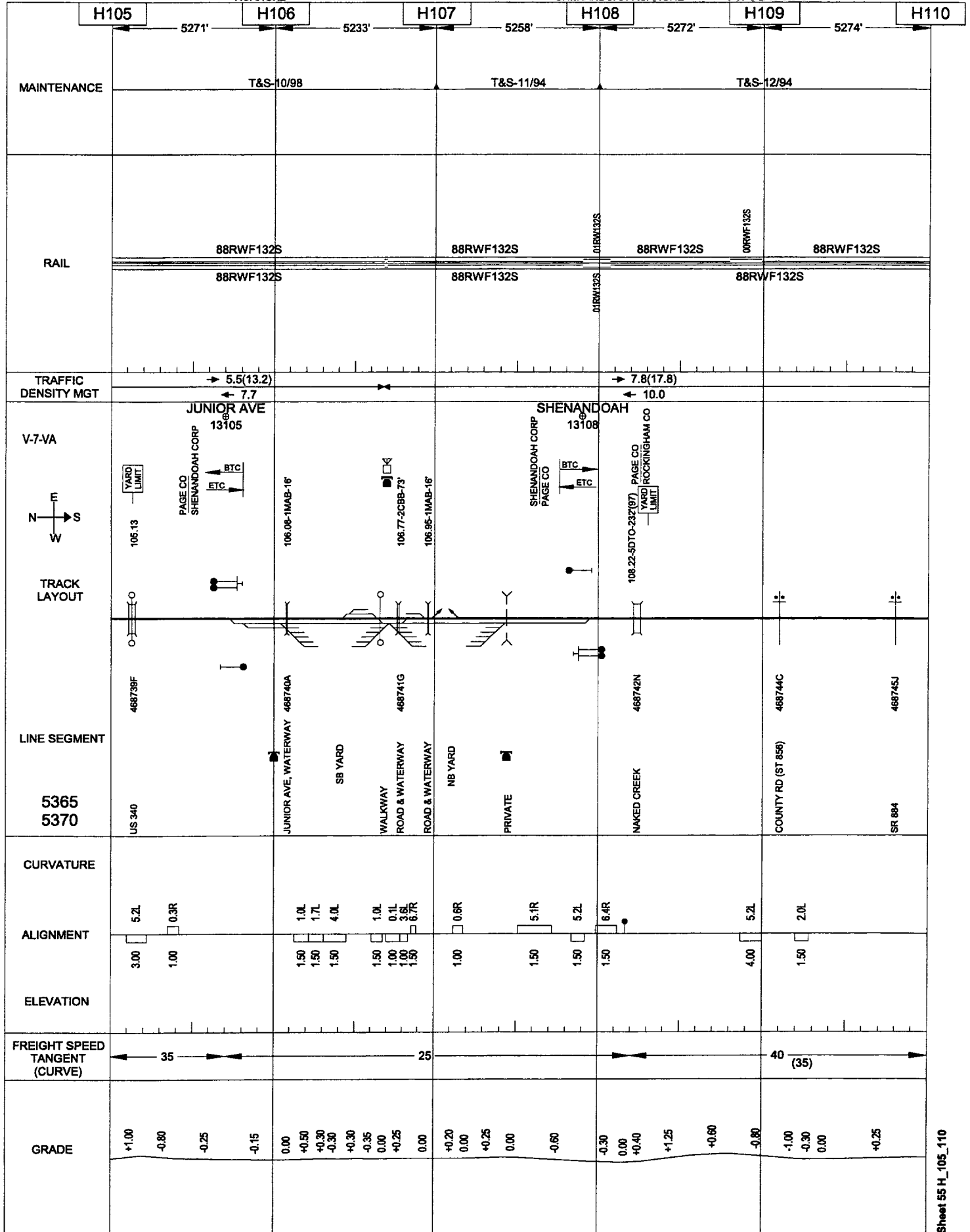


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

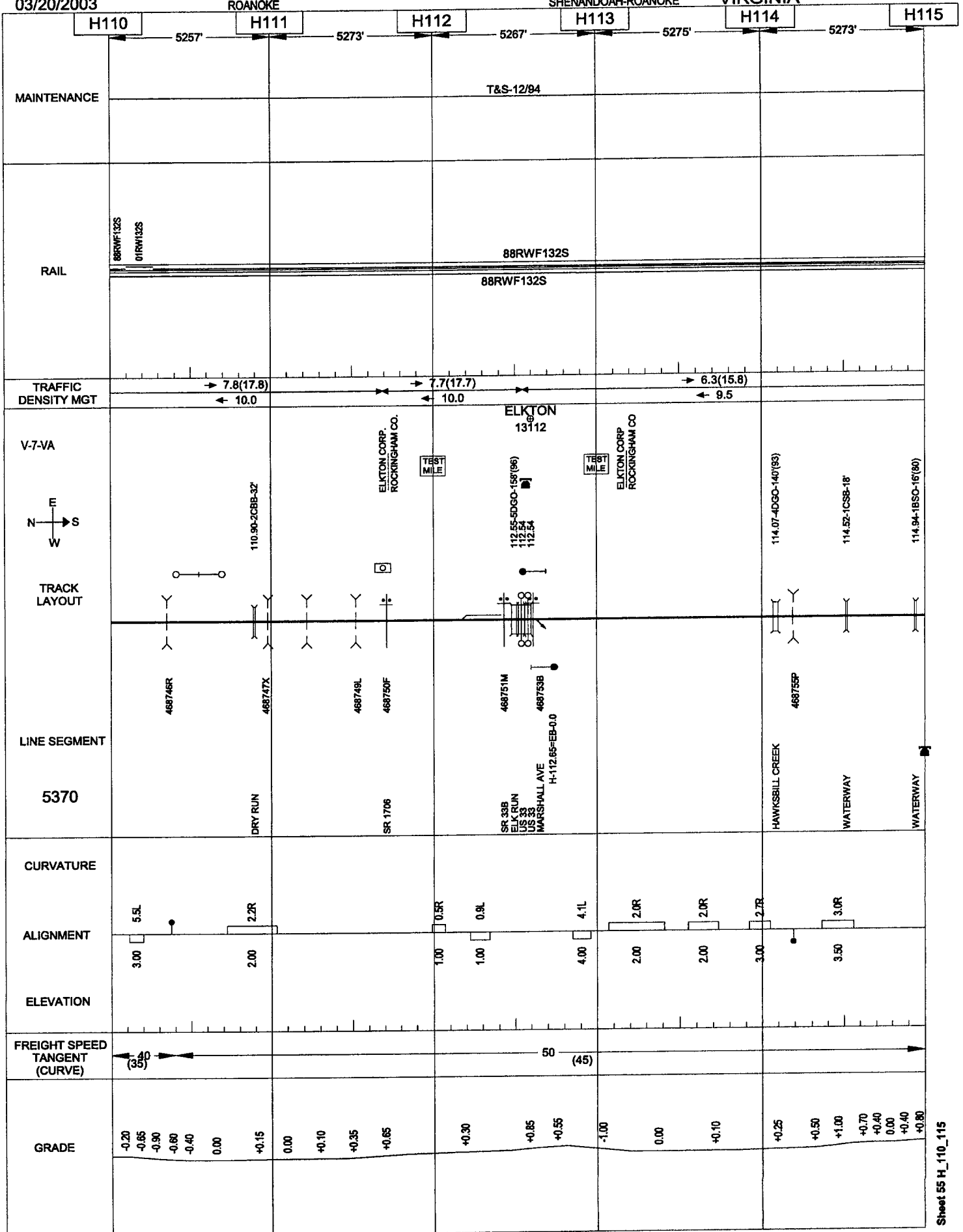


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

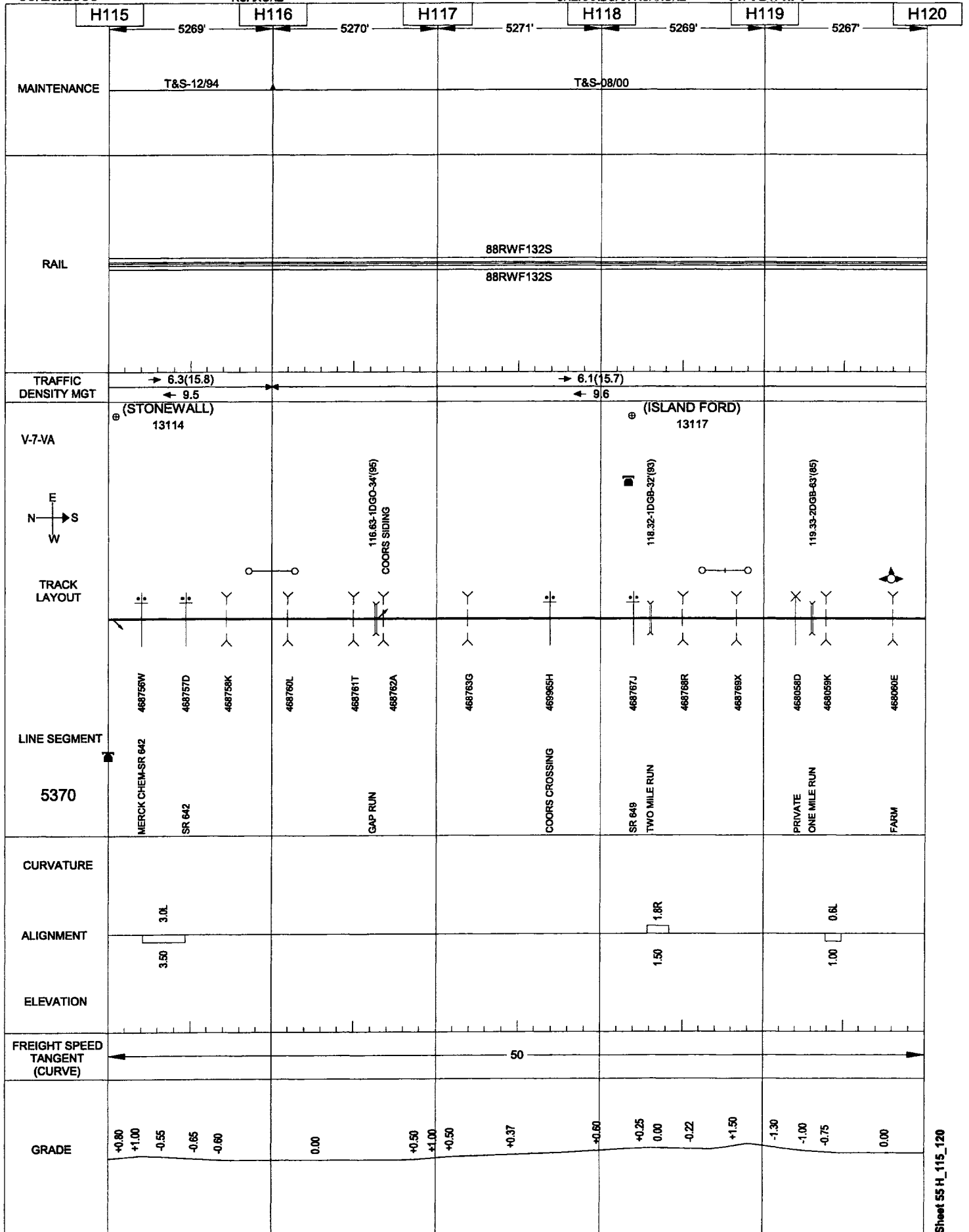


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

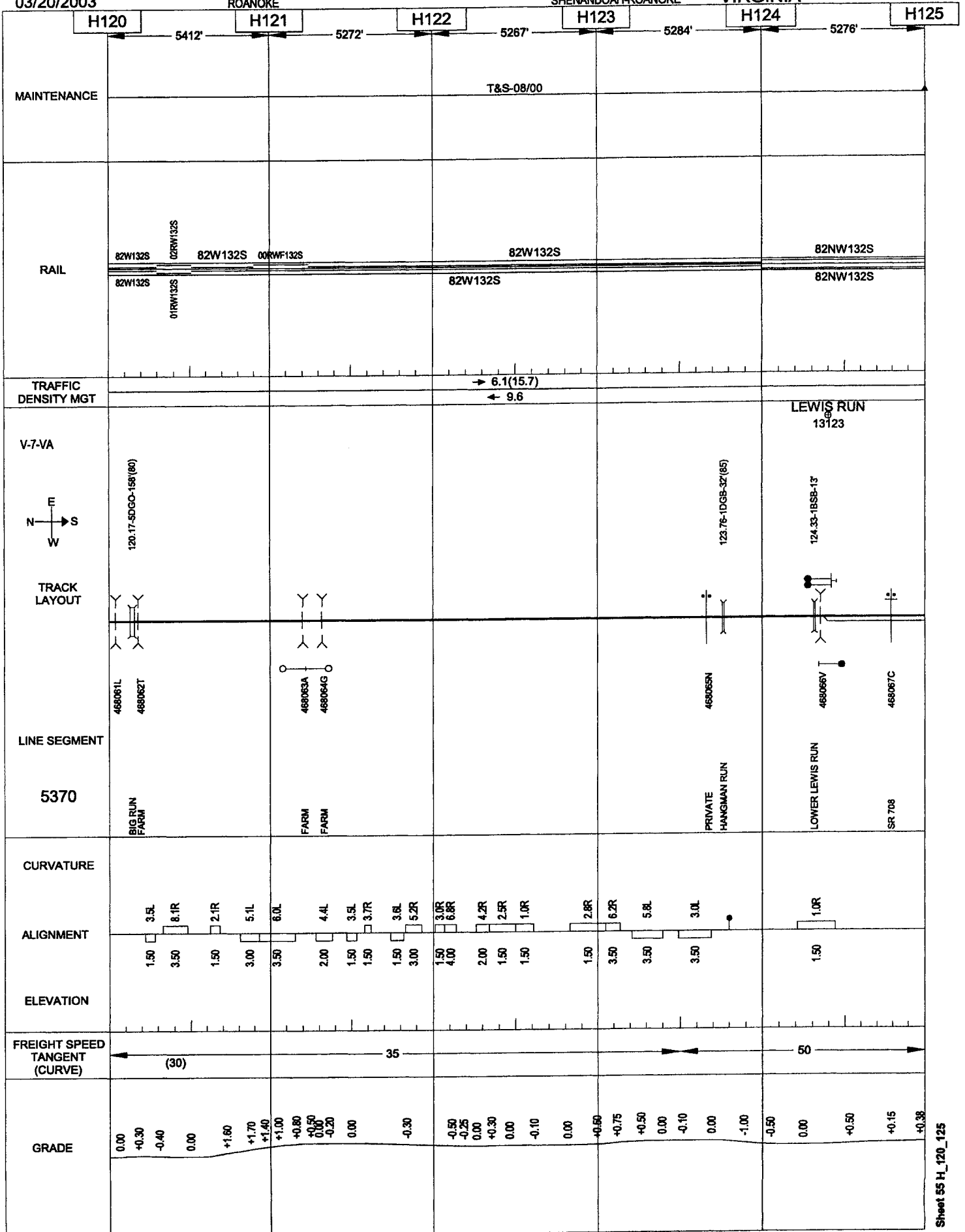


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

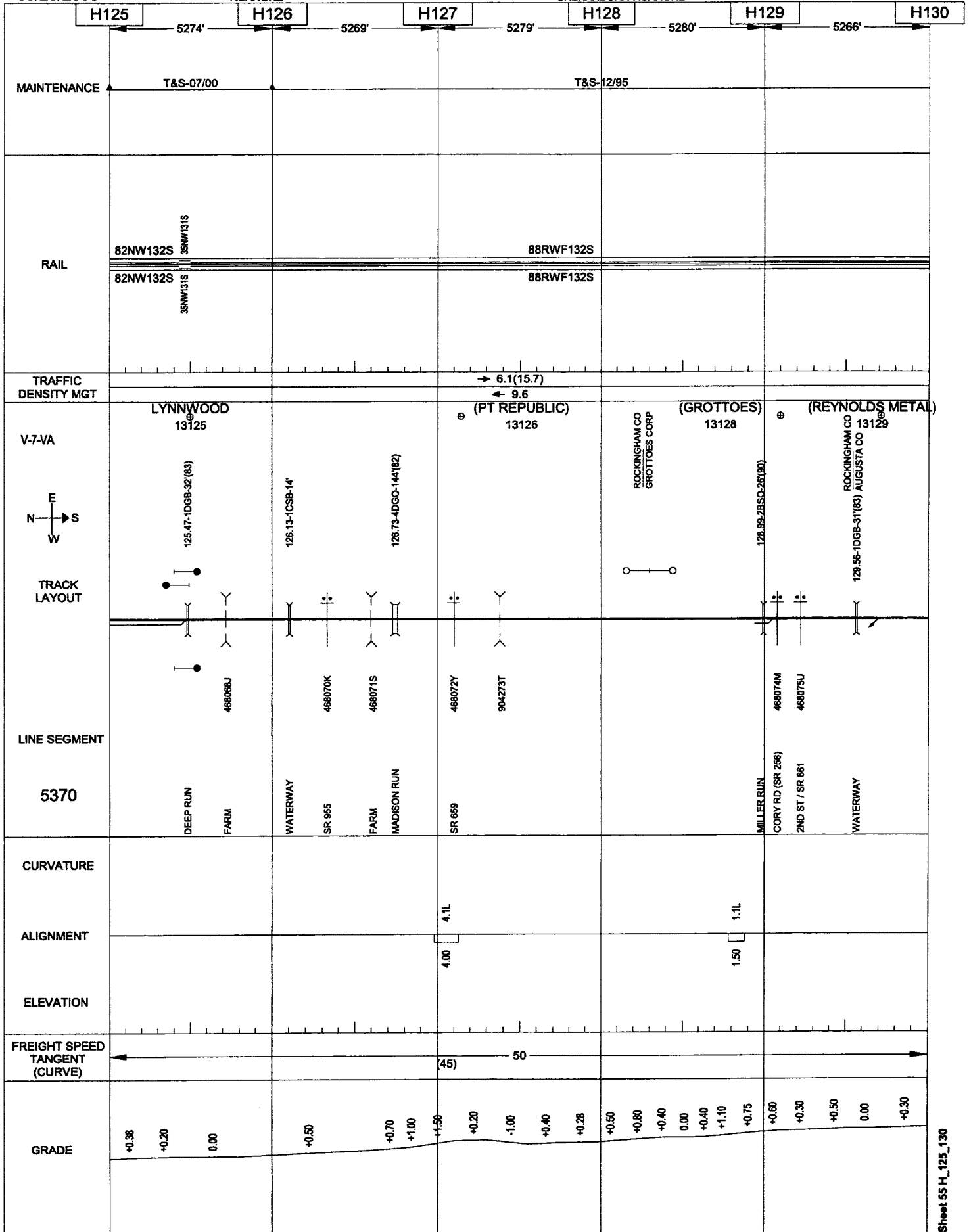


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

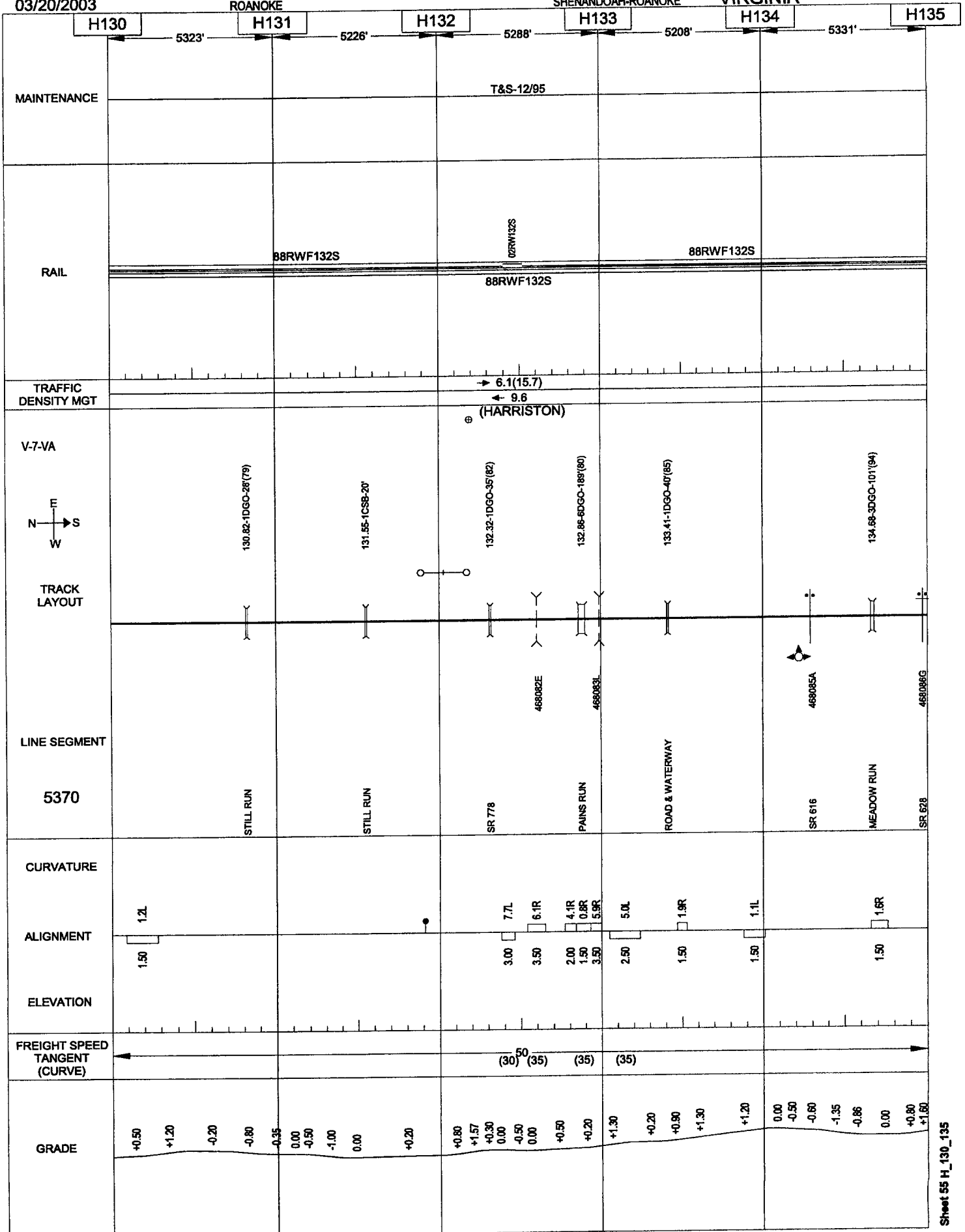


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ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

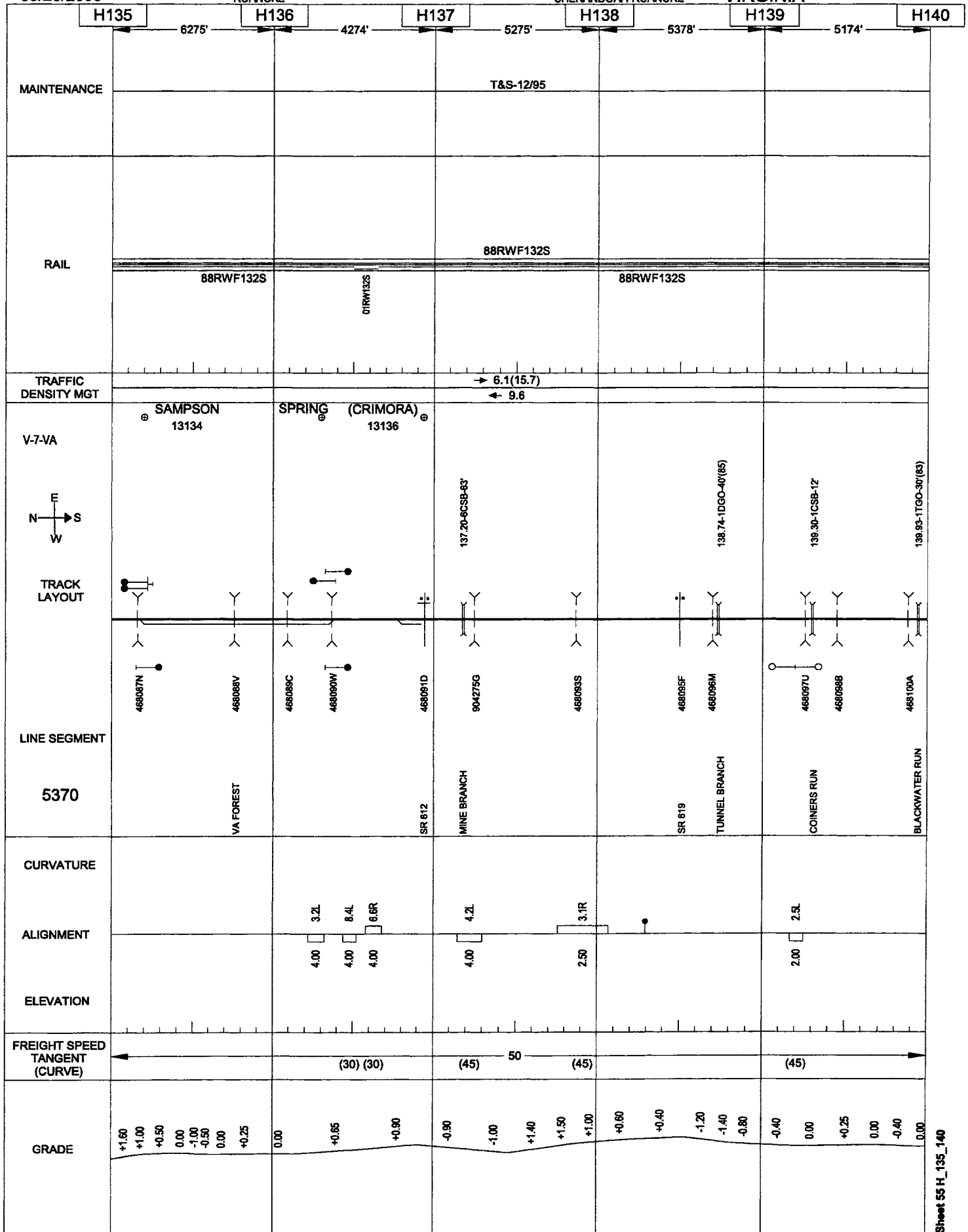


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

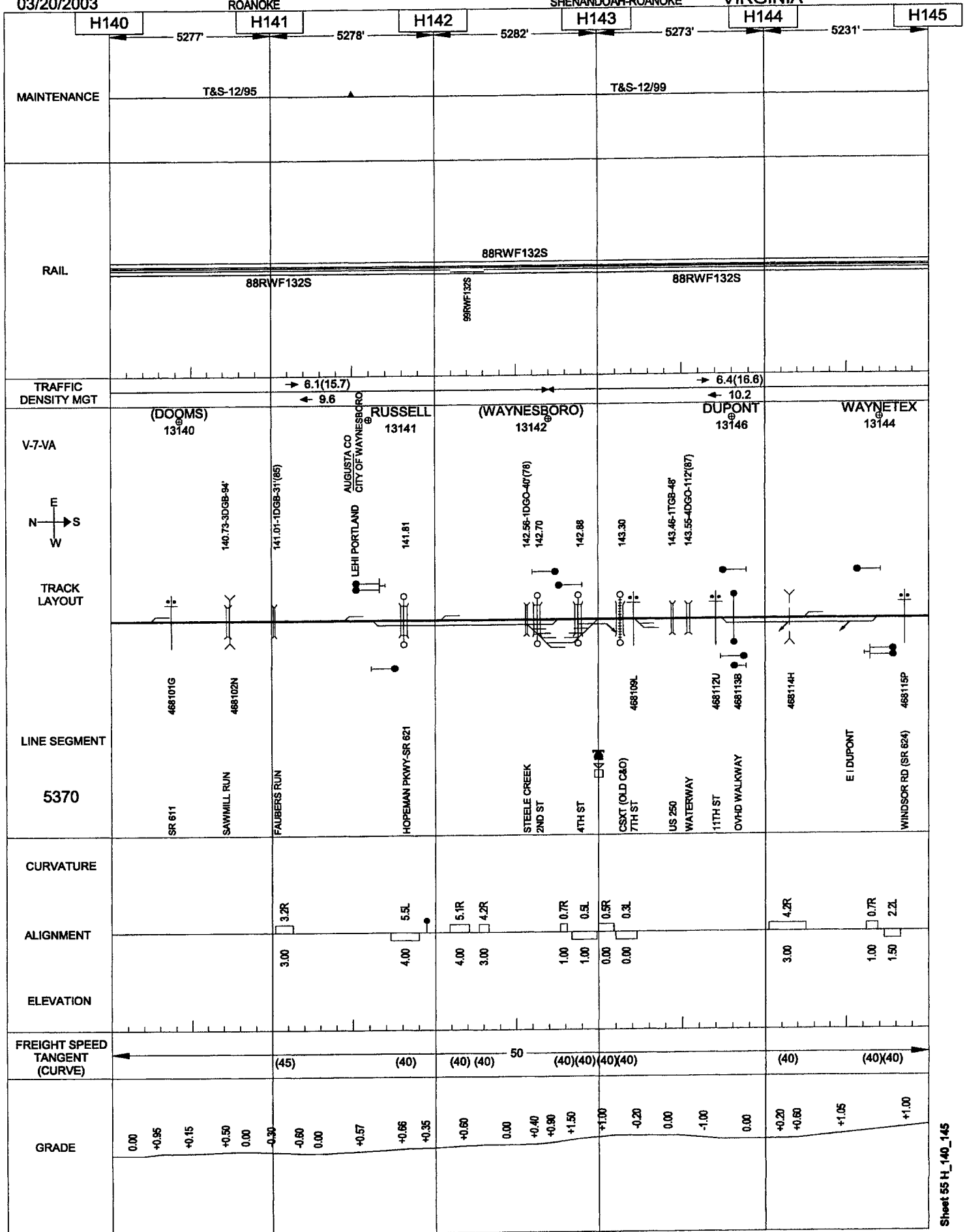


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

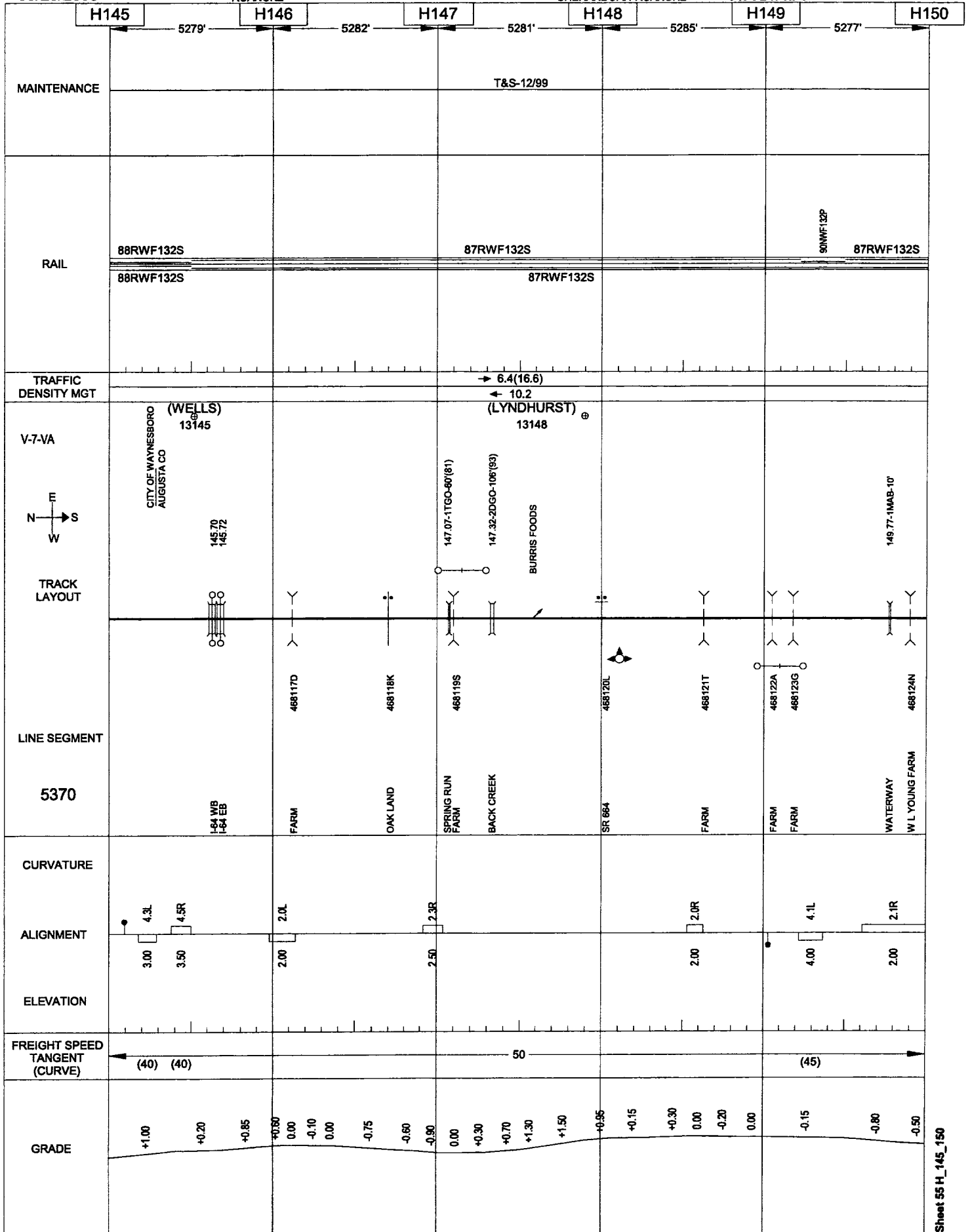


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

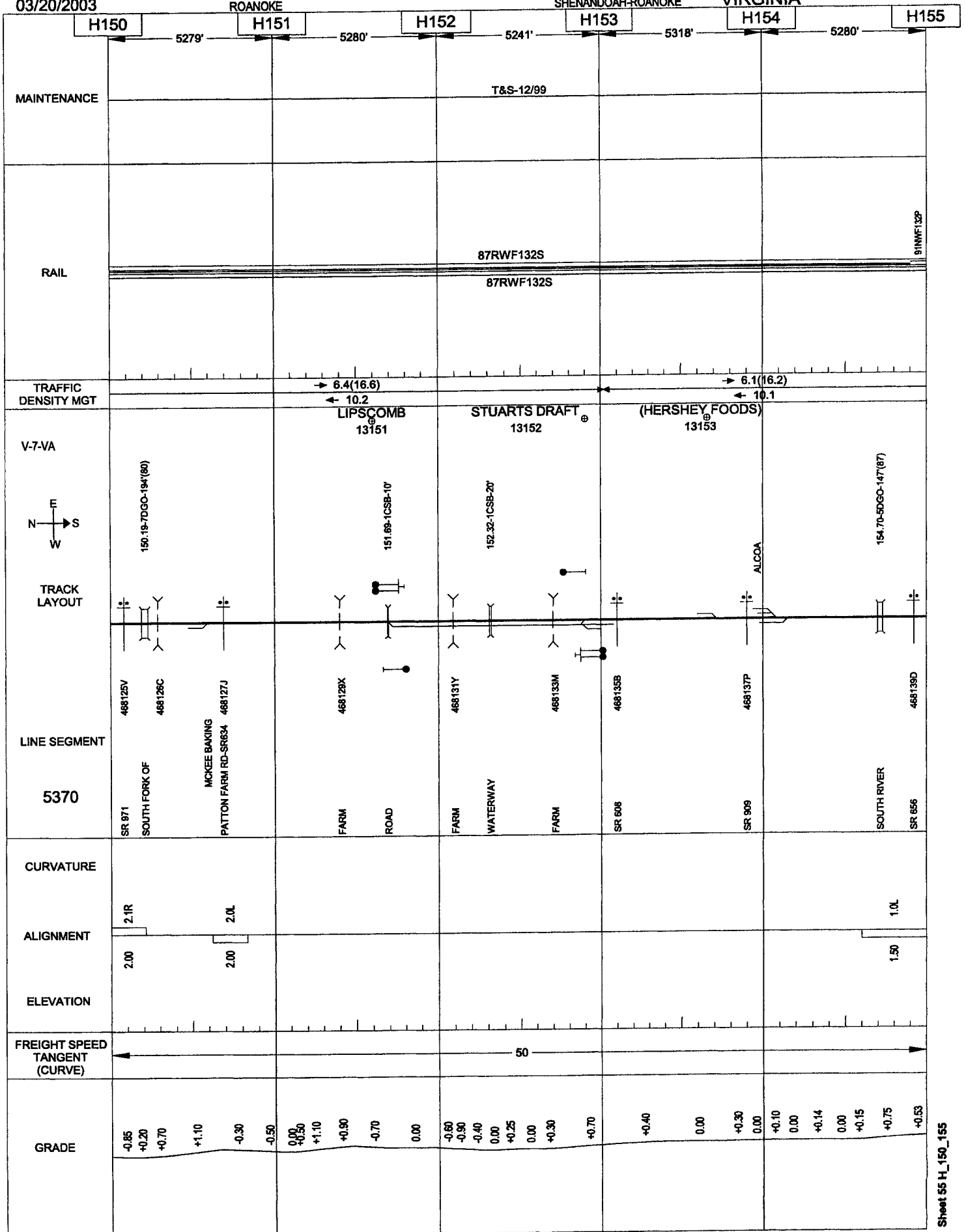


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

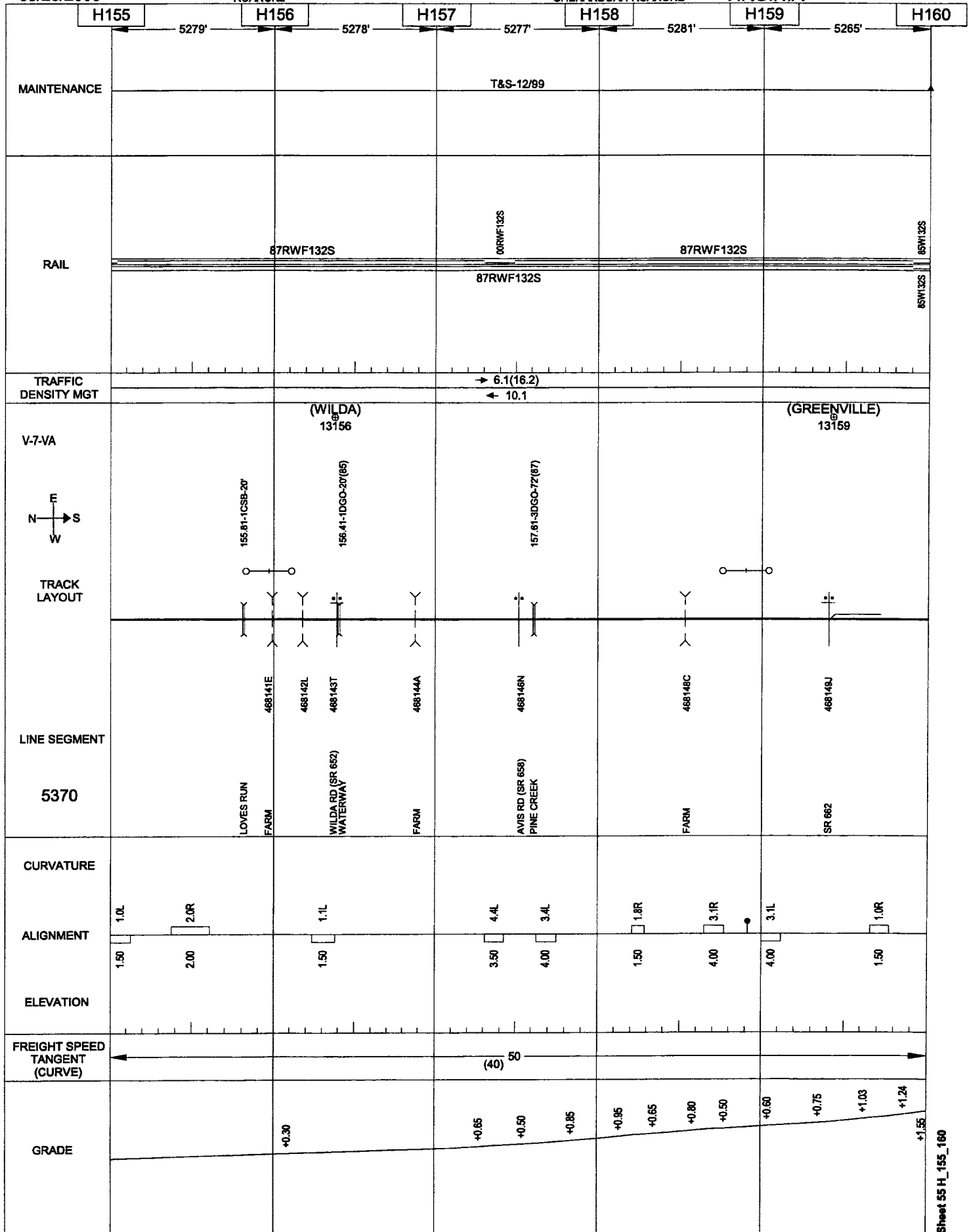


03/20/2003

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VIRGINIA

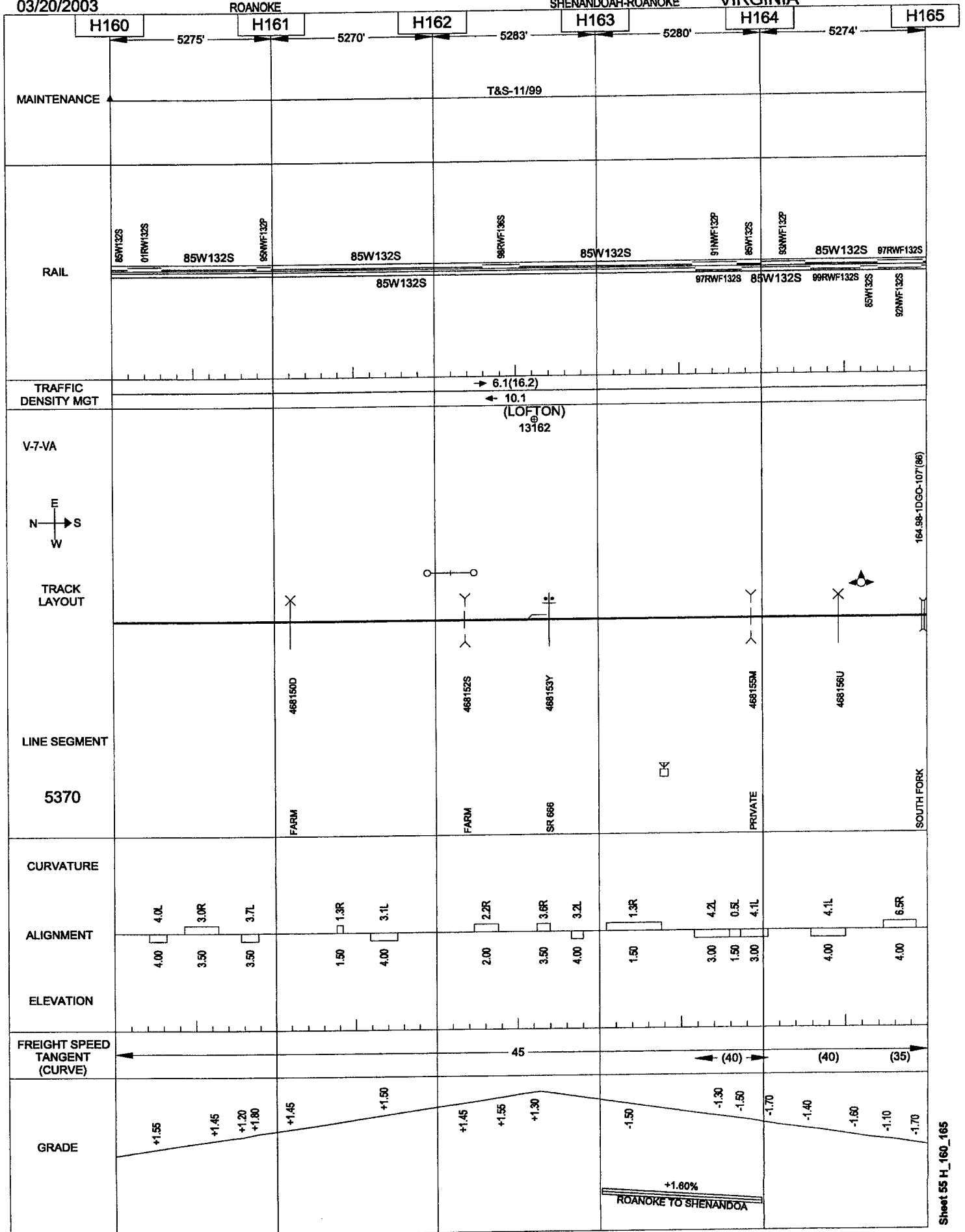


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

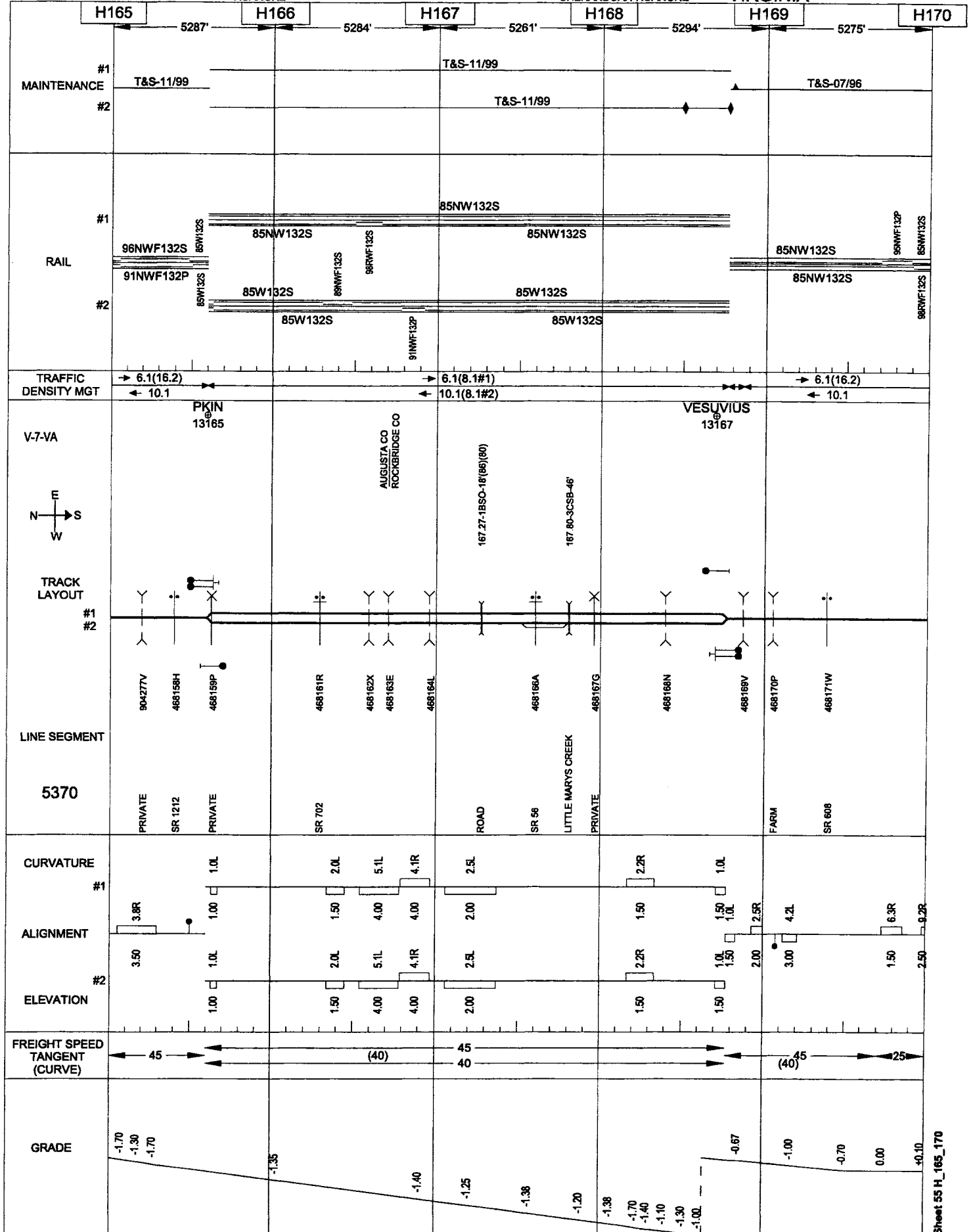


03/20/2003

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SHENANDOAH-ROANOKE

VIRGINIA

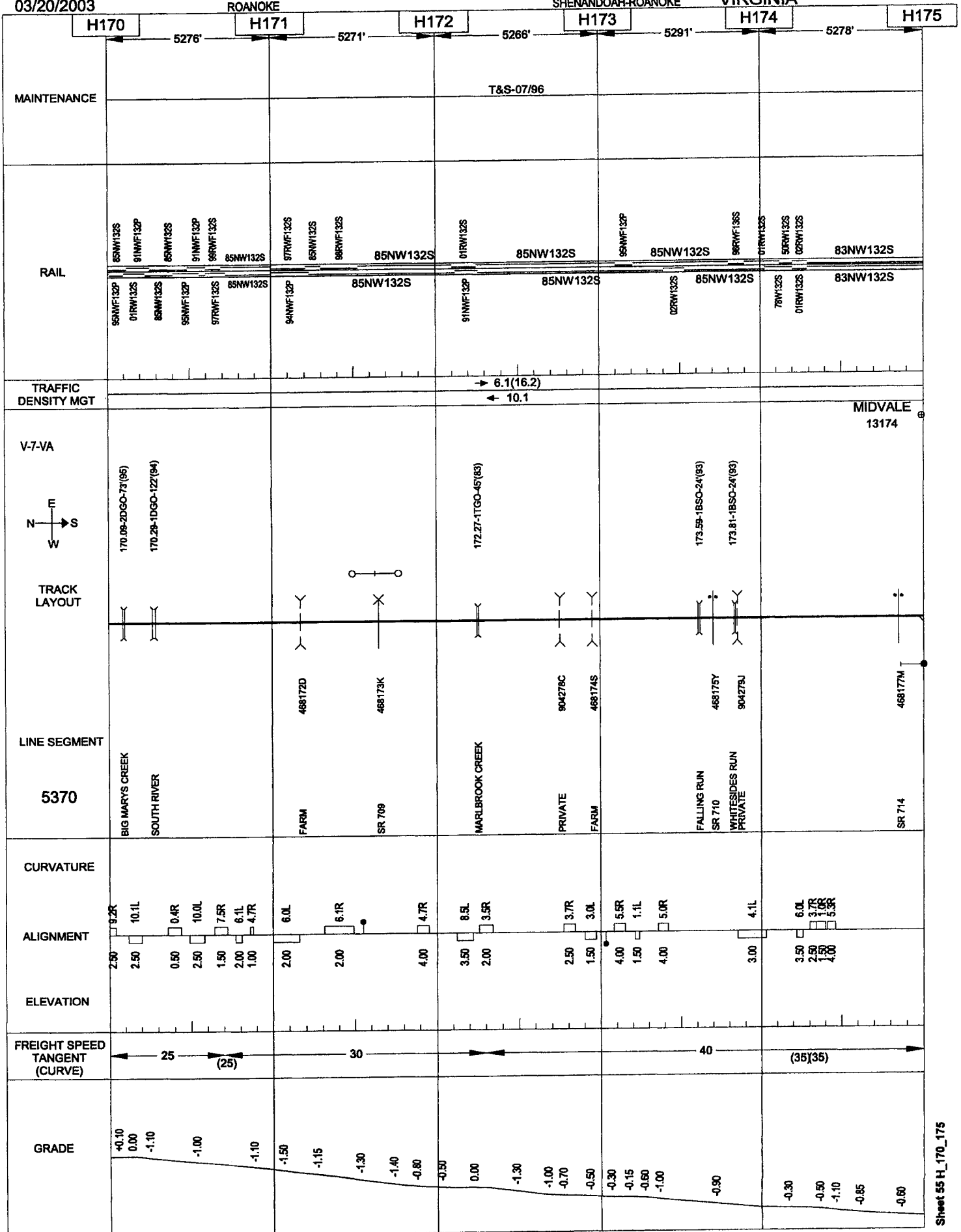


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VIRGINIA

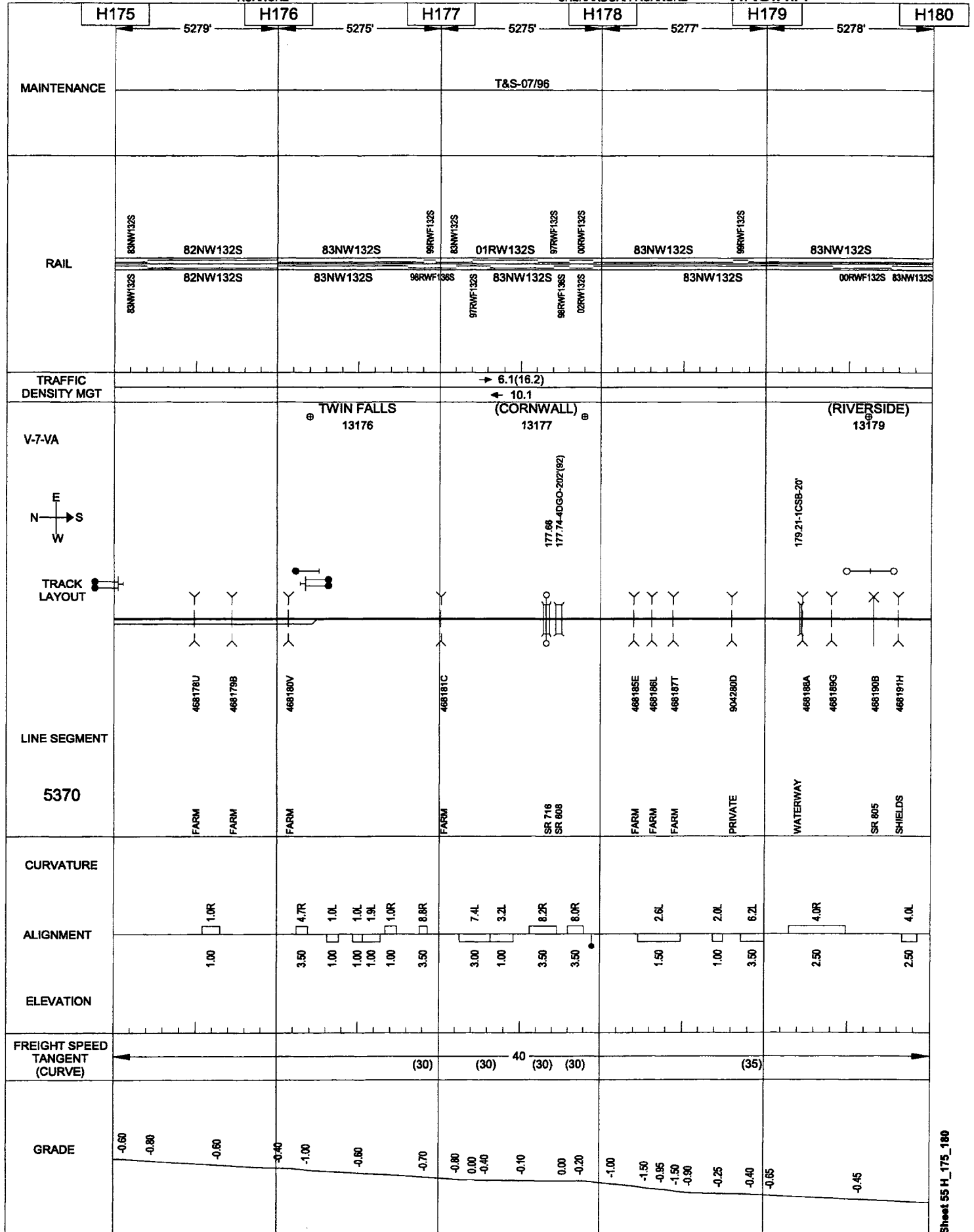


03/20/2003

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SHENANDOAH-ROANOKE

VIRGINIA

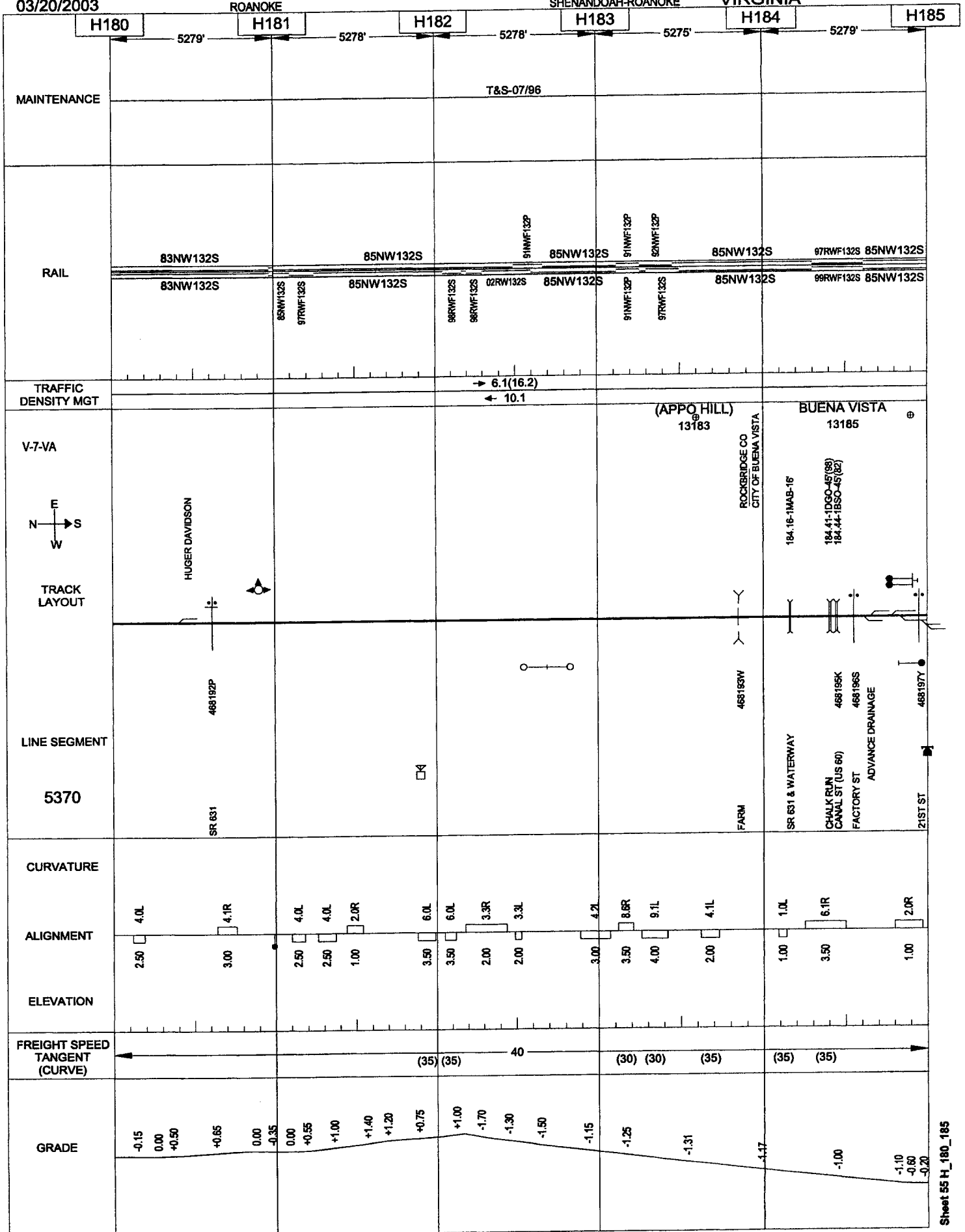


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA



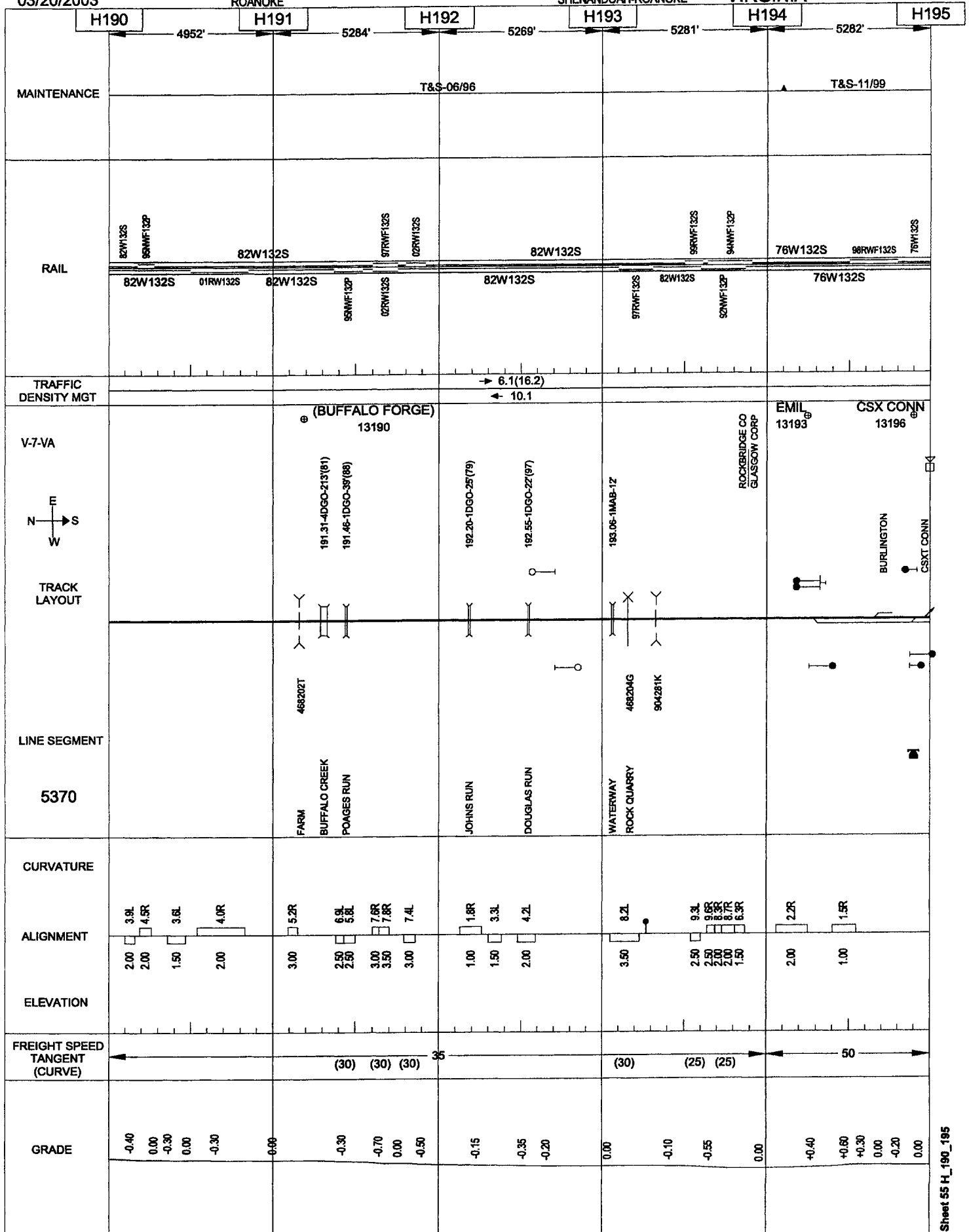
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03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA

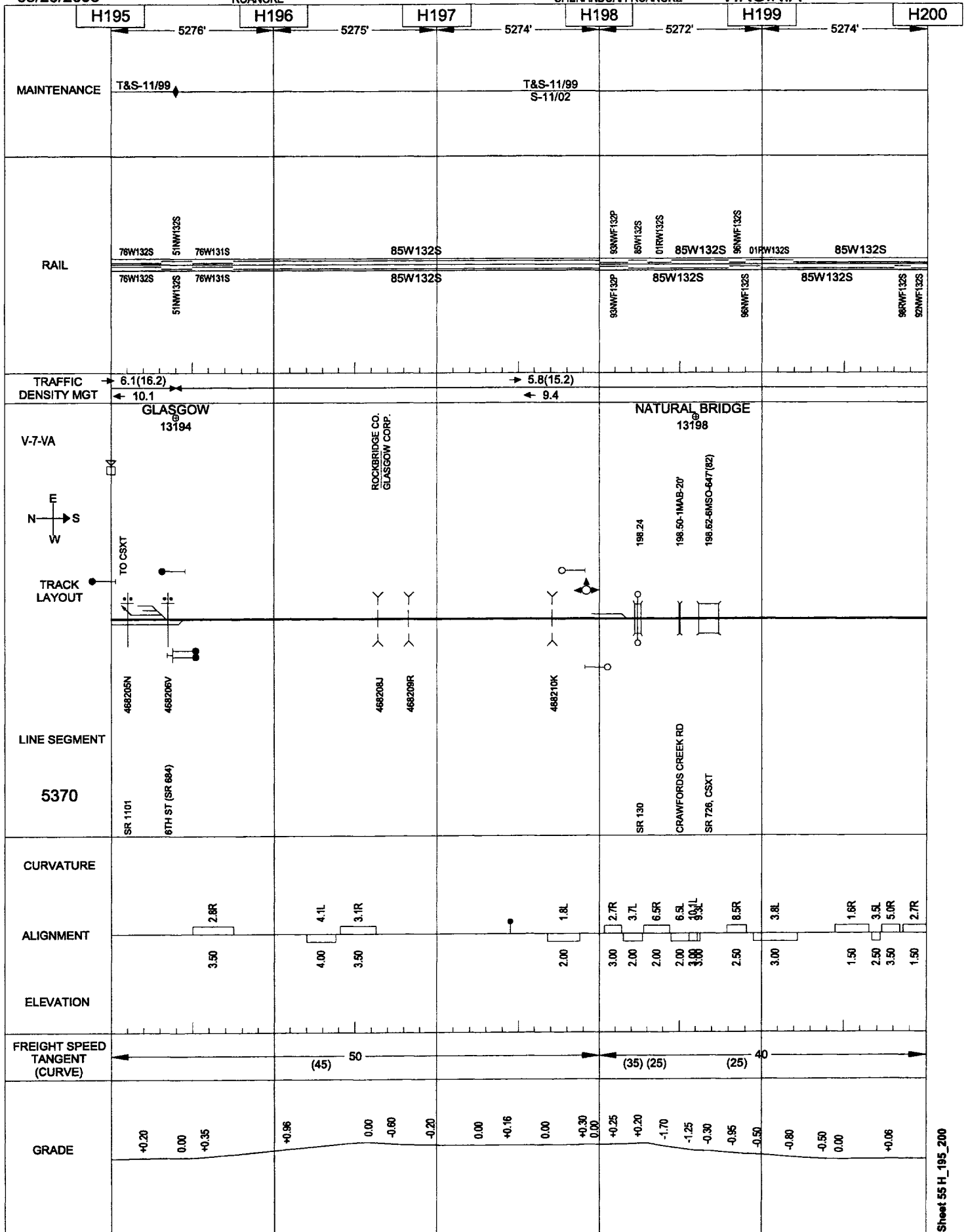


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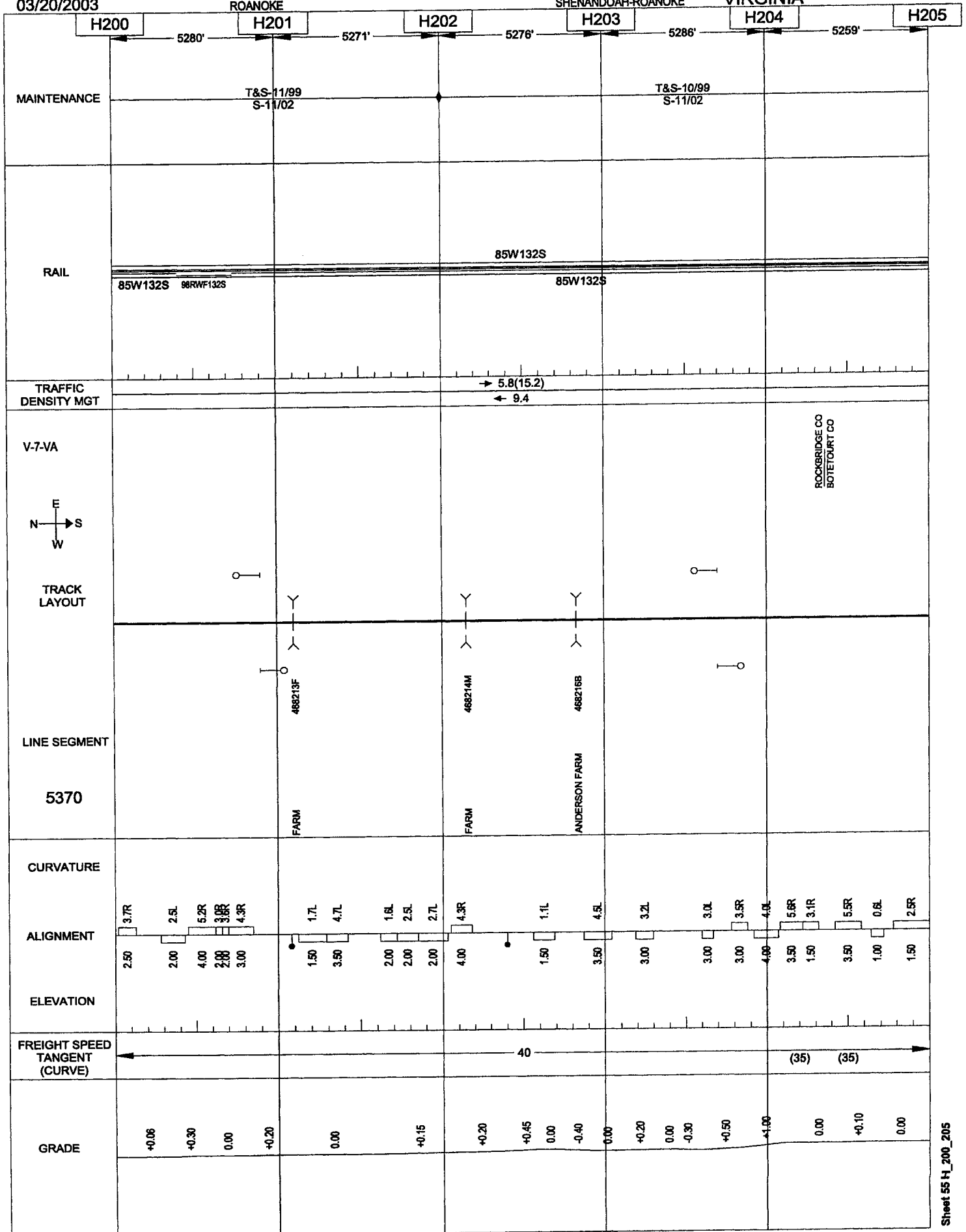


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SHENANDOAH-ROANOKE

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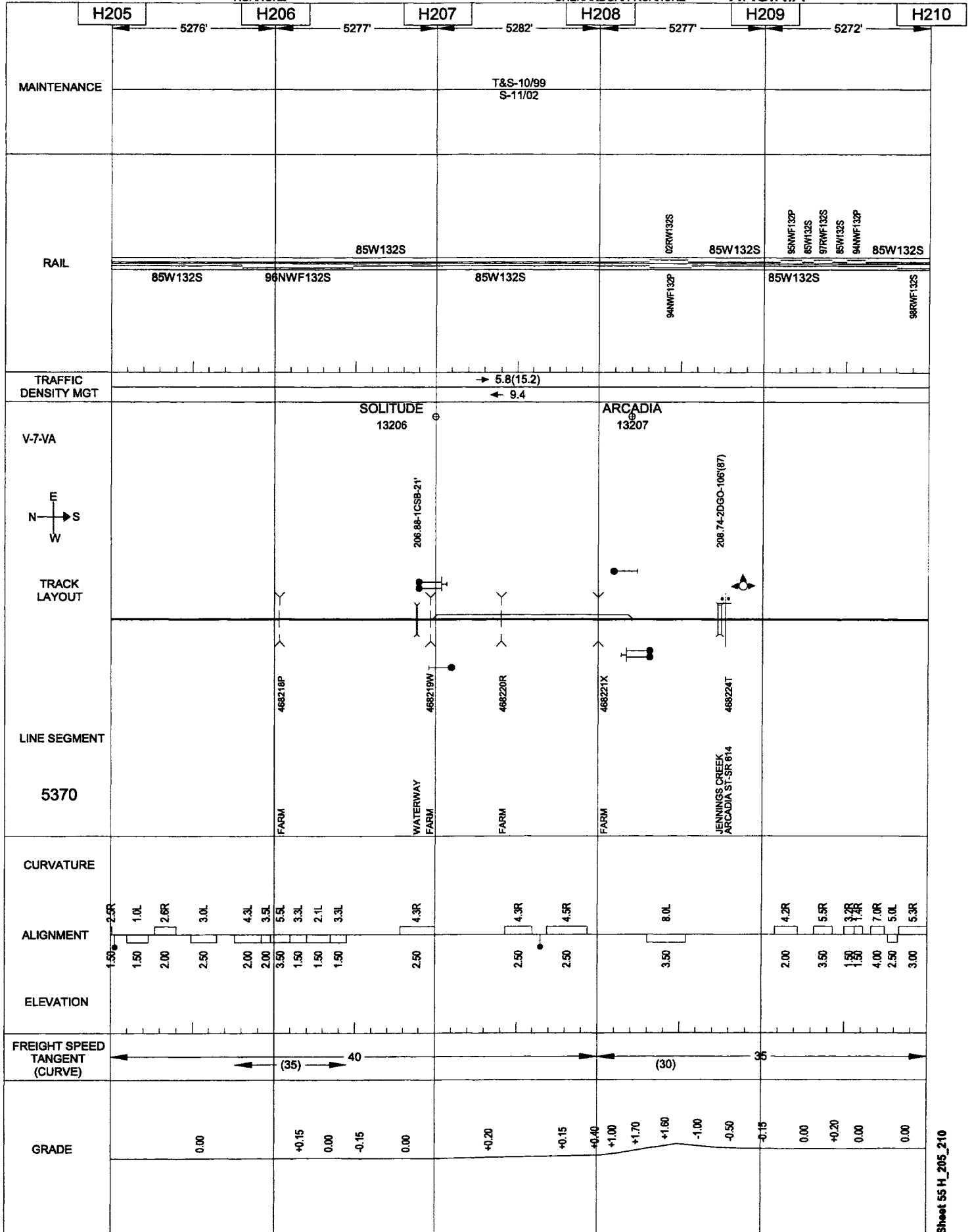


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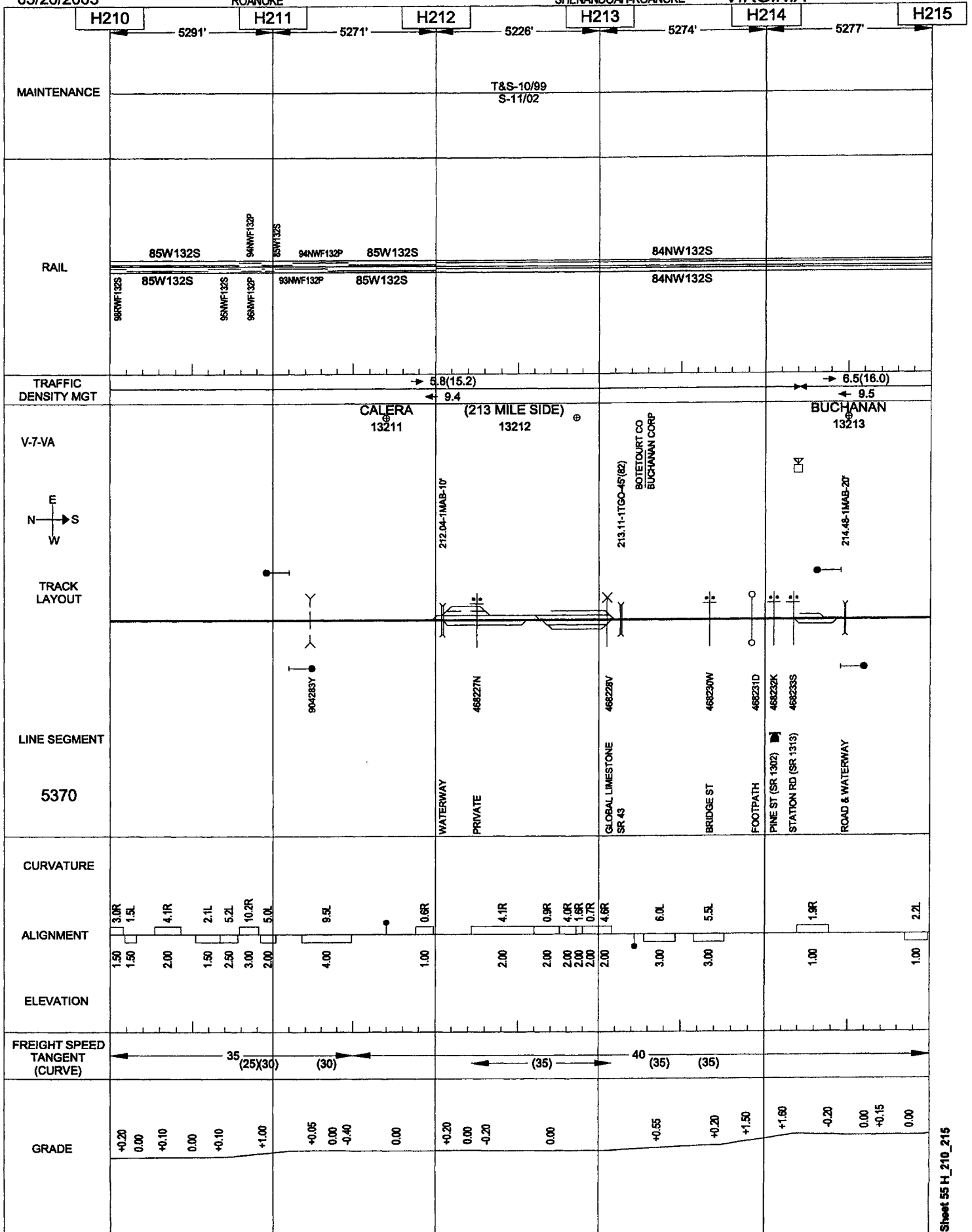


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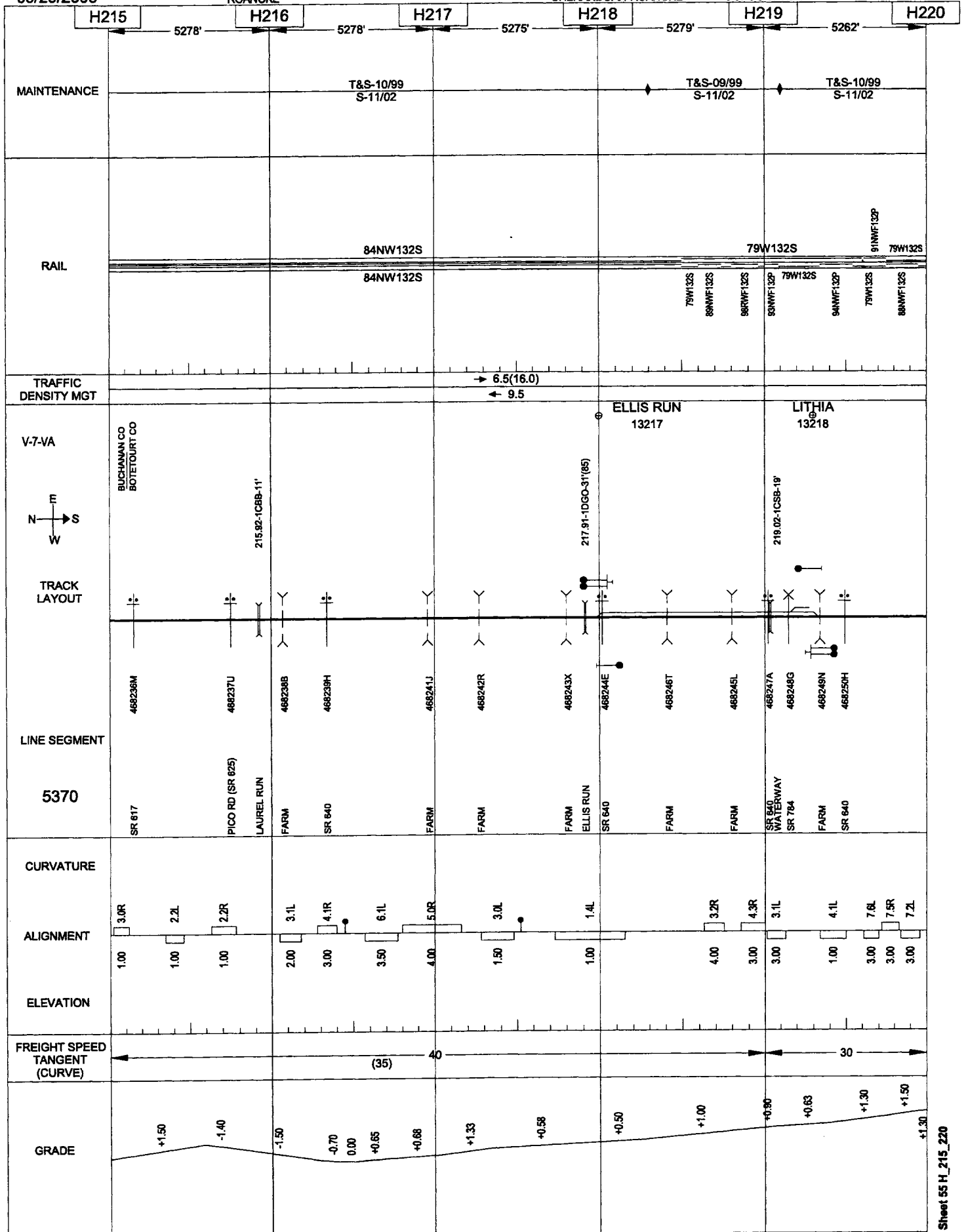


03/20/2003

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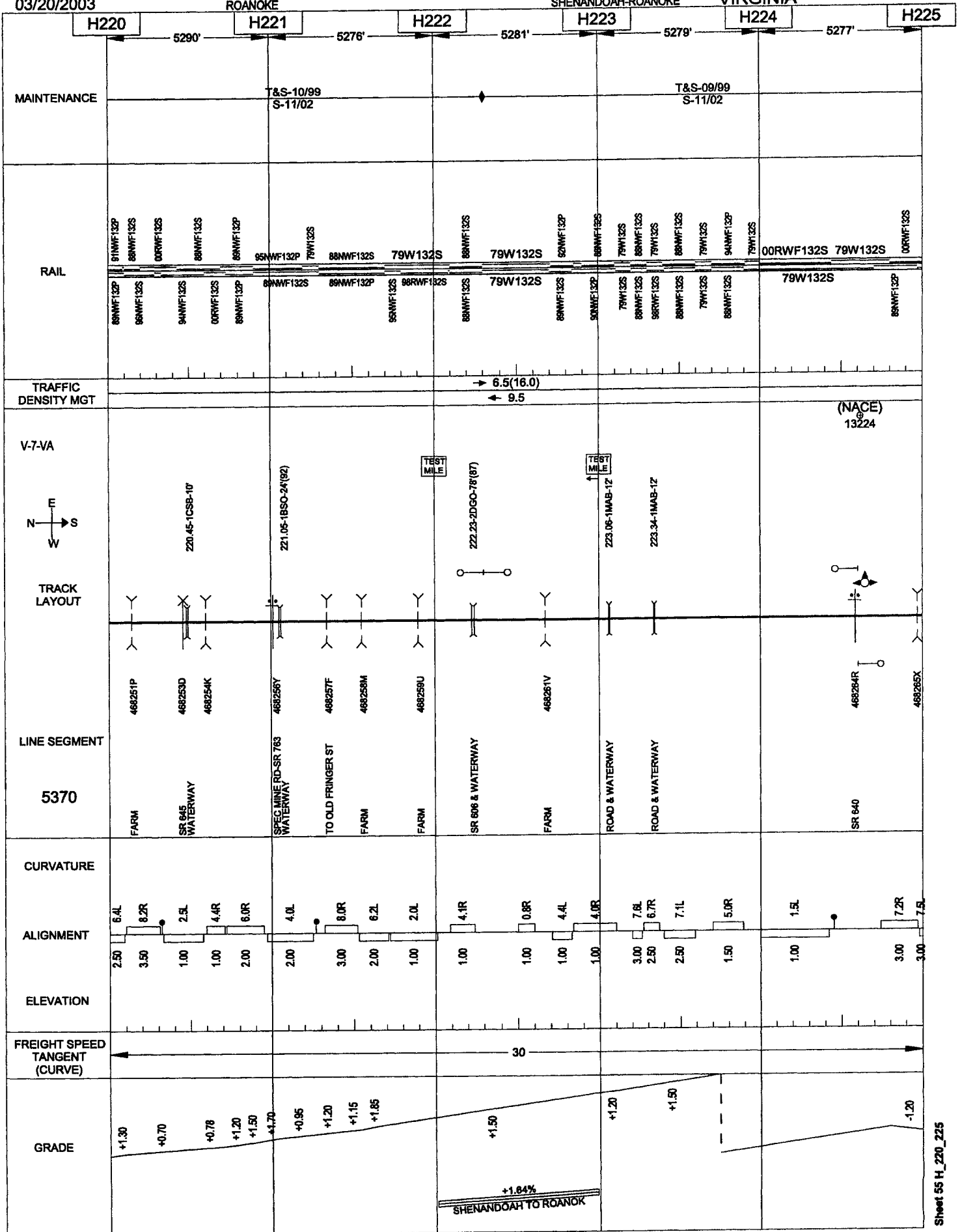


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SHENANDOAH-ROANOKE

VIRGINIA

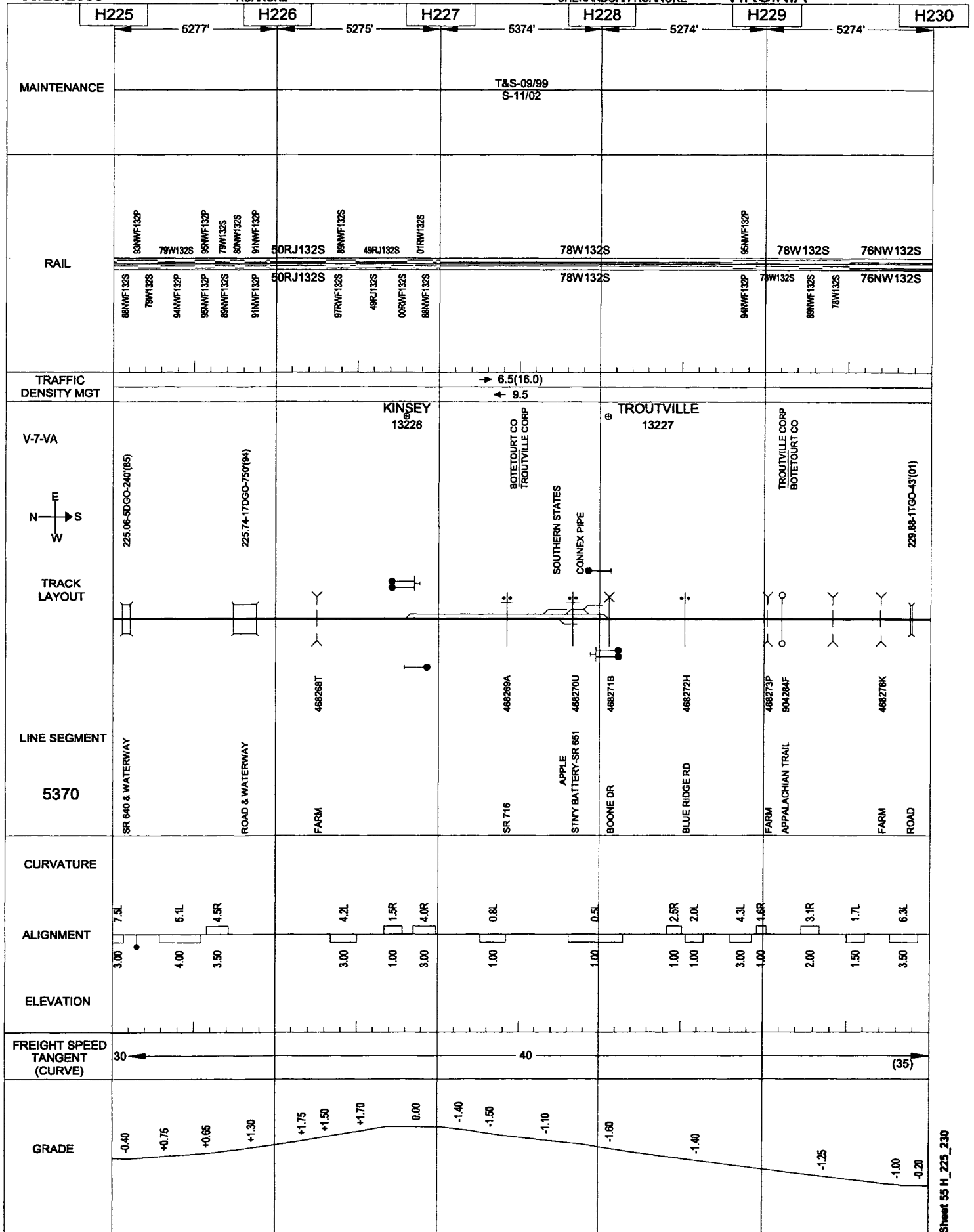


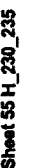
03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA



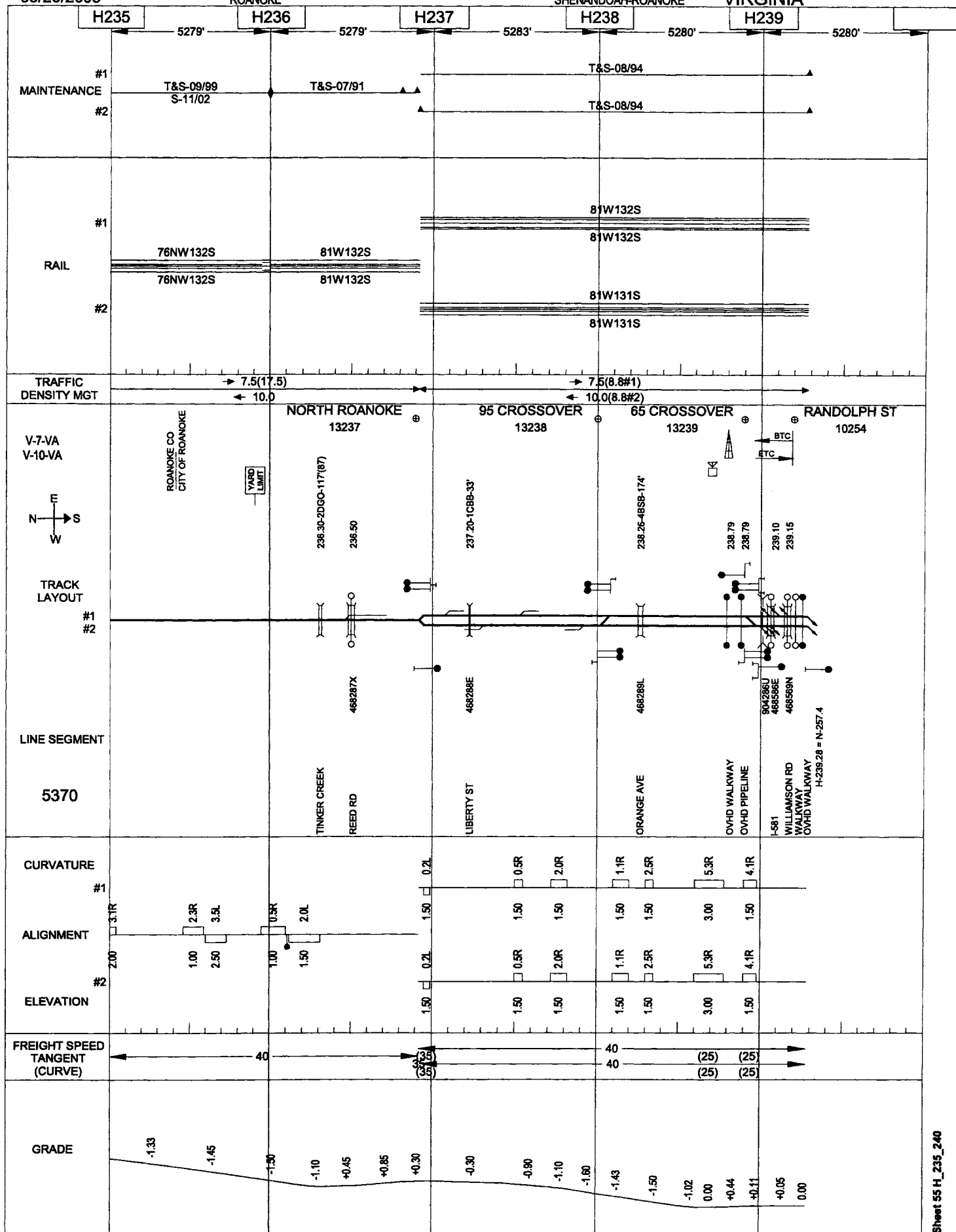


03/20/2003

ROANOKE

SHENANDOAH-ROANOKE

VIRGINIA



03/20/2003

HAGERSTOWN

WILLIAMSPORT I.T.

HAGERSTOWN-WILLIAMSP

VIRGINIA

HW75

1594'

5370'

MAINTENANCE

RAIL

TRAFFIC
DENSITY MGT

42N/130S
42N/130S

0.1

HAGER

E
N → S
W

TRACK
LAYOUT

HW-074.80H-1.31
074.87



534898T

LINE SEGMENT

2443

BURHANS BLVD

CURVATURE

ALIGNMENT

ELEVATION

2.0R
1.00

FREIGHT SPEED
TANGENT
(CURVE)

10

GRADE

+0.75
-0.31

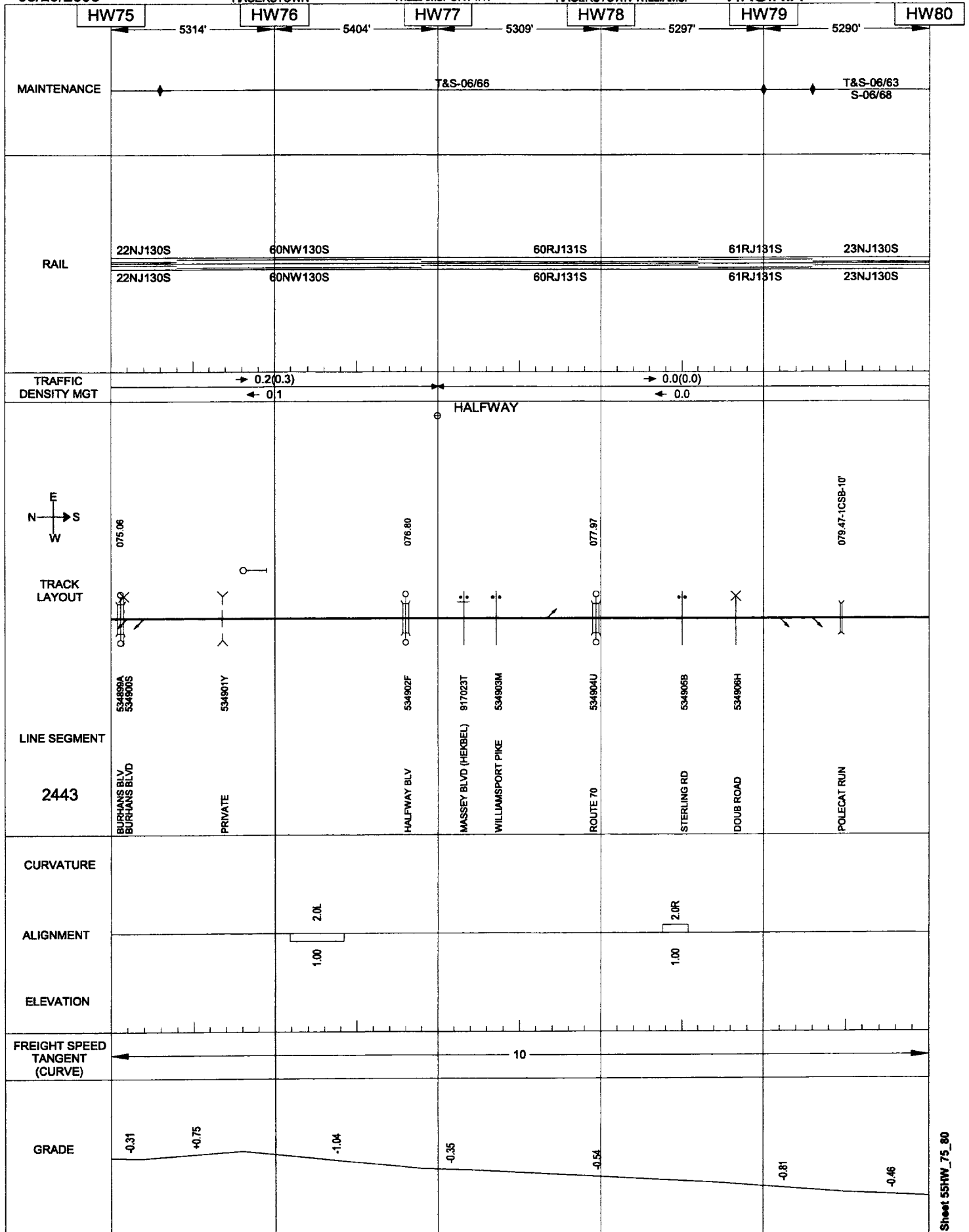
03/20/2003

HAGERSTOWN

WILLIAMSPORT I.T.

HAGERSTOWN-WILLIAMSP

VIRGINIA



03/20/2003

HAGERSTOWN

WILLIAMSPORT I.T.

HAGERSTOWN-WILLIAMSP

VIRGINIA

HW80

5286'

MAINTENANCE

T&S-06/63
S-06/68

RAIL

23NJ130S
23NJ130S

TRAFFIC
DENSITY MGT

0.0(0.0)
0.0

E
N — S
W

TRACK
LAYOUT

LINE SEGMENT

2443

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

10

GRADE

0.00

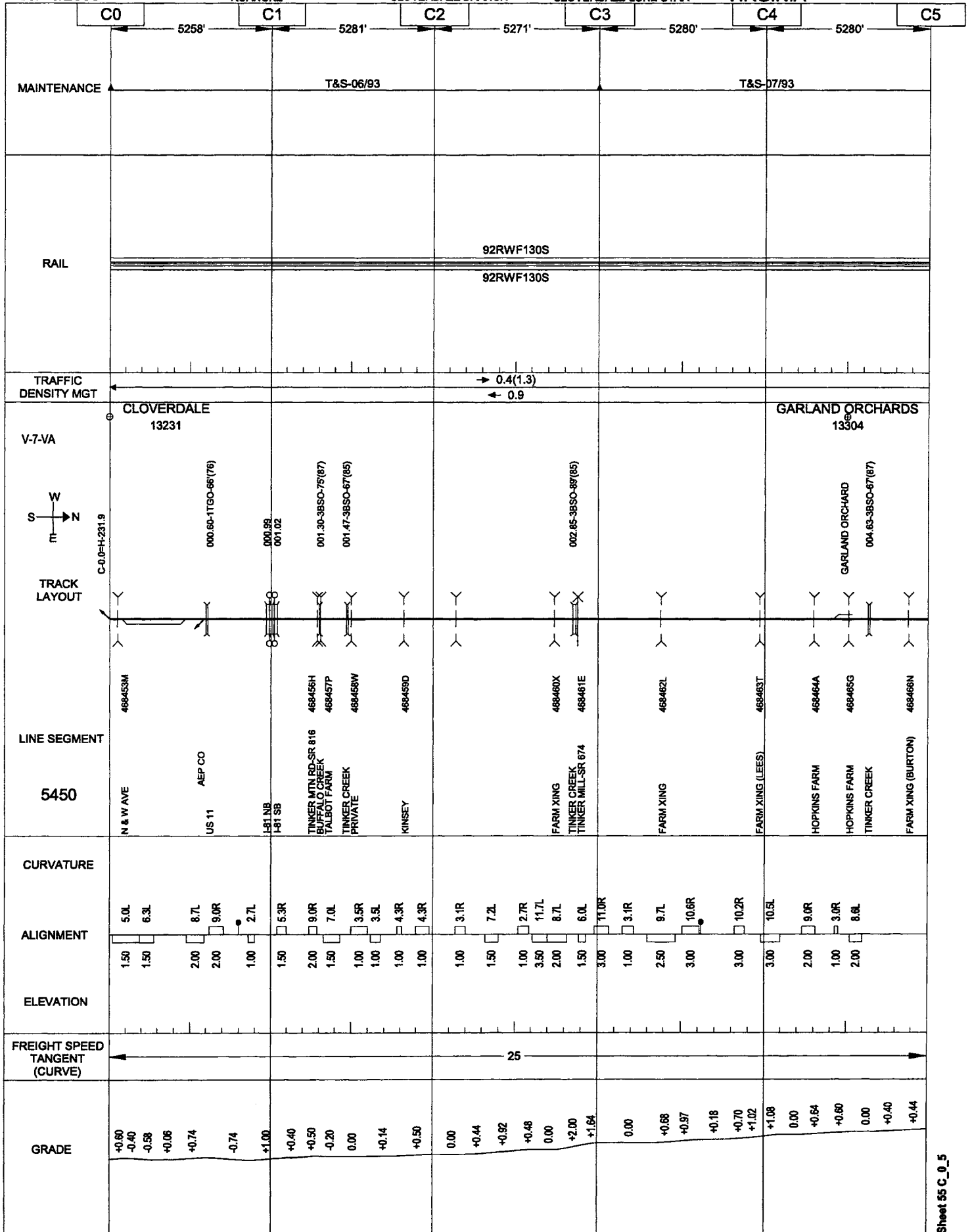
03/20/2003

ROANOKE

CLOVERDALE BRANCH

CLOVERDALE-LONE STAR

VIRGINIA



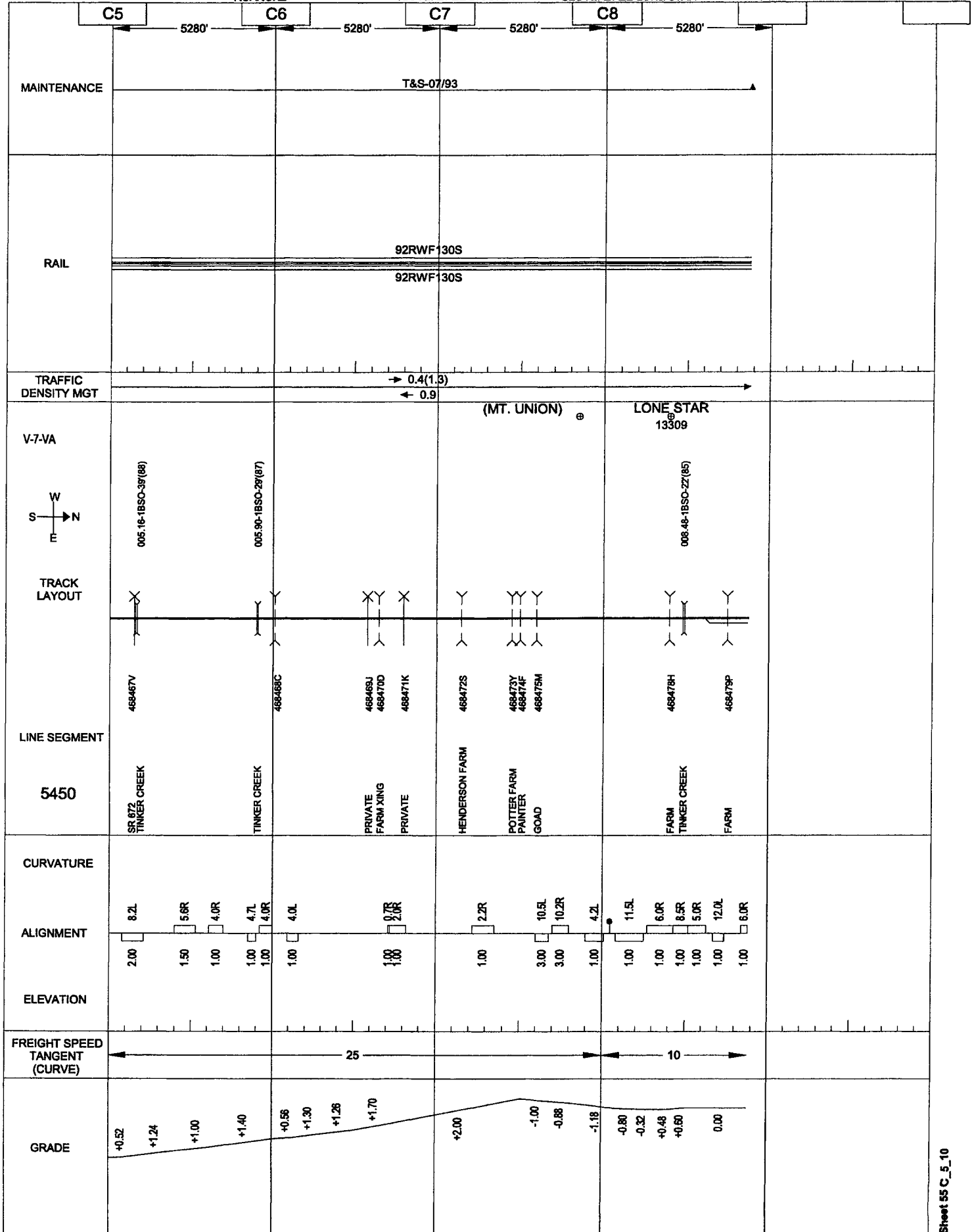
03/20/2003

ROANOKE

CLOVERDALE BRANCH

CLOVERDALE-LONE STAR

VIRGINIA

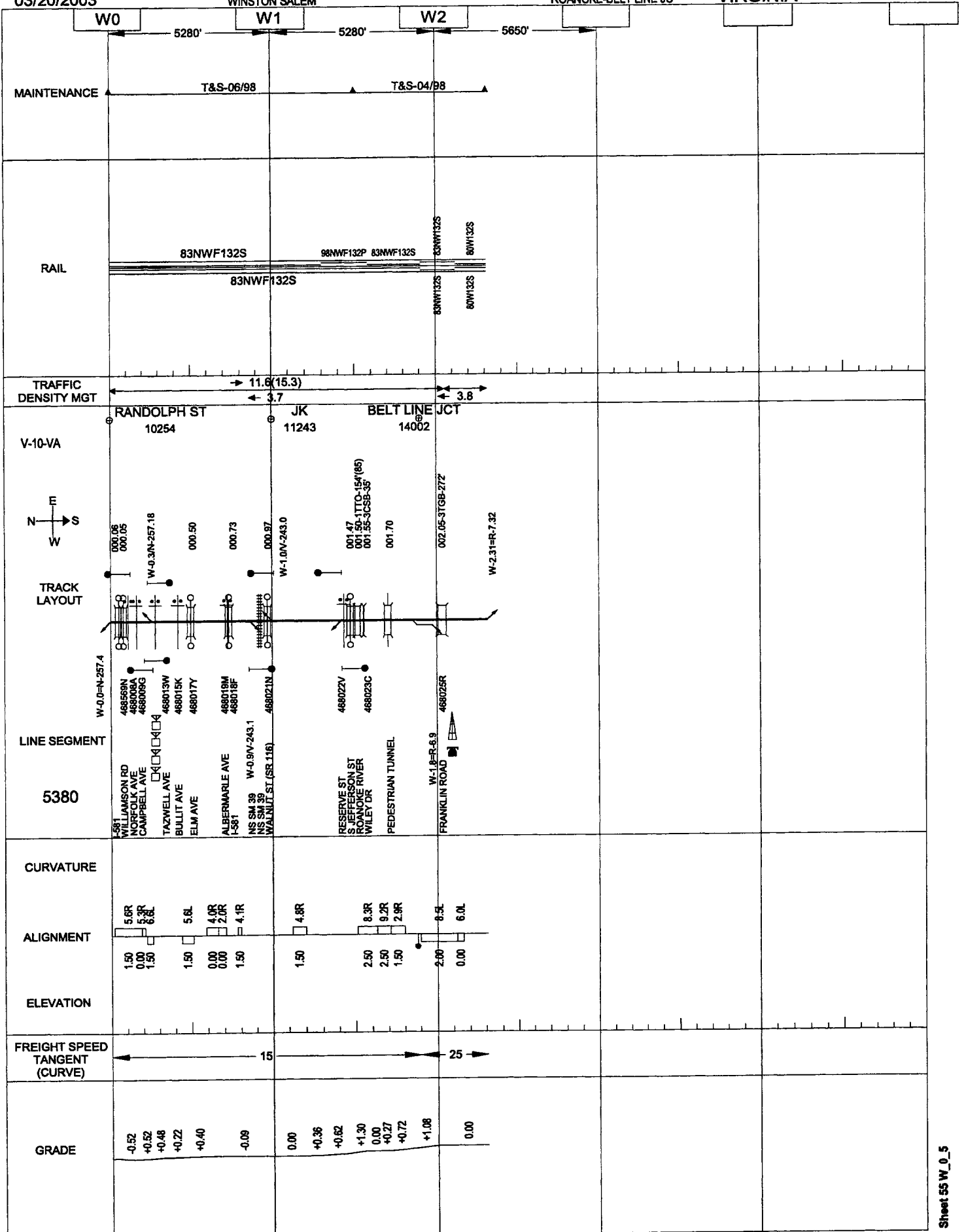


03/20/2003

WINSTON SALEM

ROANOKE-BELT LINE JC

VIRGINIA



03/20/2003

ROANOKE TERMINAL

ROANOKE BELT LINE

BELT LINE XING-JUNCT

VIRGINIA

R4

R5

5280'

5280'

MAINTENANCE

T&S-05/85

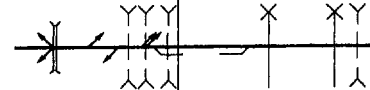
RAIL

26RJ130S

26RJ130S

TRAFFIC
DENSITY MGT0.1(0.2)
← 0.1

V-10-VA

TRACK
LAYOUTBELT LINE CROSSING
11247R-3.33-N-260.74
003 51-7MSO-233(98)
R-3.55V-246.4

LINE SEGMENT

5570

TO RDWAY MATL YARD
ROANOKE RIVER468480J
468481R
468482X468483E
BRIDGE ST468486A
PRIVATE
BEDFORD ST

CURVATURE

ALIGNMENT

ELEVATION

15.2L
0.50
8.8L
0.50
5.5R
0.50
6.0R
0.50
5.8L
0.50
0.2L
0.50
0.1R

FREIGHT SPEED
TANGENT
(CURVE)

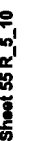
15

GRADE

-0.10

-0.70

-0.10

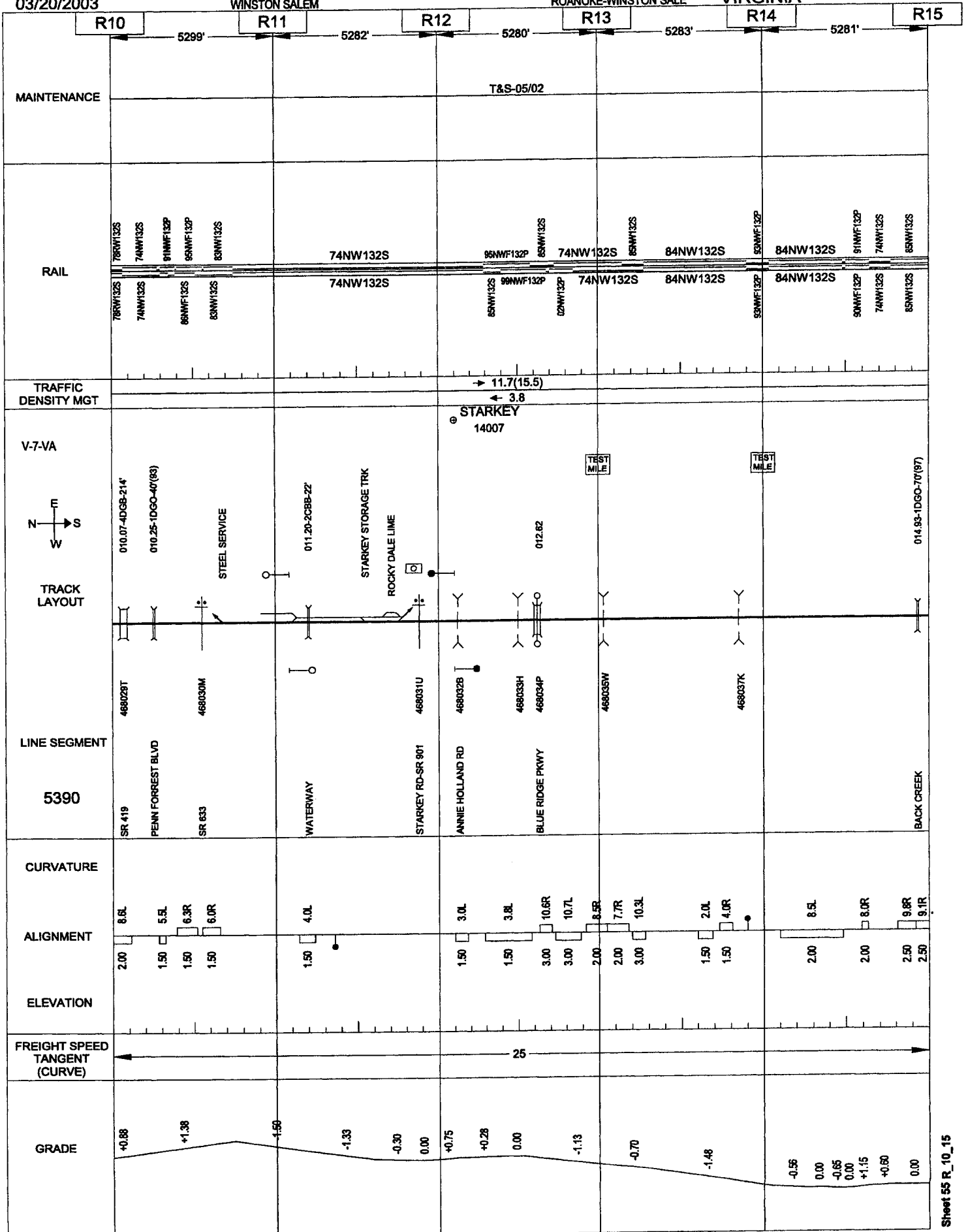


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

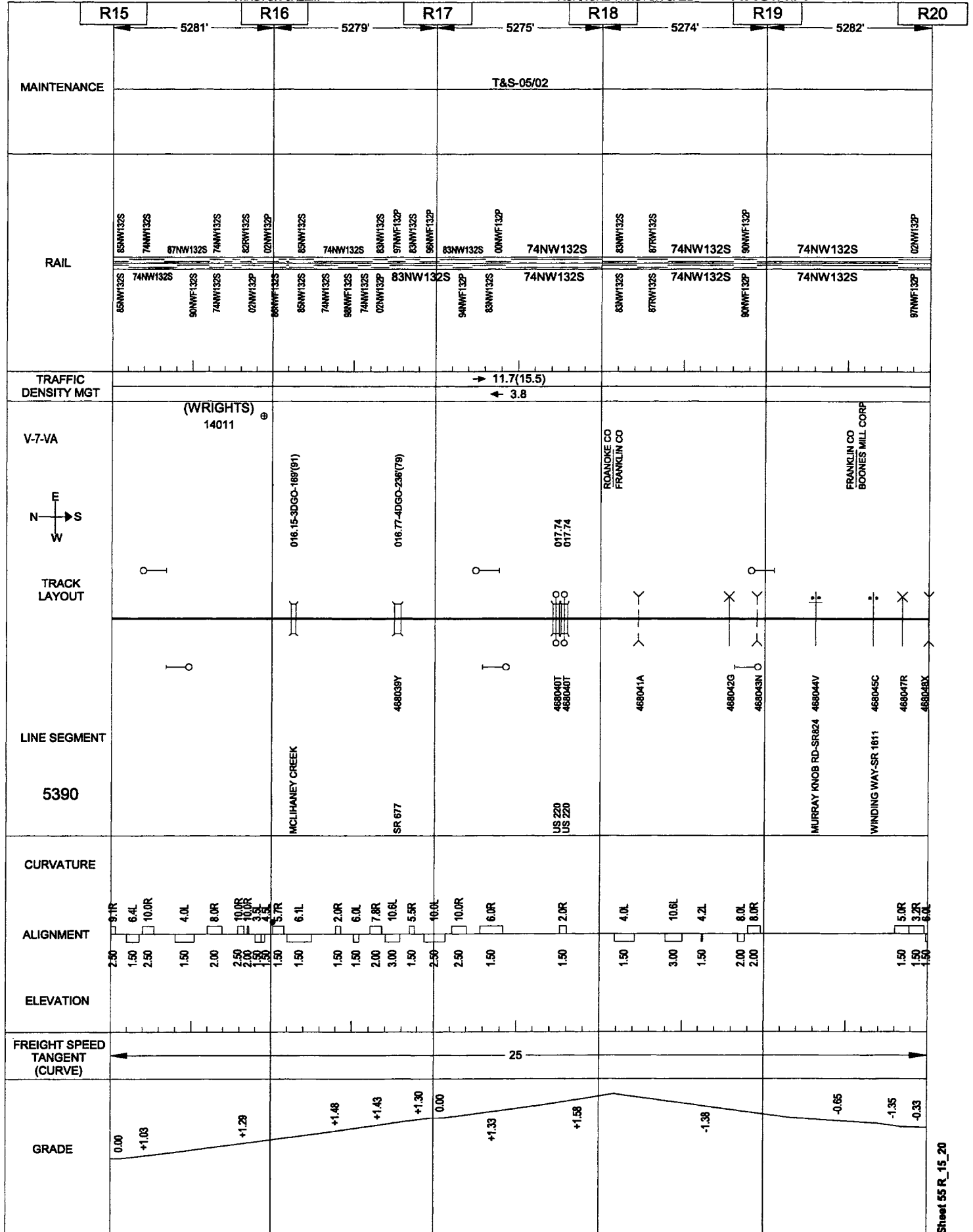


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

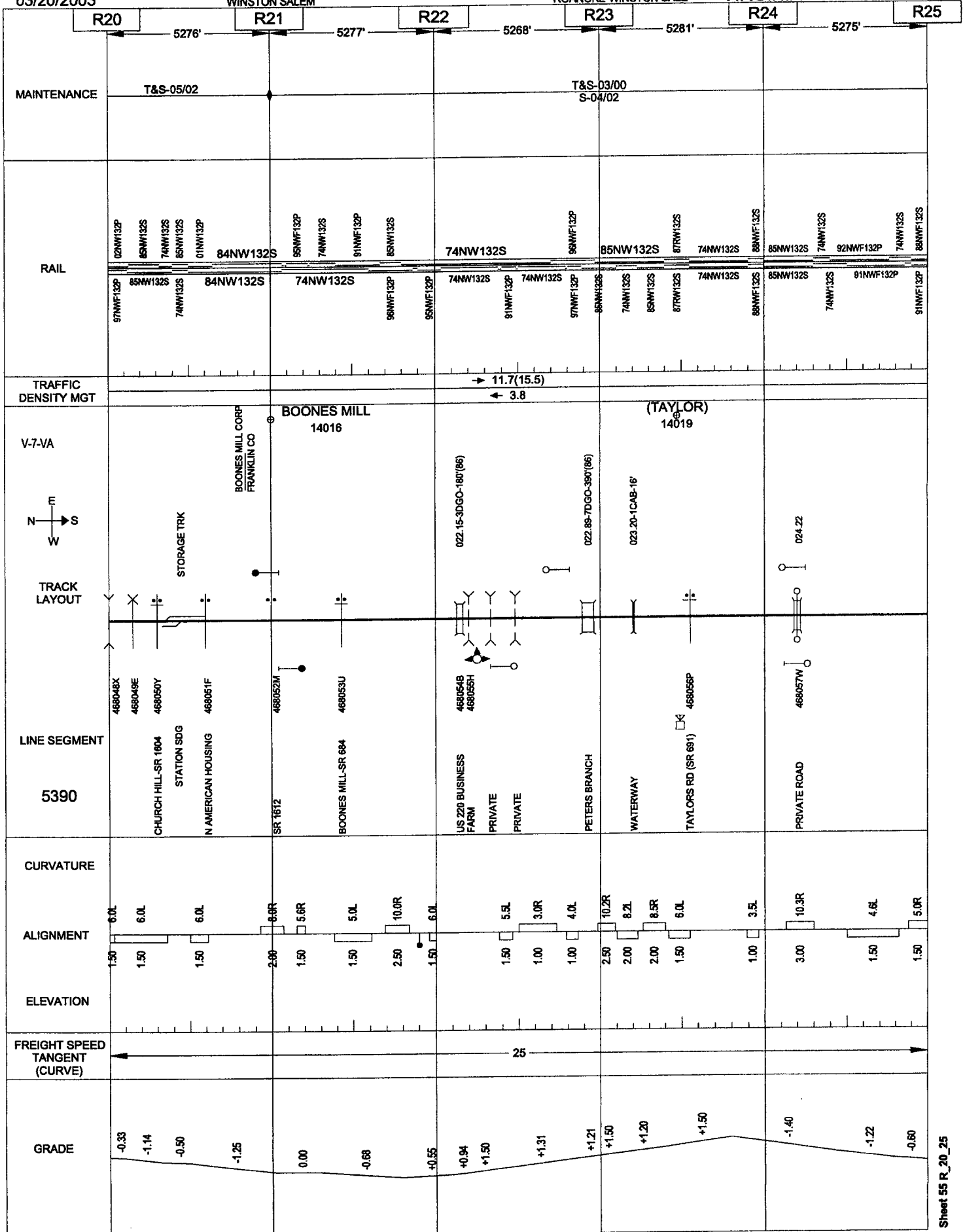


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

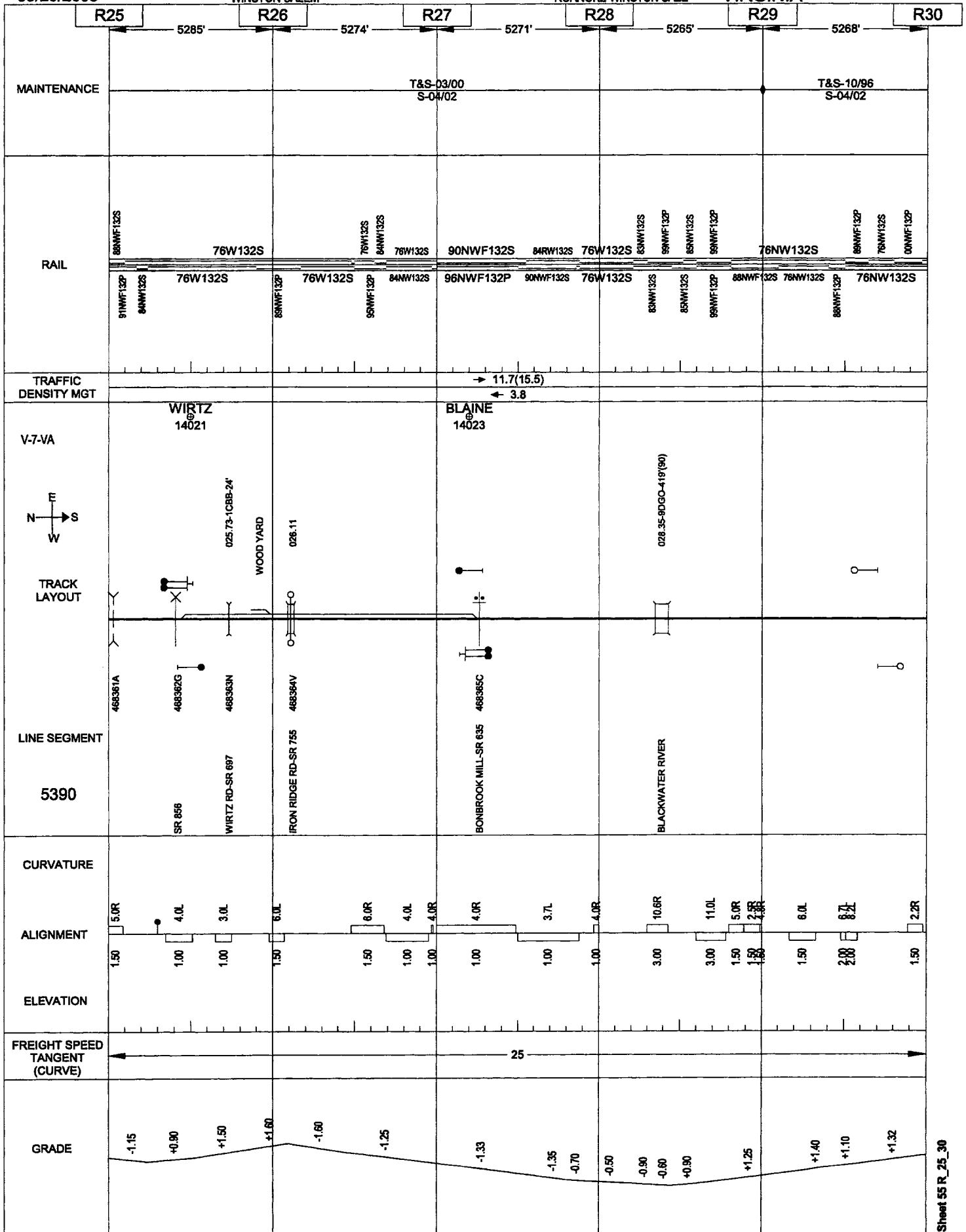


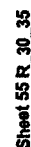
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WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA



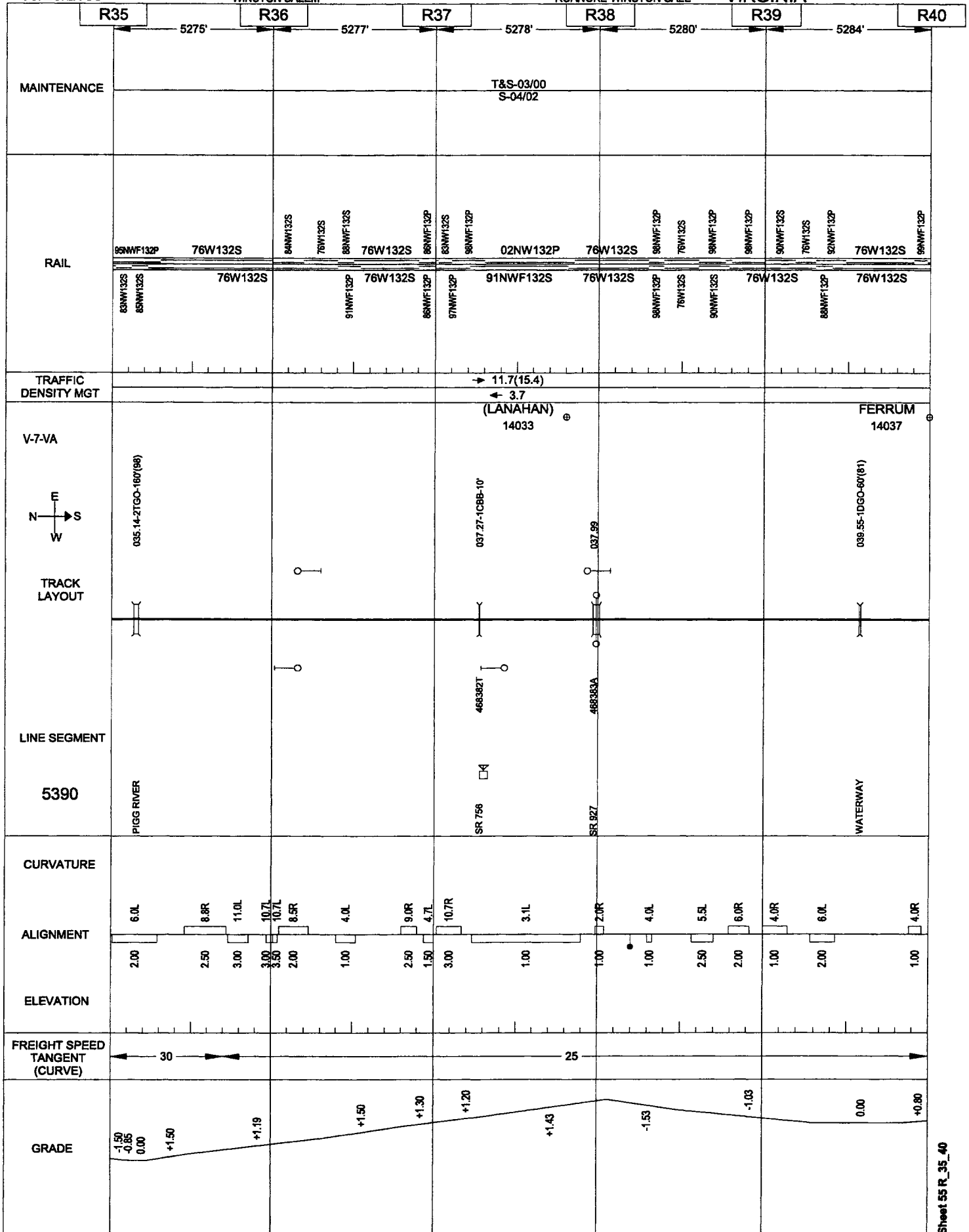


03/20/2003

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VIRGINIA

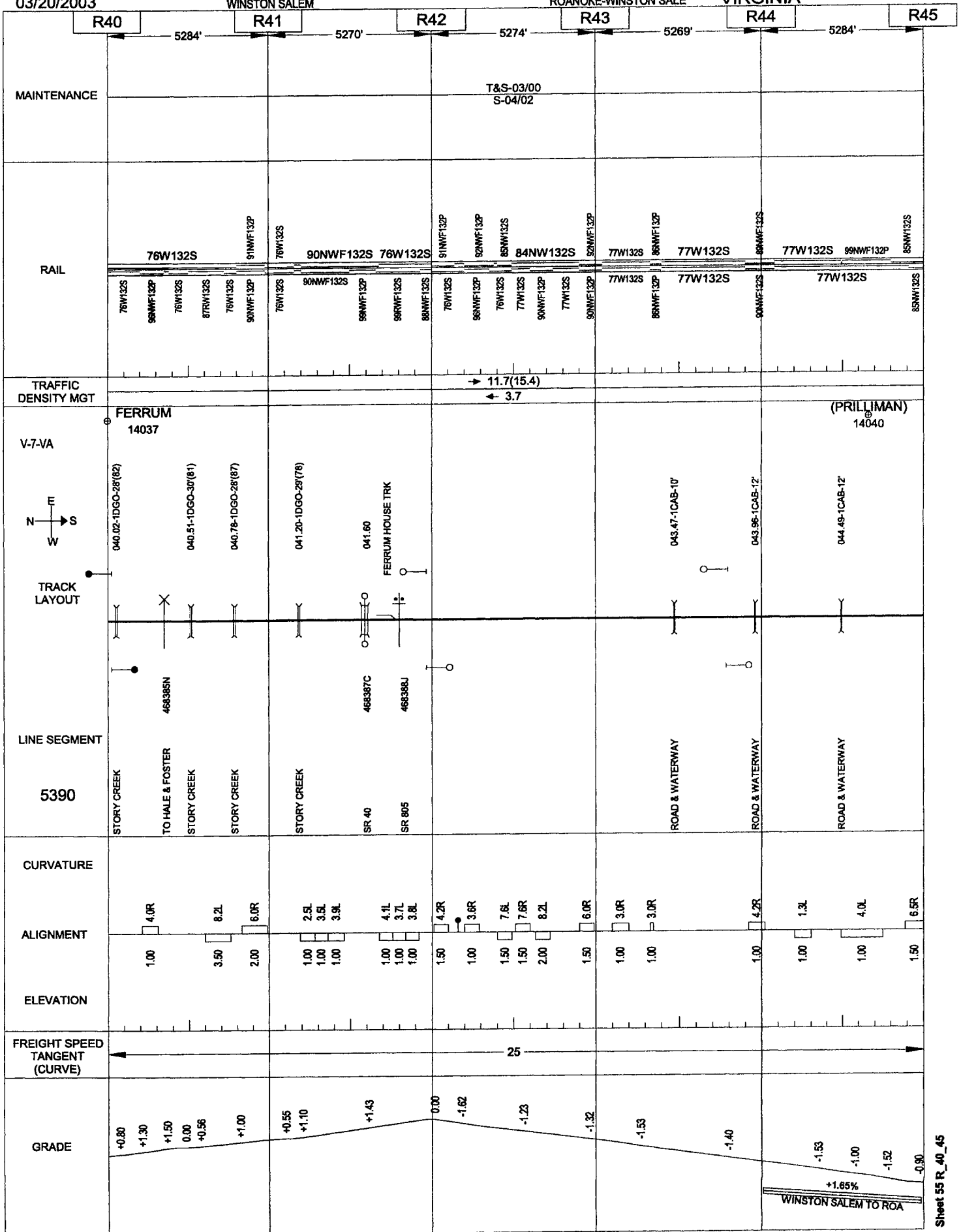


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

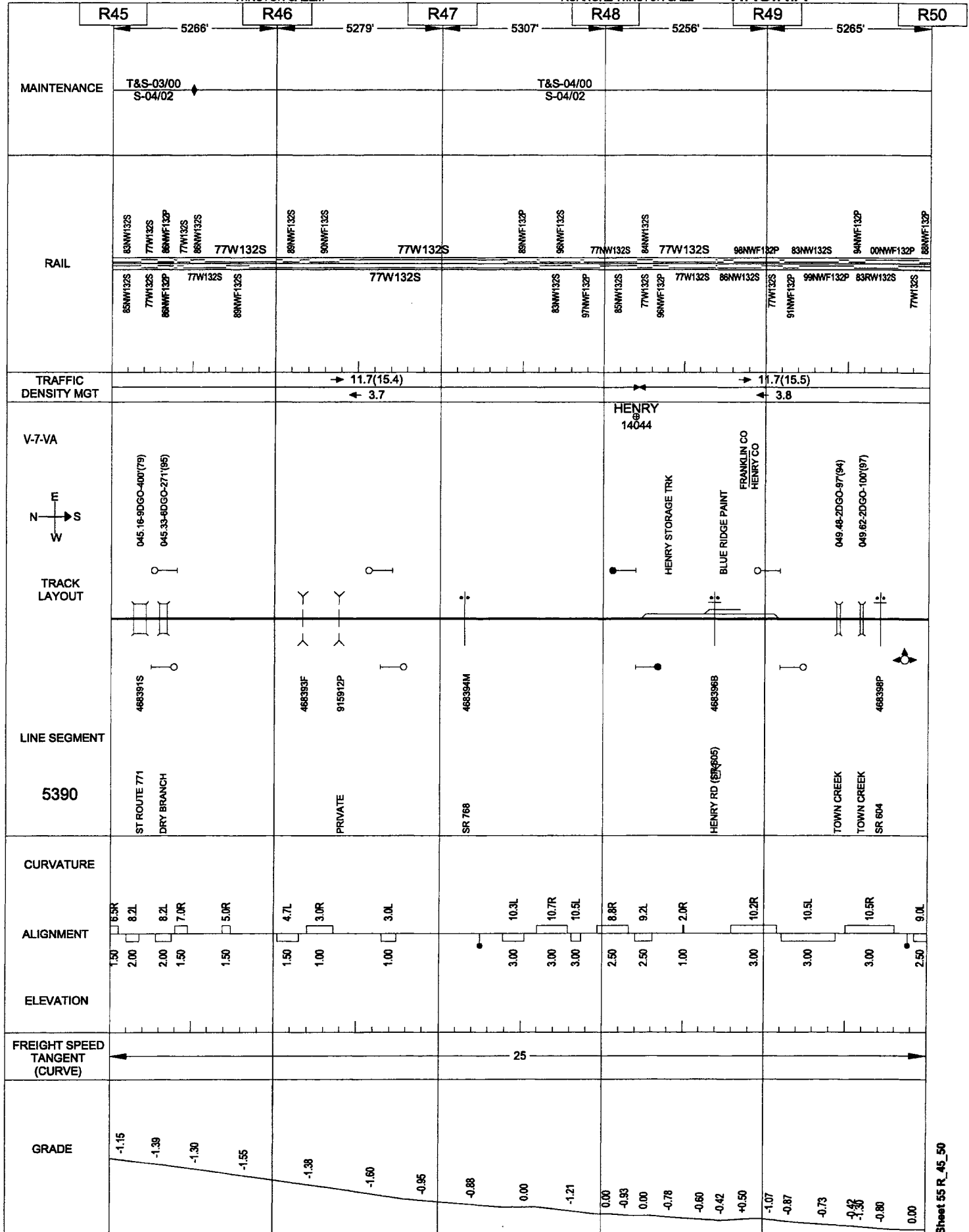


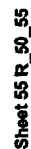
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WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA



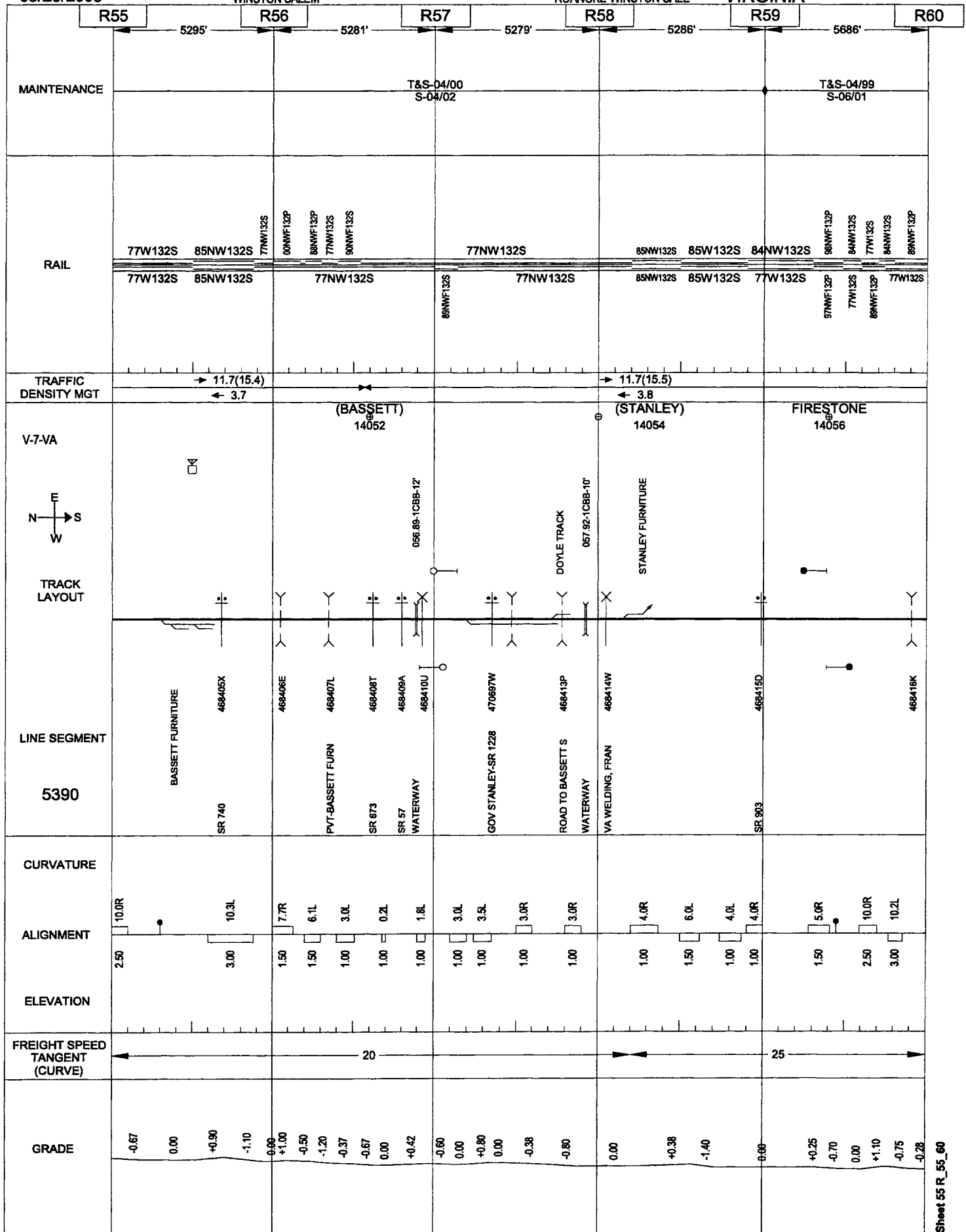


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA



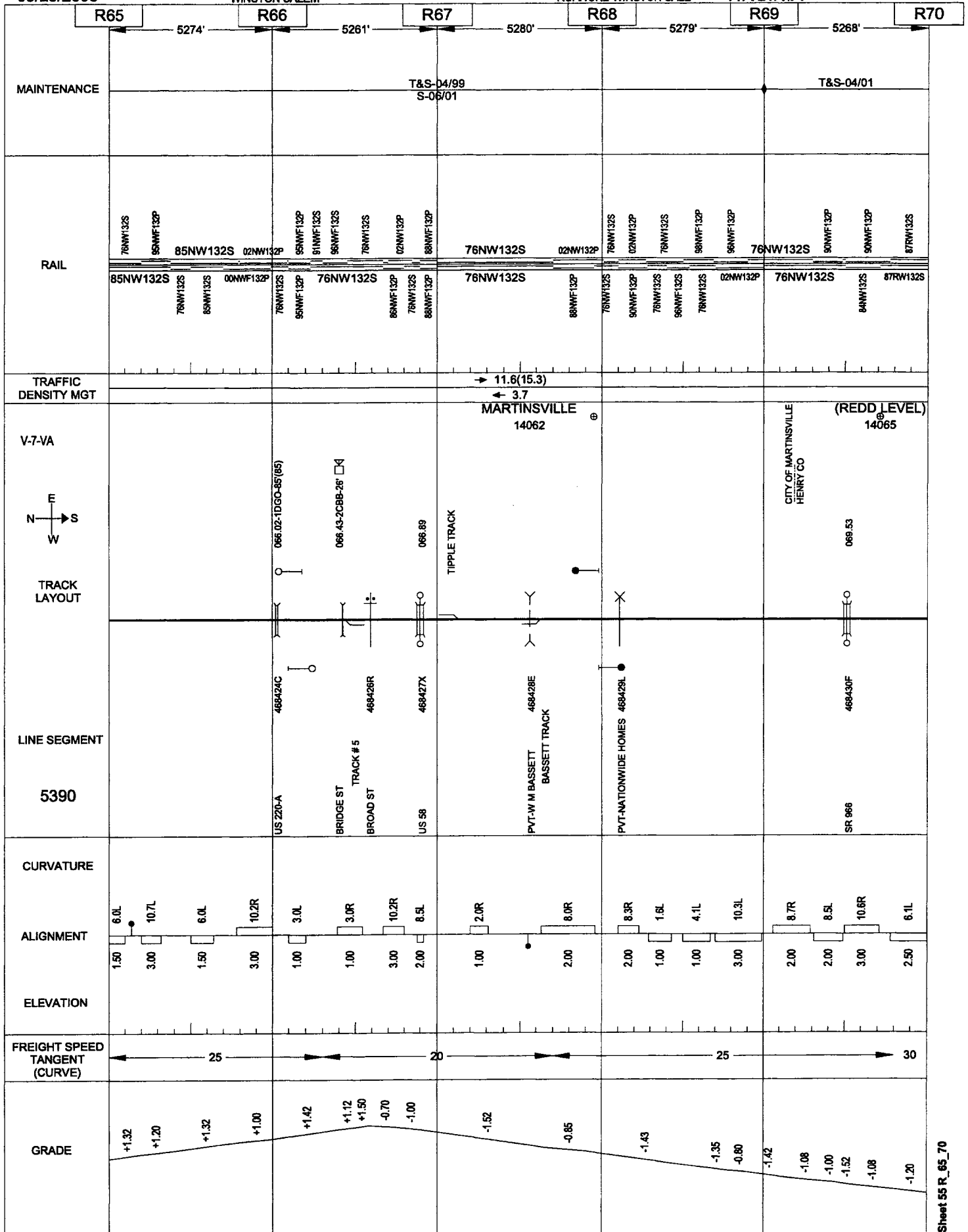
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03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

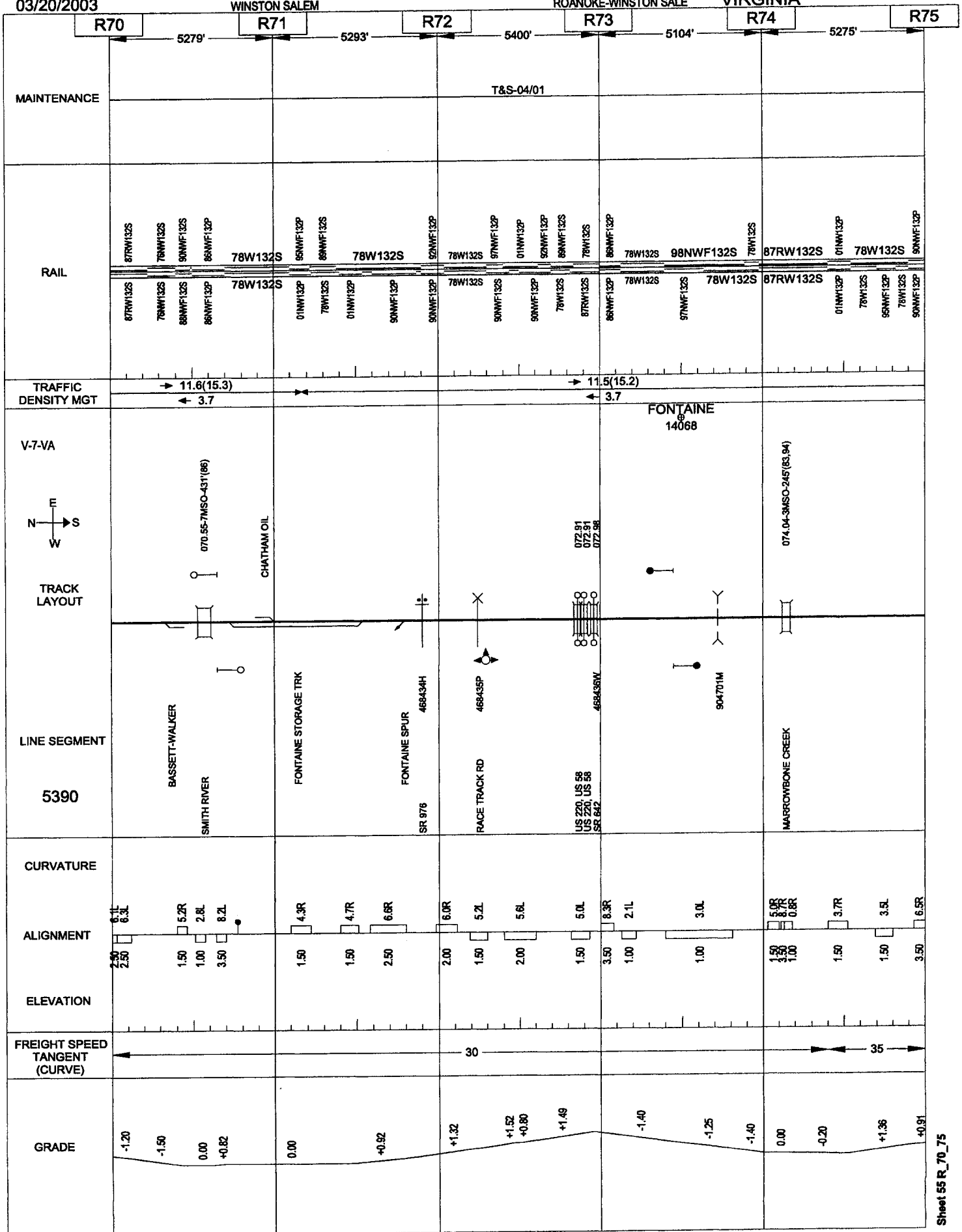


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA



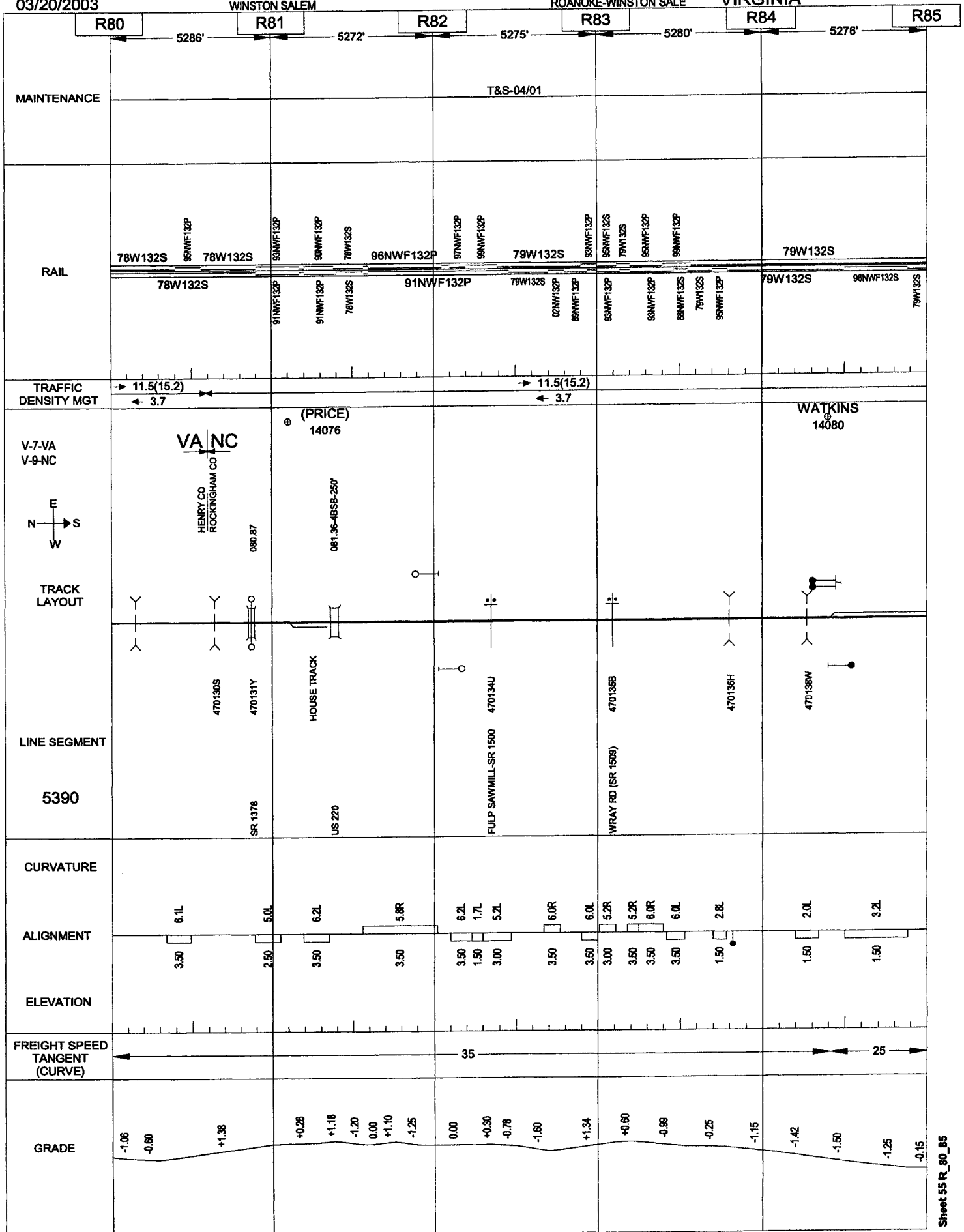
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03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

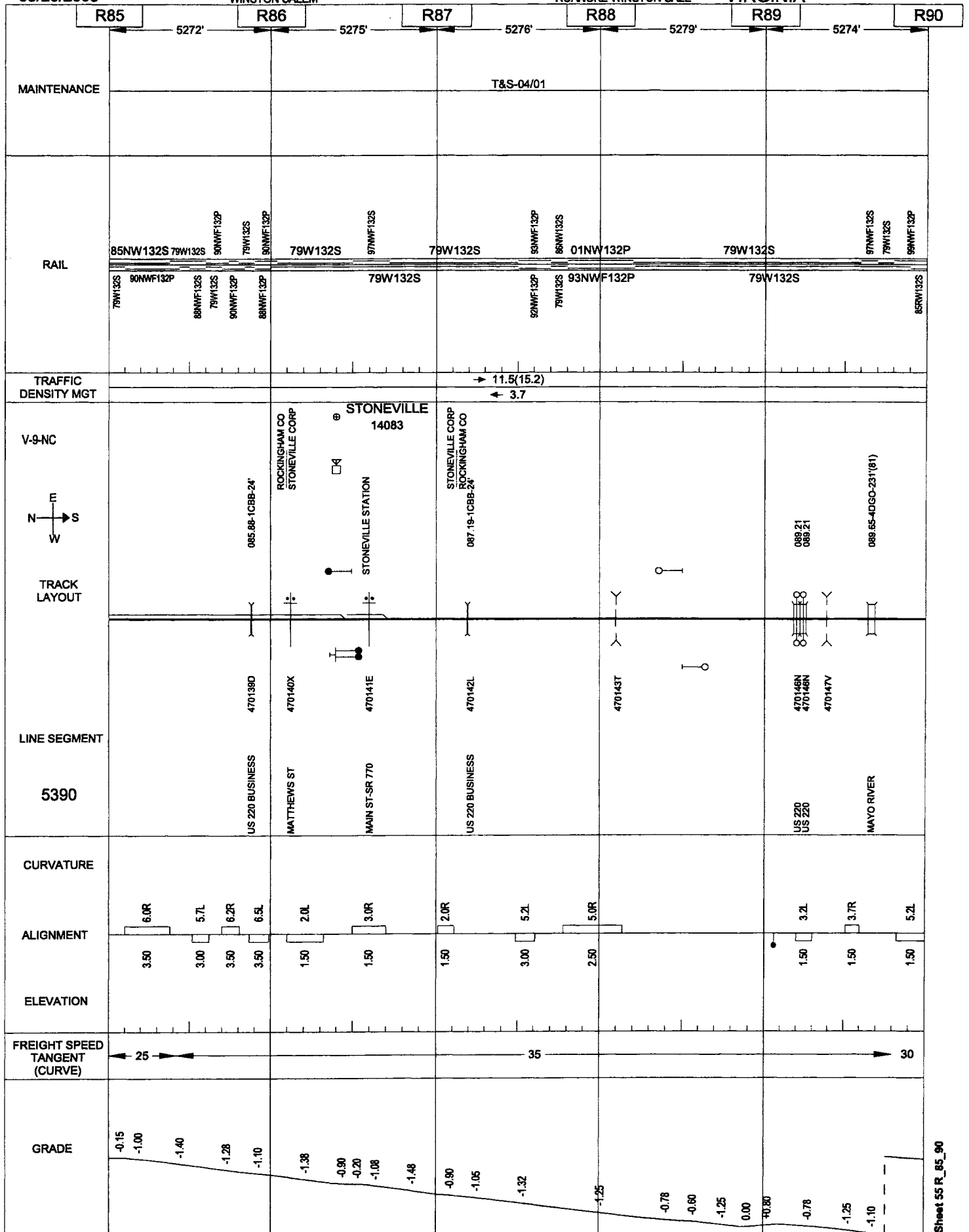


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

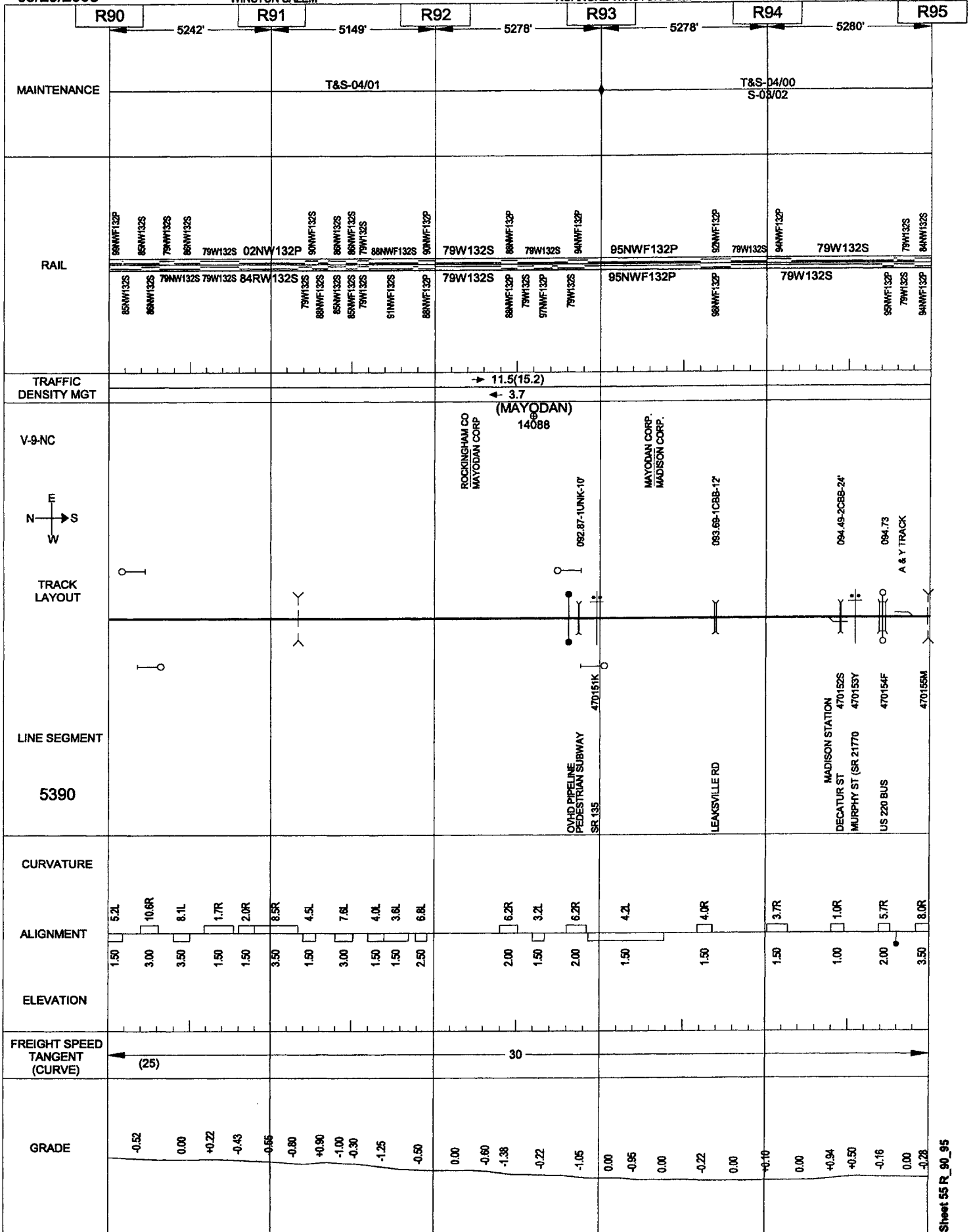


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA



Sheet 55 R 95 100

Sheet 55 R_100_105

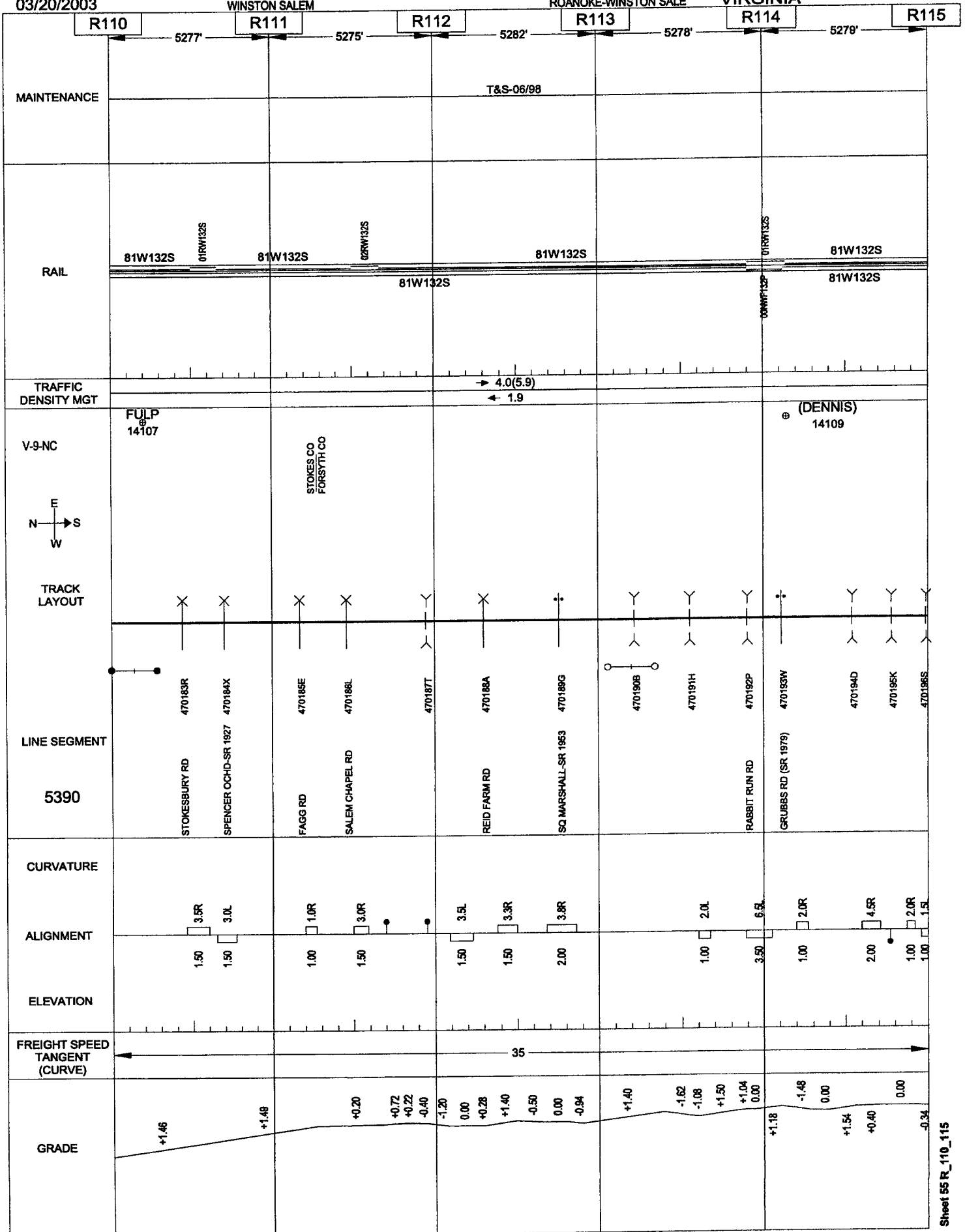
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03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

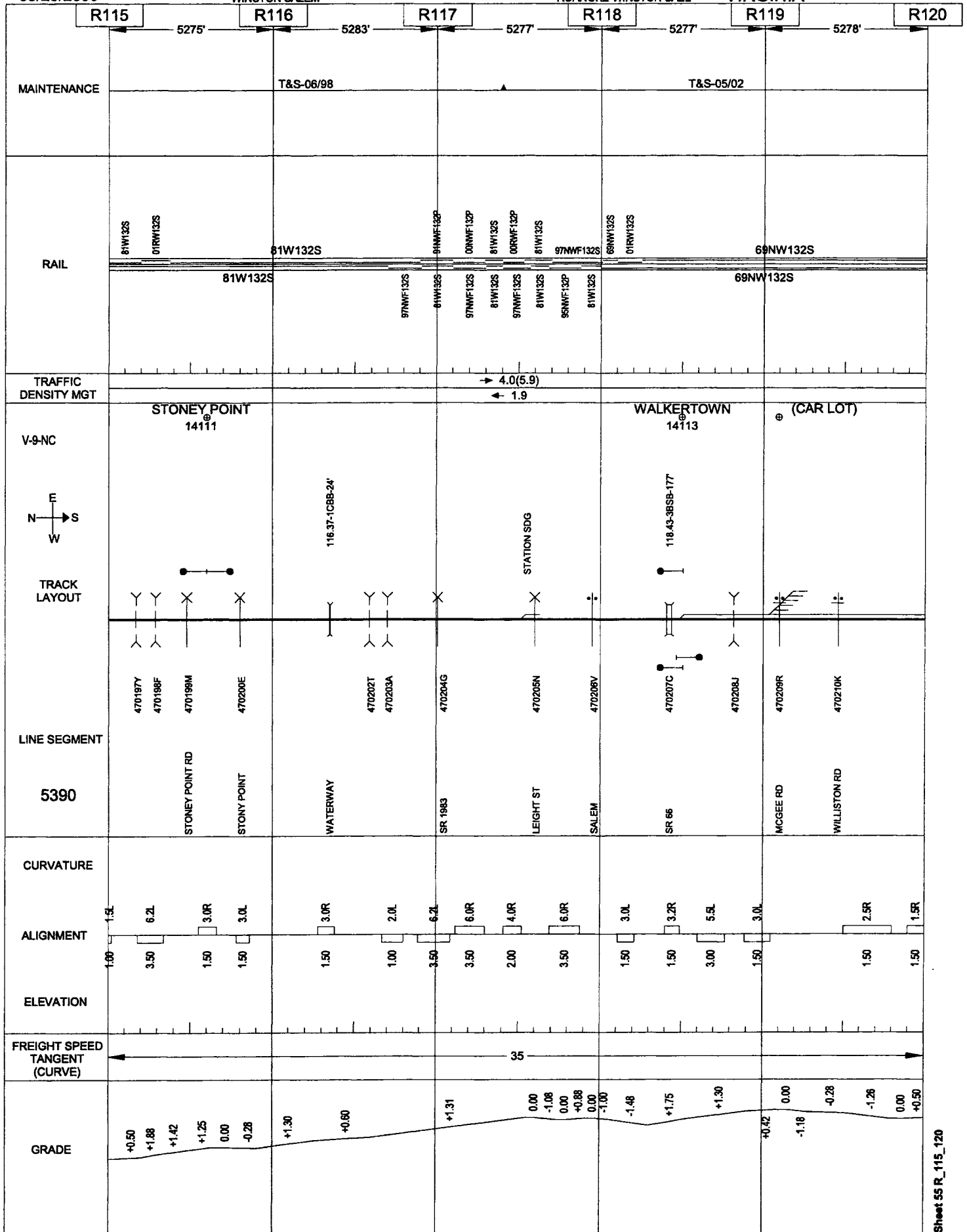


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA

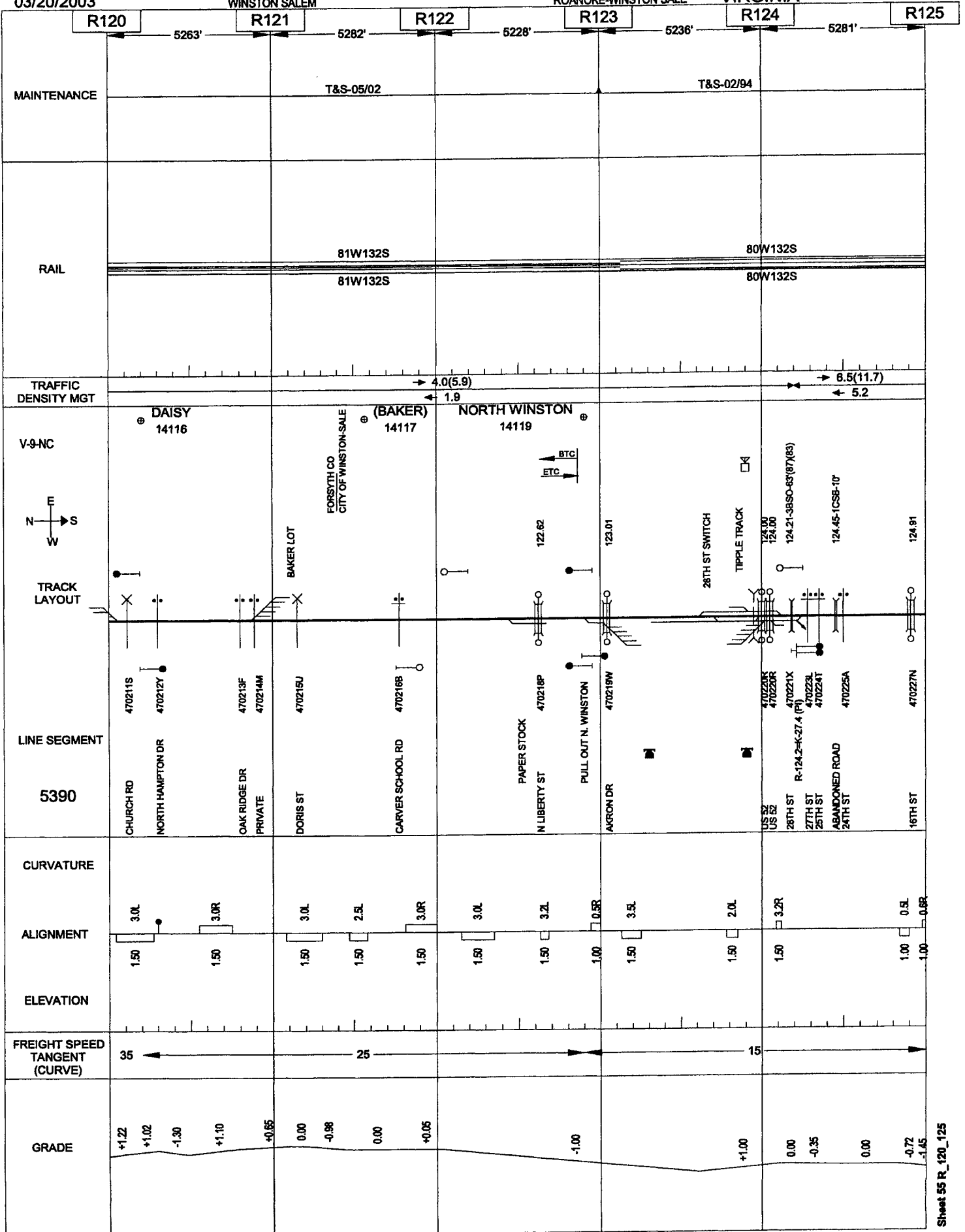


03/20/2003

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VIRGINIA

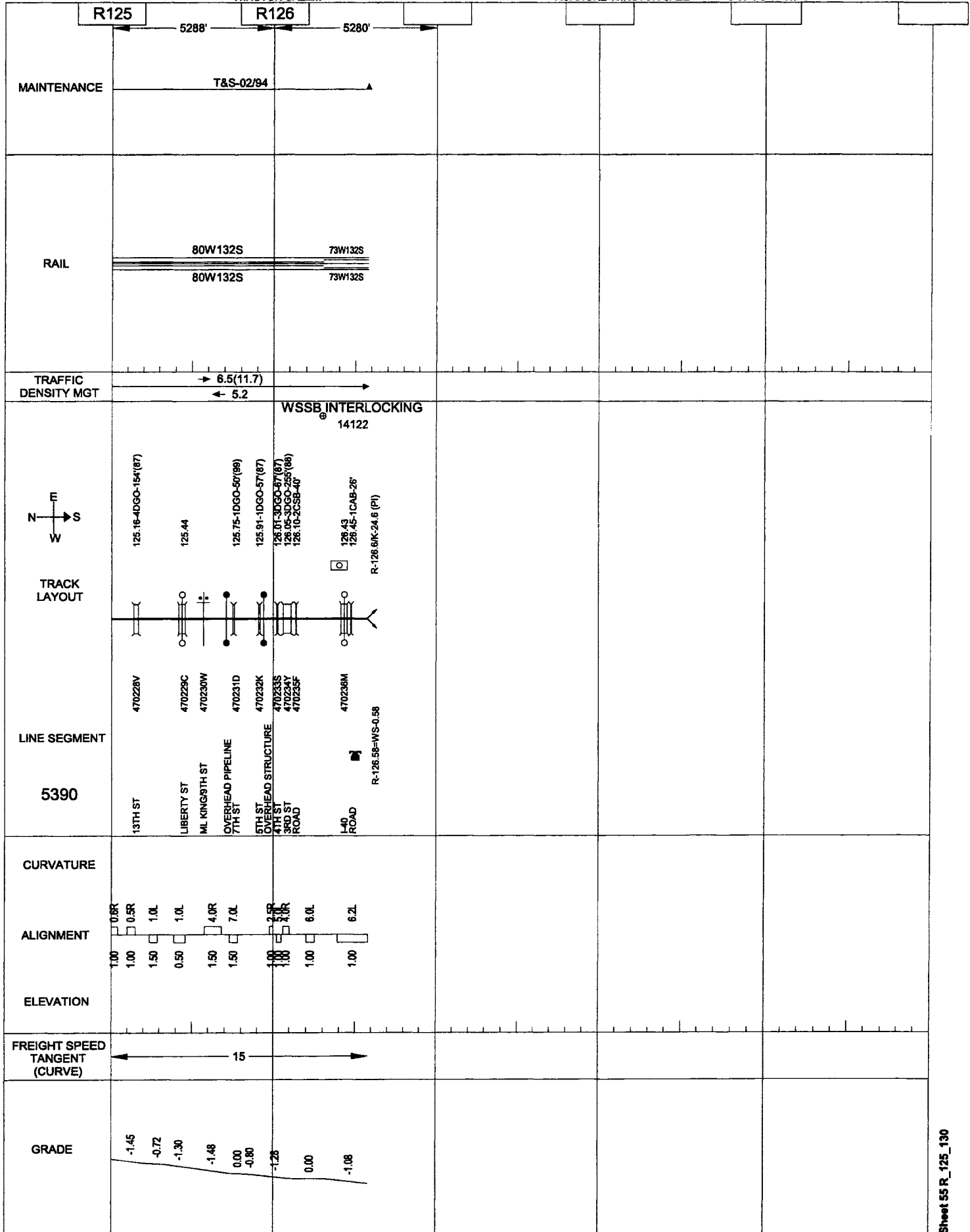


03/20/2003

WINSTON SALEM

ROANOKE-WINSTON SALE

VIRGINIA



03/20/2003

WINSTON SALEM

W SALEM SOUTHBOUND

W SALEM-SOUTH W SALE

VIRGINIA

WS1

WS2

WS3

WS4

3346'

5272'

5274'

5282'

5280'

MAINTENANCE

T&S-06/92

RAIL

73RW132S

73RW132S

TRAFFIC
DENSITY MGT

0.1(0.1)

0.0

WINSTON-SALEM

WS-0.58-R-126.58

001.08-1DGO-58'

001.28-15DGO-693'

001.75-4BSB-384'

NISSEN SPUR

002.43

002.58

003.20-7BSB-336'

YARD
LIMIT

E
N → S
W

TRACK
LAYOUT

LINE SEGMENT

5400

NS M 38
IDABELL ST

SALEM CREEK

EXPRESSWAY

WAUGHTOWN TOWN

SPRAGUE RD

ST ROUTE 150

STARLIGHT DR

CASSELL ST

CURVATURE

ALIGNMENT

10.0R
1.00

7.0R
1.00

1.8R
1.00

6.1R
1.00

6.0L
1.00

2.0R
1.00

2.0R
1.00

2.0R
1.00

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

10

GRADE

0.00

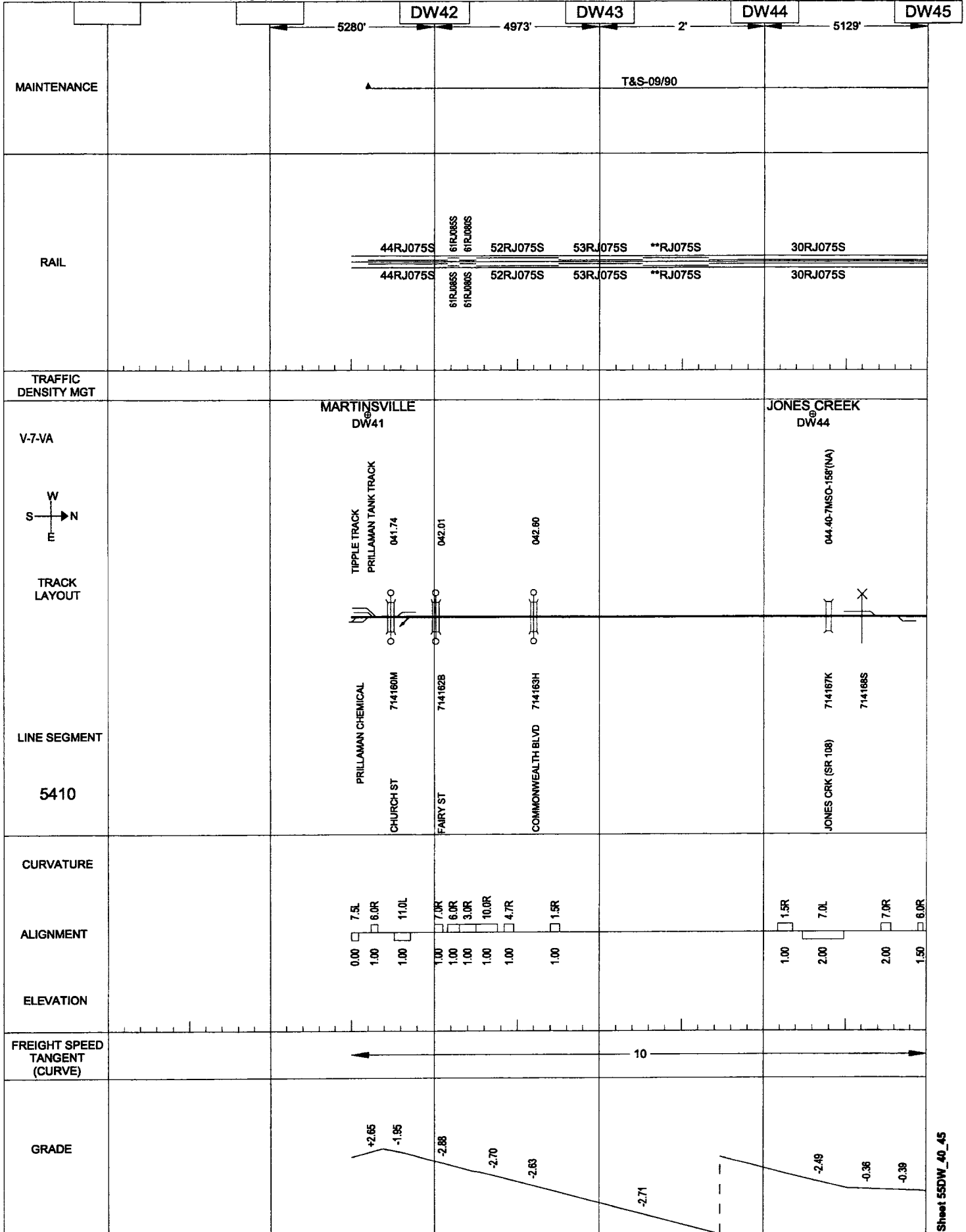
03/20/2003

WINSTON SALEM

HILL TOP BRANCH

MARTINSVILLE-FIELDAL

VIRGINIA



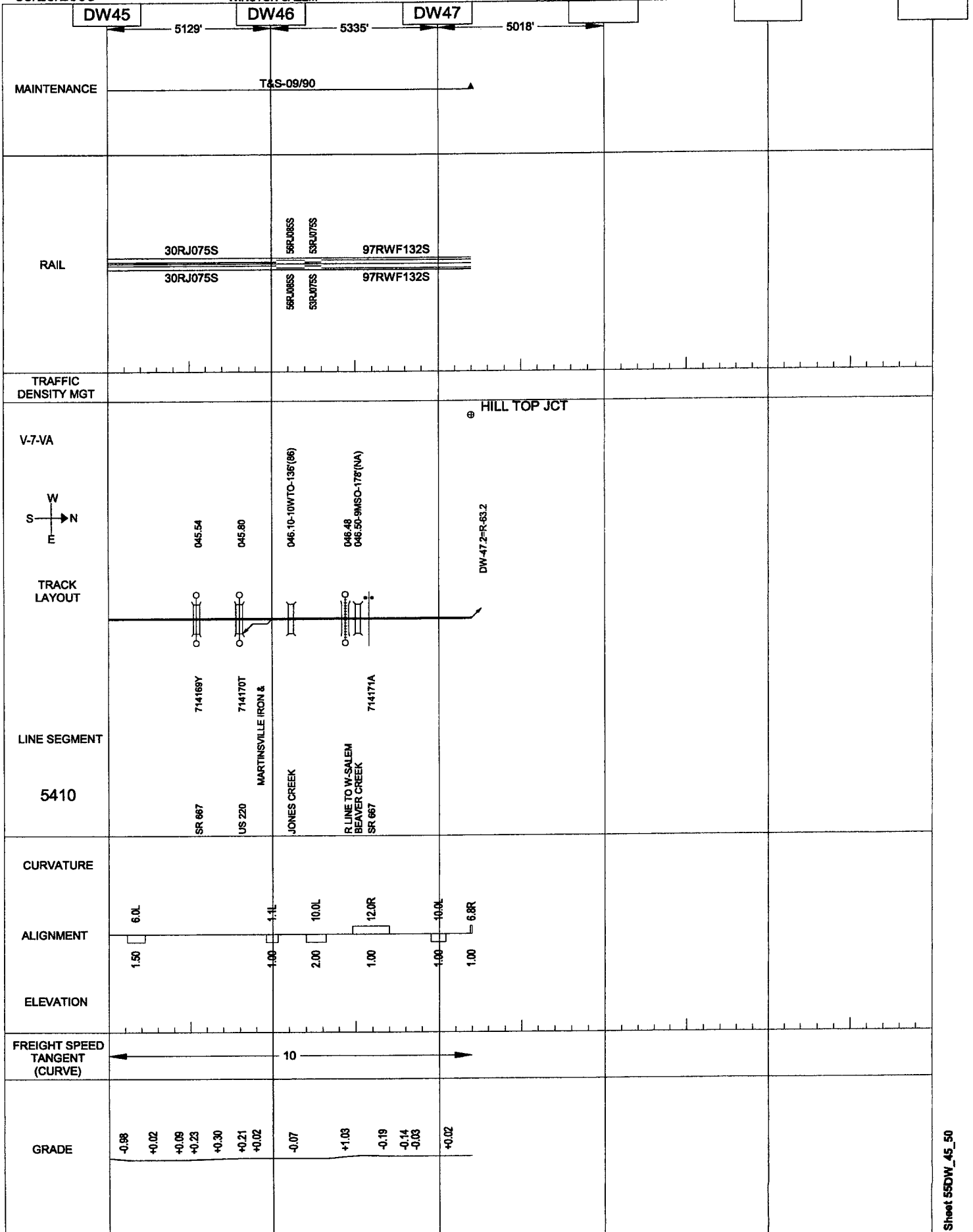
03/20/2003

WINSTON SALEM

HILL TOP BRANCH

MARTINSVILLE-FIELDAL

VIRGINIA



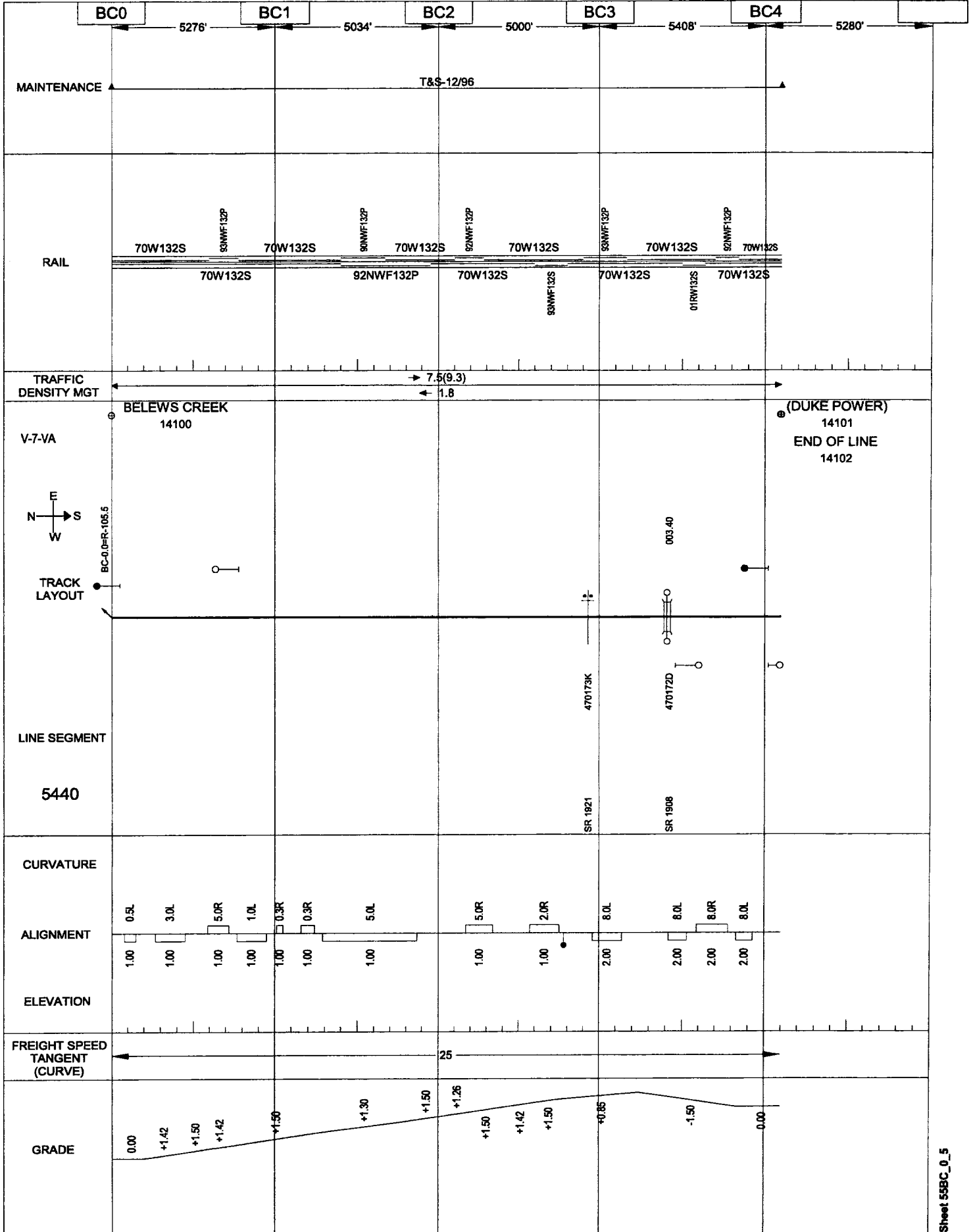
03/20/2003

WINSTON SALEM

BELEWS CREEK SPUR

JUNCTION-DUKE POWER

VIRGINIA

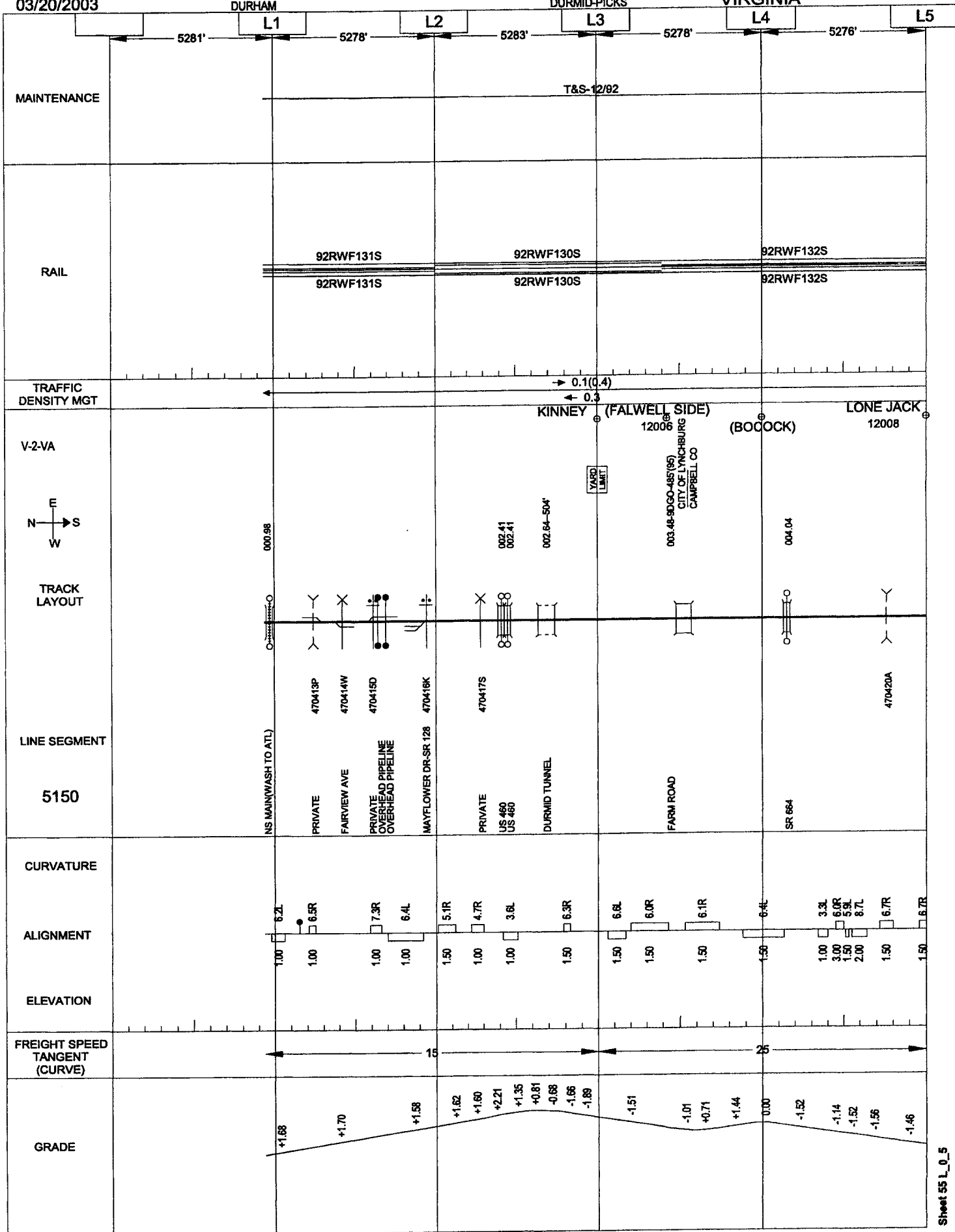


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA

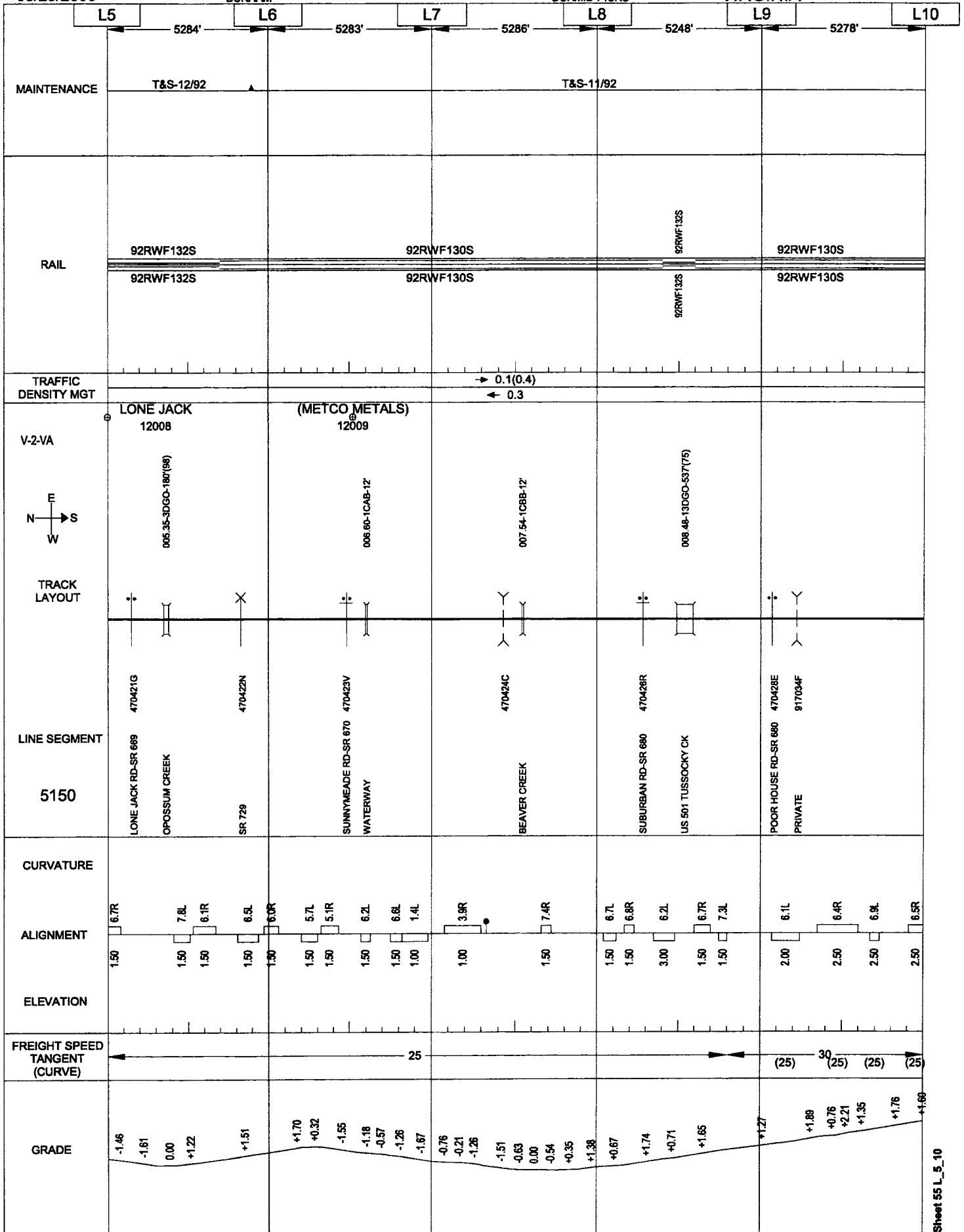


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA

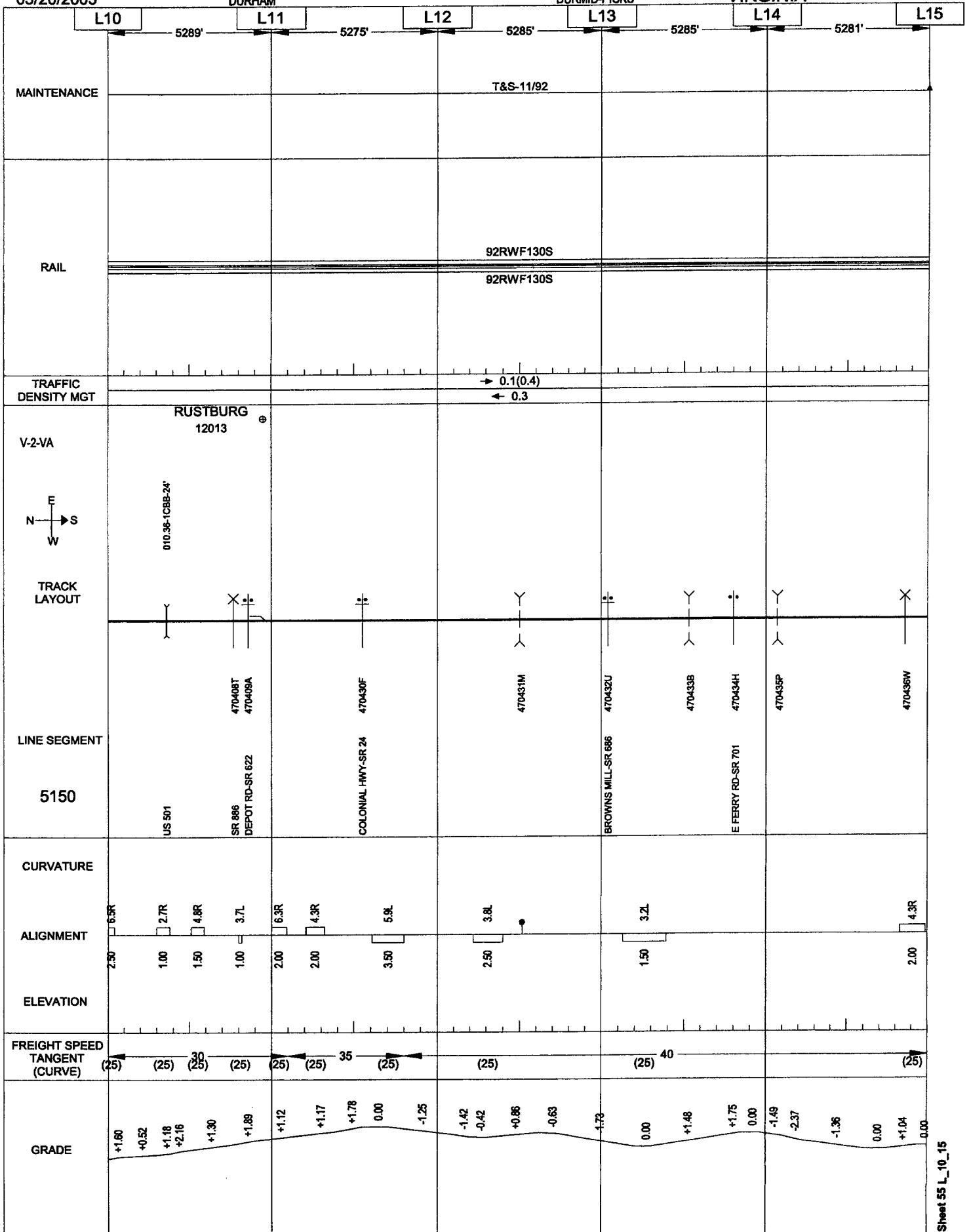


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA

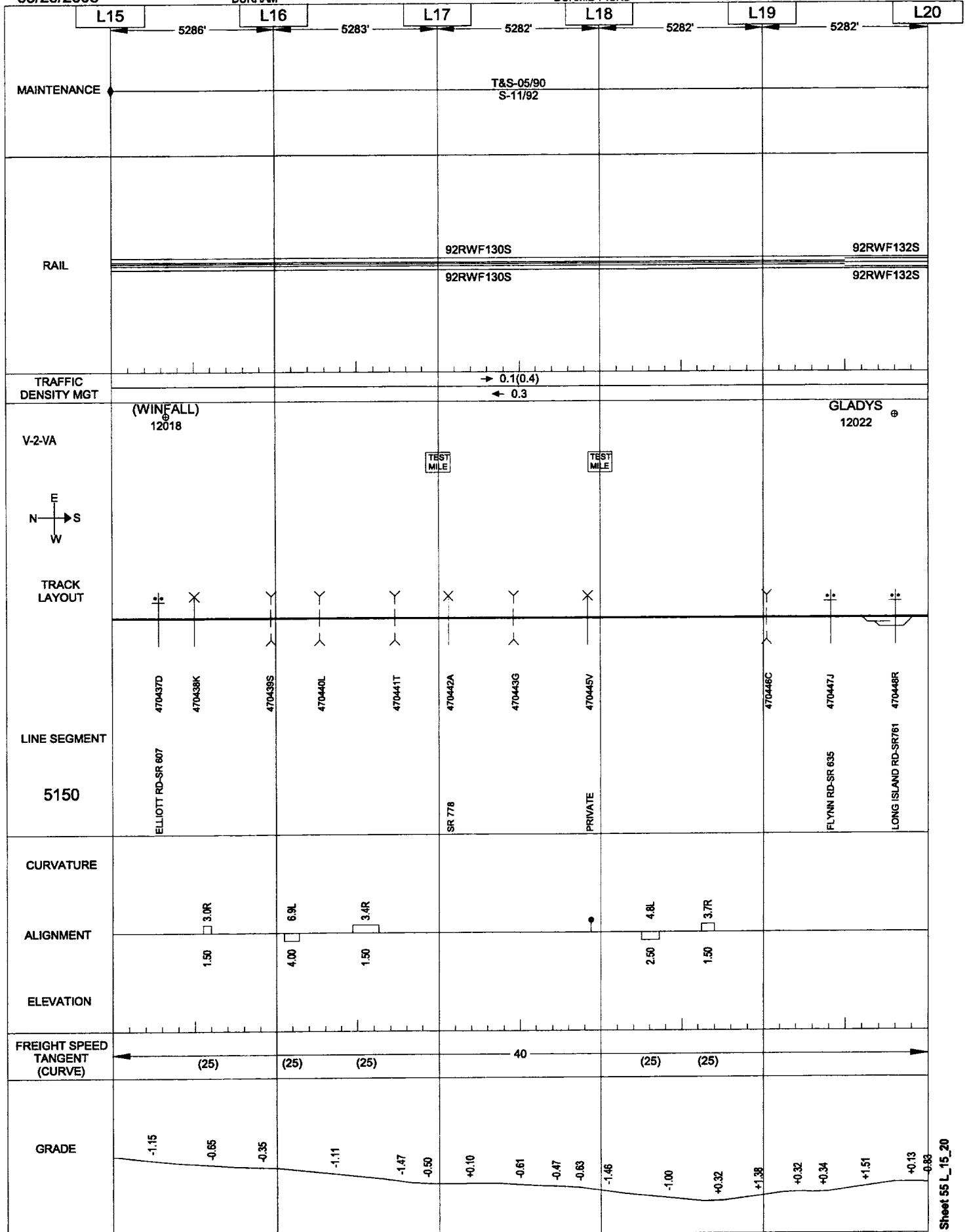


03/20/2003

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DURMID-PICKS

VIRGINIA

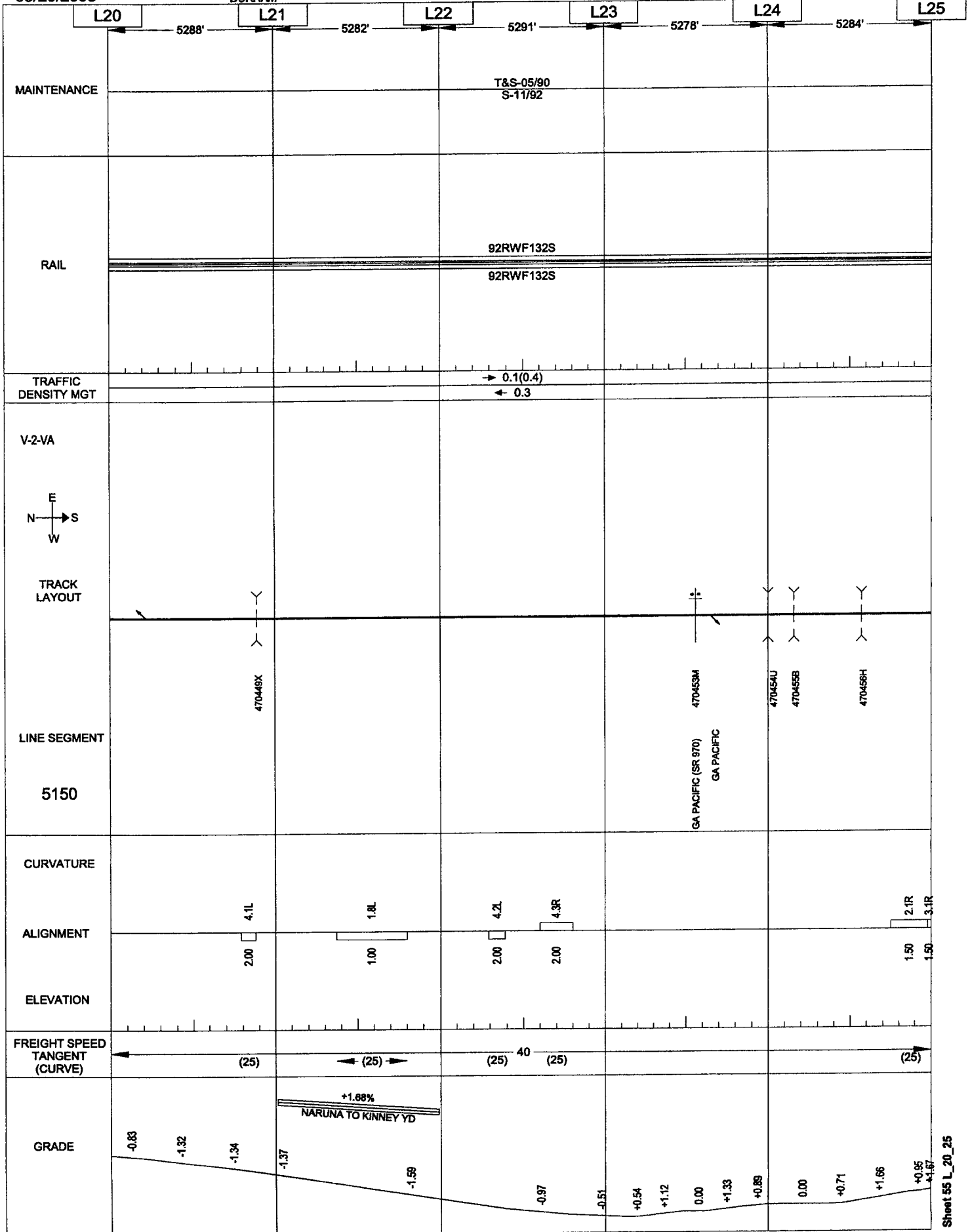


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA

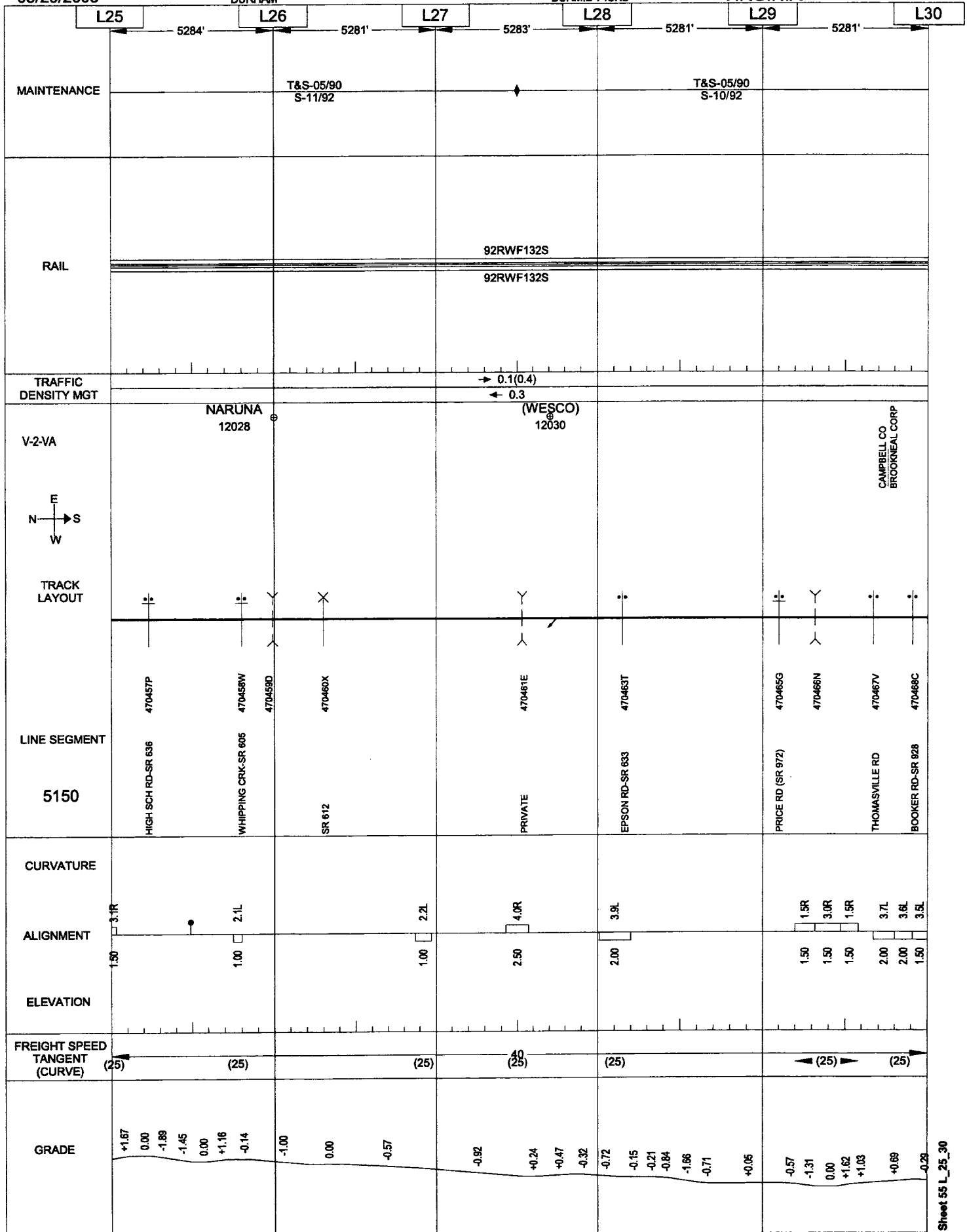


03/20/2003

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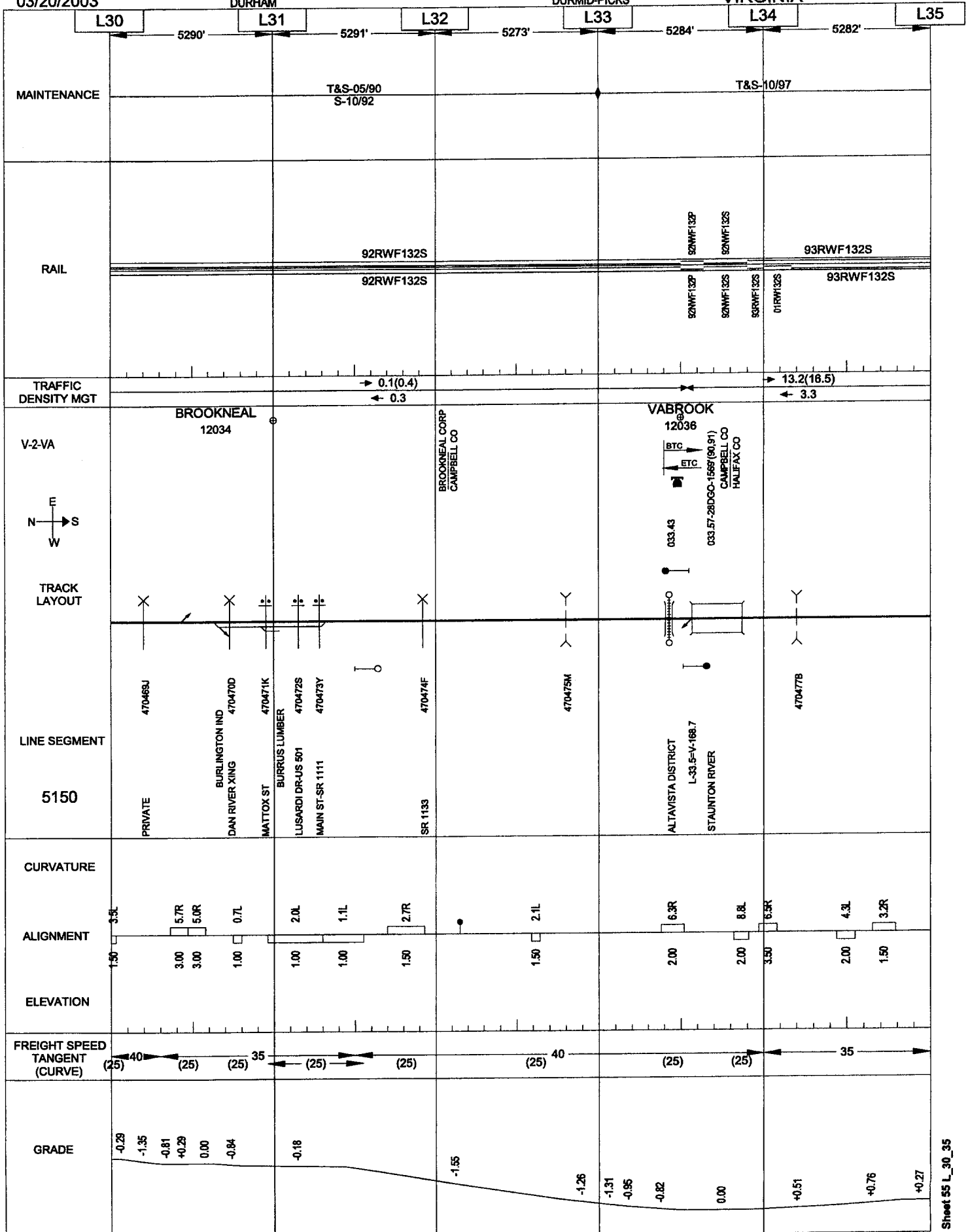


03/20/2003

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DURMID-PICKS

VIRGINIA

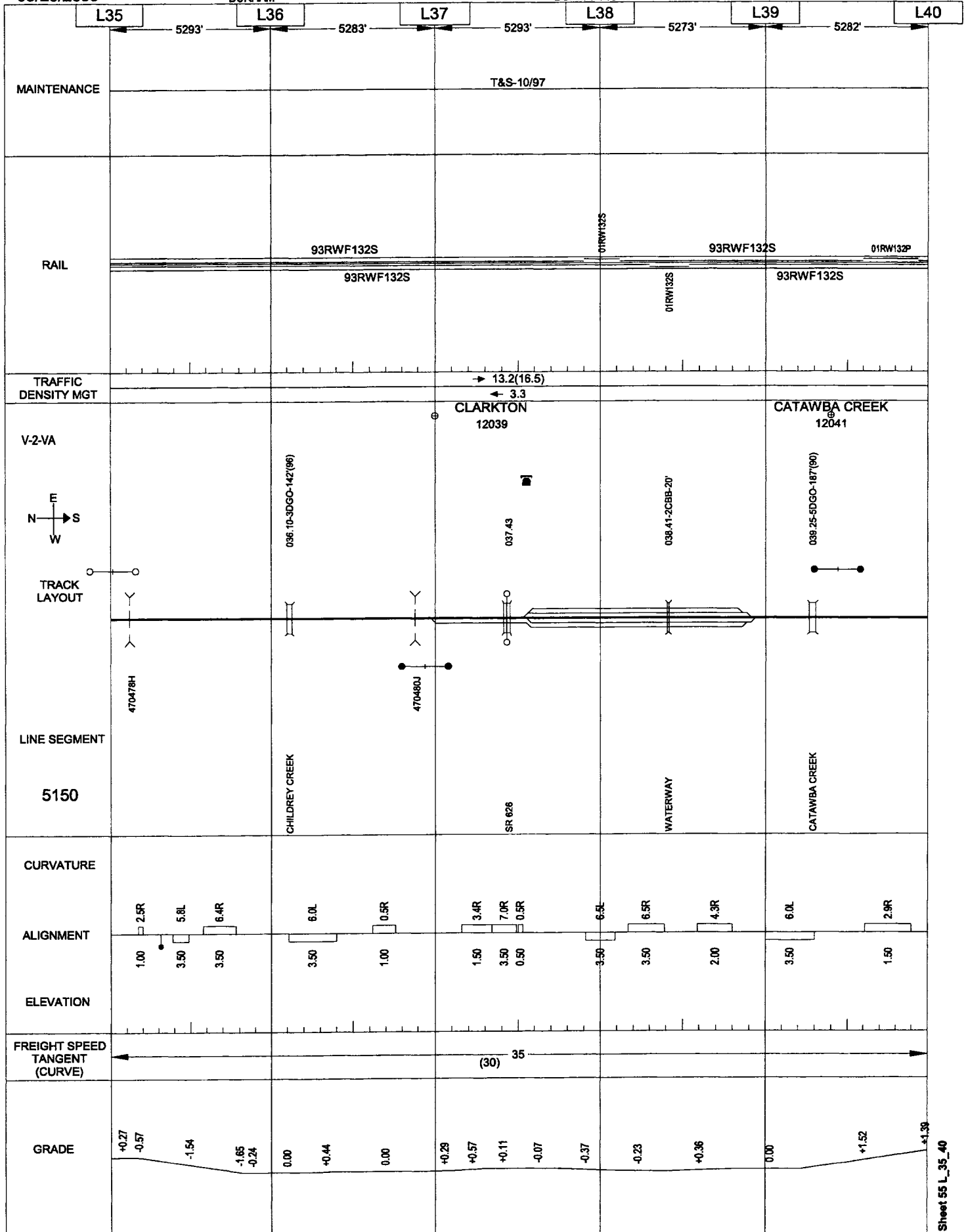


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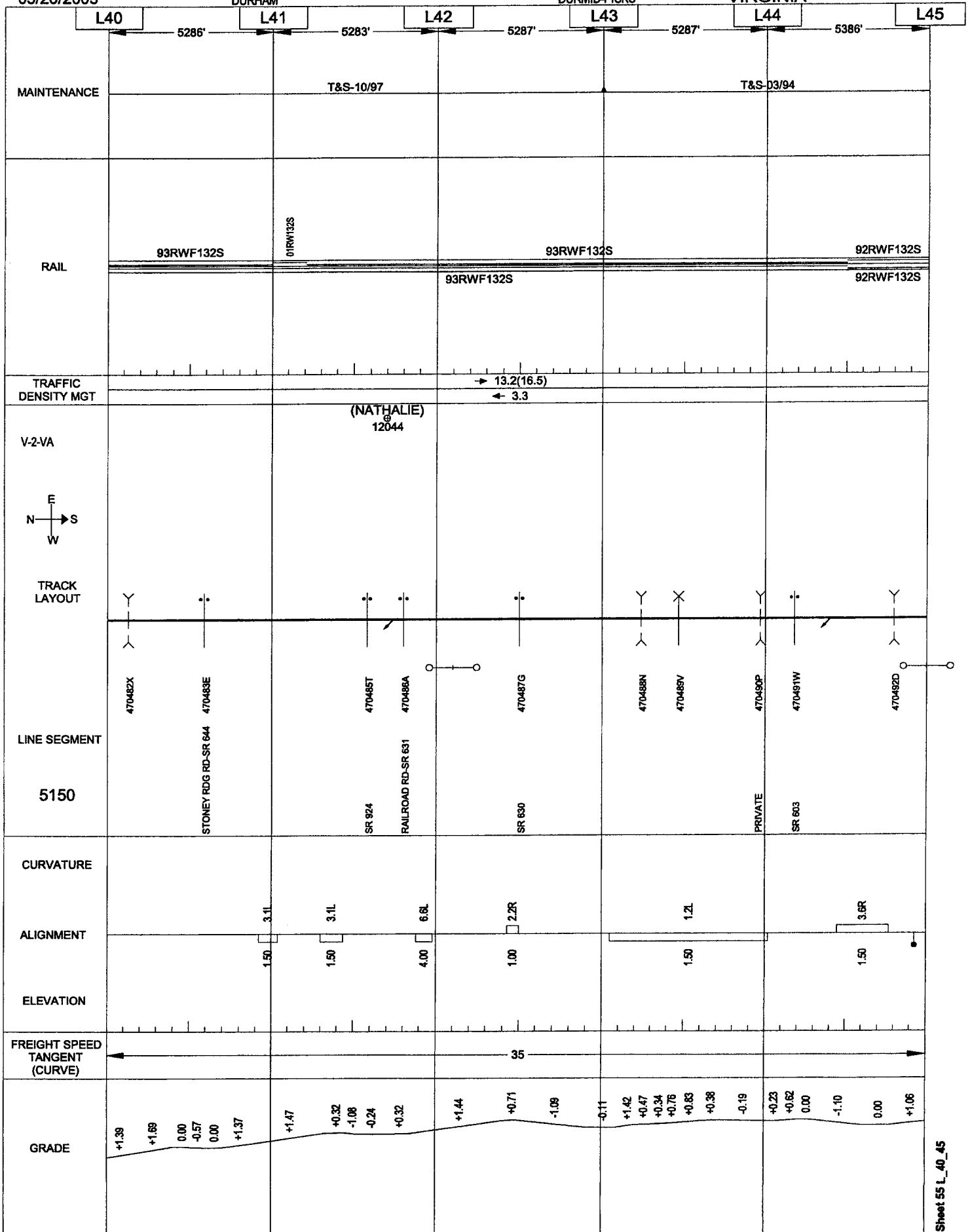


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DURMID-PICKS

VIRGINIA

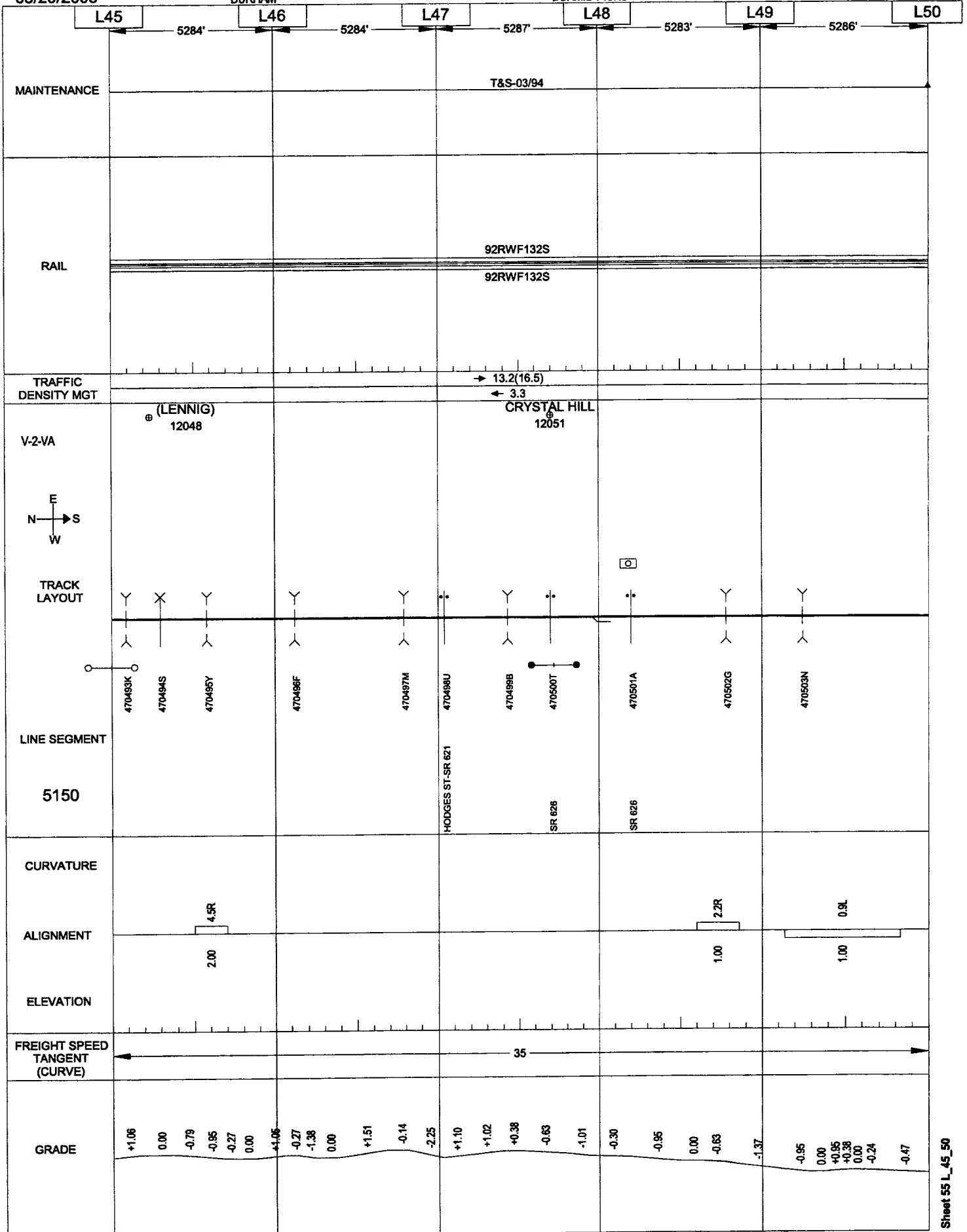


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DURMID-PICKS

VIRGINIA

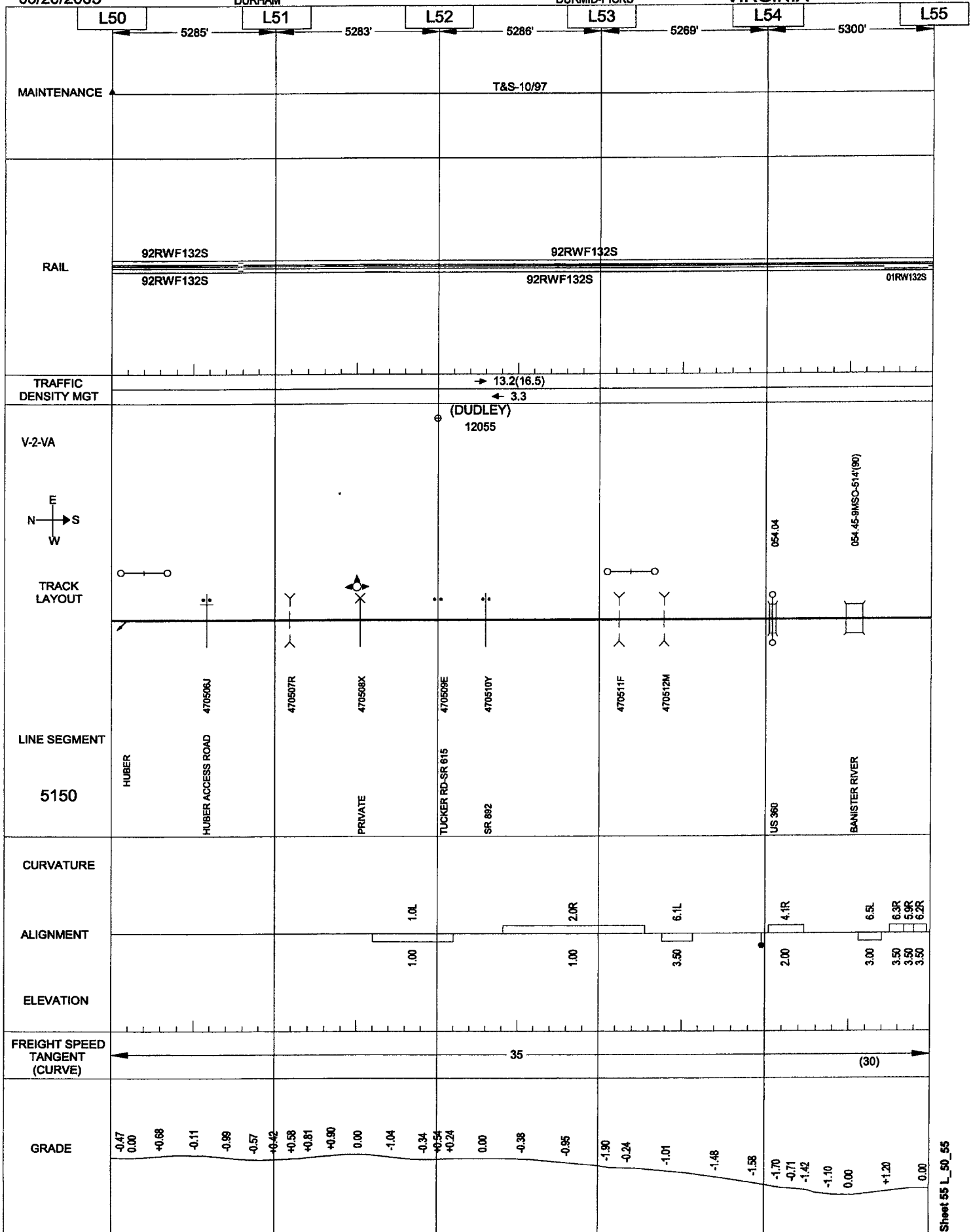


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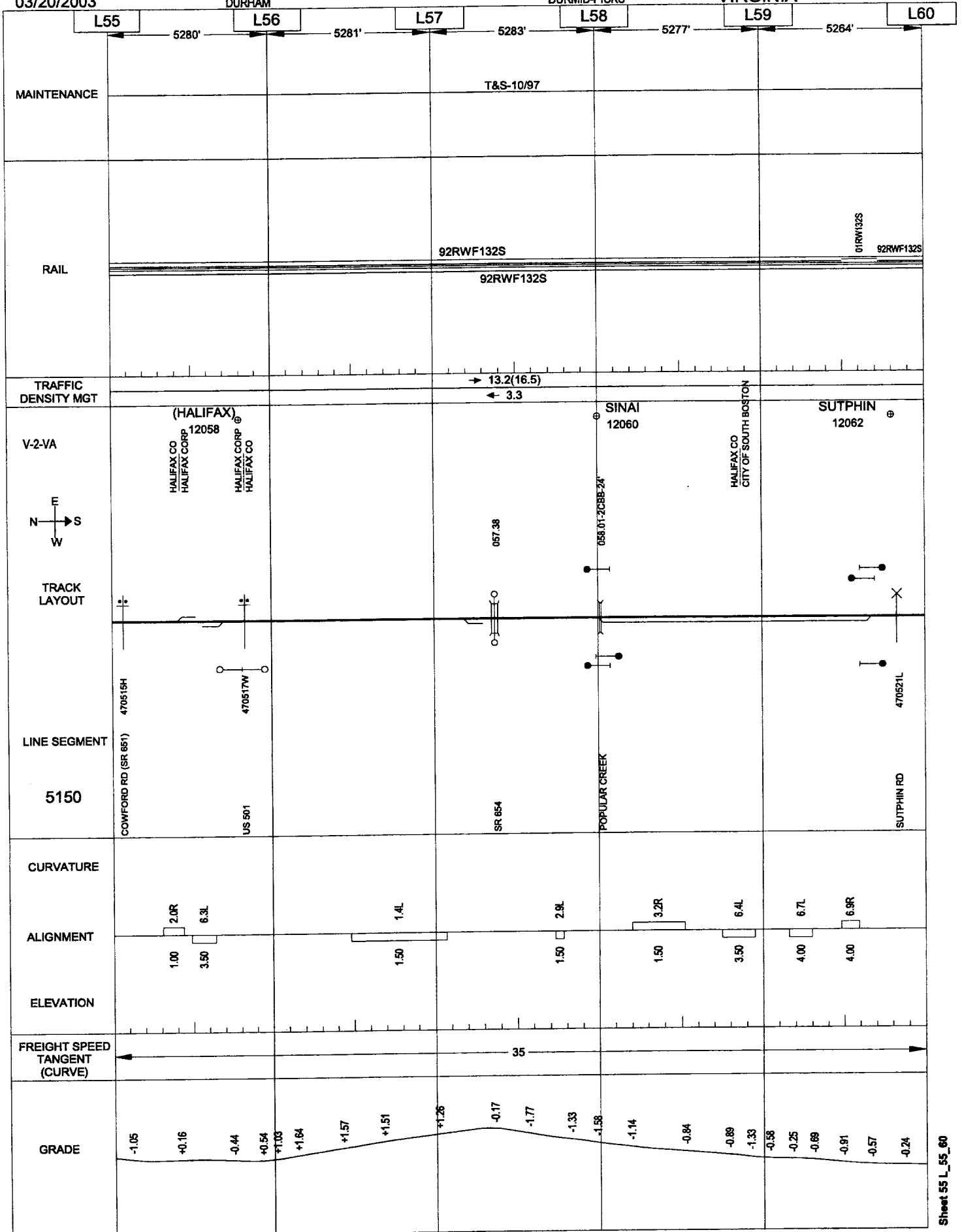


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DURMID-PICKS

VIRGINIA

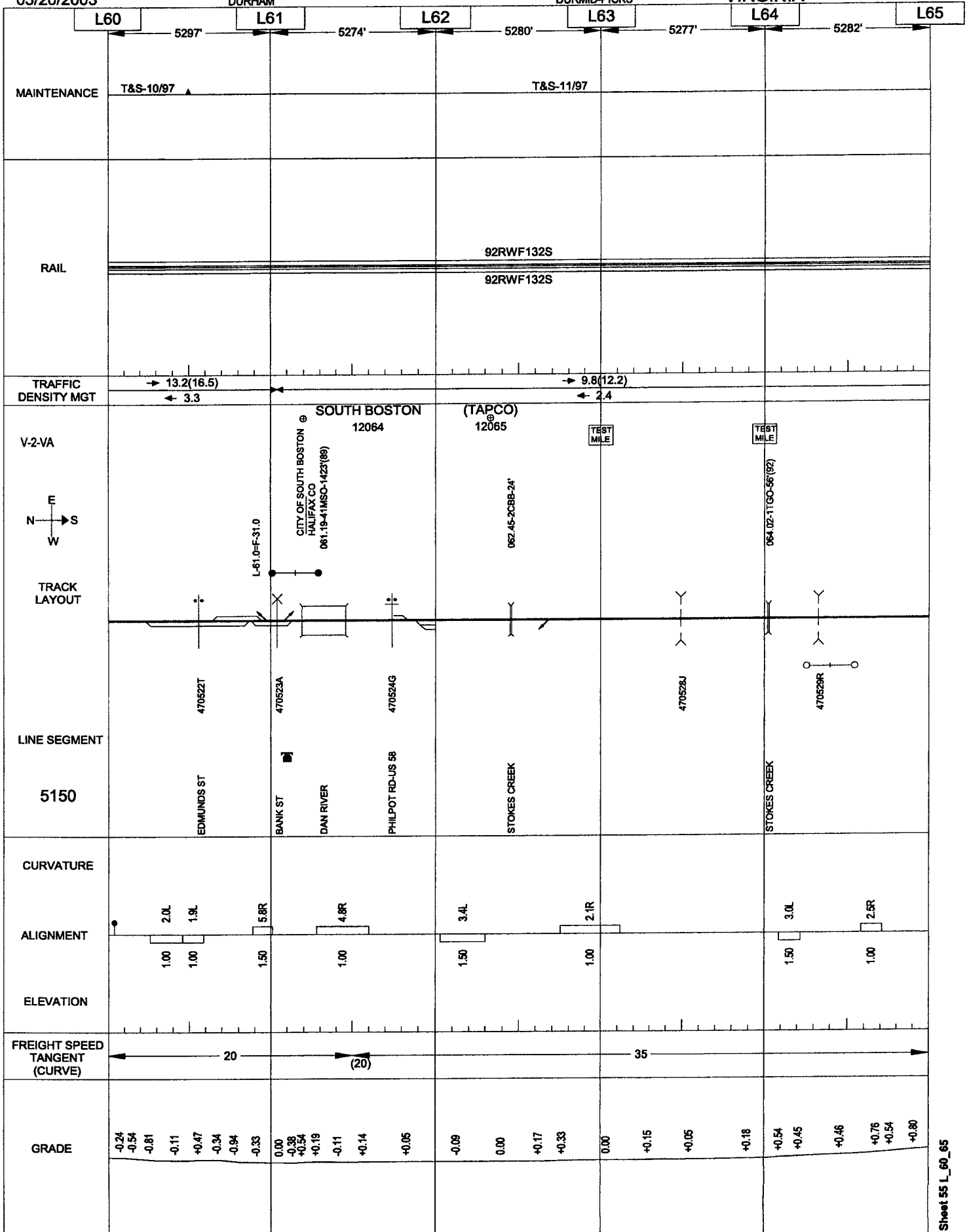


03/20/2003

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DURMID-PICKS

VIRGINIA

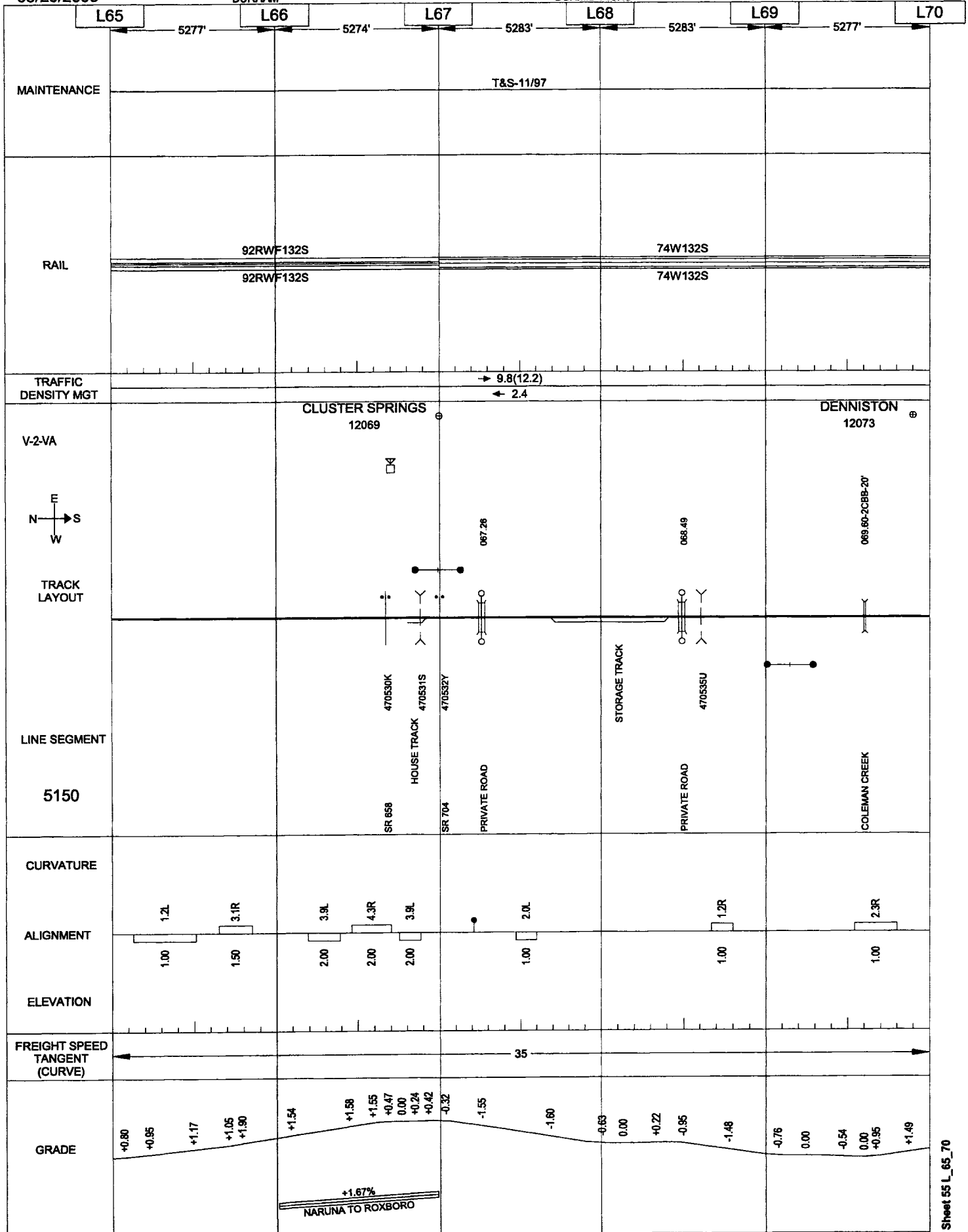


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DURHAM

DURMID-PICKS

VIRGINIA

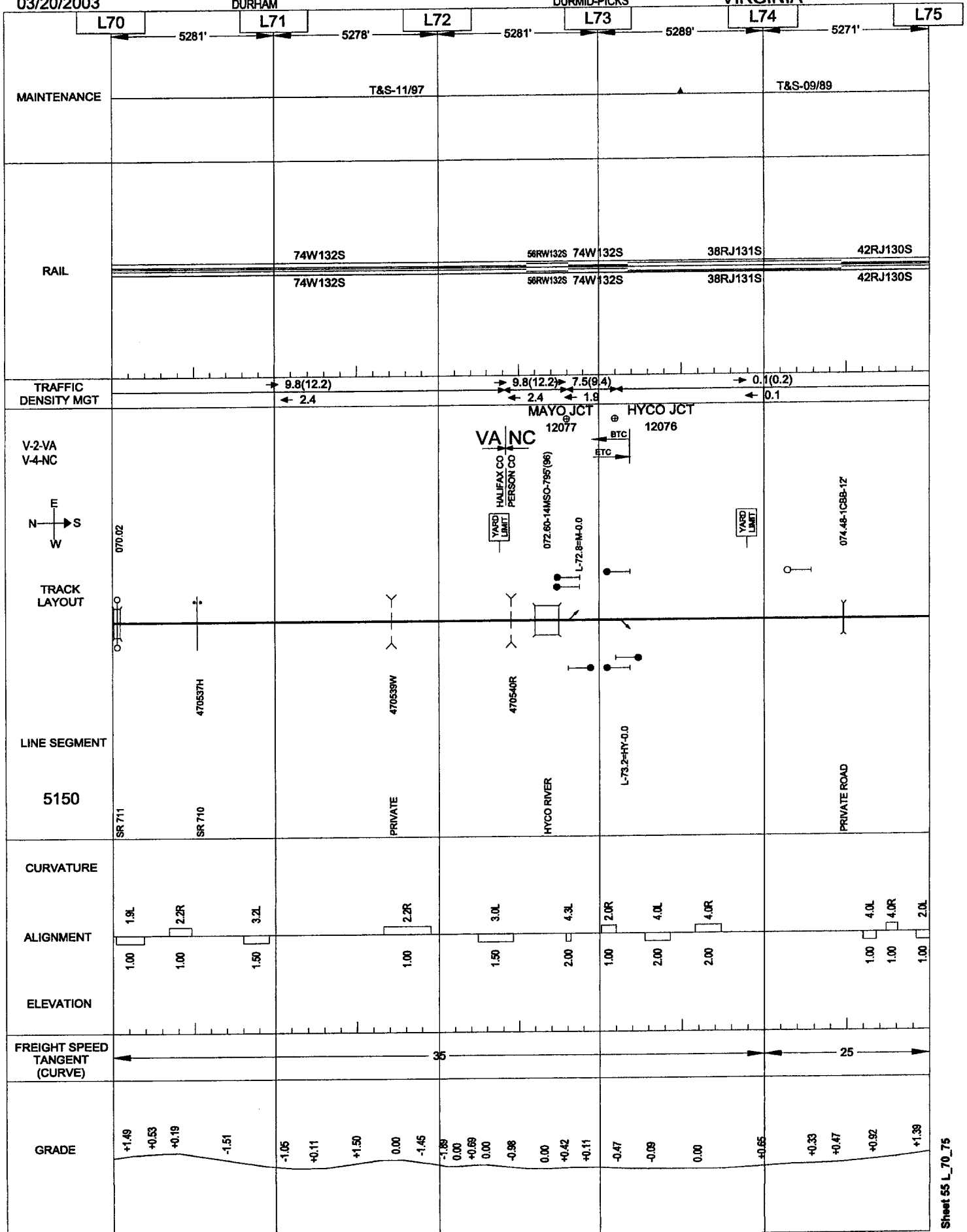


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DURMID-PICKS

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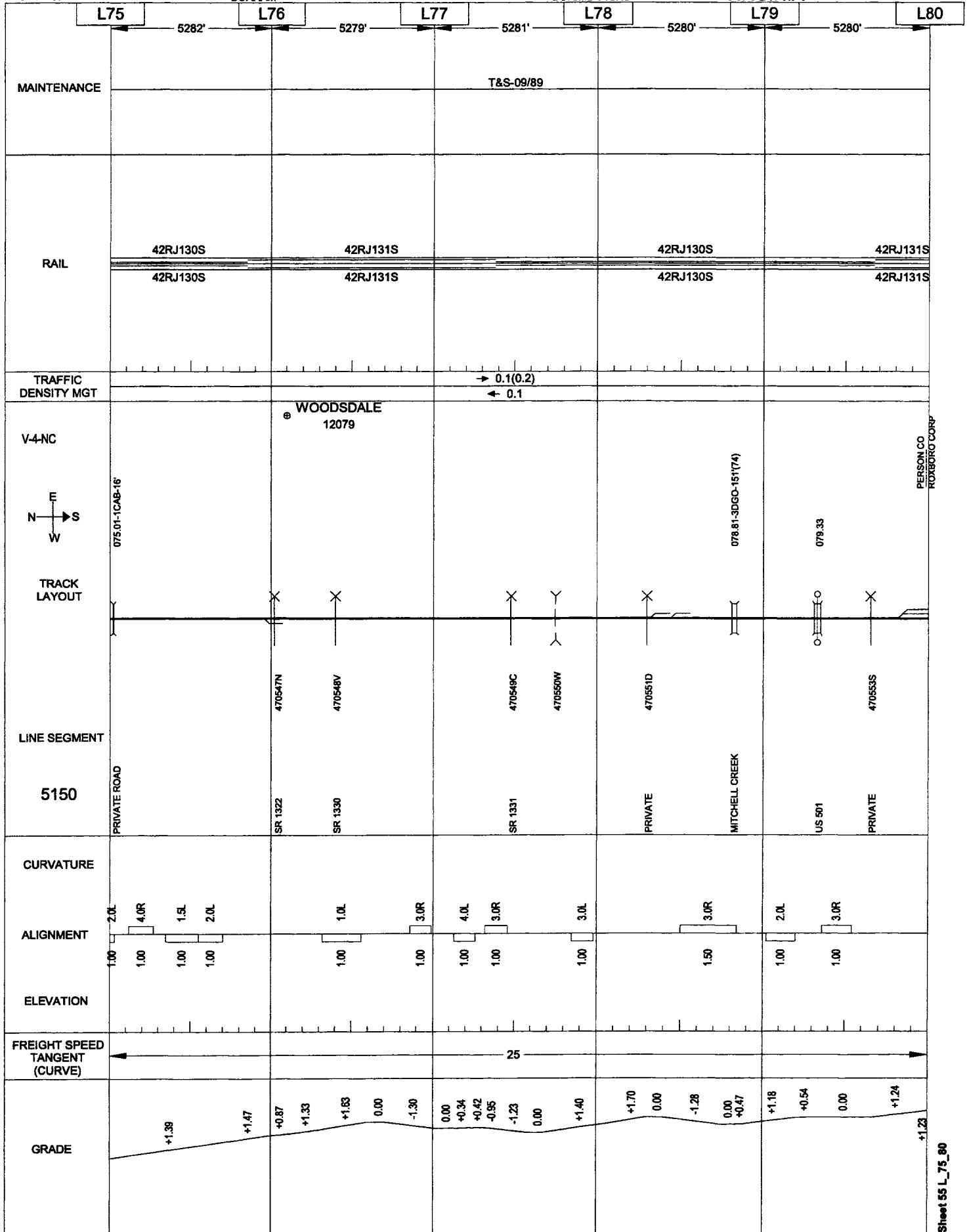


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA

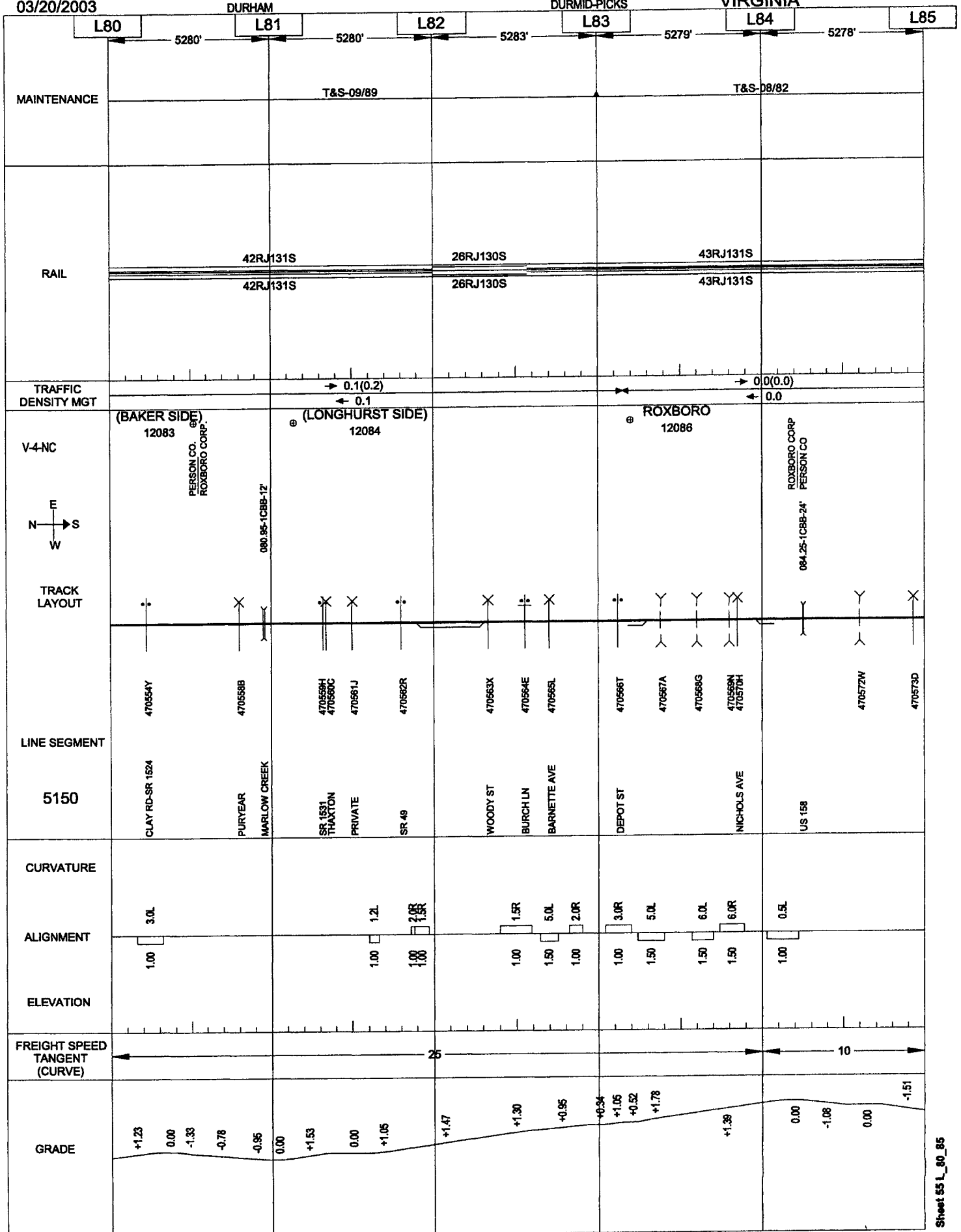


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA

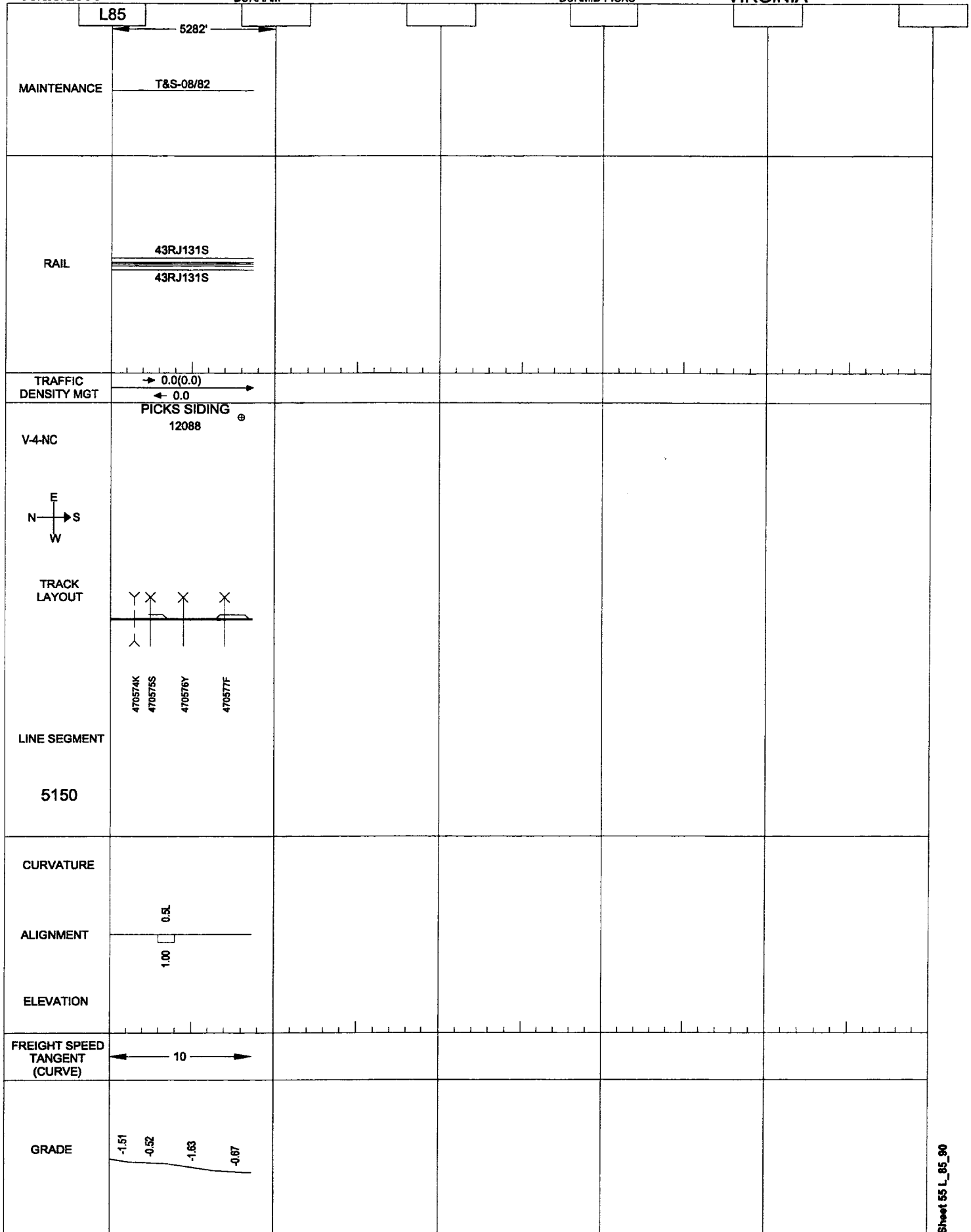


03/20/2003

DURHAM

DURMID-PICKS

VIRGINIA



265

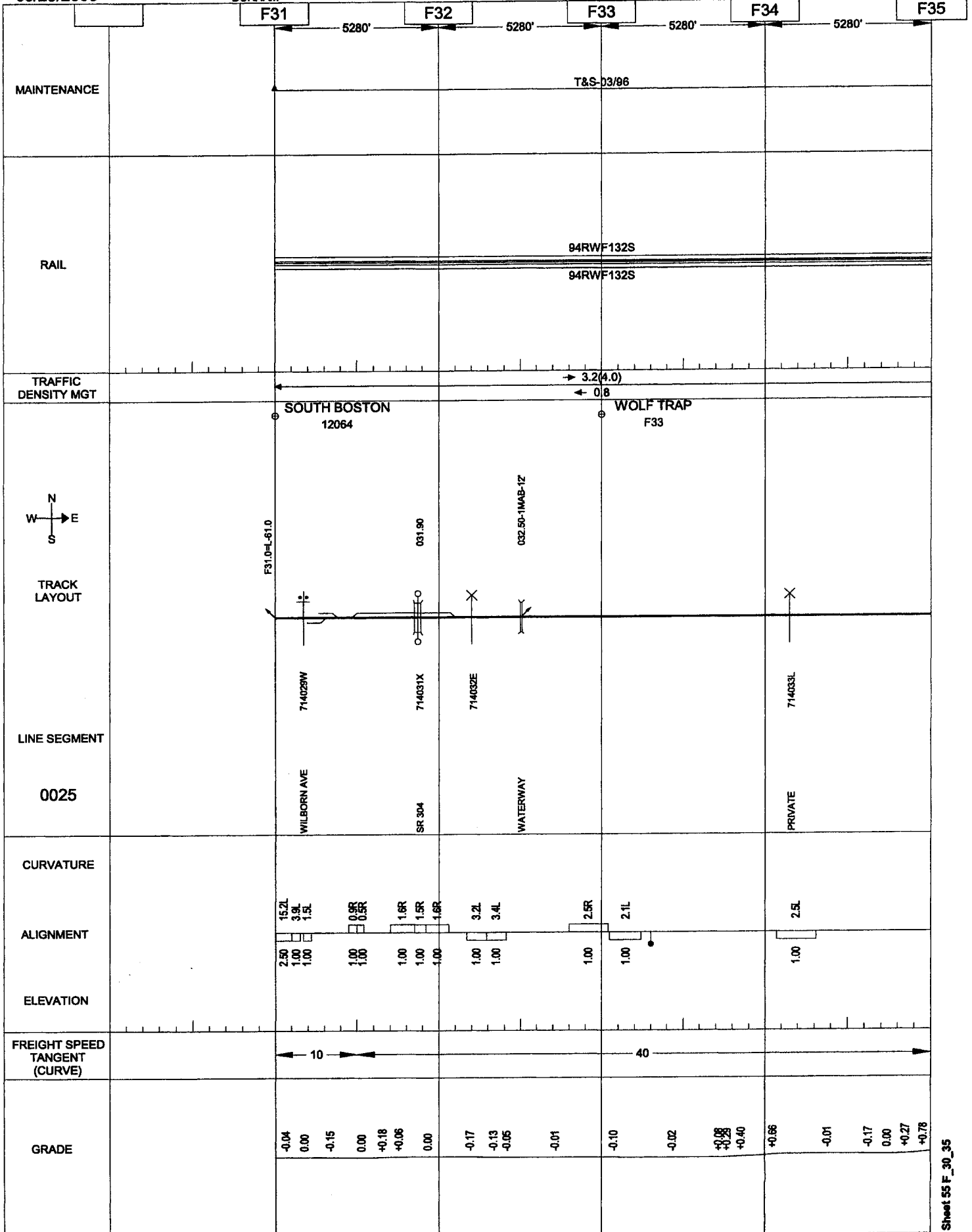
03/20/2003

DURHAM

CLOVER SPUR

SOUTH BOSTON-CLOVER

VIRGINIA



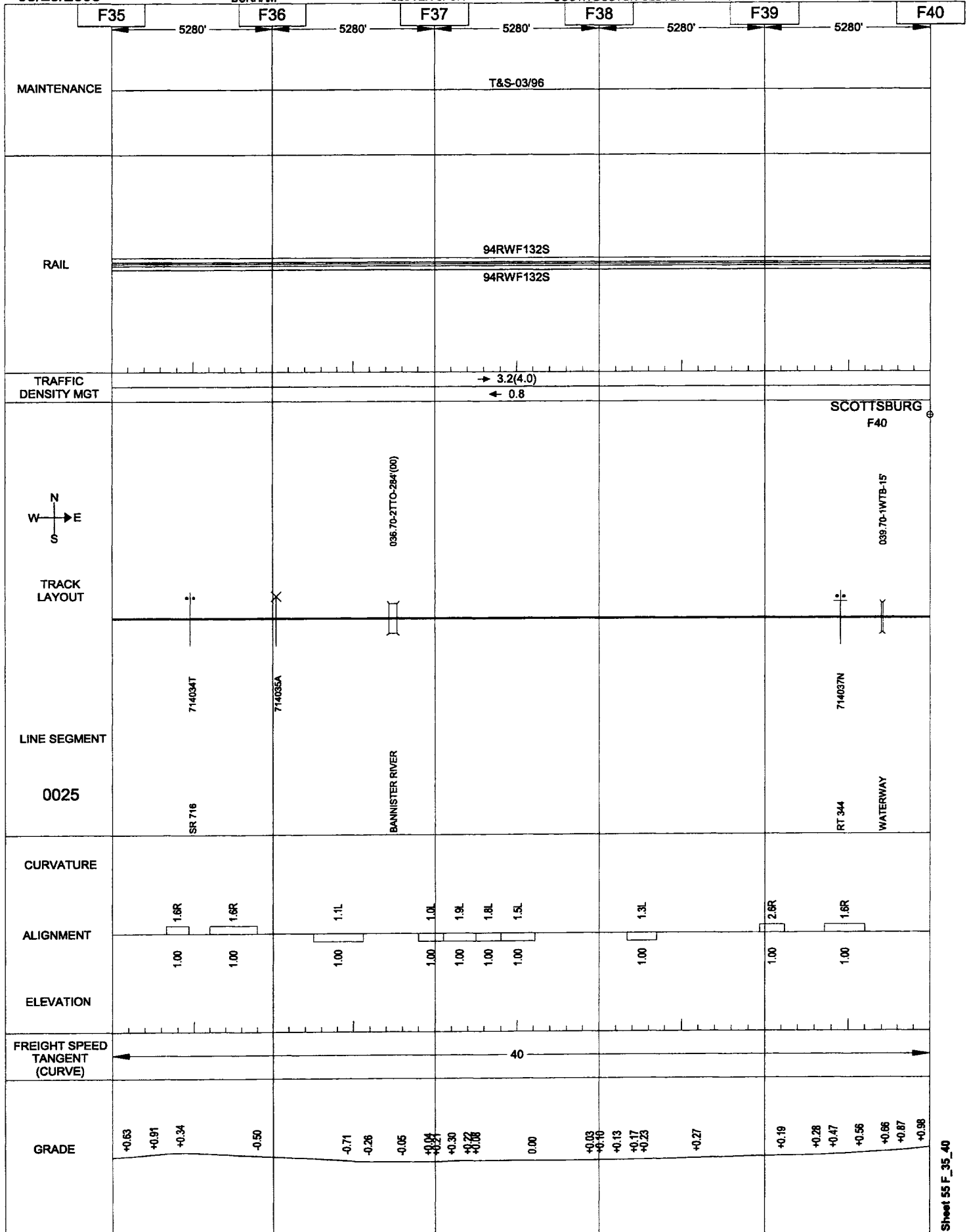
03/20/2003

DURHAM

CLOVER SPUR

SOUTH BOSTON-CLOVER

VIRGINIA



SCOTTSBURG
F40



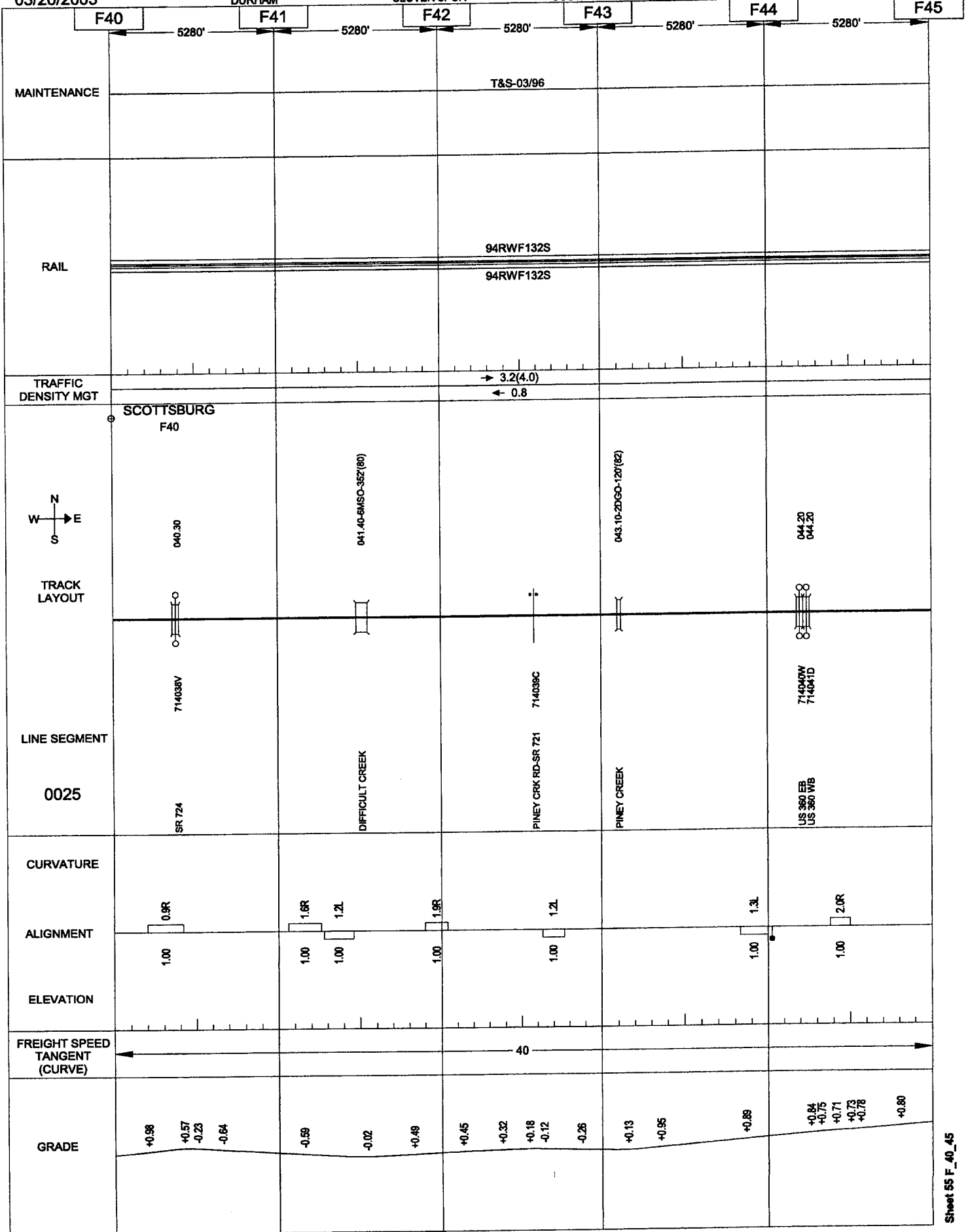
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CLOVER SPUR

SOUTH BOSTON-CLOVER

VIRGINIA



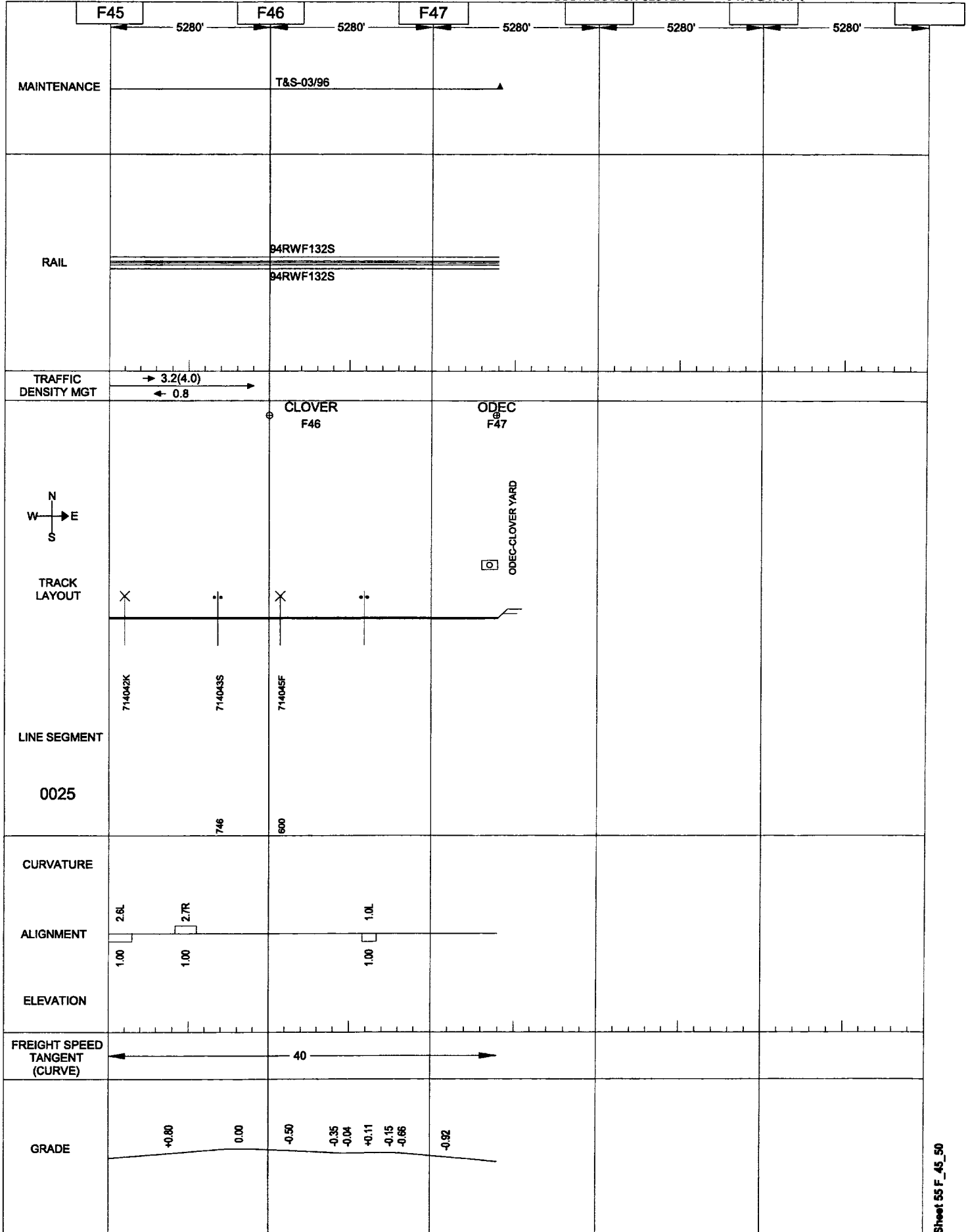
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CLOVER SPUR

SOUTH BOSTON-CLOVER

VIRGINIA



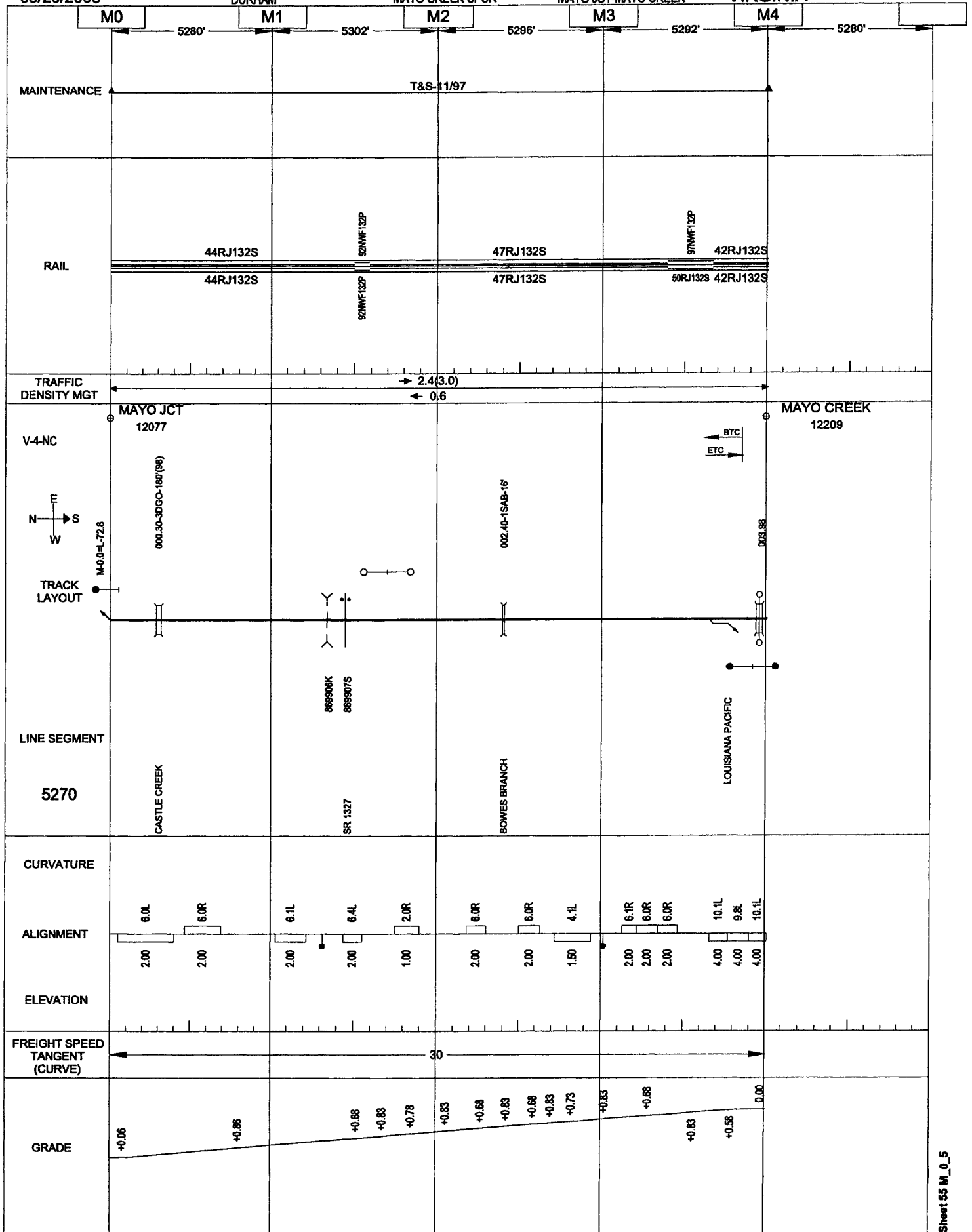
03/20/2003

DURHAM

MAYO CREEK SPUR

MAYO JCT-MAYO CREEK

VIRGINIA



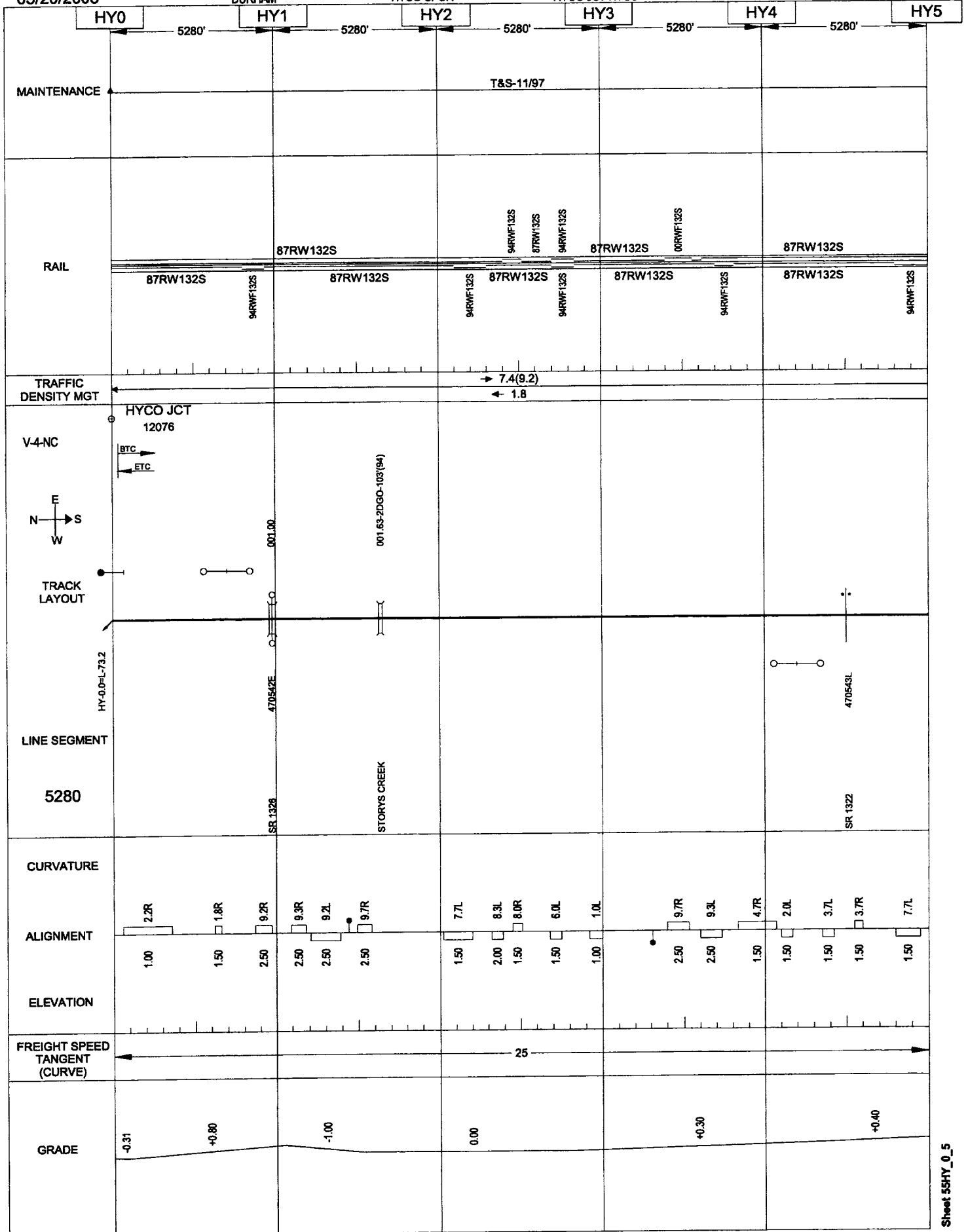
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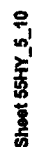
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HYCO SPUR

HYCO JCT-HYCO

VIRGINIA



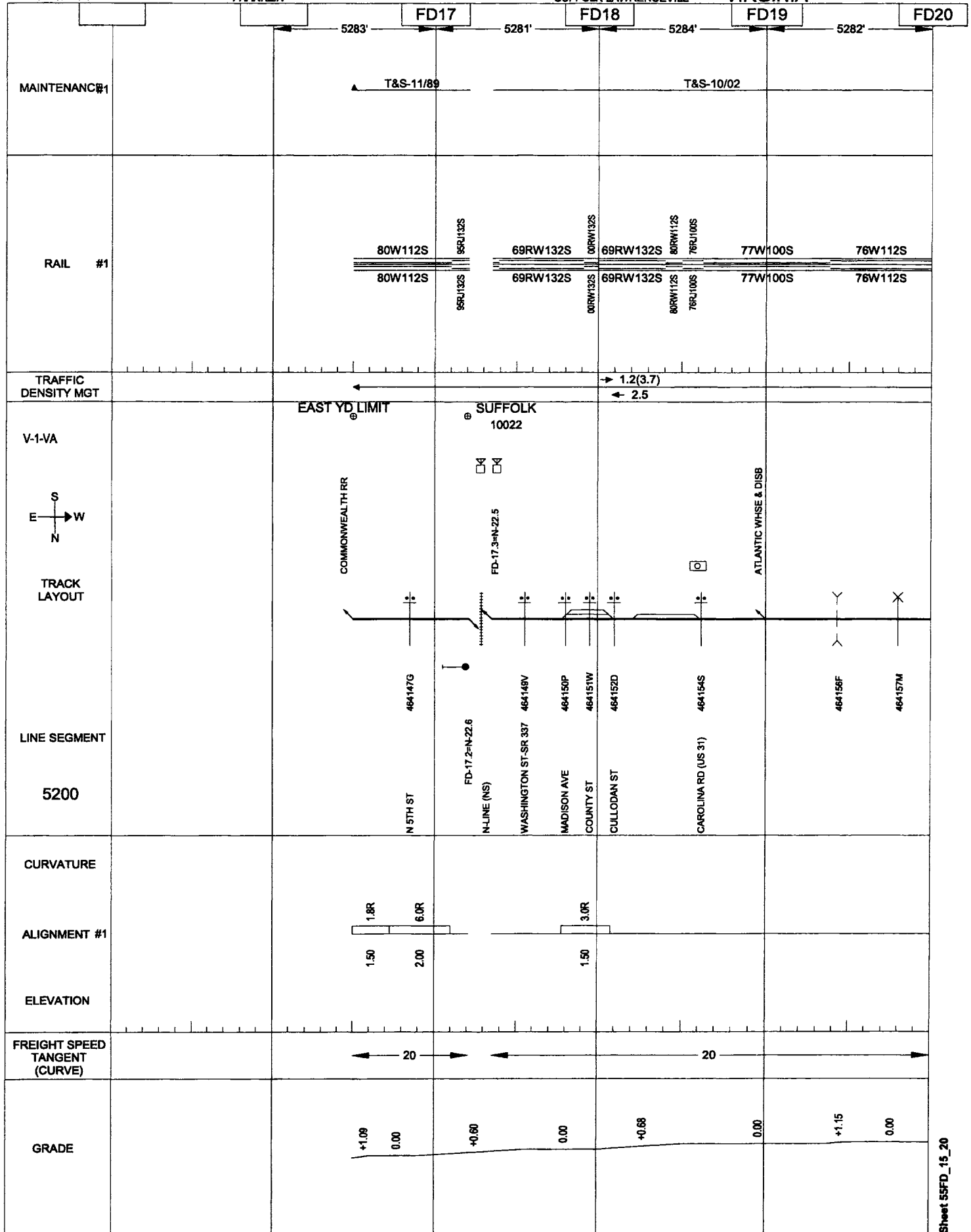


03/20/2003

FRANKLIN

SUFFOLK-LAWRENCEVILL

VIRGINIA

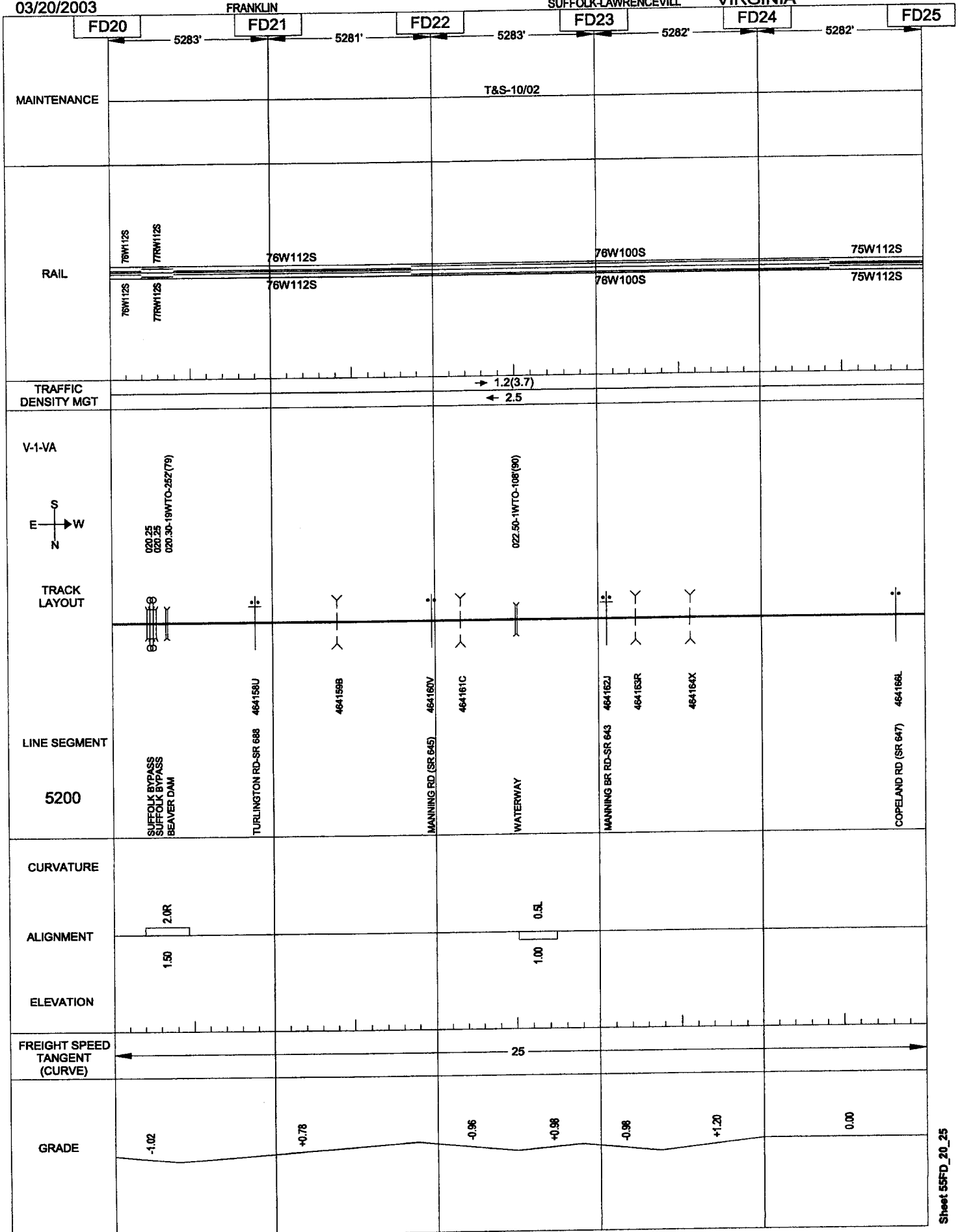


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VIRGINIA

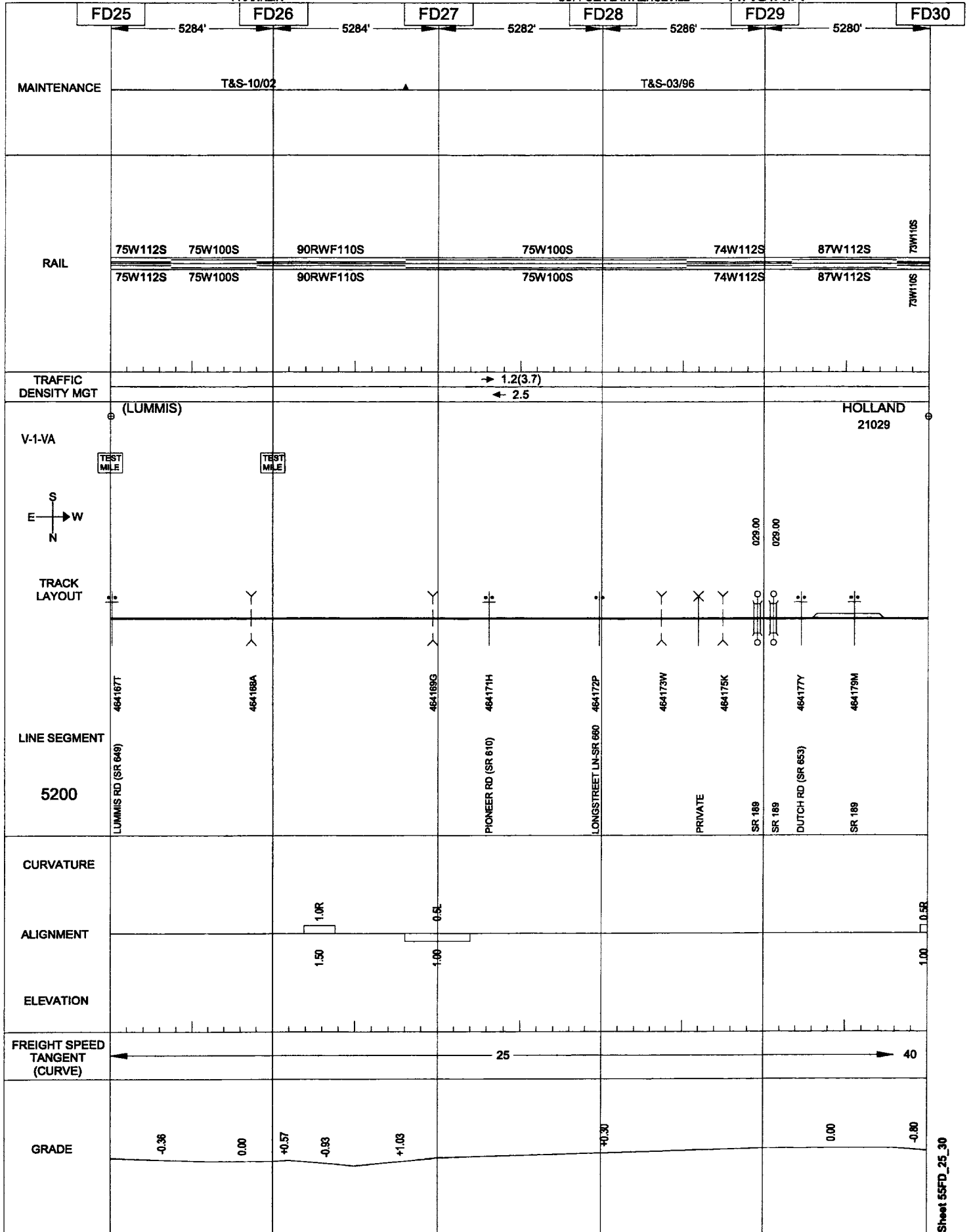


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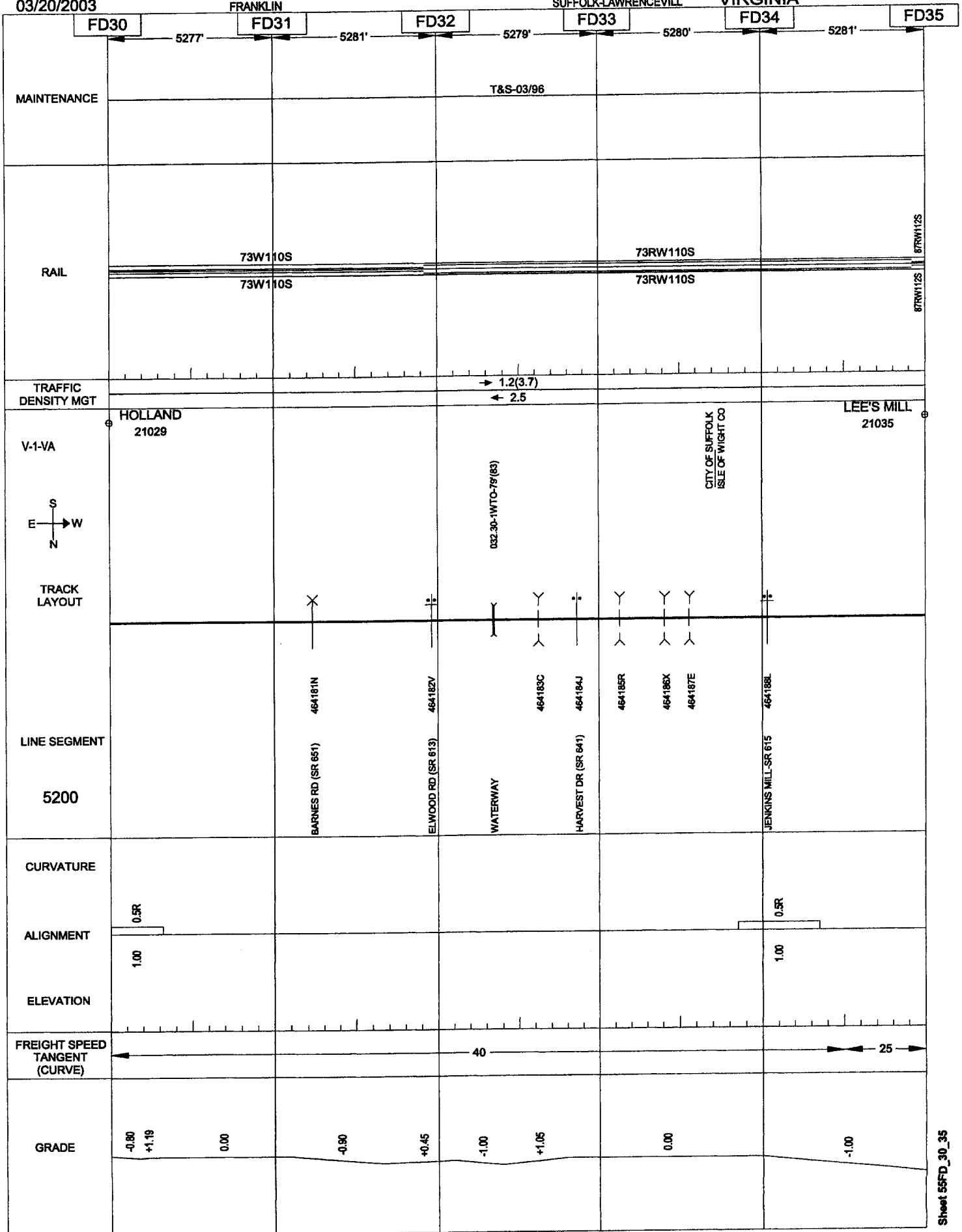


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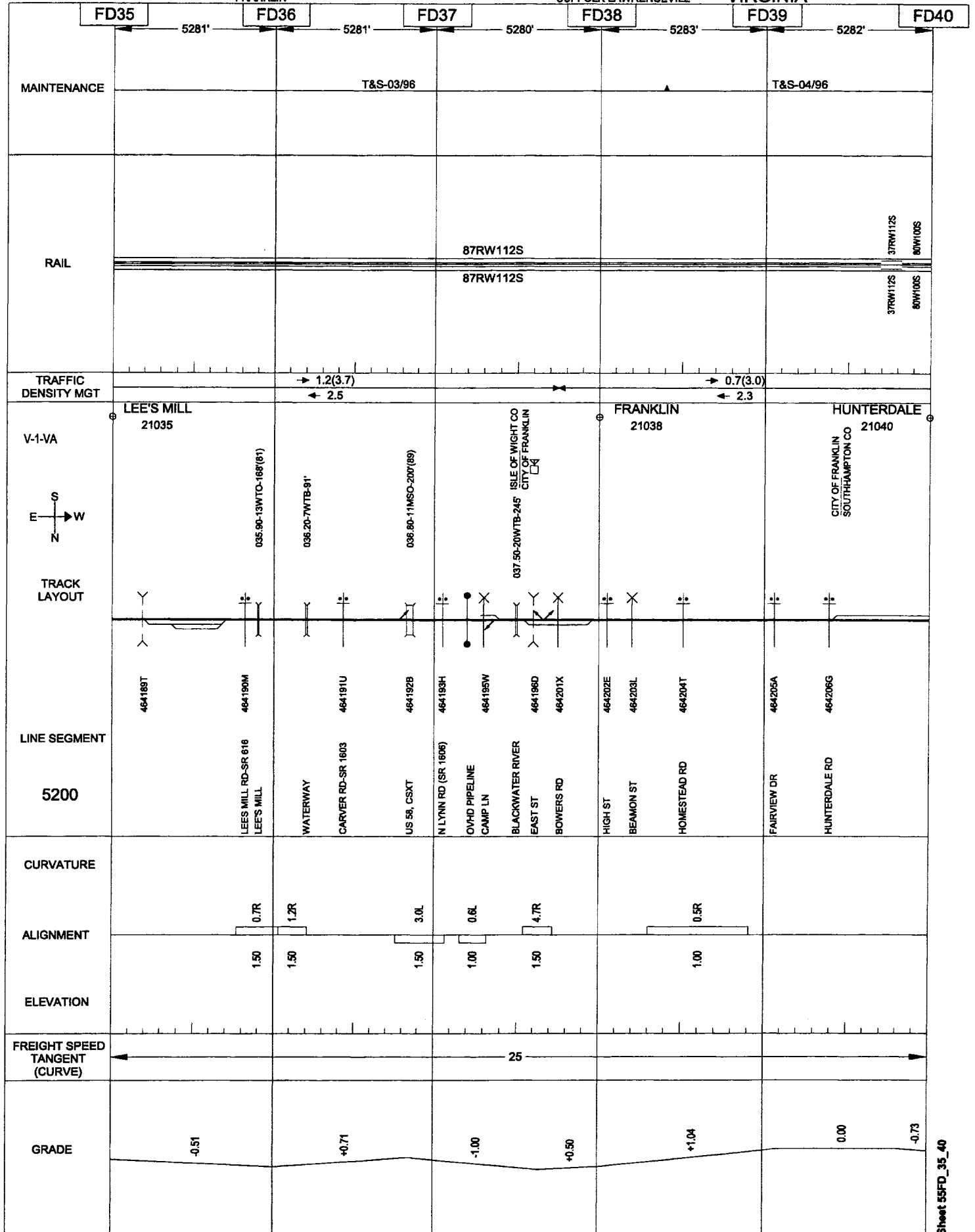


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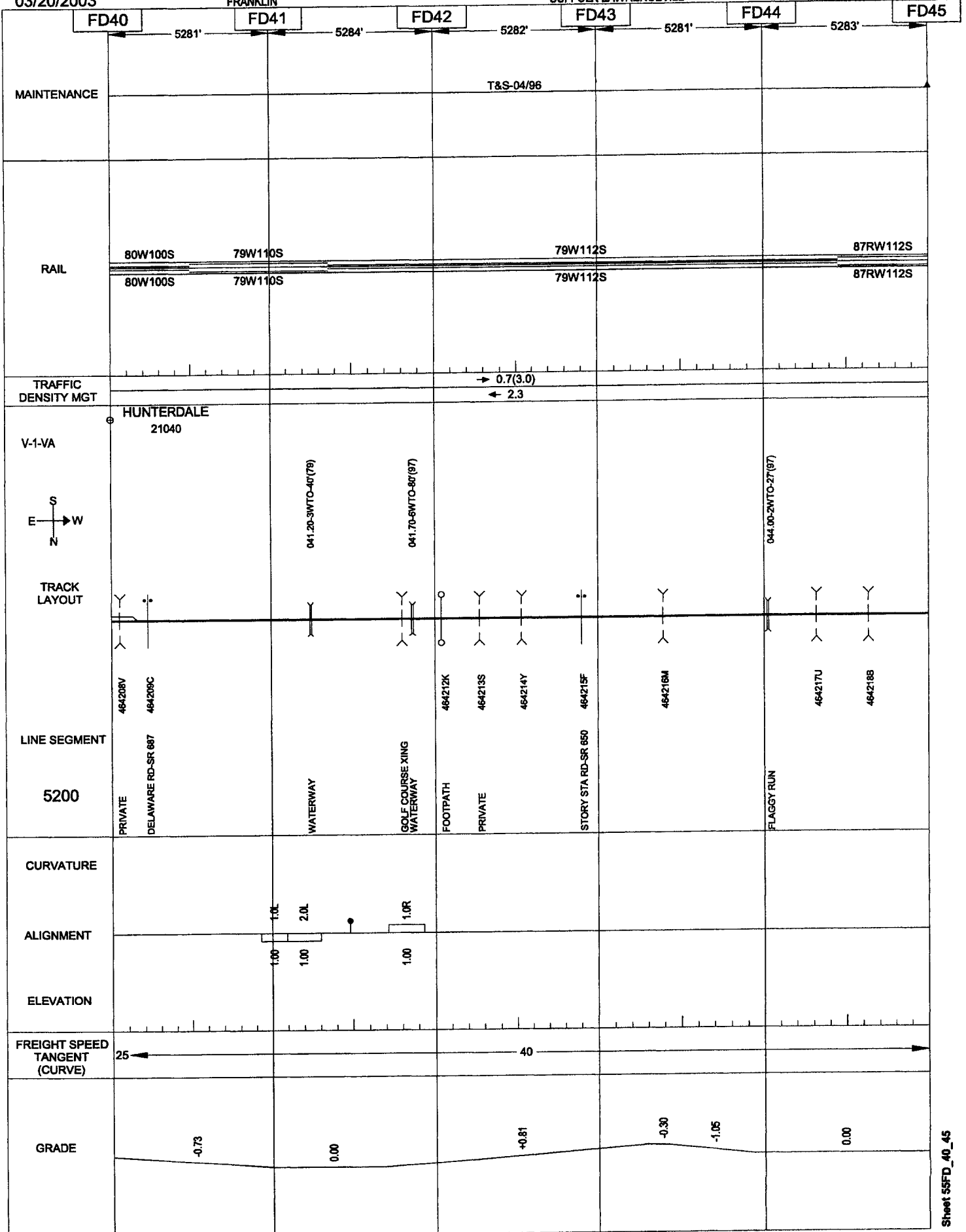


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SUFFOLK-LAWRENCEVILL

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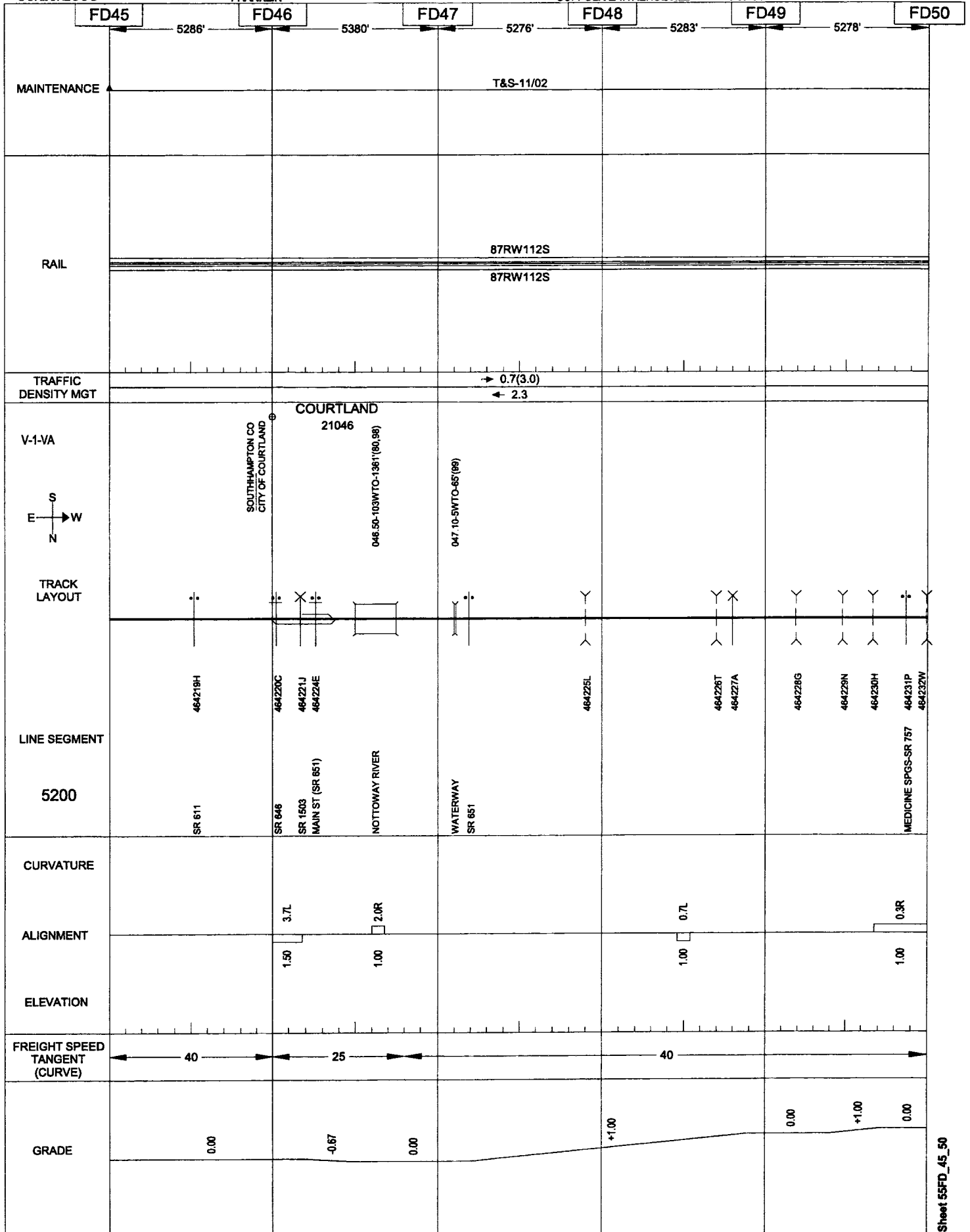


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FRANKLIN

SUFFOLK-LAWRENCEVILL

VIRGINIA

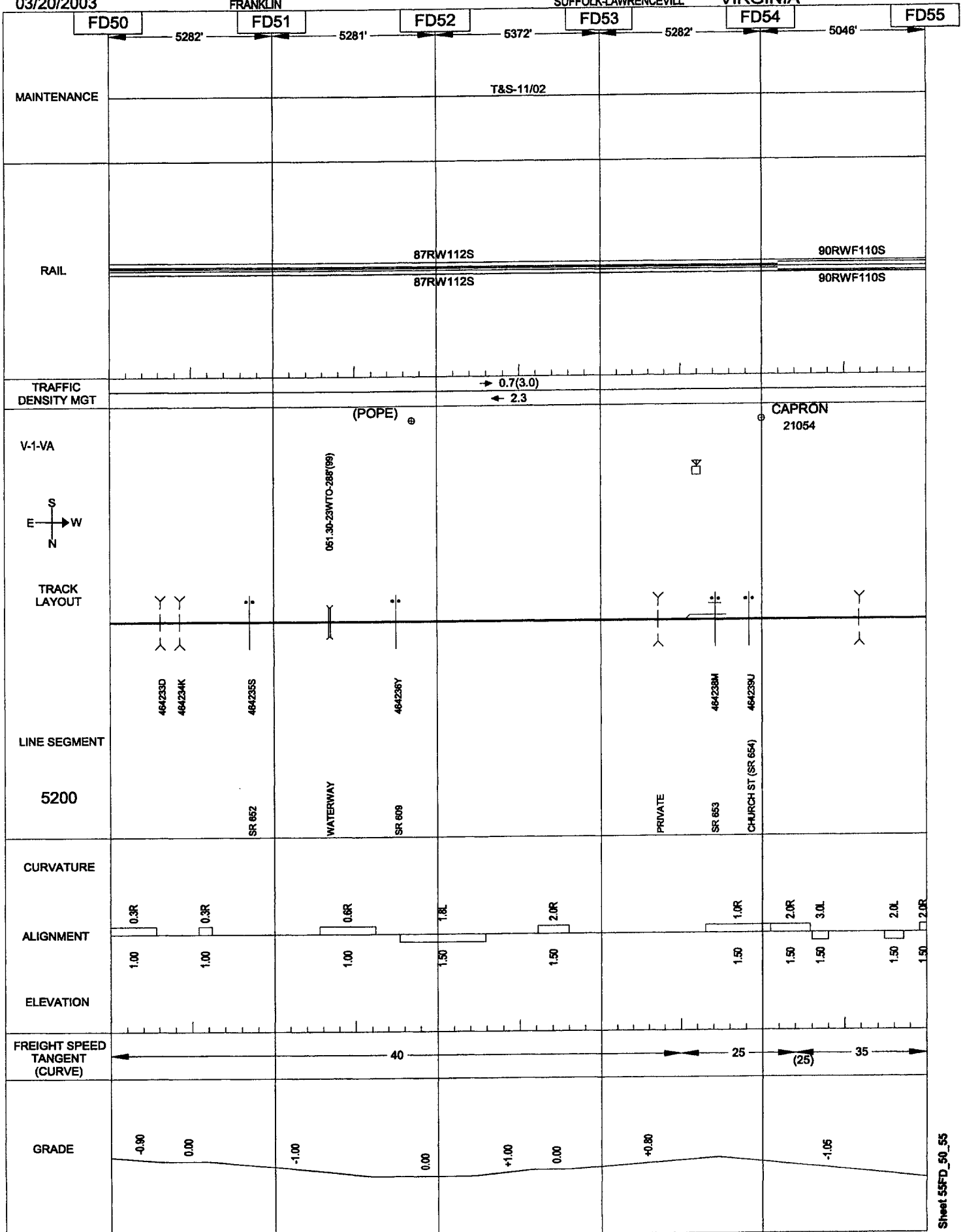


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SUFFOLK-LAWRENCEVILL

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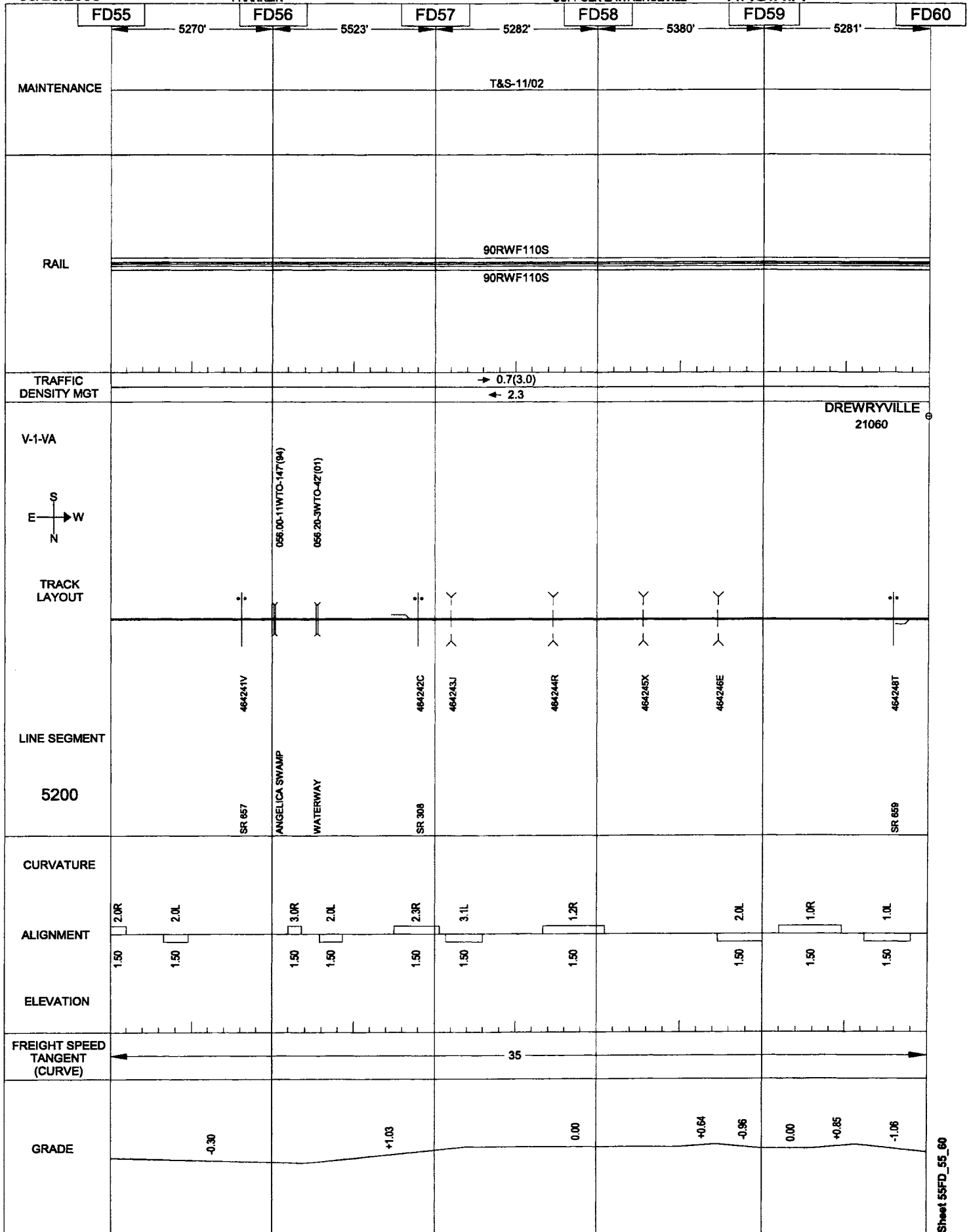


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FRANKLIN

SUFFOLK-LAWRENCEVILL

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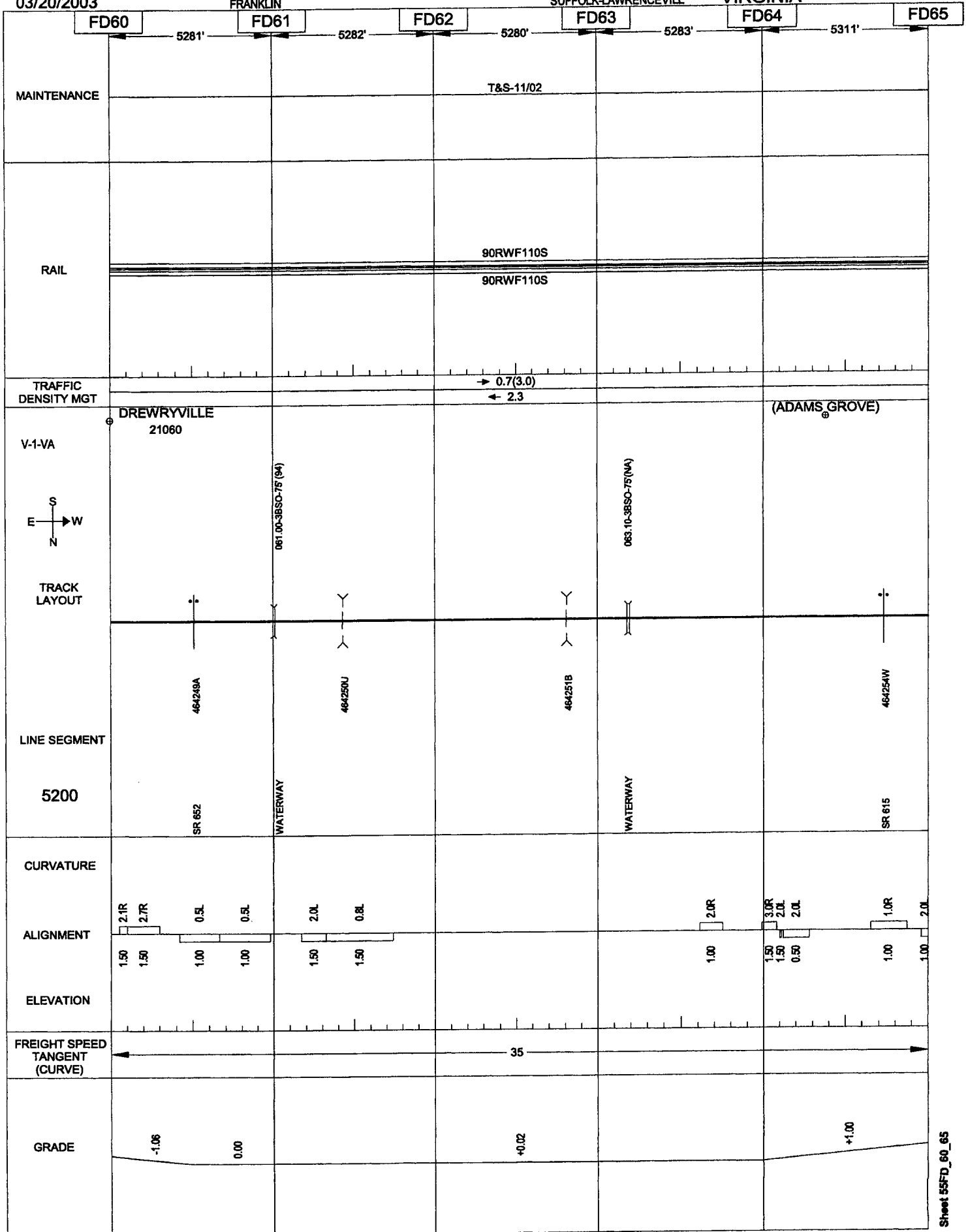


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SUFFOLK-LAWRENCEVILLE

VIRGINIA

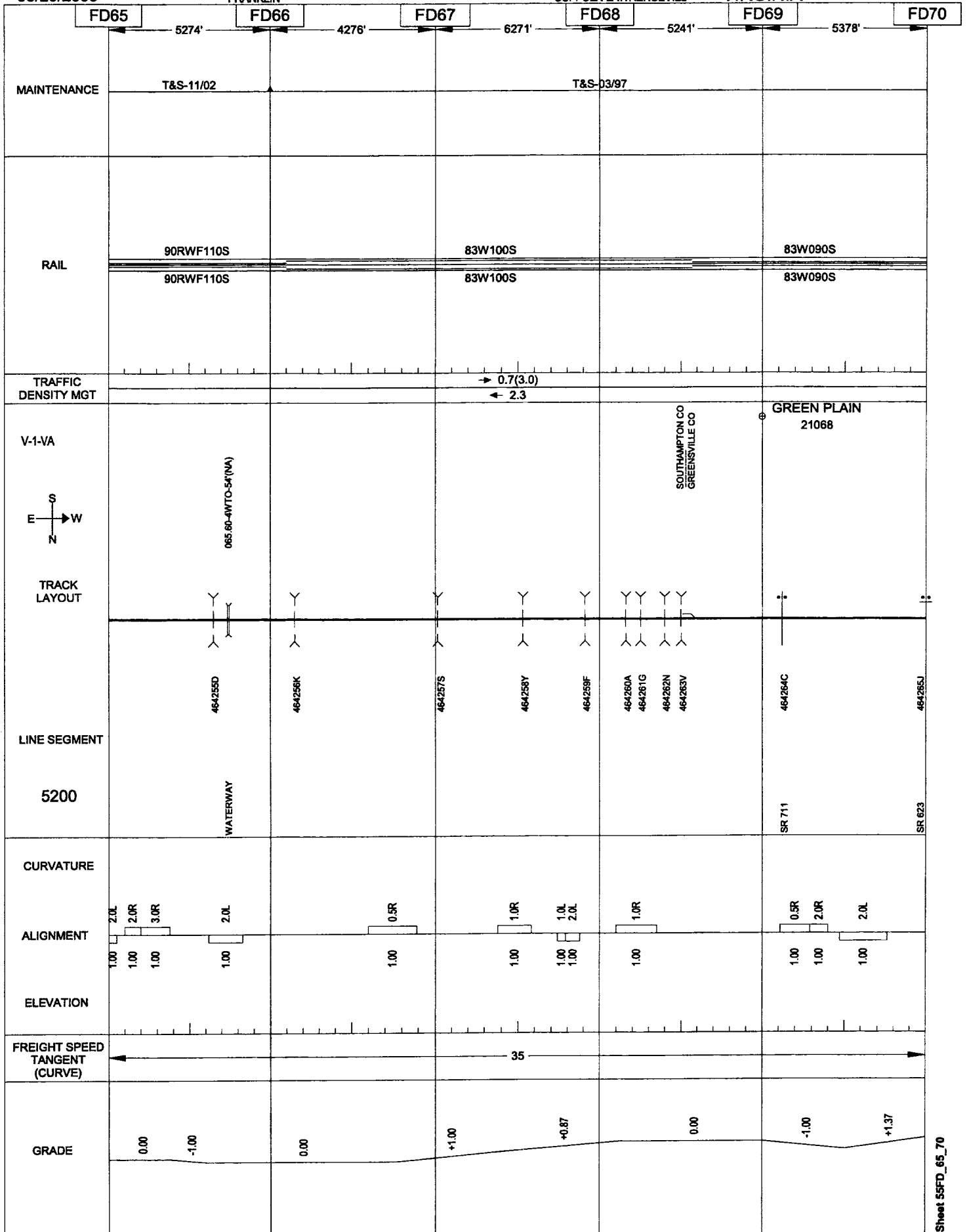


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SUFFOLK-LAWRENCEVILL

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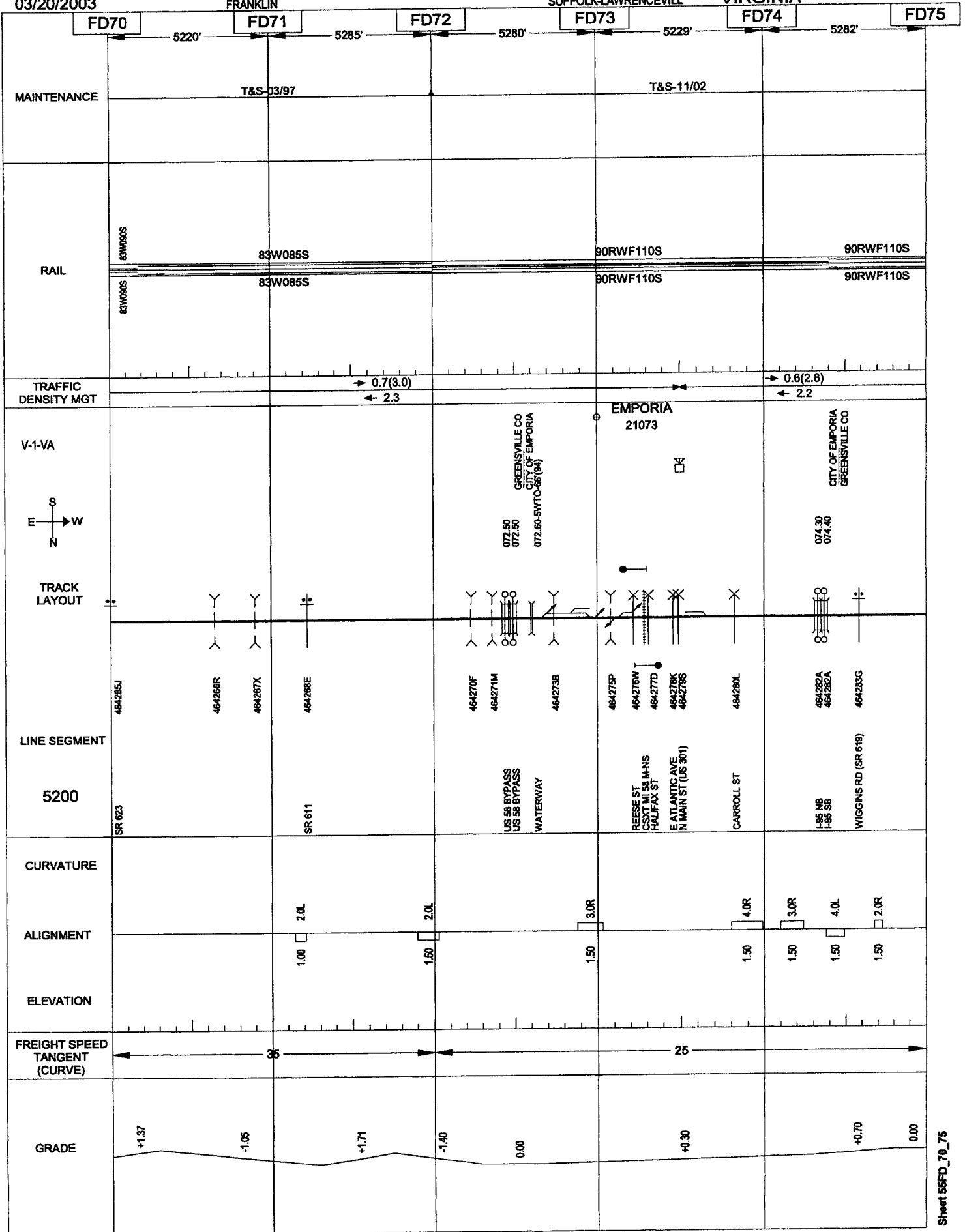


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SUFFOLK-LAWRENCEVILL

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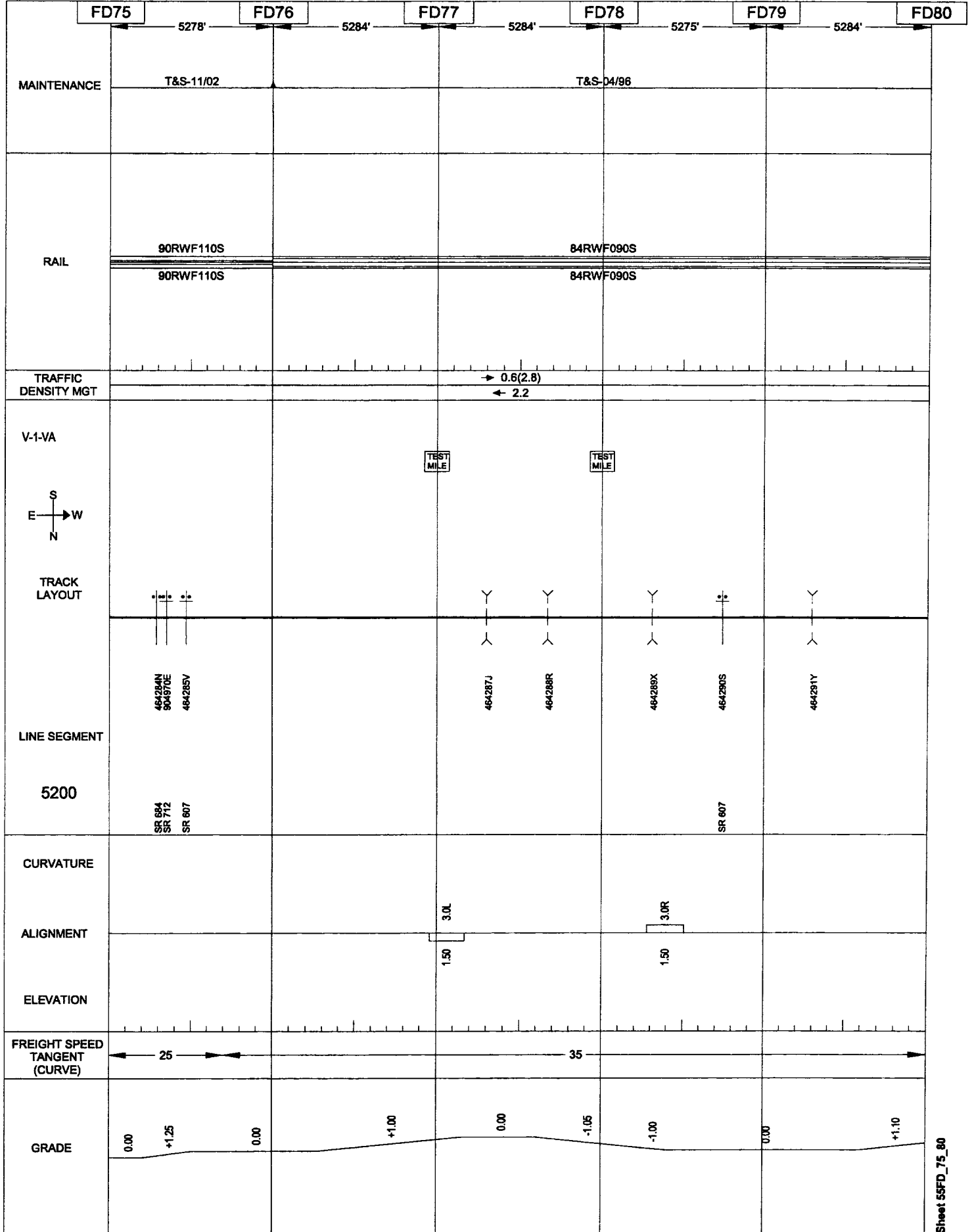


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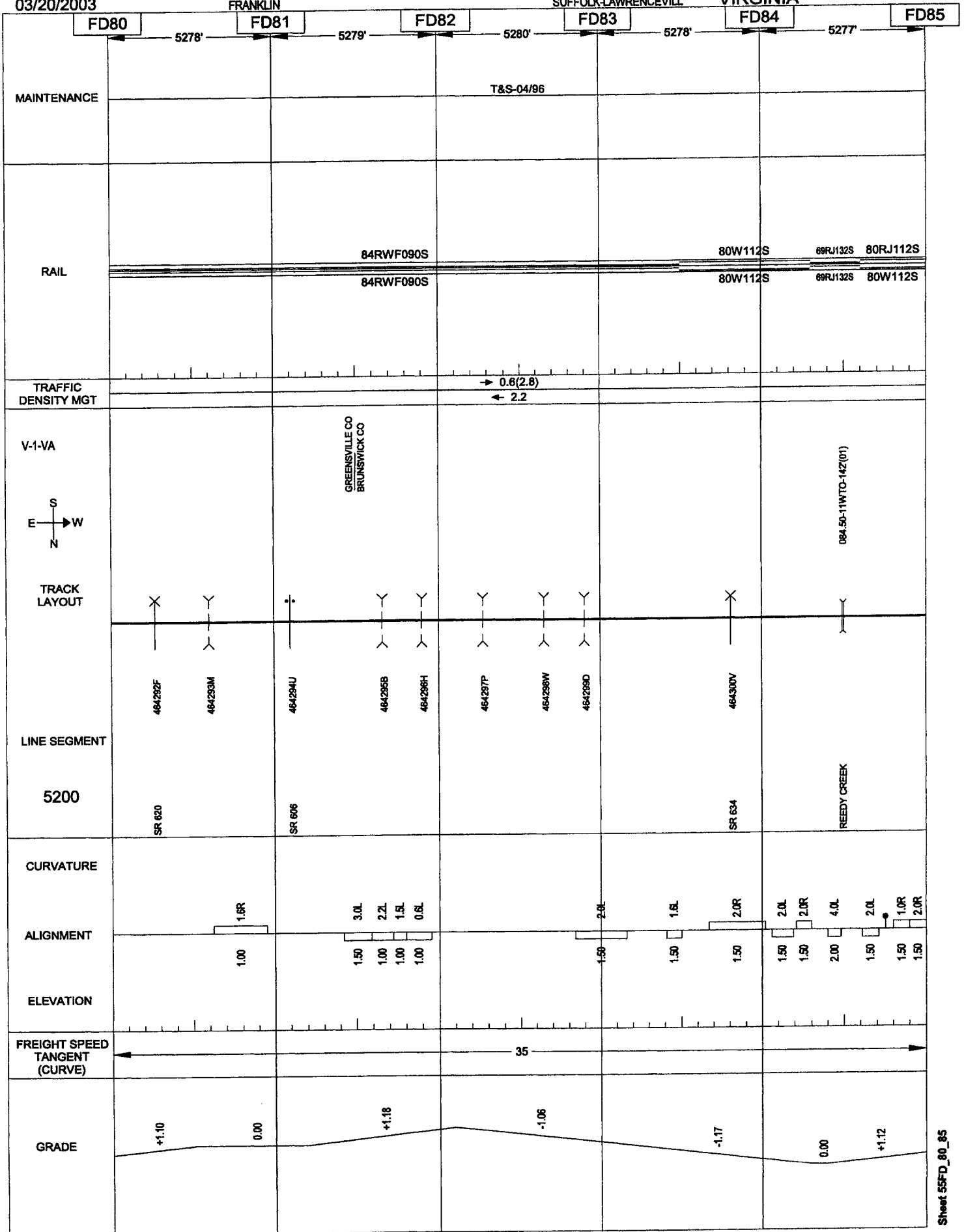


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SUFFOLK-LAWRENCEVILL

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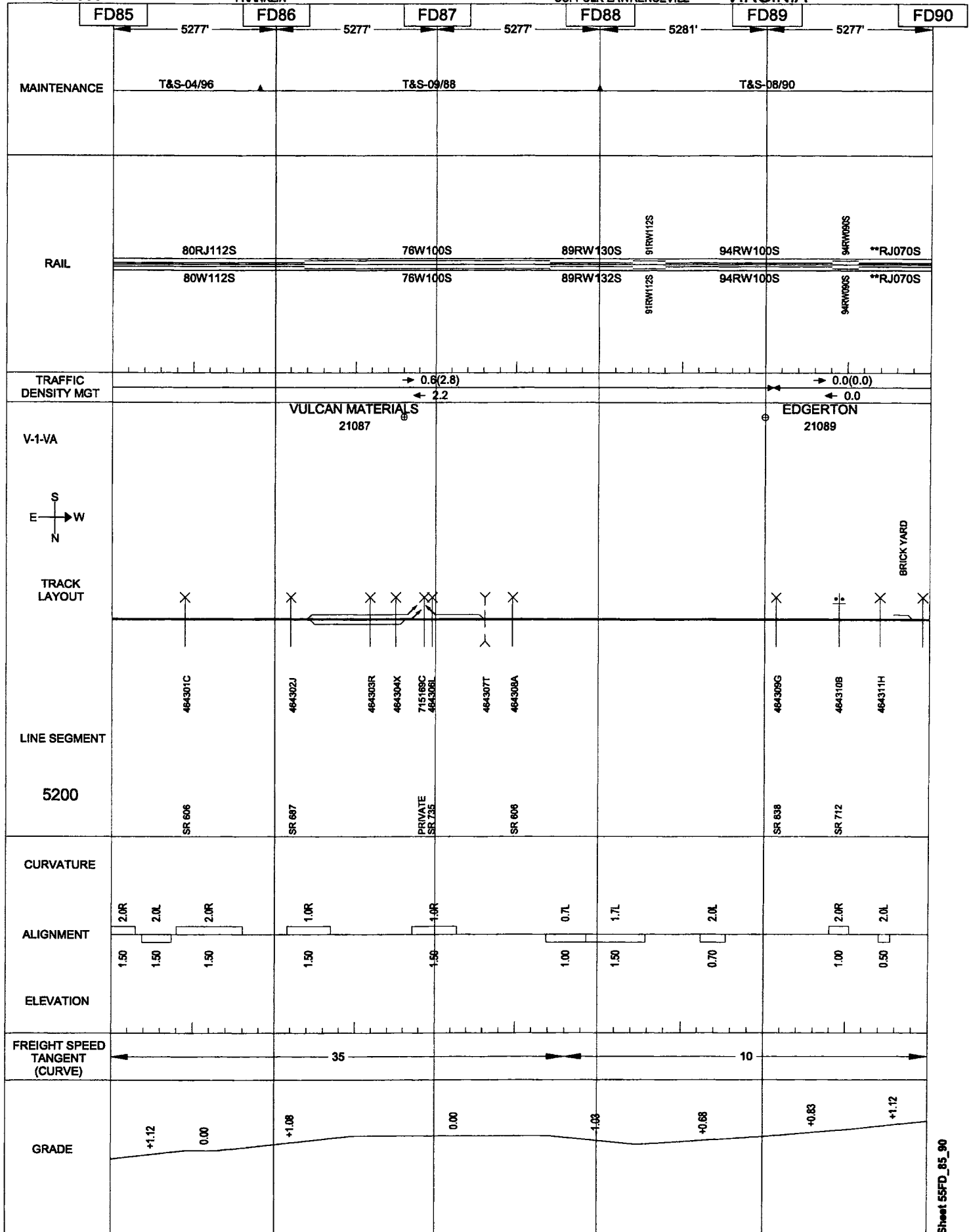


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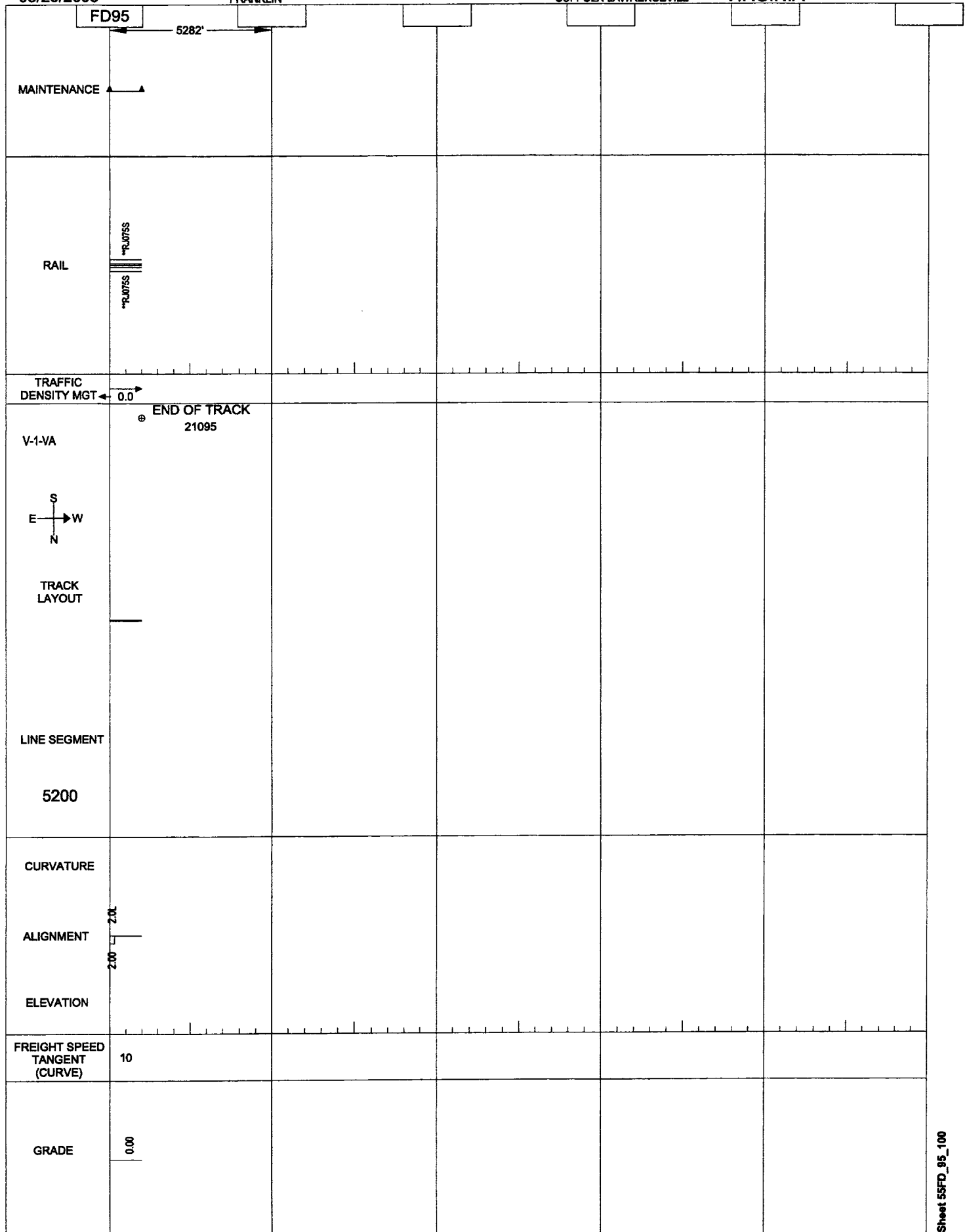
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03/20/2003

FRANKLIN

SUFFOLK-LAWRENCEVILL

VIRGINIA



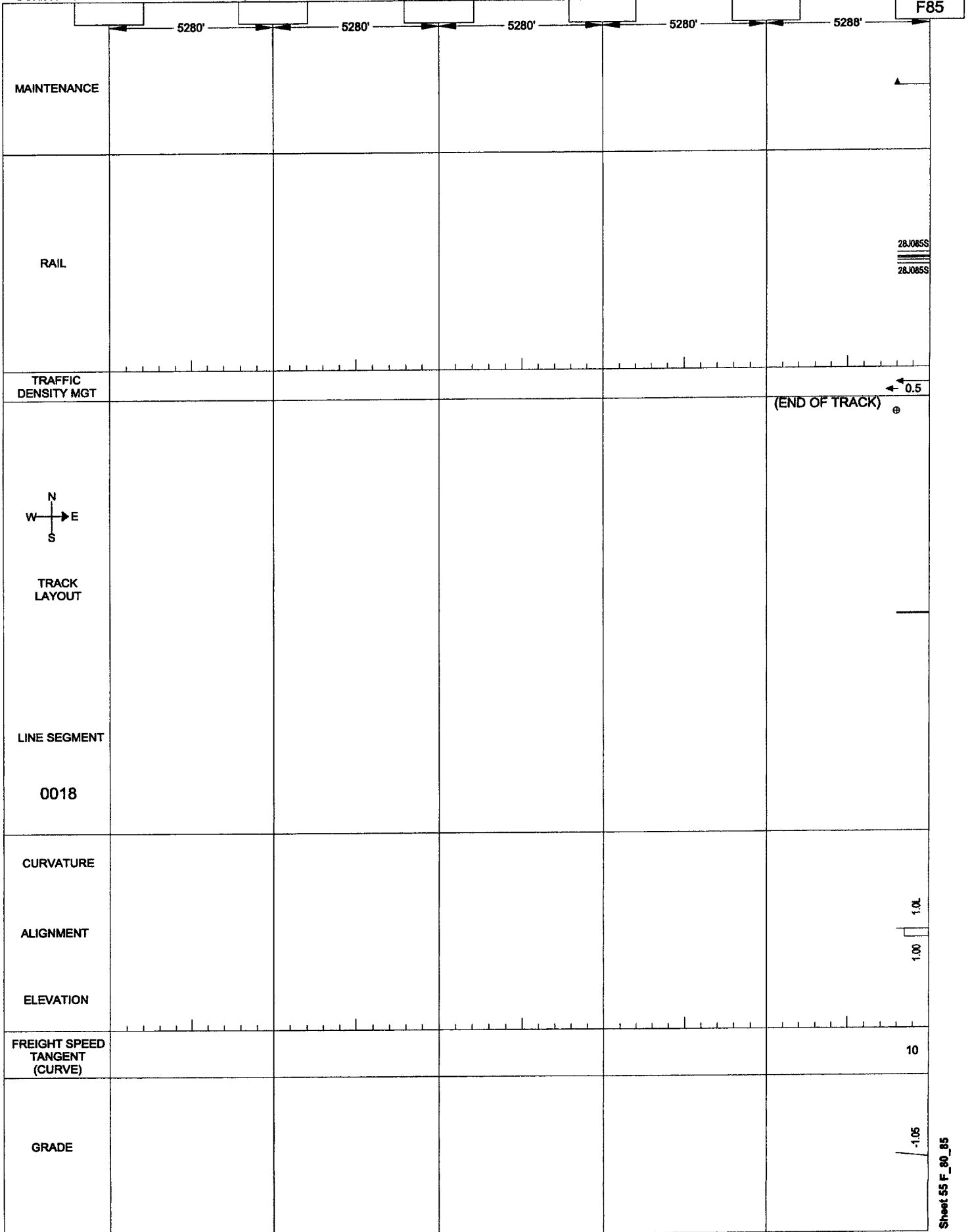
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RICHMOND

BURKEVILLE-RICHMOND

VIRGINIA

F85

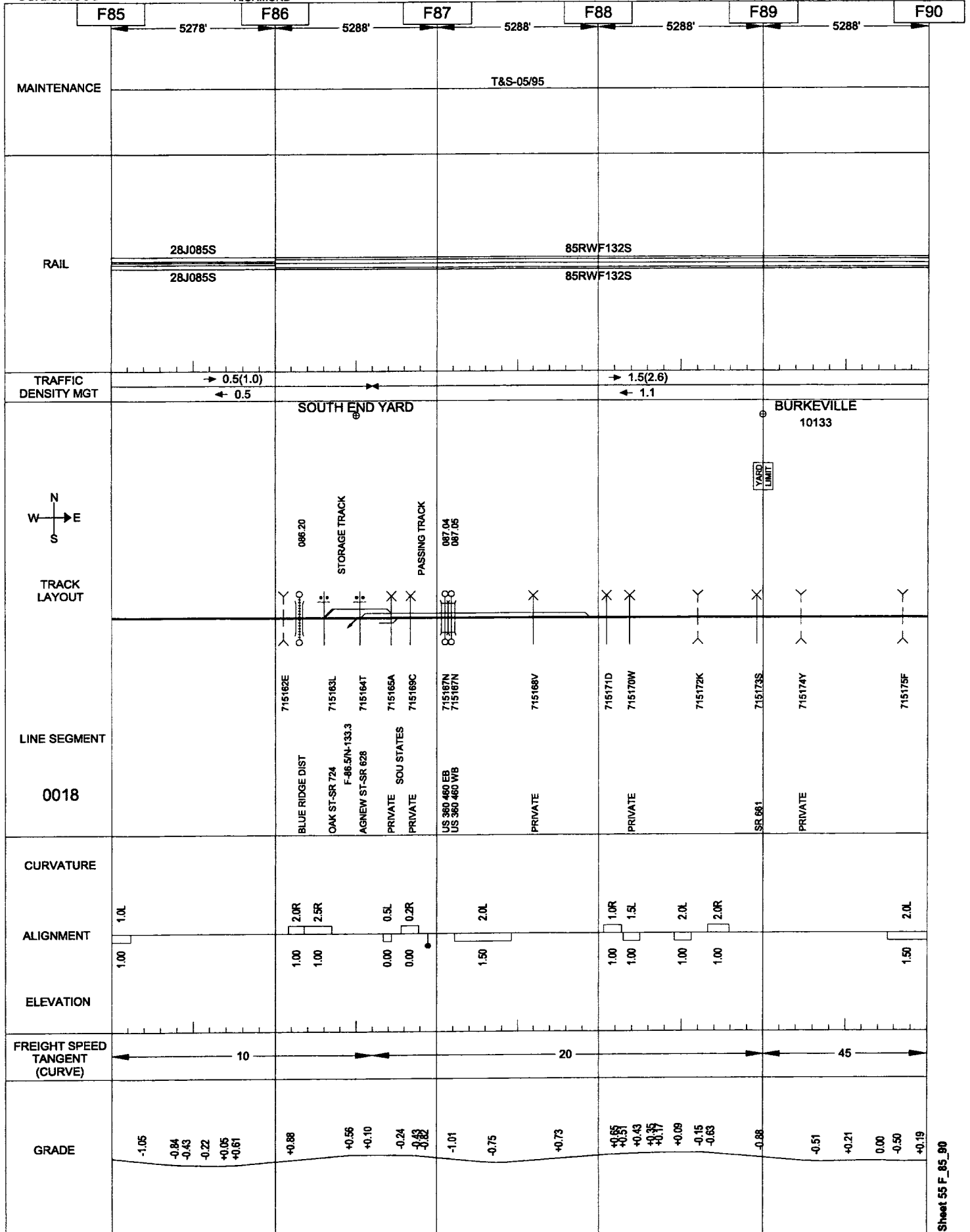


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BURKEVILLE-RICHMOND

VIRGINIA



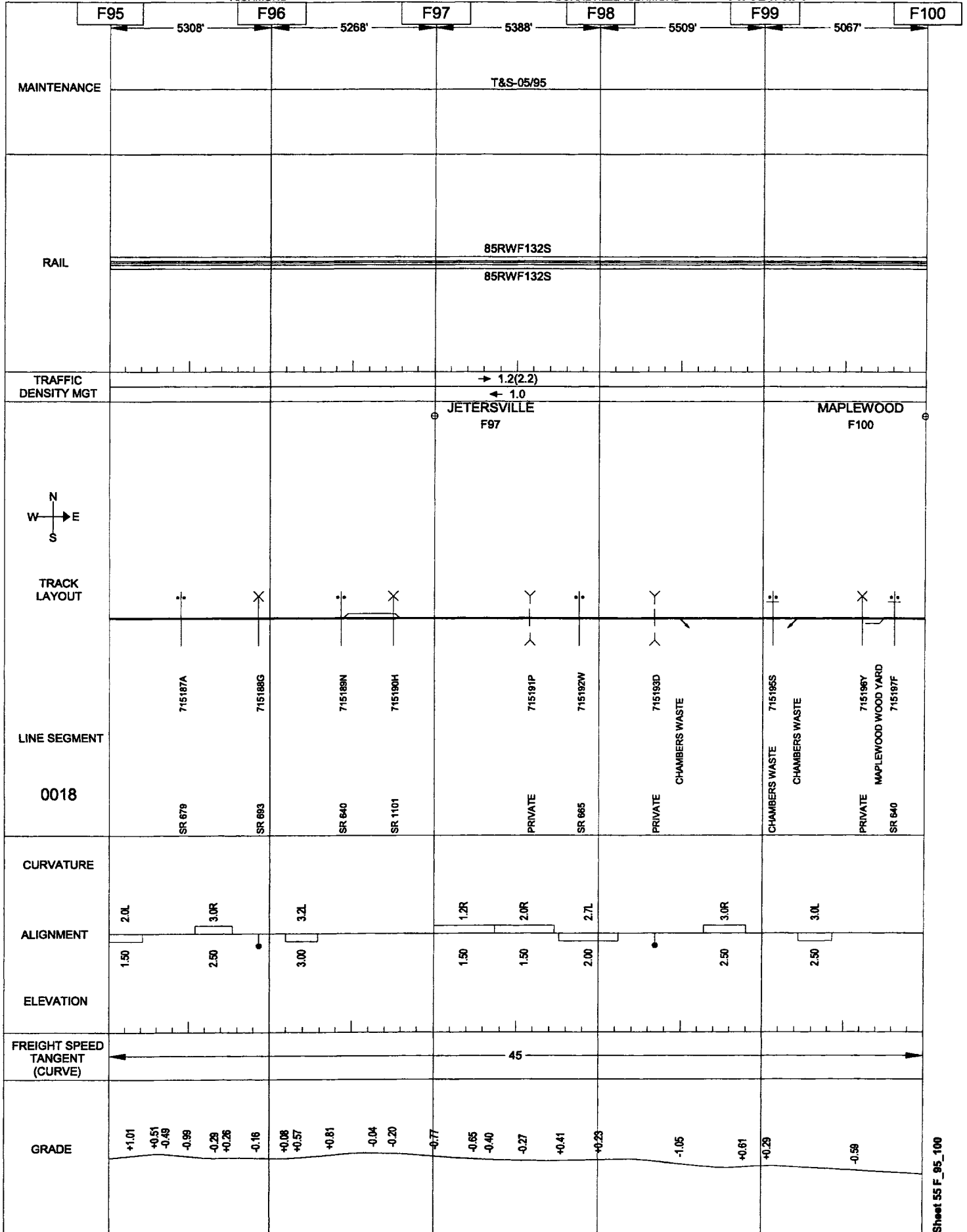
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03/20/2003

RICHMOND

BURKEVILLE-RICHMOND

VIRGINIA

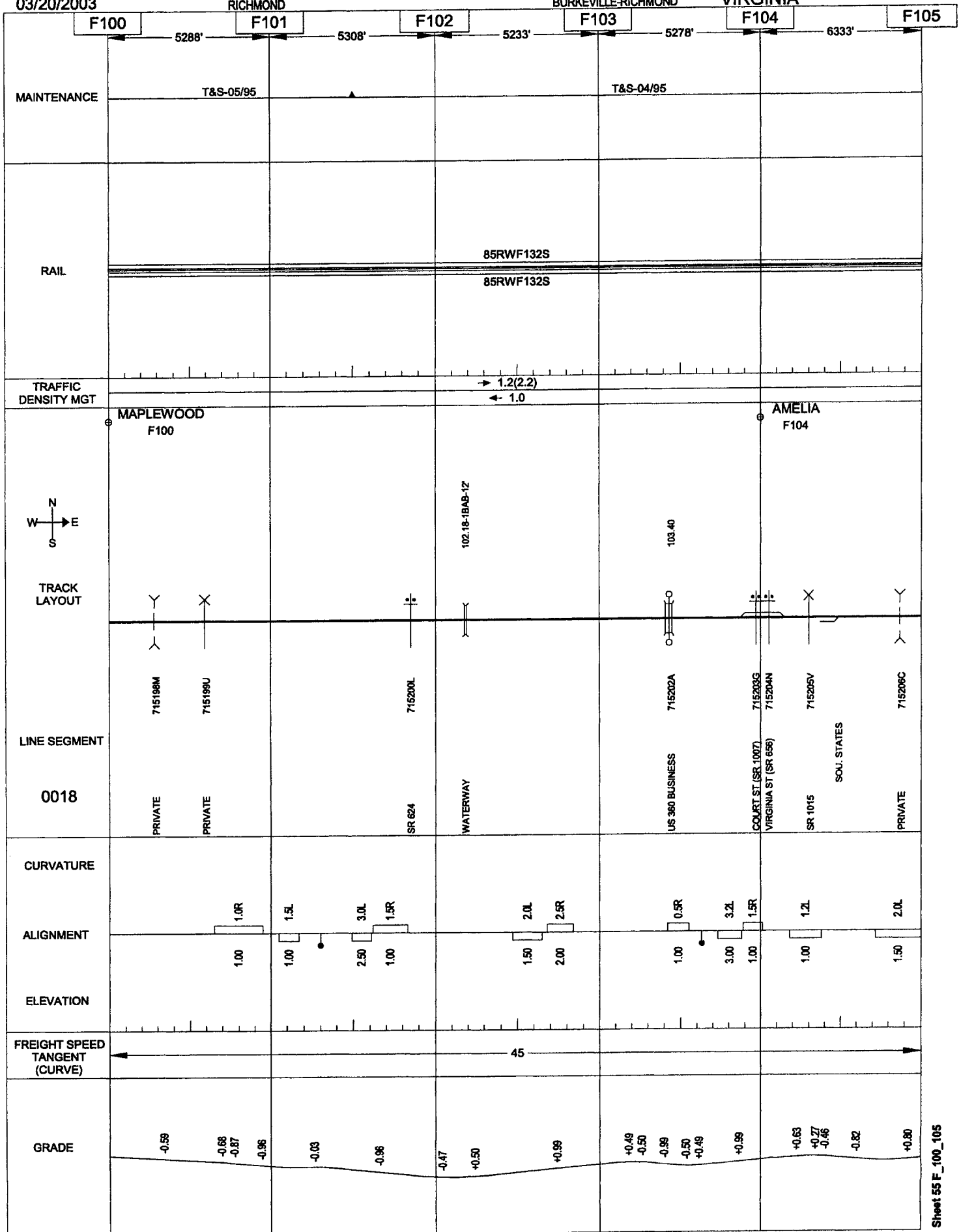


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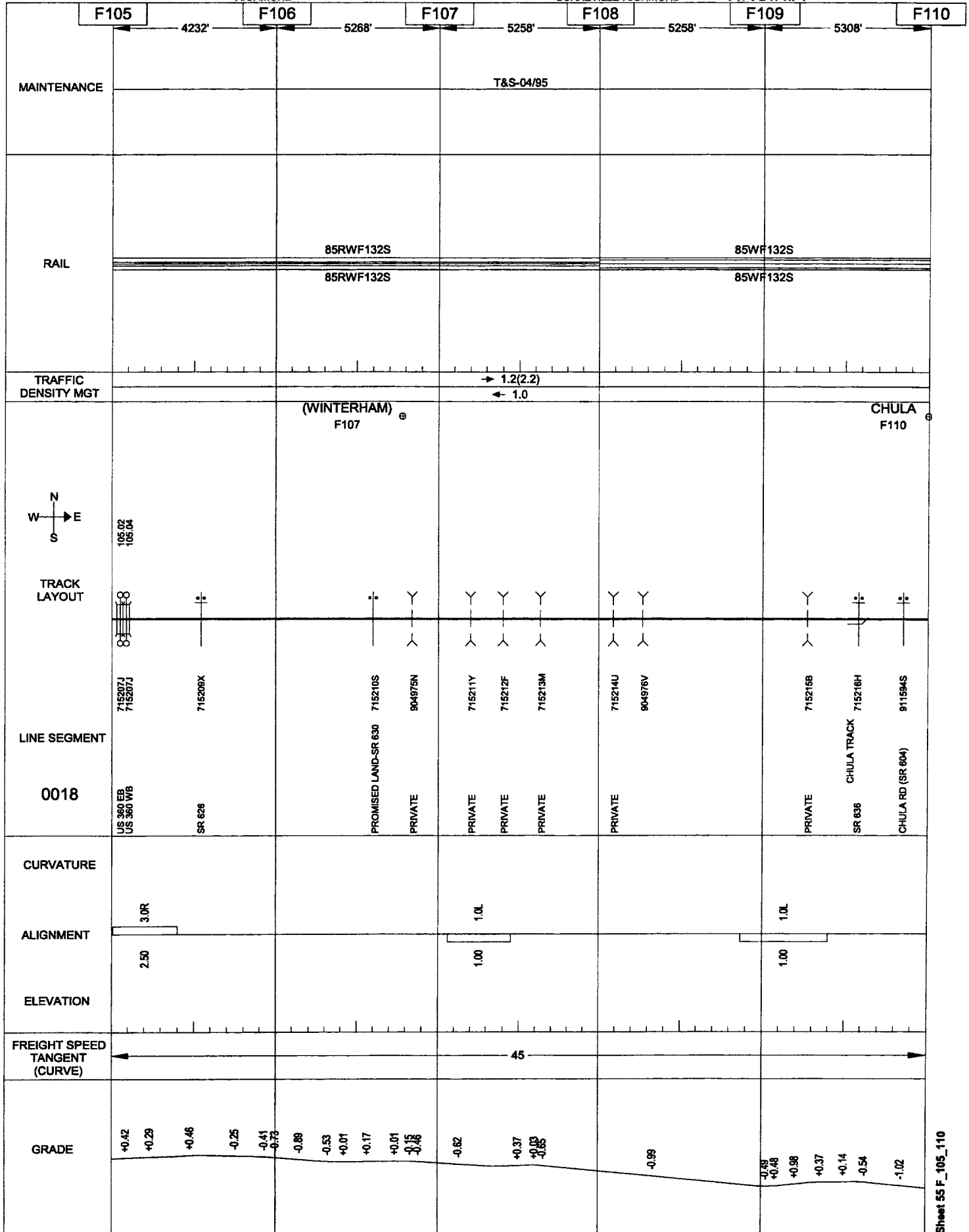


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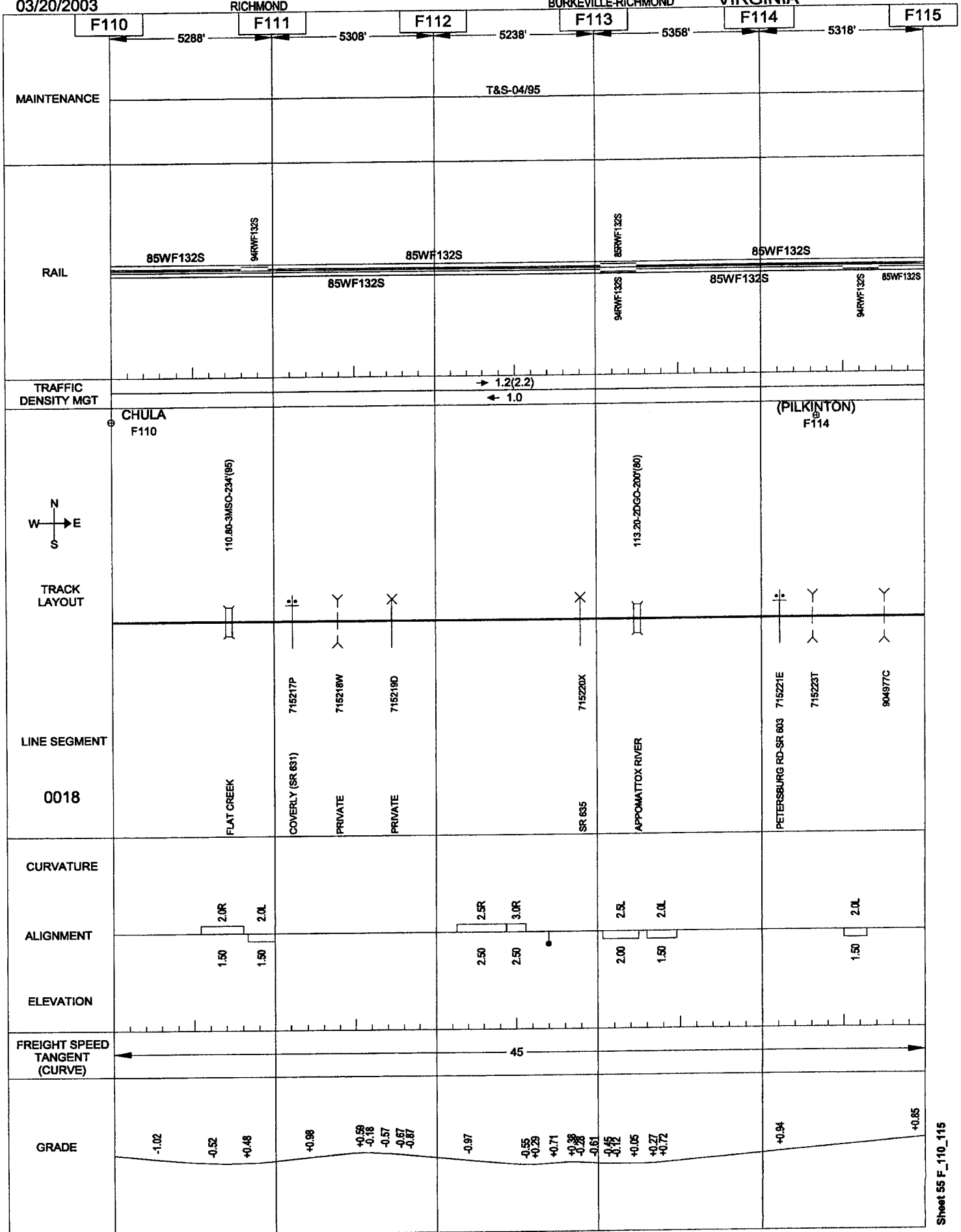


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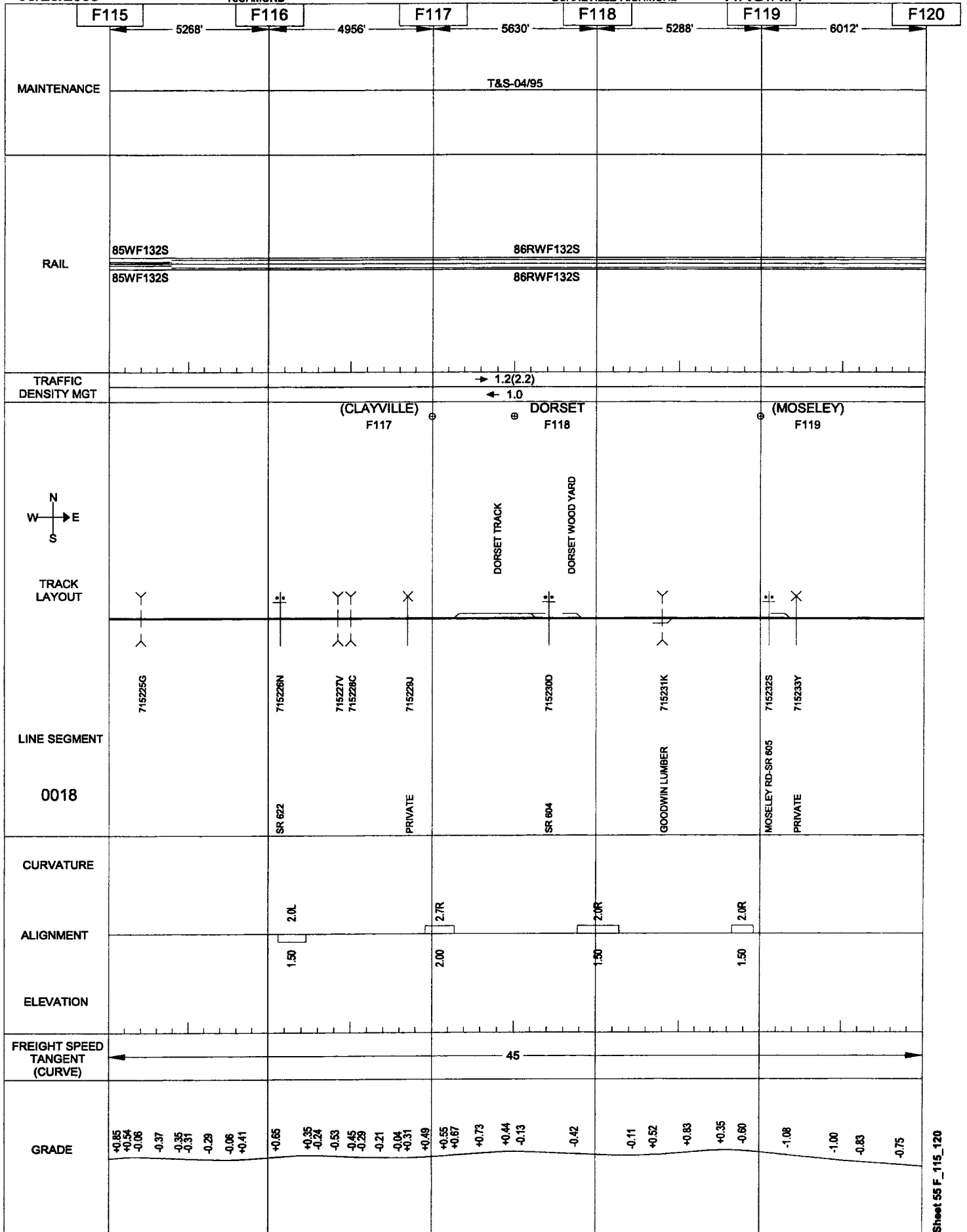


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BURKEVILLE-RICHMOND

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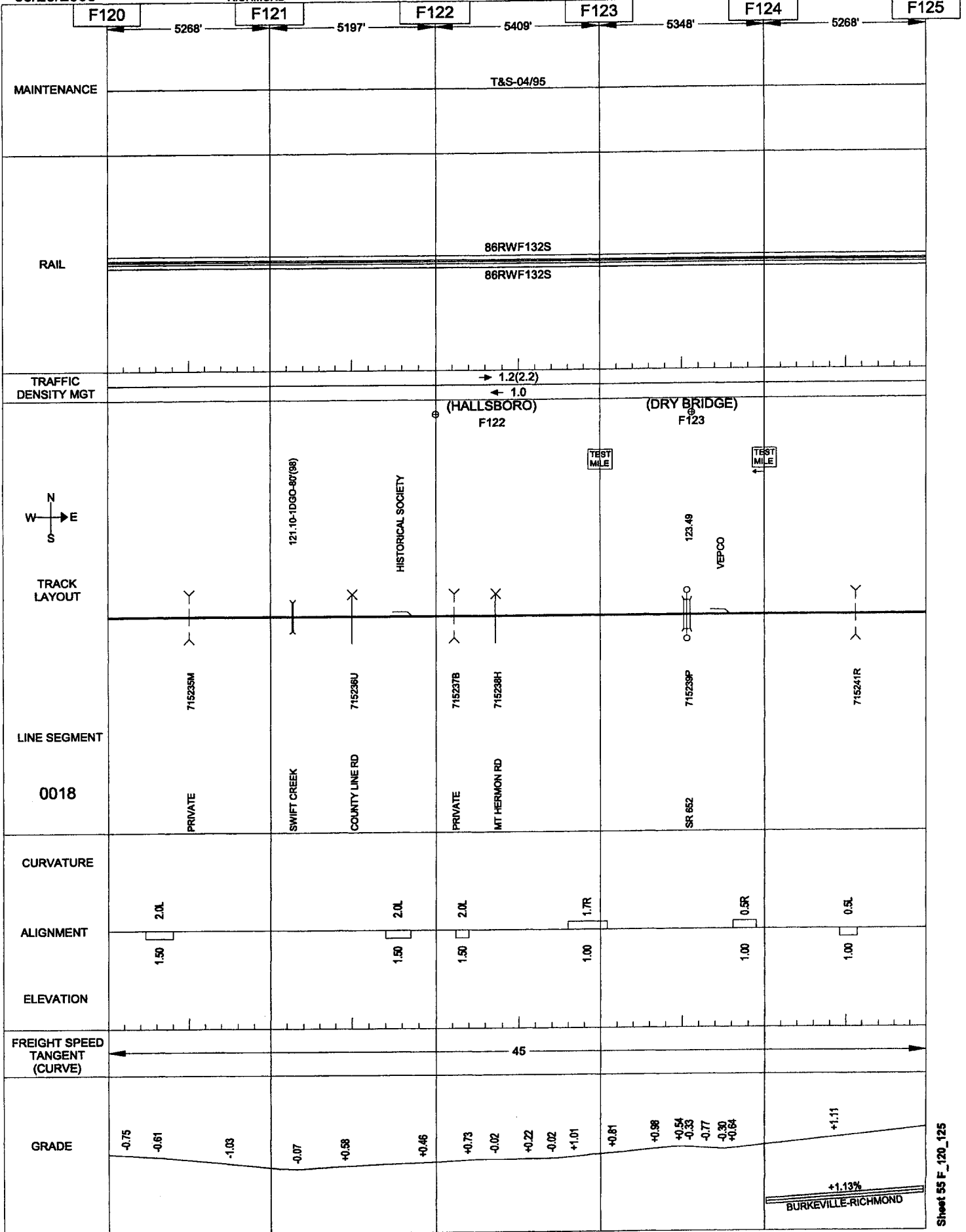


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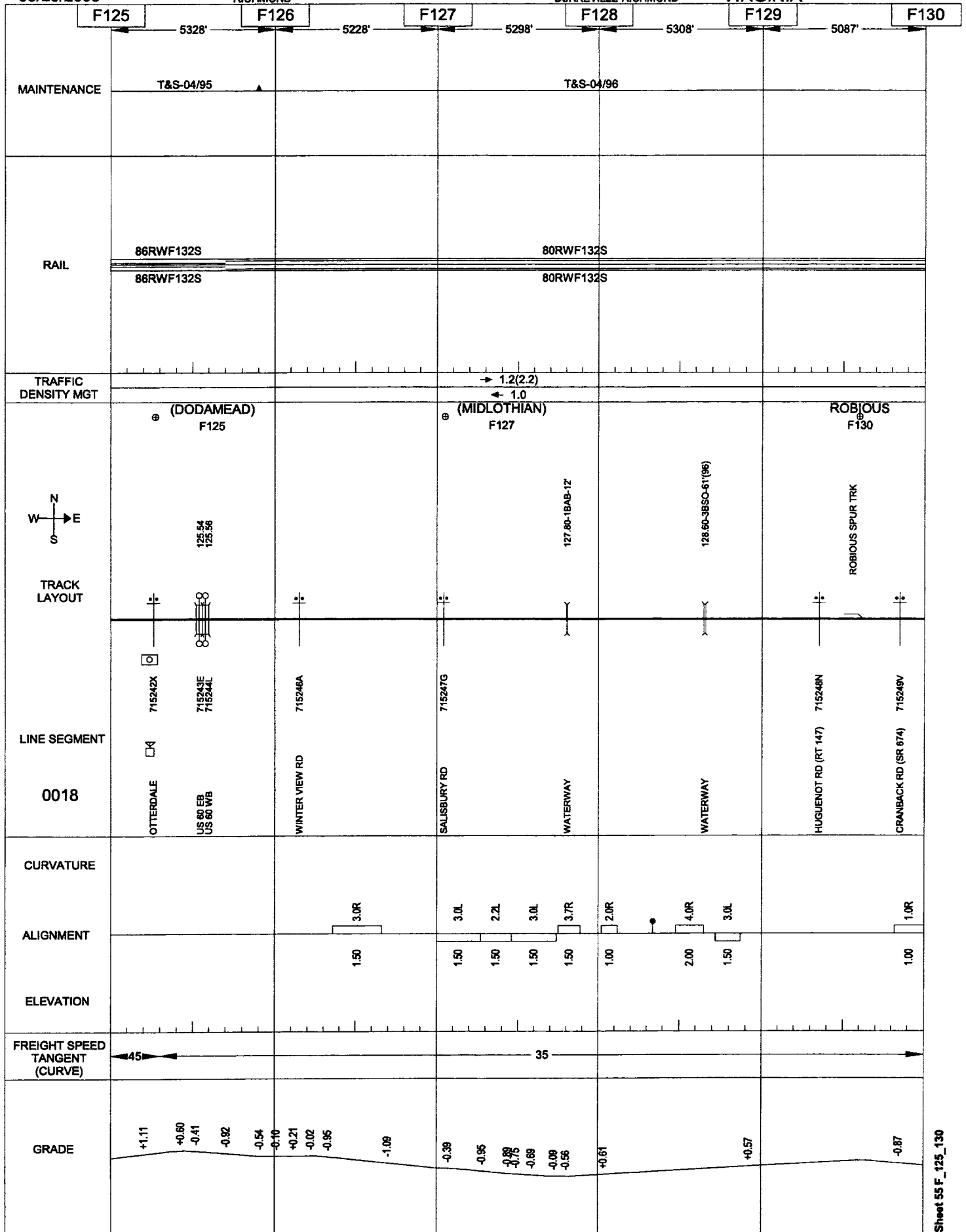


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BURKEVILLE-RICHMOND

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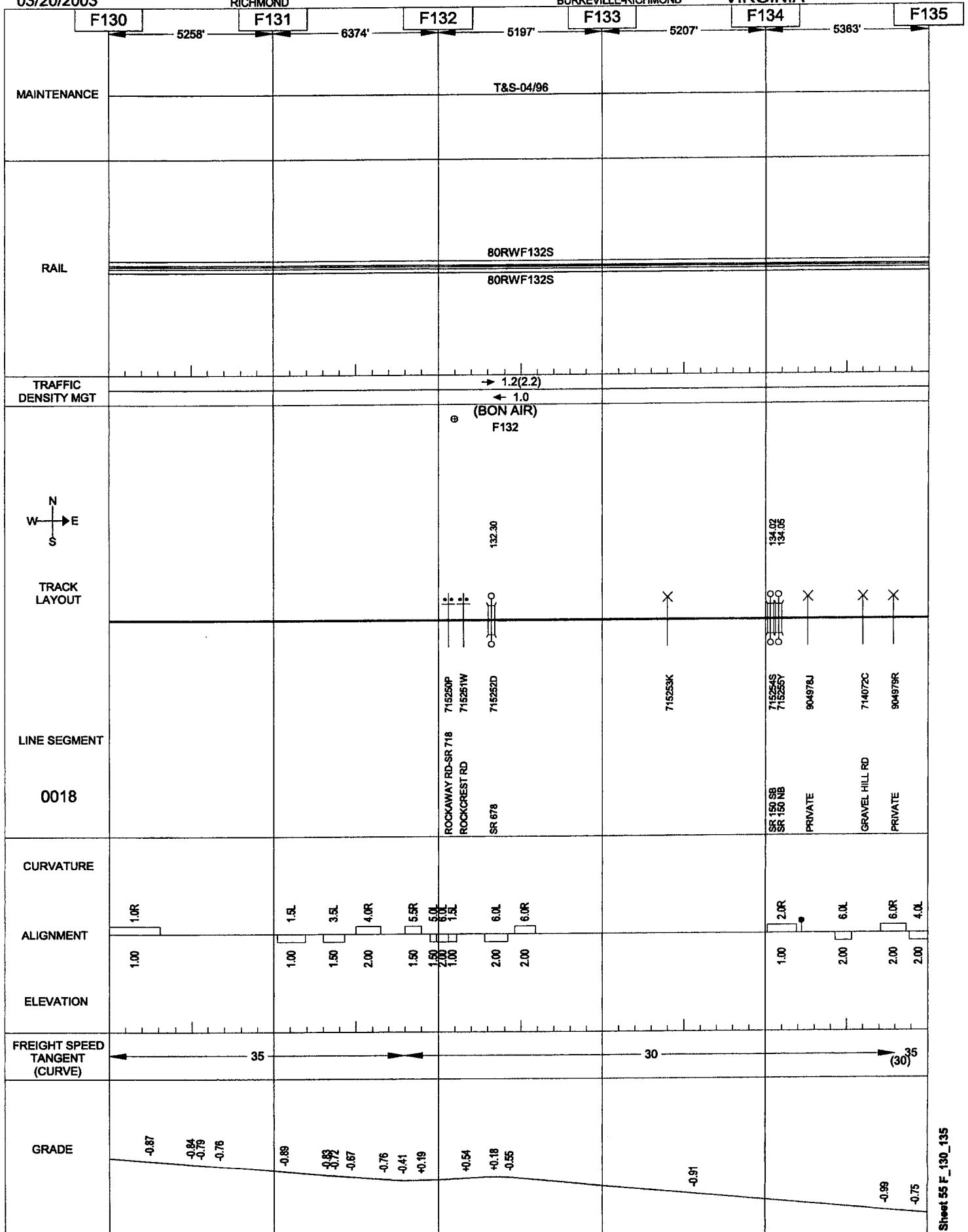


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BURKEVILLE-RICHMOND

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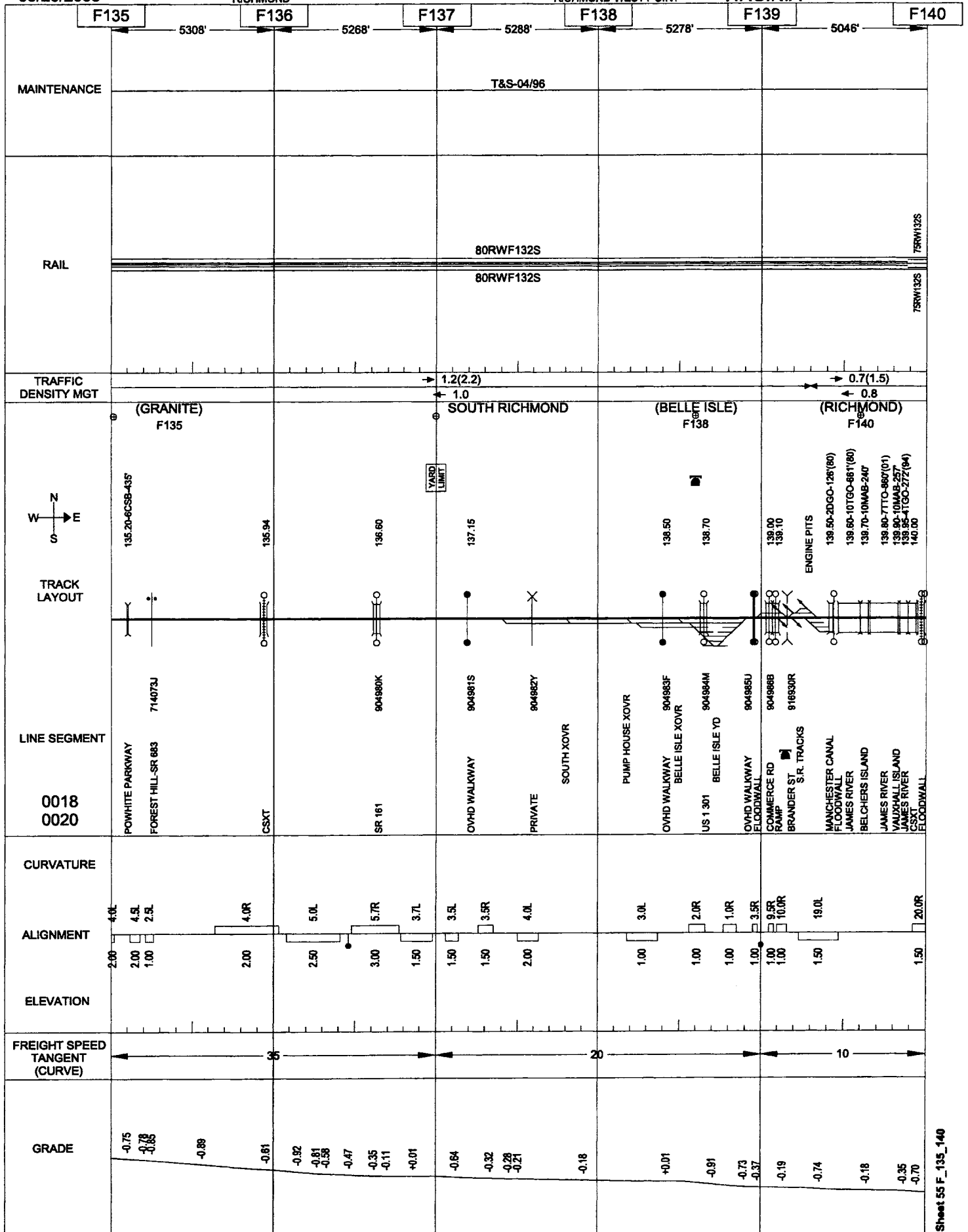


03/20/2003

RICHMOND

RICHMOND-WEST POINT

VIRGINIA

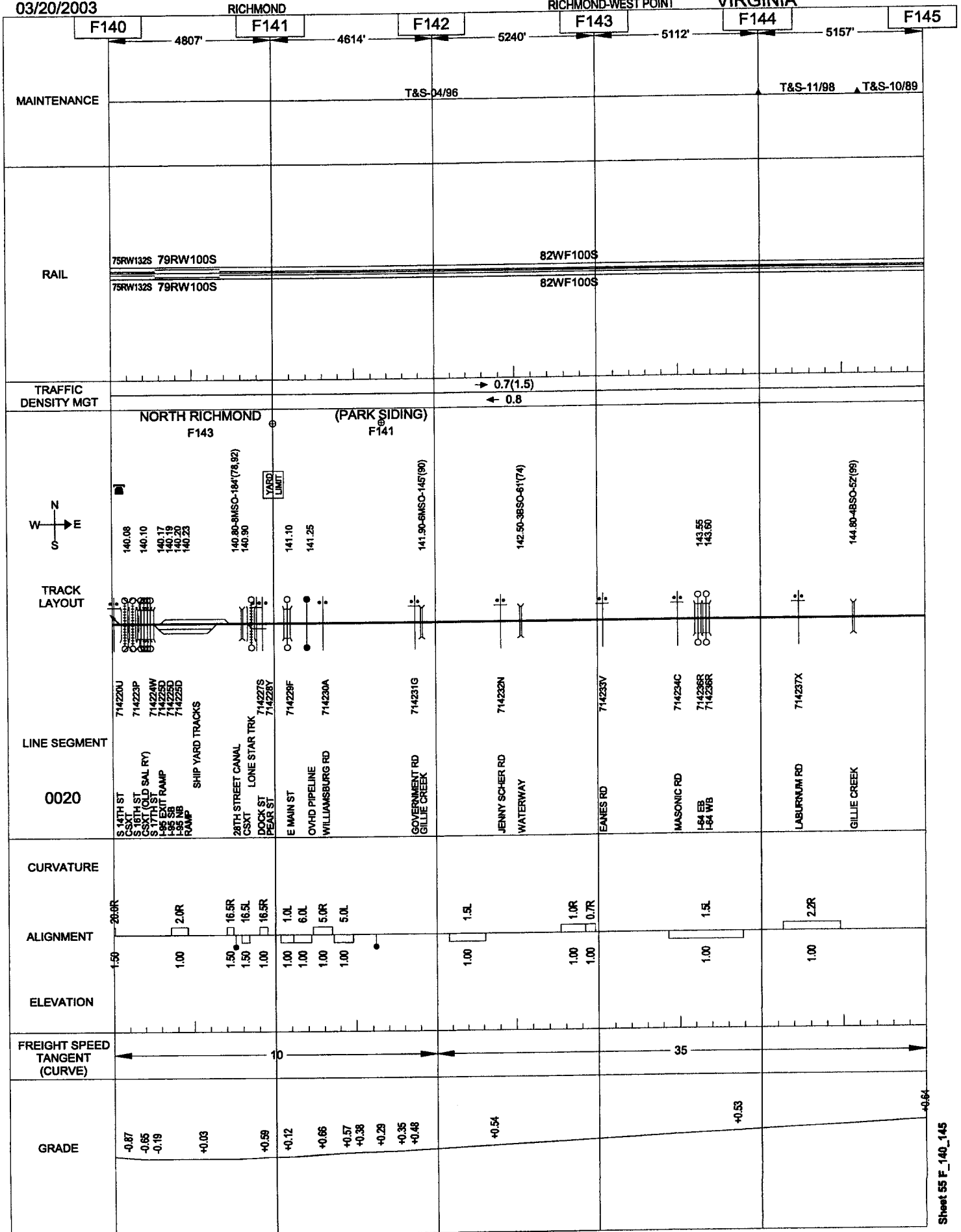


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RICHMOND

RICHMOND-WEST POINT

VIRGINIA



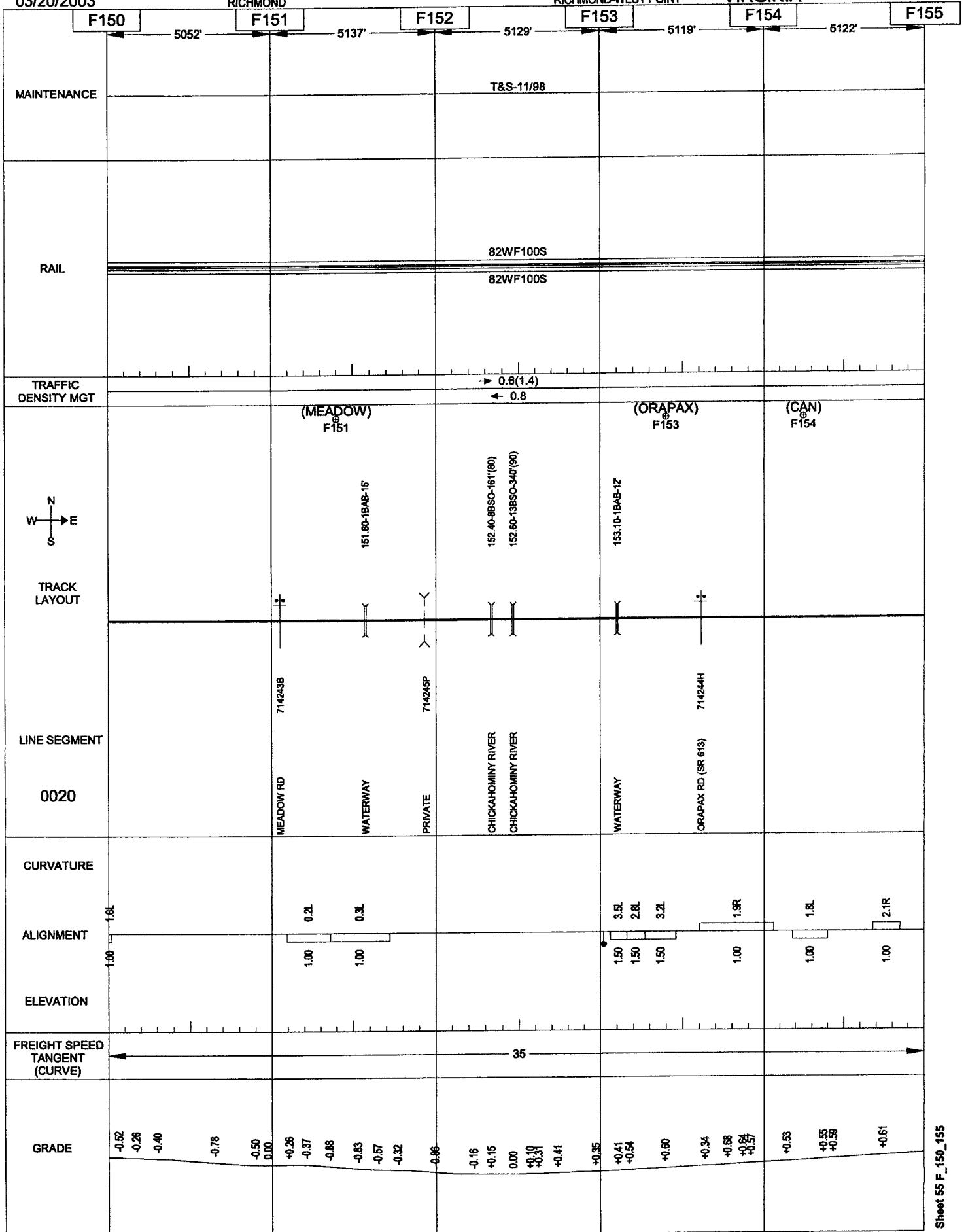
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03/20/2003

RICHMOND

RICHMOND-WEST POINT

VIRGINIA

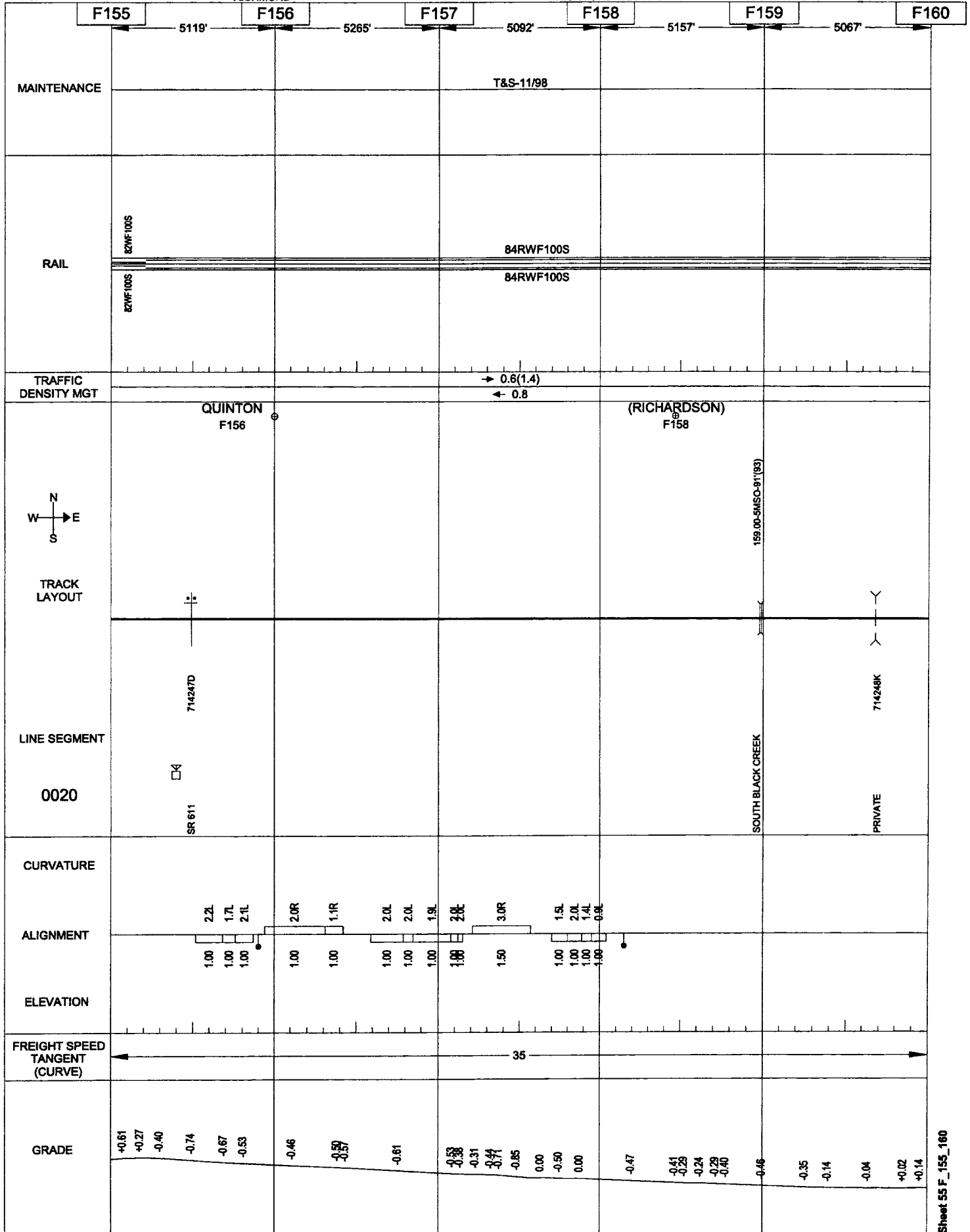


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RICHMOND-WEST POINT

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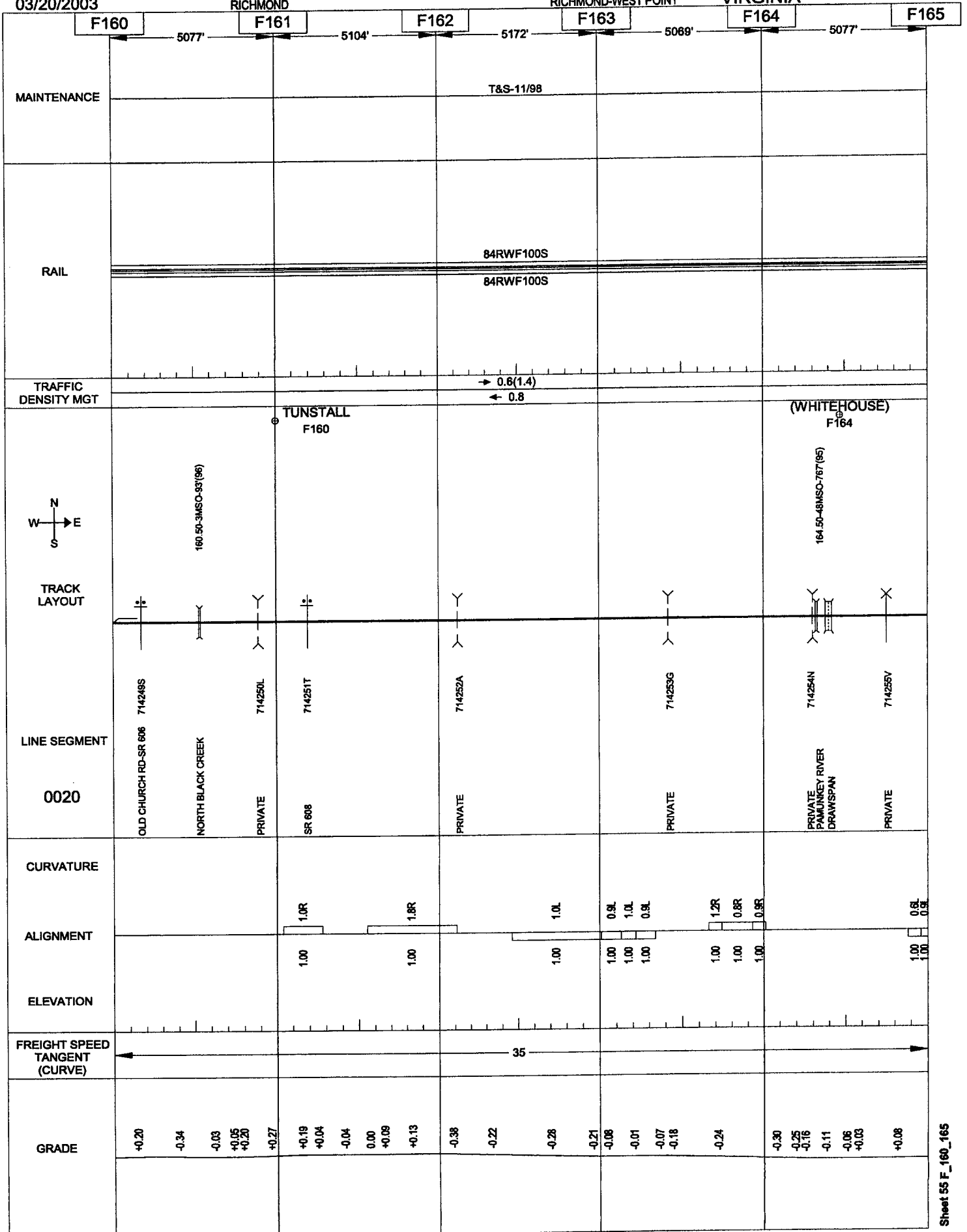


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RICHMOND-WEST POINT

VIRGINIA

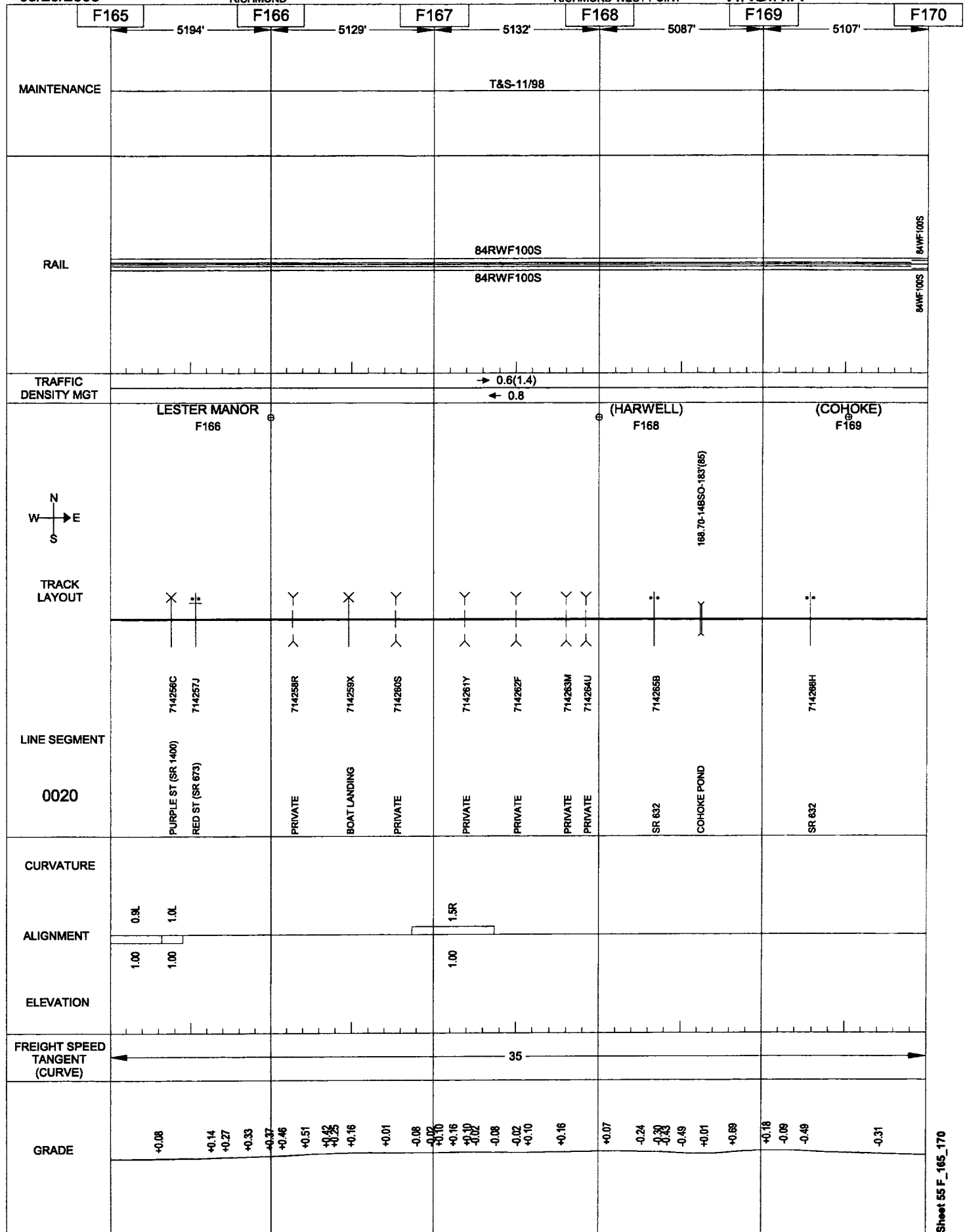


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RICHMOND

RICHMOND-WEST POINT

VIRGINIA

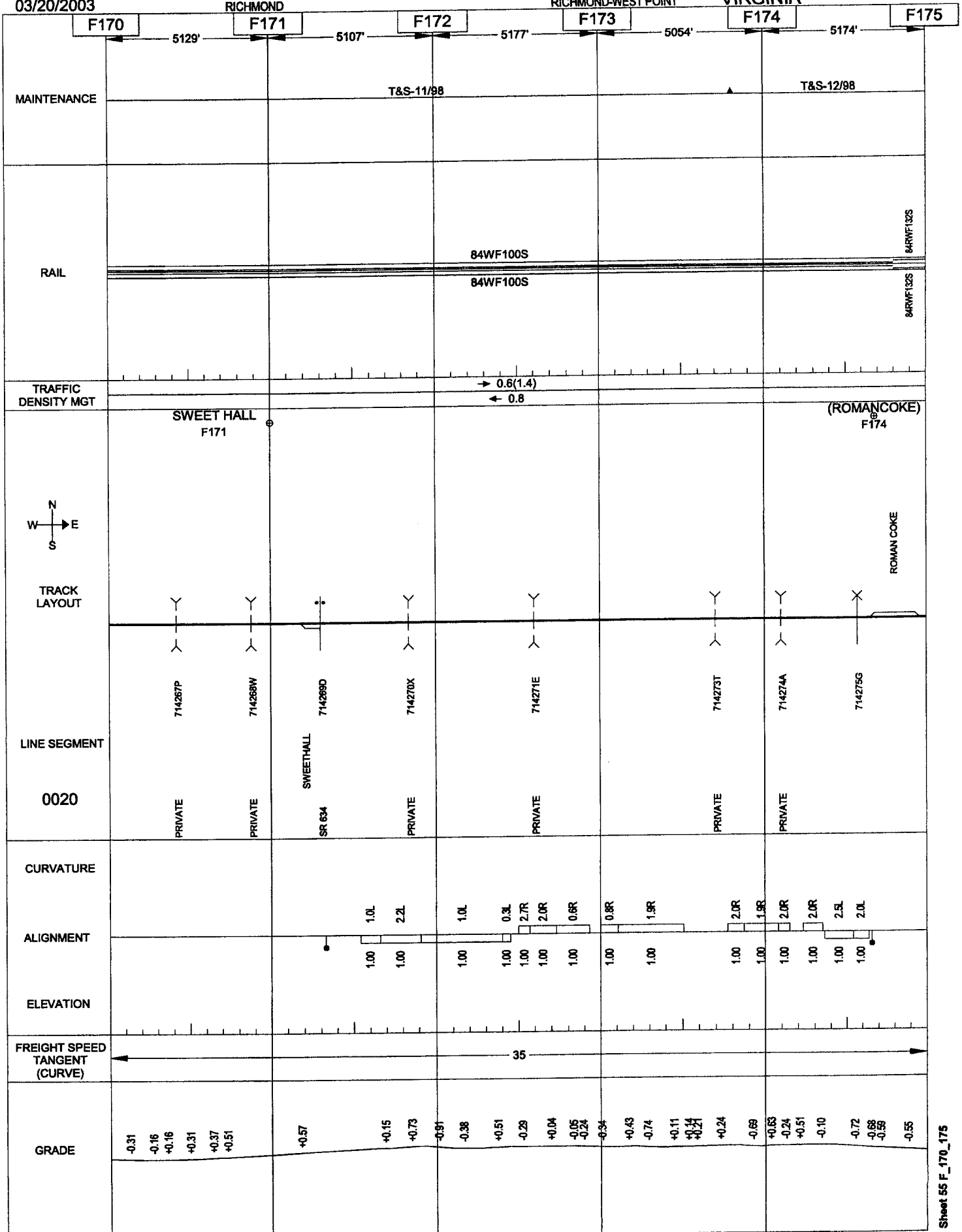


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RICHMOND

RICHMOND-WEST POINT

VIRGINIA



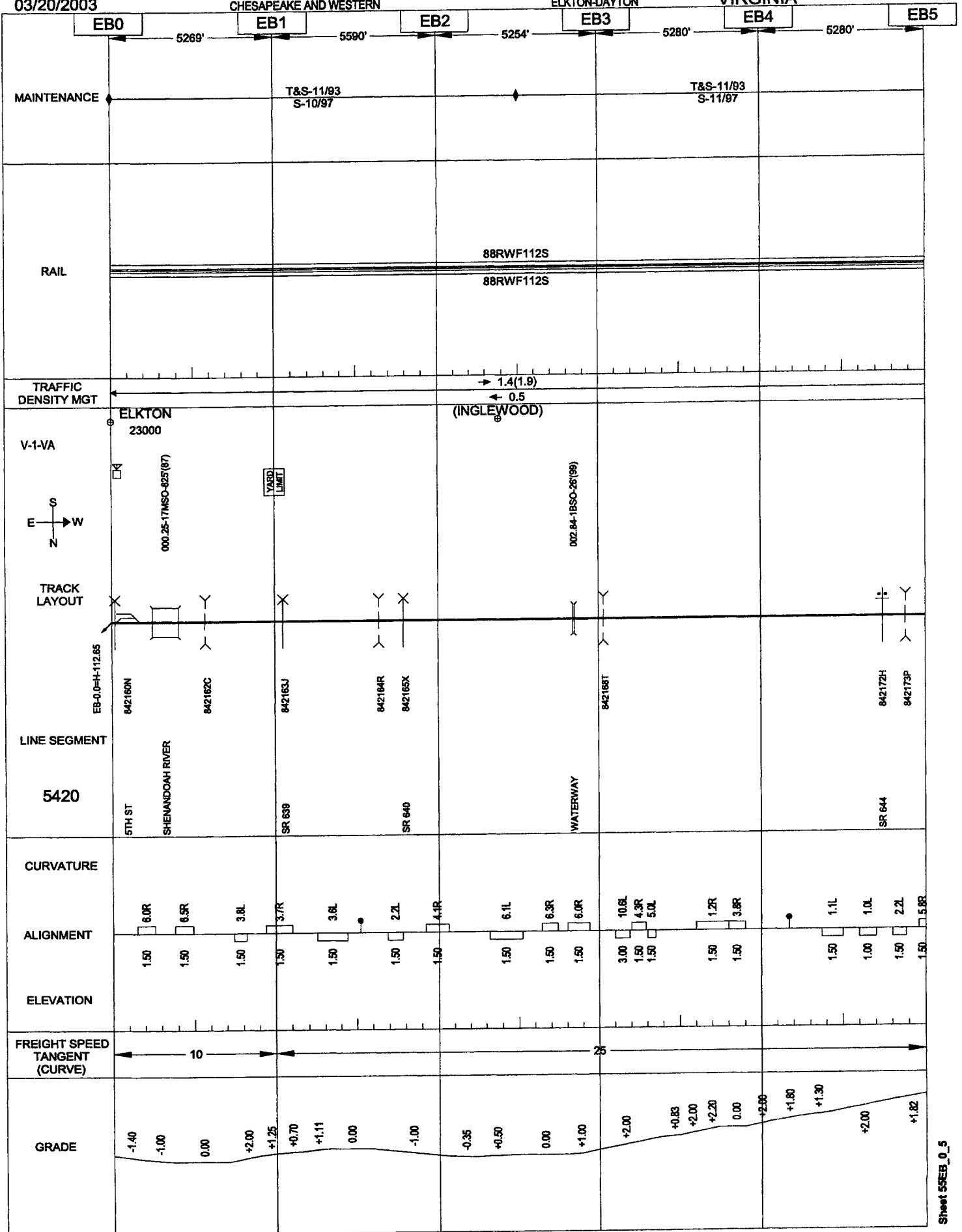
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03/20/2003

CHESAPEAKE AND WESTERN

ELKTON-DAYTON

VIRGINIA

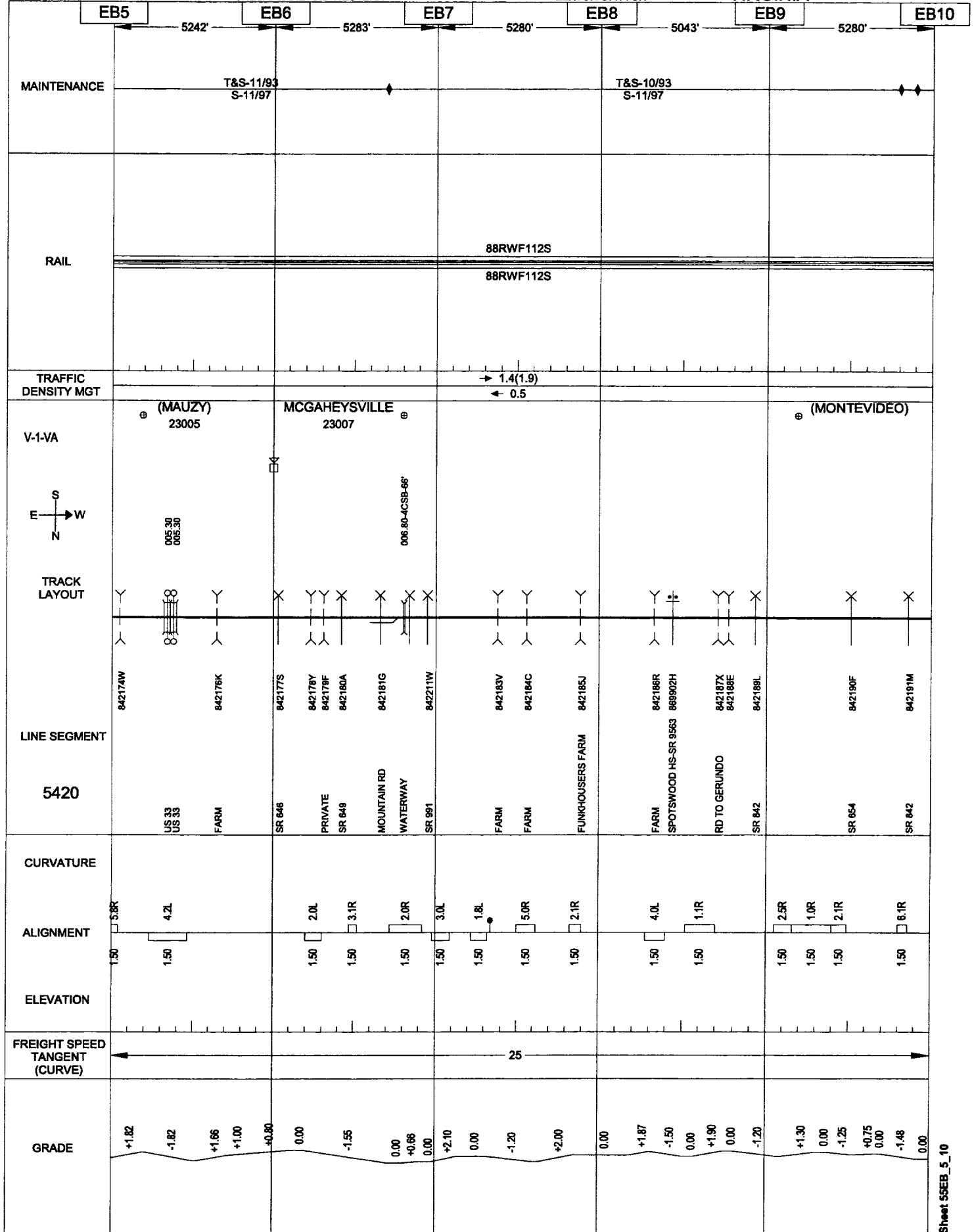


03/20/2003

CHESAPEAKE AND WESTERN

ELKTON-DAYTON

VIRGINIA

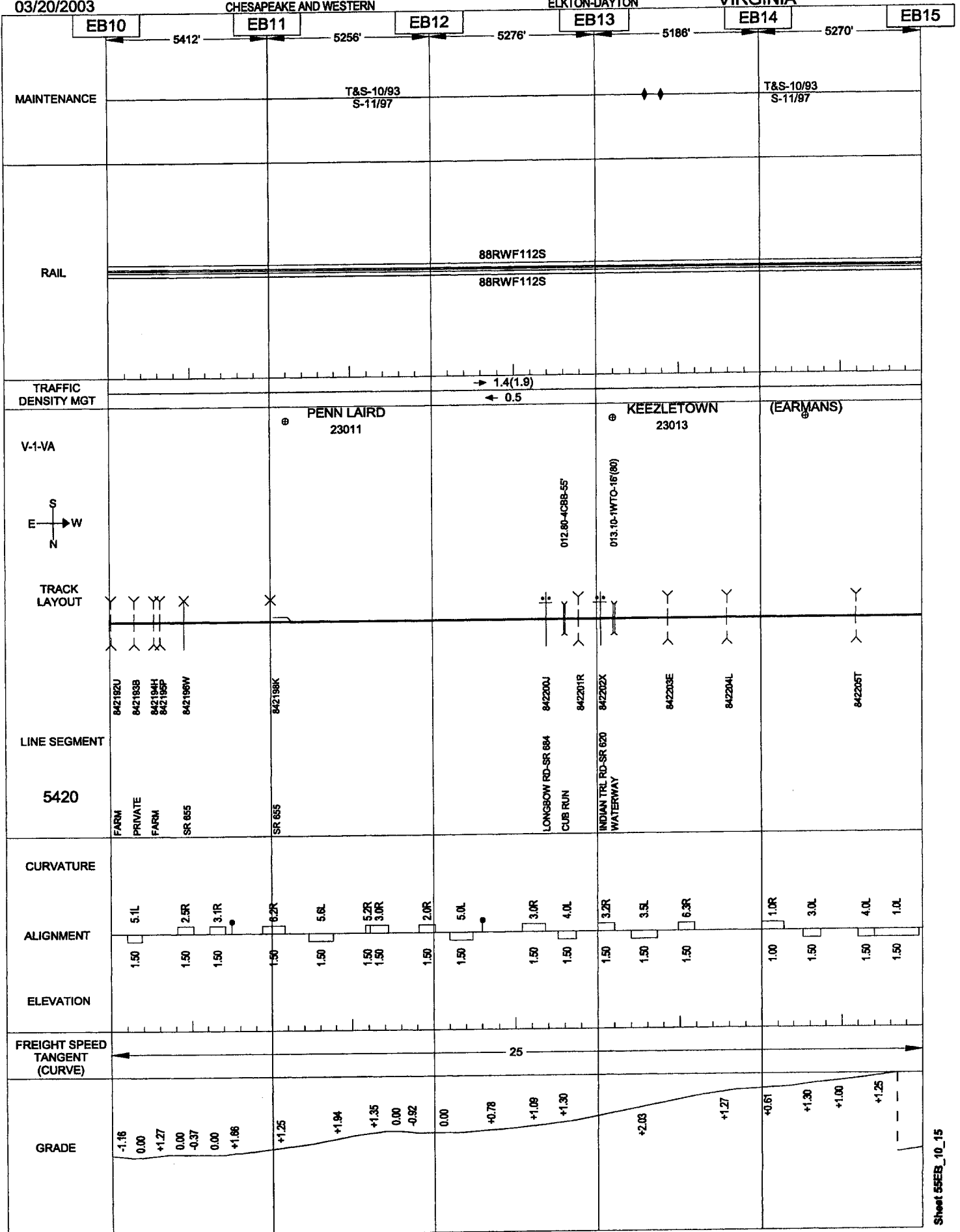


03/20/2003

CHESAPEAKE AND WESTERN

ELKTON-DAYTON

VIRGINIA

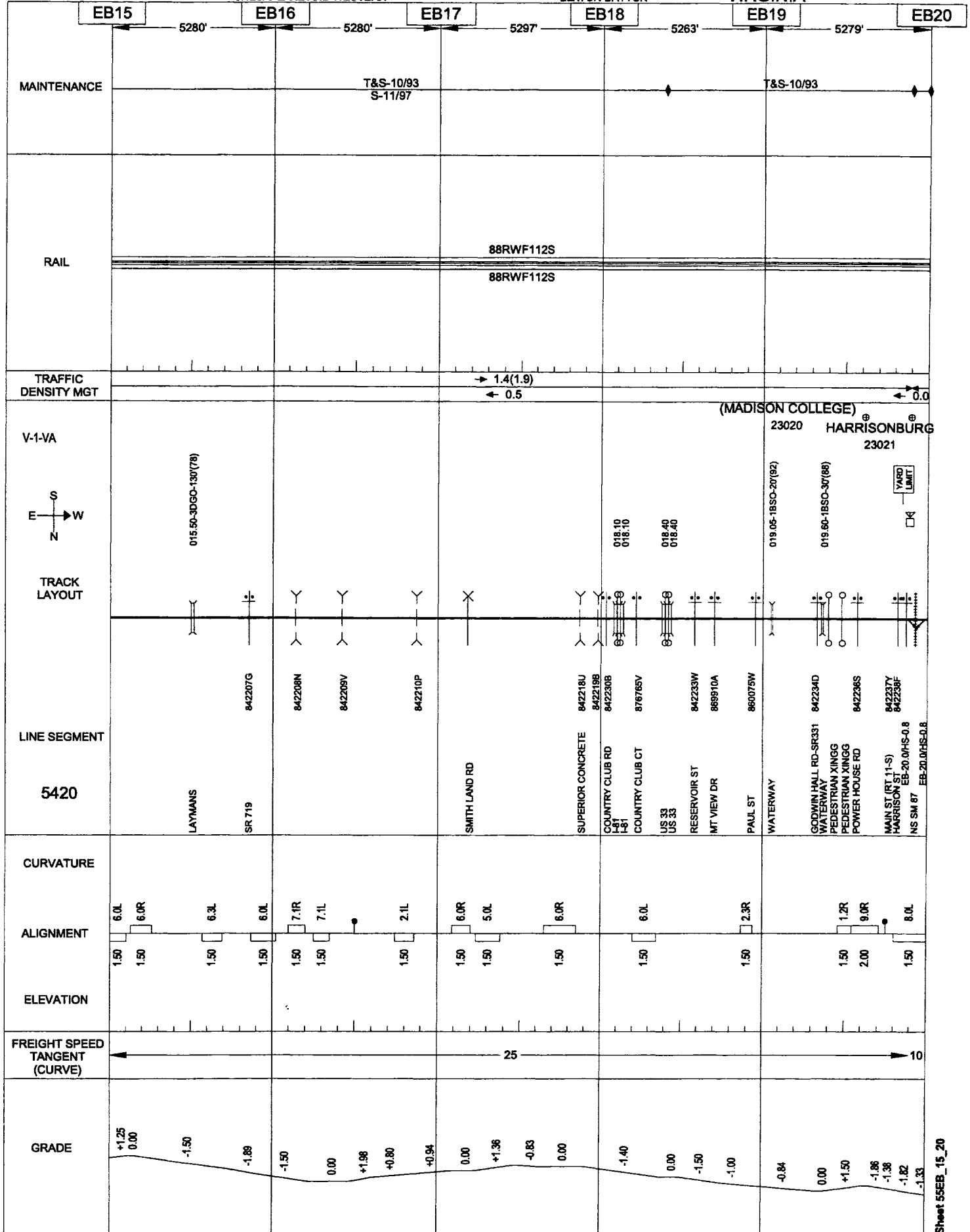


03/20/2003

CHESAPEAKE AND WESTERN

ELKTON-DAYTON

VIRGINIA

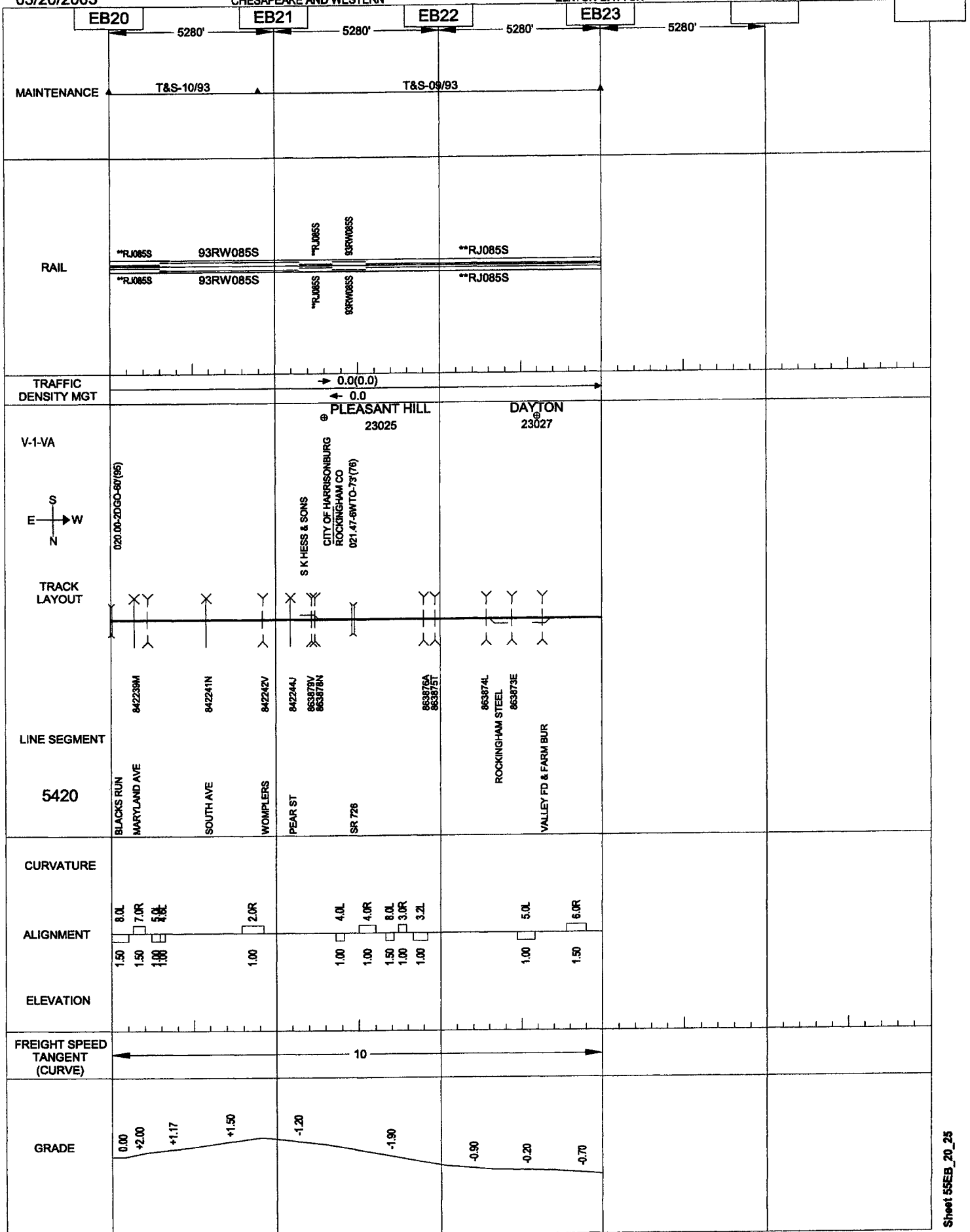


03/20/2003

CHESAPEAKE AND WESTERN

ELKTON-DAYTON

VIRGINIA



03/20/2003

CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

VIRGINIA

CW84

CW85

5358'

MAINTENANCE

T&S-07/88

RAIL

**RJ085S

**RJ085S

TRAFFIC
DENSITY MGT→ 0.1(0.2)
← 0.1

S
E — W
N

TRACK
LAYOUTBOWMAN
23039084.40-1WTB-13"
084.50-1WTB-13"

LINE SEGMENT

5003

714537L 714538T 714539A 714540U
WATERWAY BOWMAN APPLE RD
WATERWAY SR 703 / 282

CURVATURE

ALIGNMENT

ELEVATION

FREIGHT SPEED
TANGENT
(CURVE)

25

GRADE

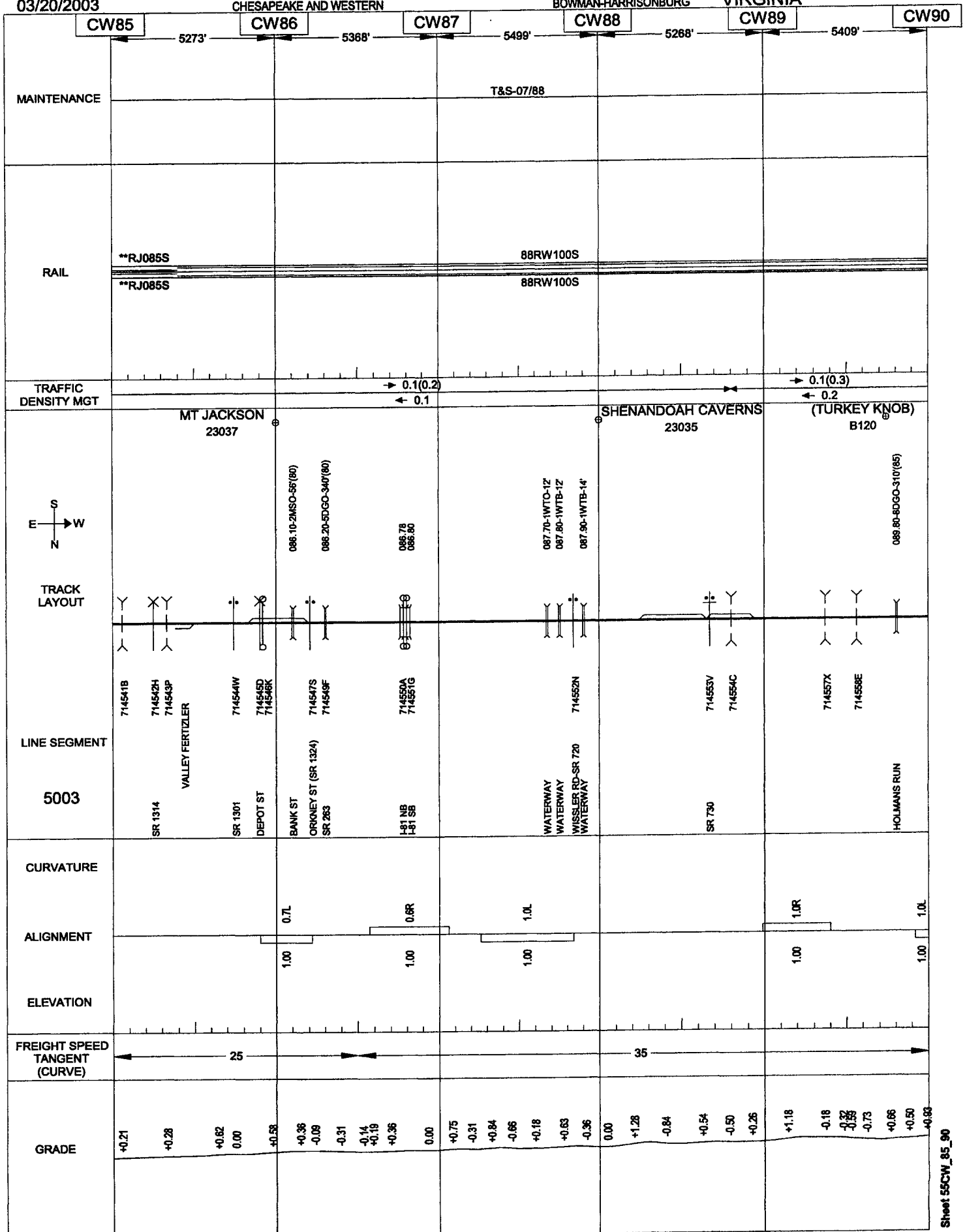
+0.16 +0.30 +0.22 0.00

03/20/2003

CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

VIRGINIA

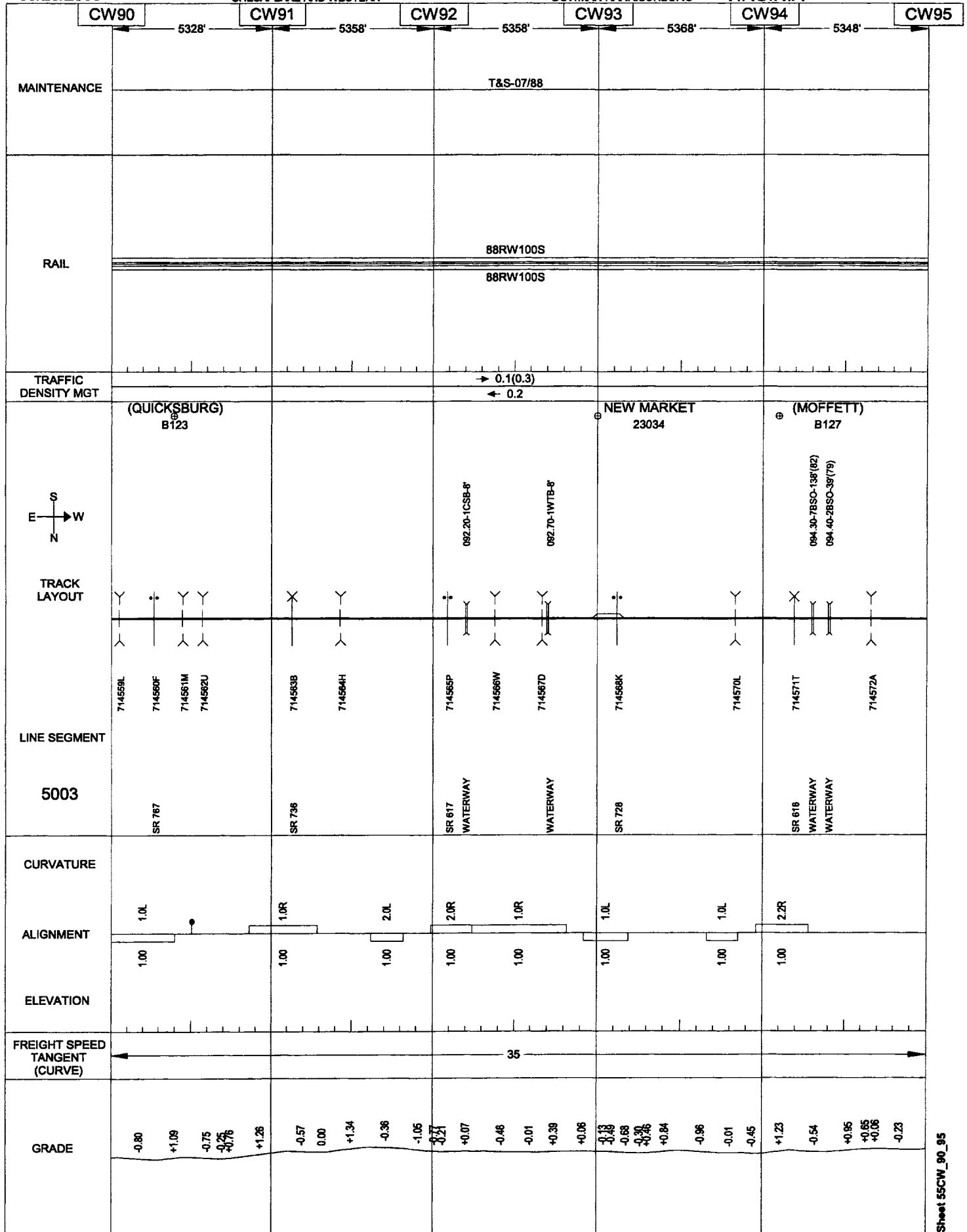


03/20/2003

CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

VIRGINIA

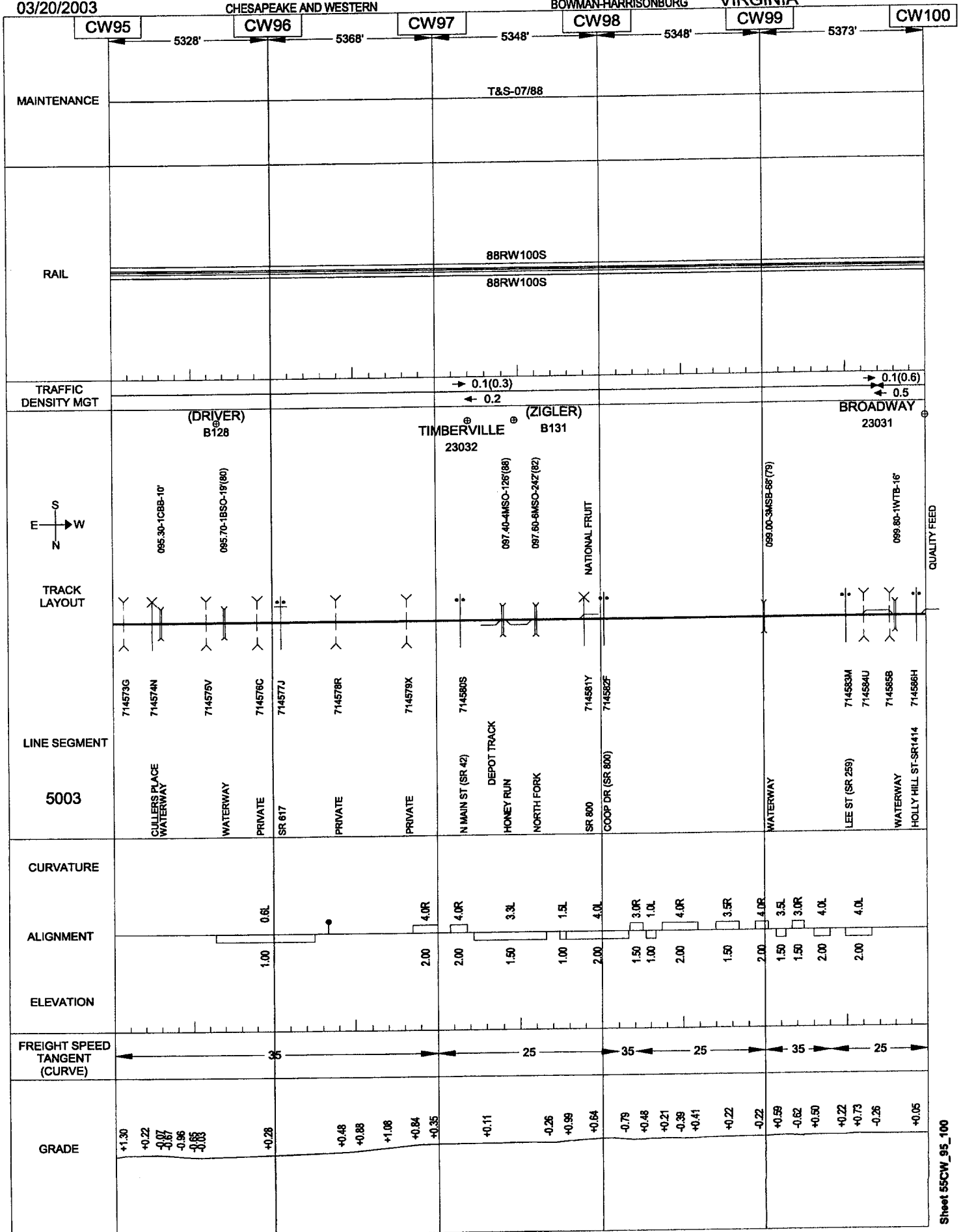


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CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

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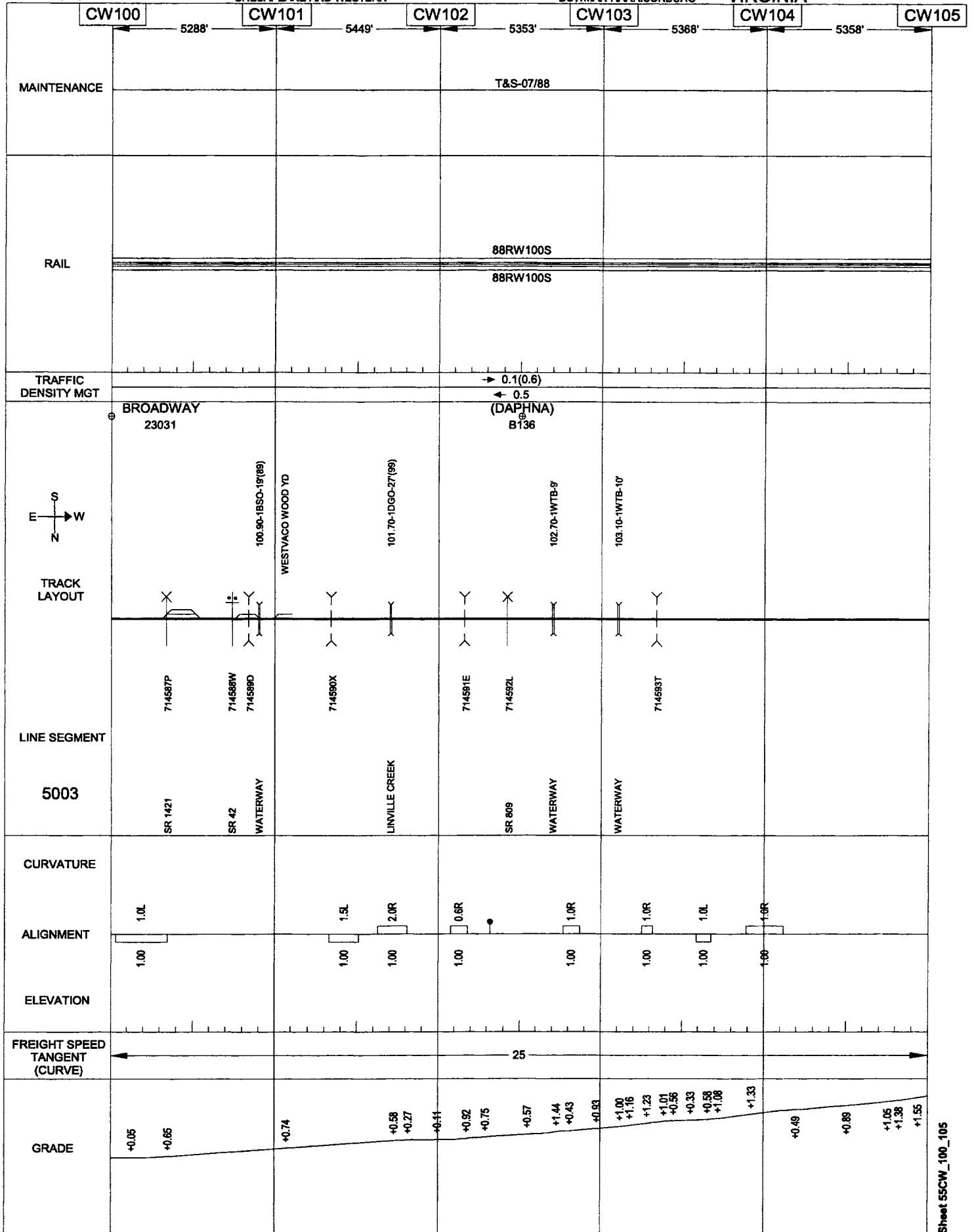


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CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

VIRGINIA

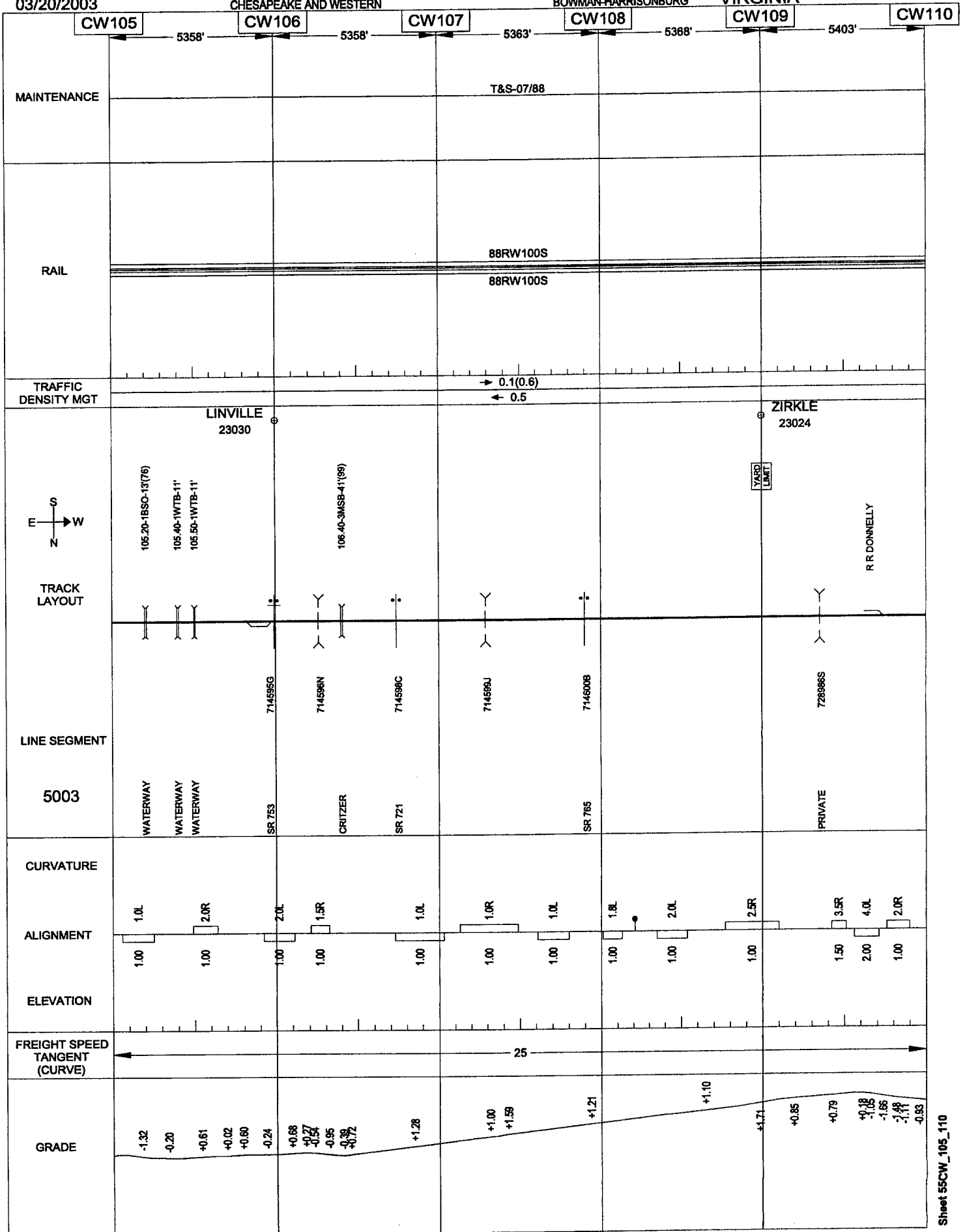


03/20/2003

CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

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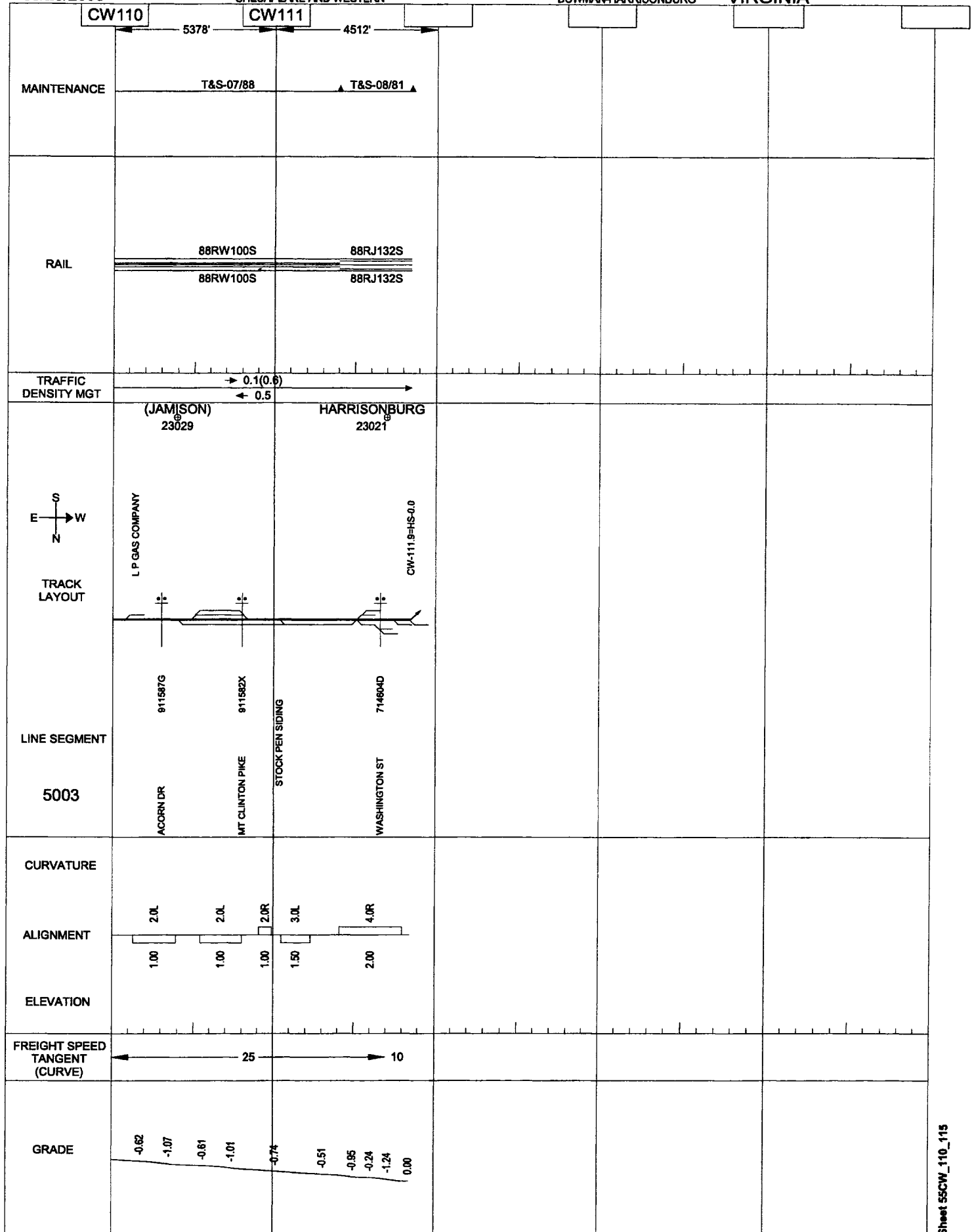


03/20/2003

CHESAPEAKE AND WESTERN

BOWMAN-HARRISONBURG

VIRGINIA

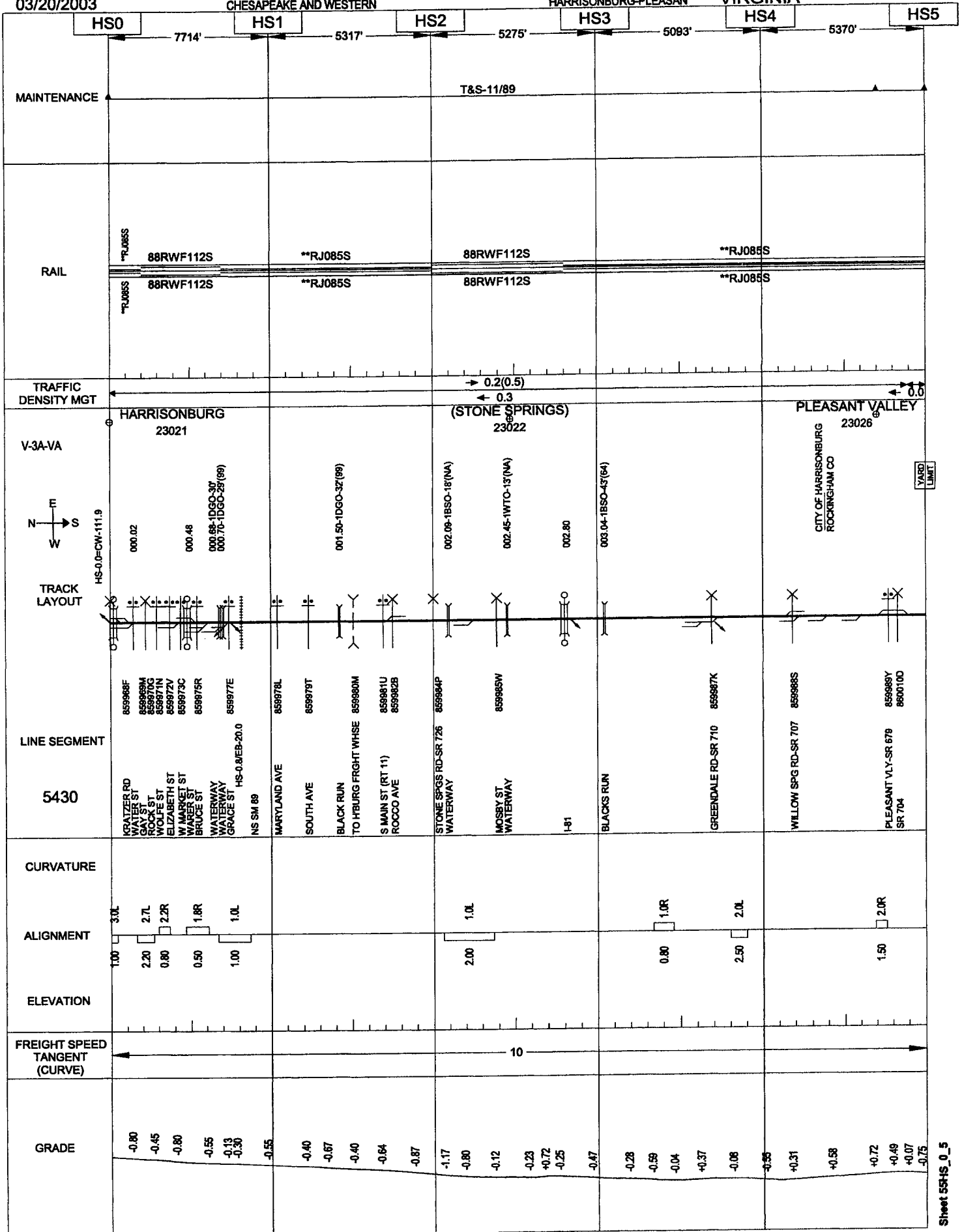


03/20/2003

CHESAPEAKE AND WESTERN

HARRISONBURG-PLEASANT

VIRGINIA



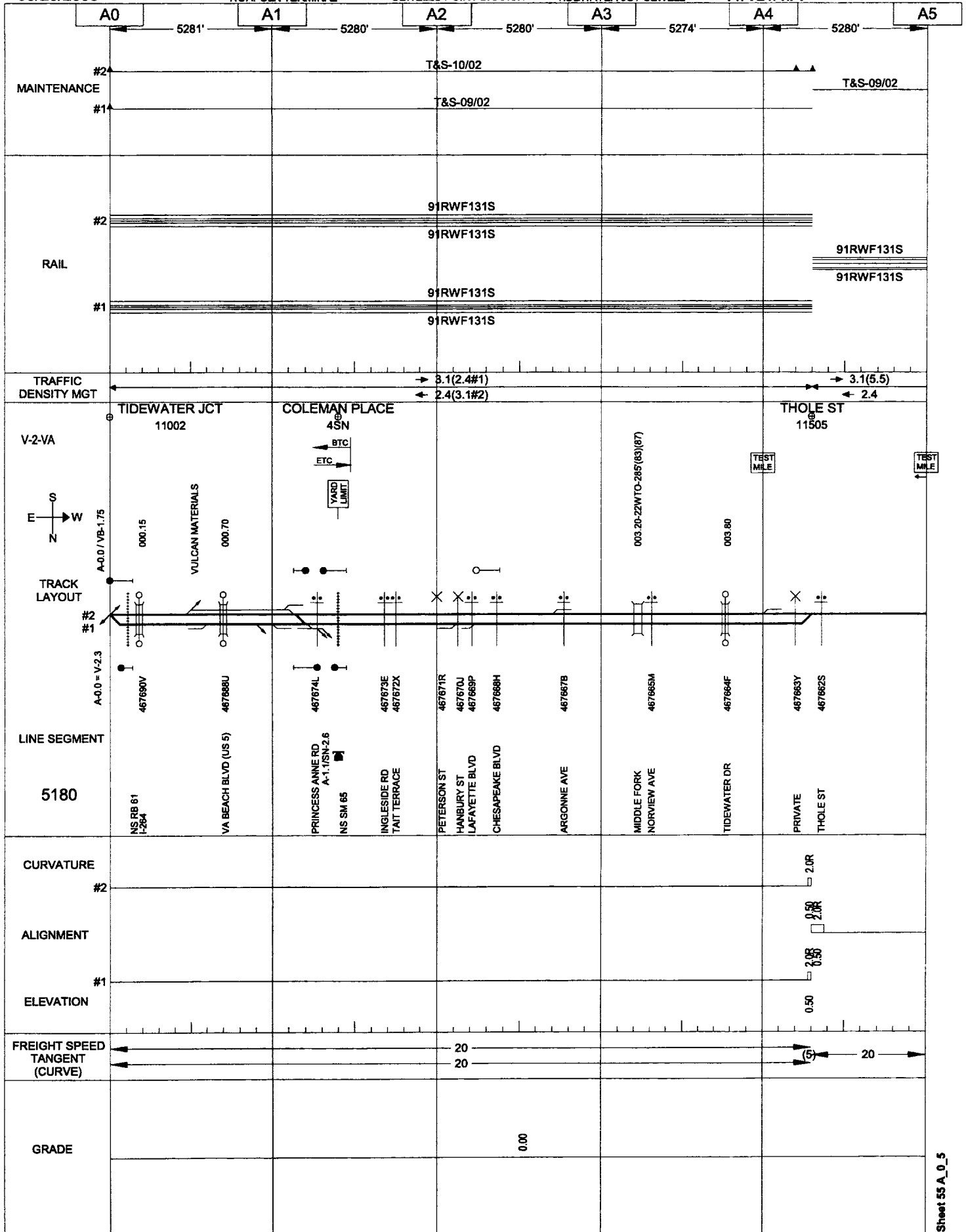
03/20/2003

NORFOLK TERMINAL

SEWELLS POINT BRANCH

TIDEWATER JCT-SEWELL

VIRGINIA



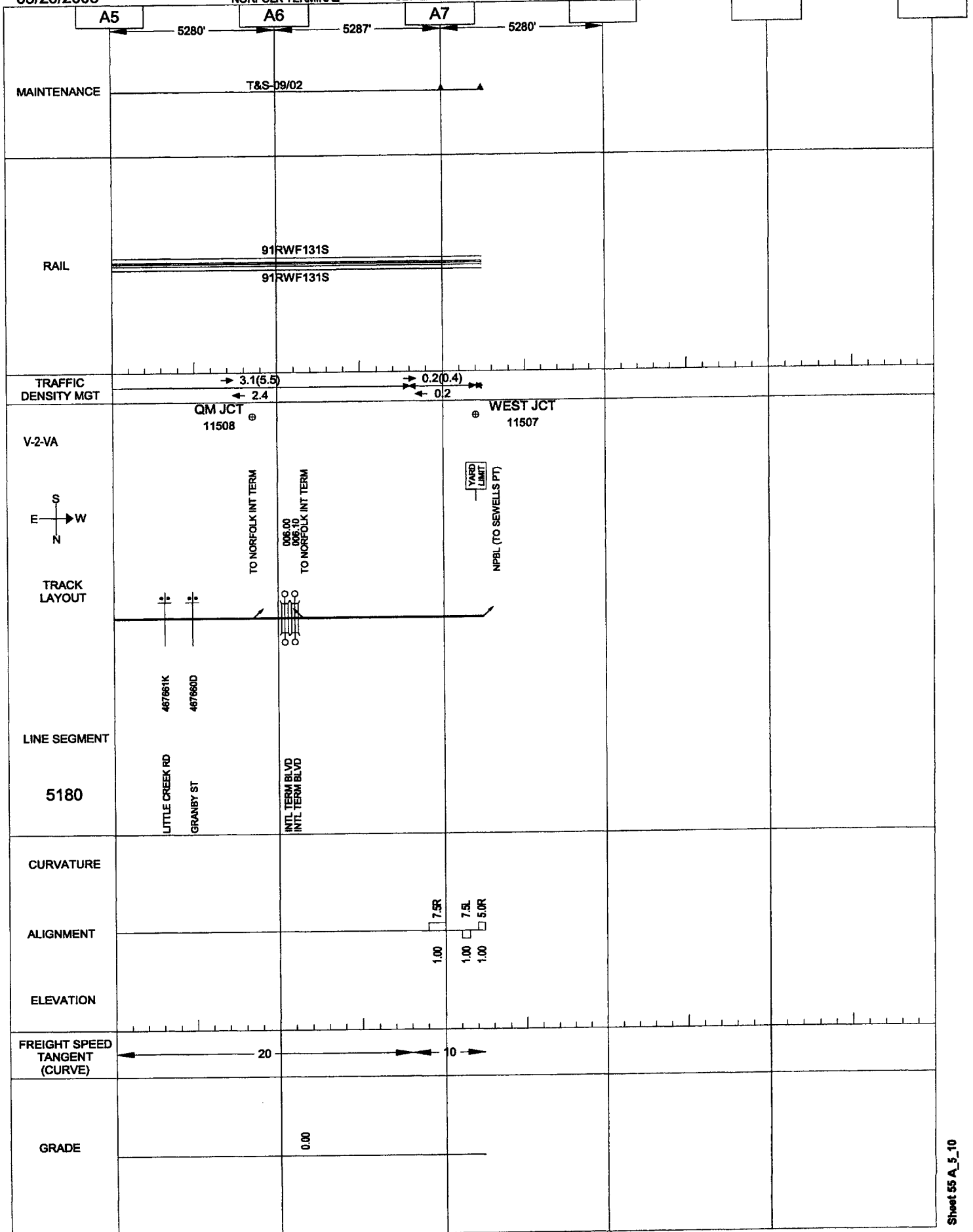
03/20/2003

NORFOLK TERMINAL

SEWELLS POINT BRANCH

TIDEWATER JCT-SEWELL

VIRGINIA



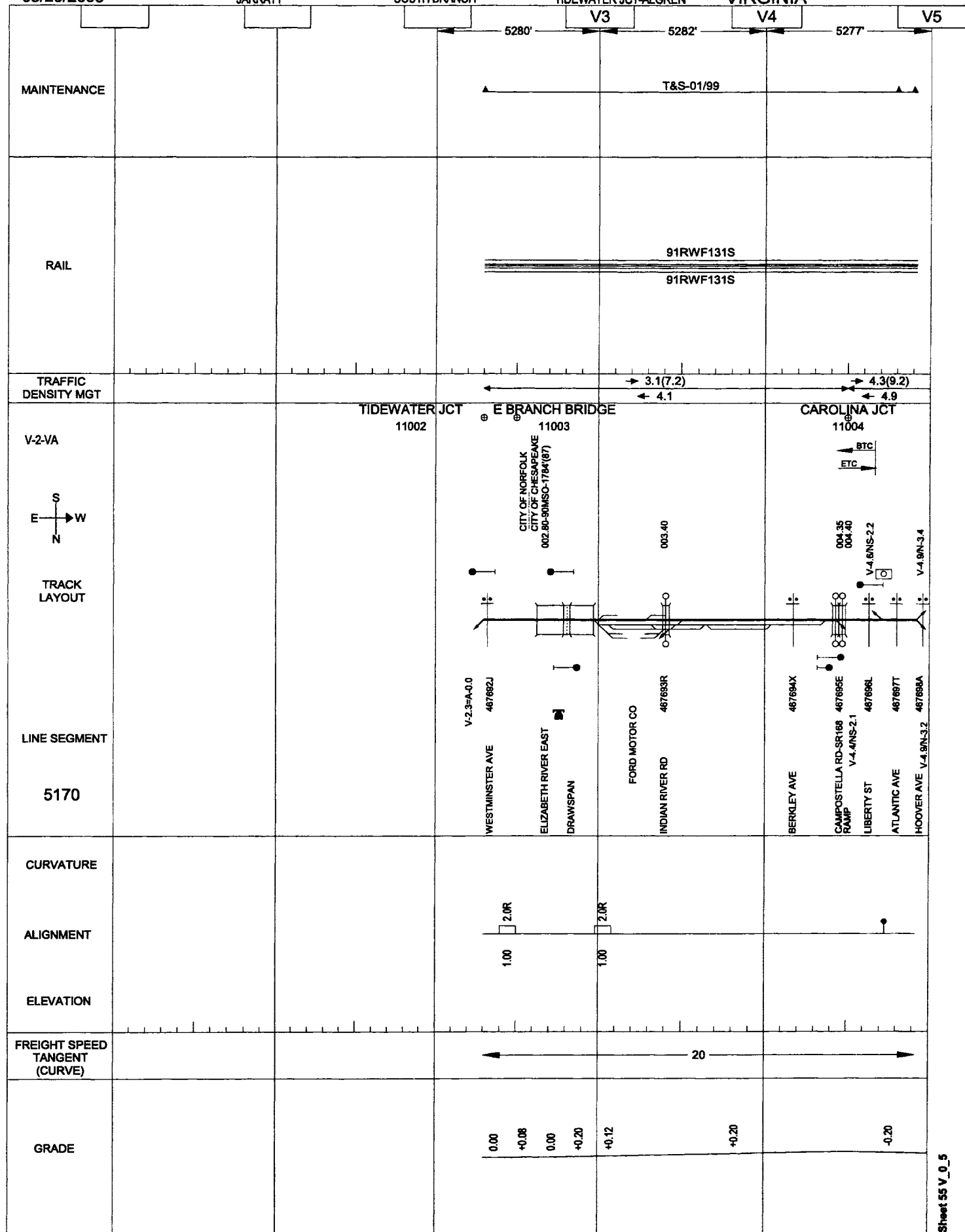
03/20/2003

JARRATT

324
SOUTH BRANCH

TIDEWATER JCT-ALGREN

VIRGINIA



325

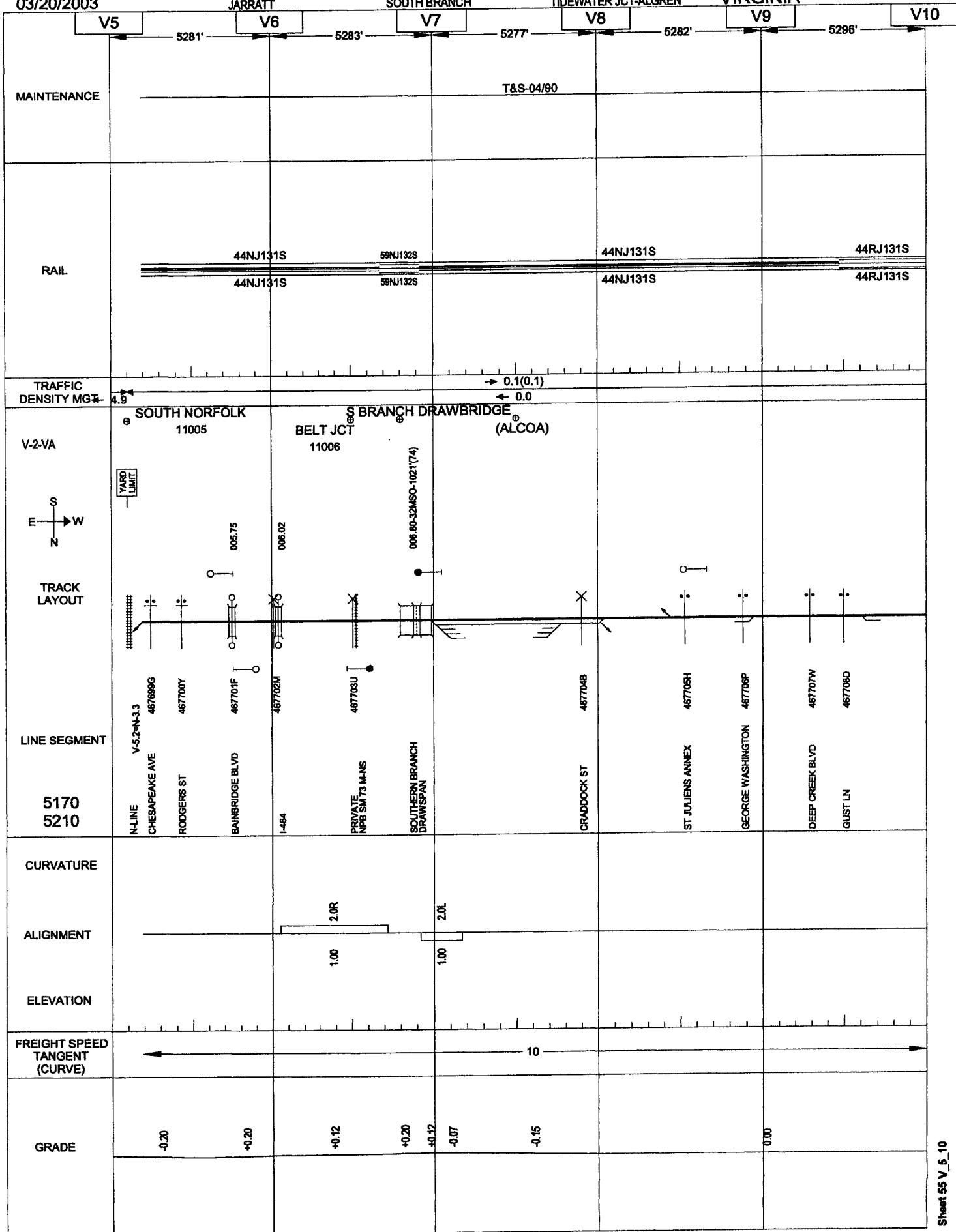
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JARRATT

SOUTH BRANCH

TIDEWATER JCT-ALGREN

VIRGINIA



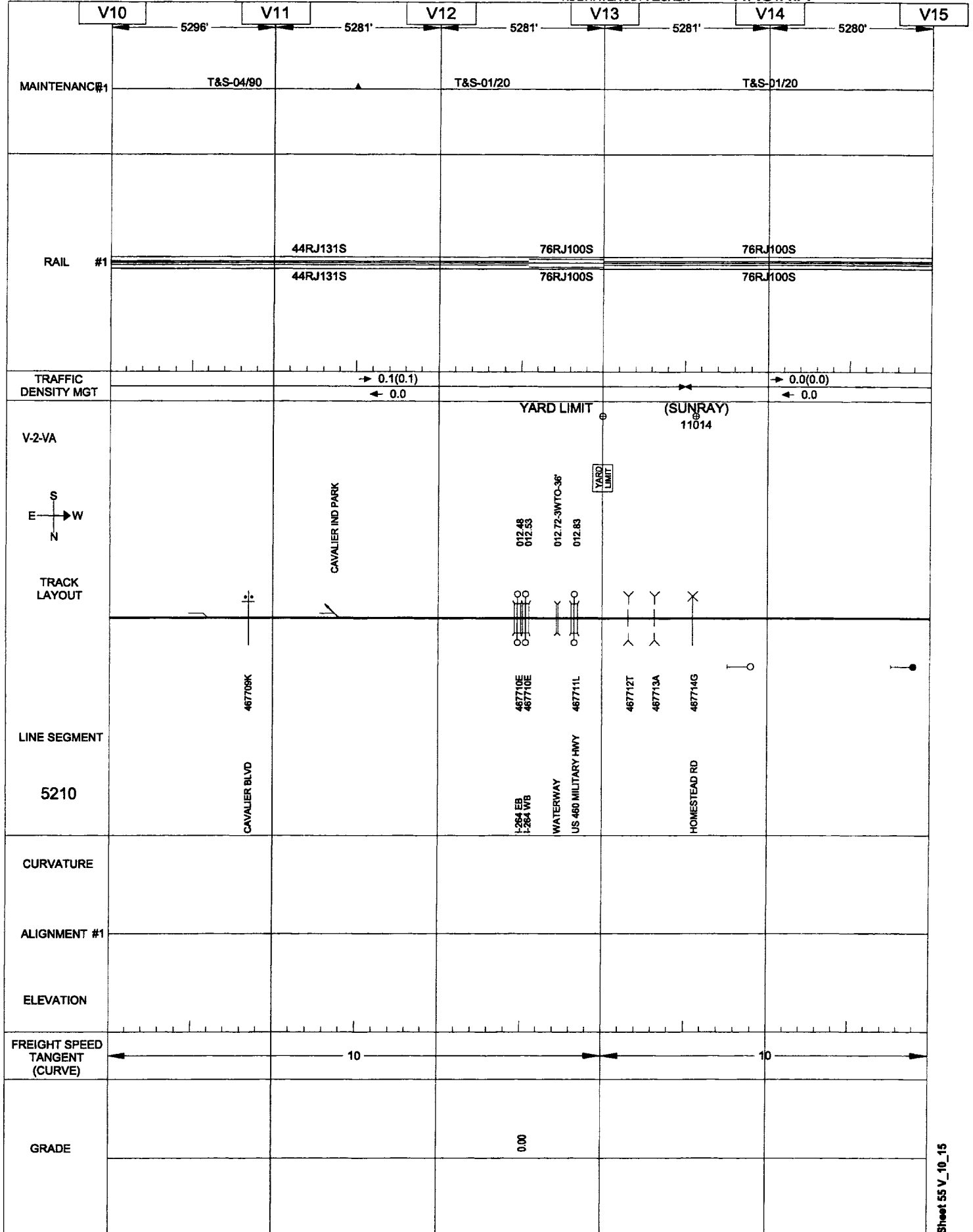
03/20/2003

JARRATT

SOUTH BRANCH

TIDEWATER JCT-ALGREN

VIRGINIA




03/20/2003

JARRATT

SOUTH BRANCH

TIDEWATER JCT-ALGREN

VIRGINIA

V15		5288'	5280'	5280'	5280'	5280'	5280'
MAINTENANCE	T&S-01/20						
RAIL	76RJ100S 76RJ100S						
TRAFFIC DENSITY MGT	0.0(0.0) 0.0						
V-2-VA	ALGREN 11515						
TRACK LAYOUT							
LINE SEGMENT	467715N						
5210	SNOWDEN ST						
CURVATURE							
ALIGNMENT							
ELEVATION							
FREIGHT SPEED TANGENT (CURVE)	10						
GRADE	0.00						

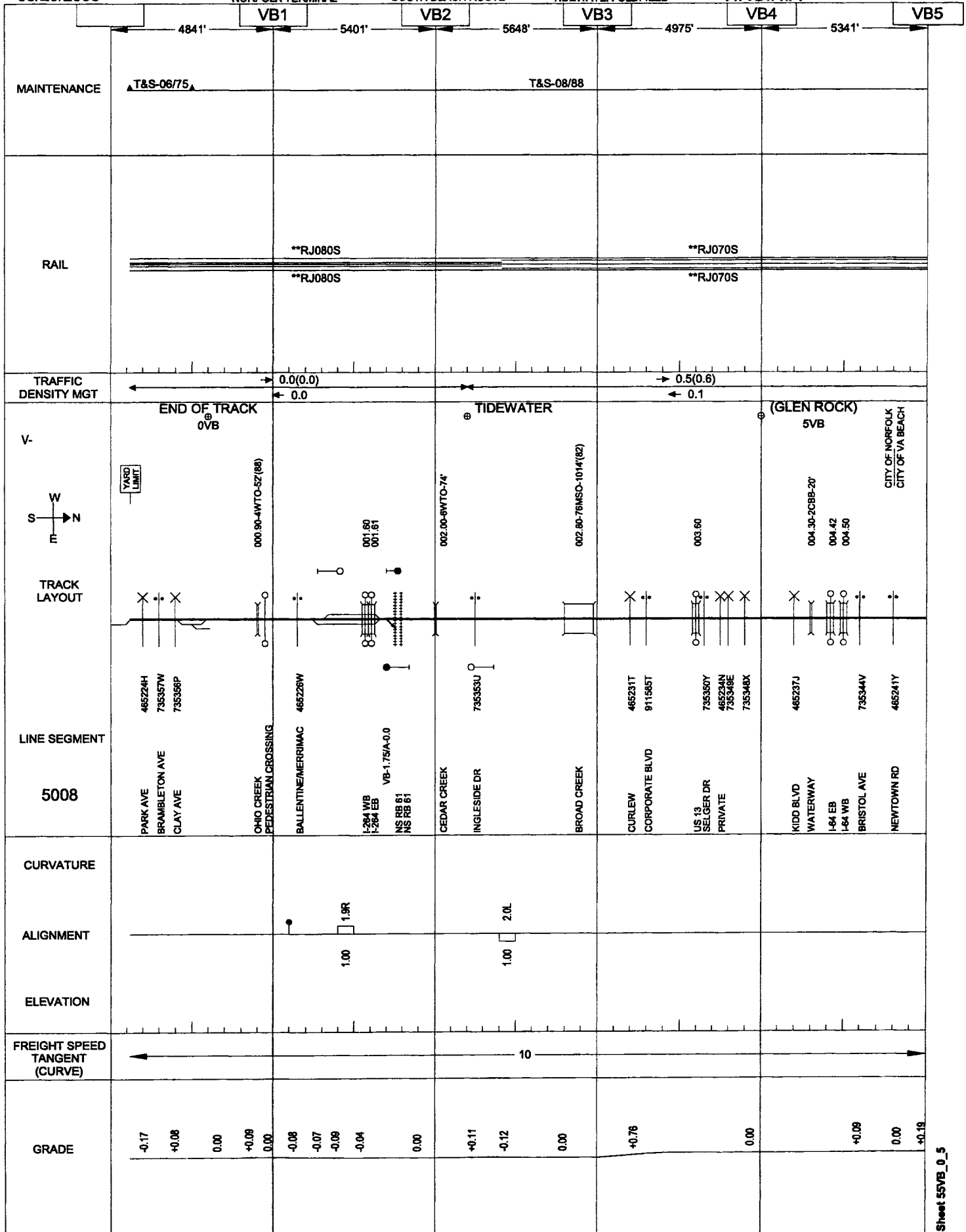
03/20/2003

NORFOLK TERMINAL

SOUTH BEACH ROUTE

TIDEWATER-OLDFIELD

VIRGINIA



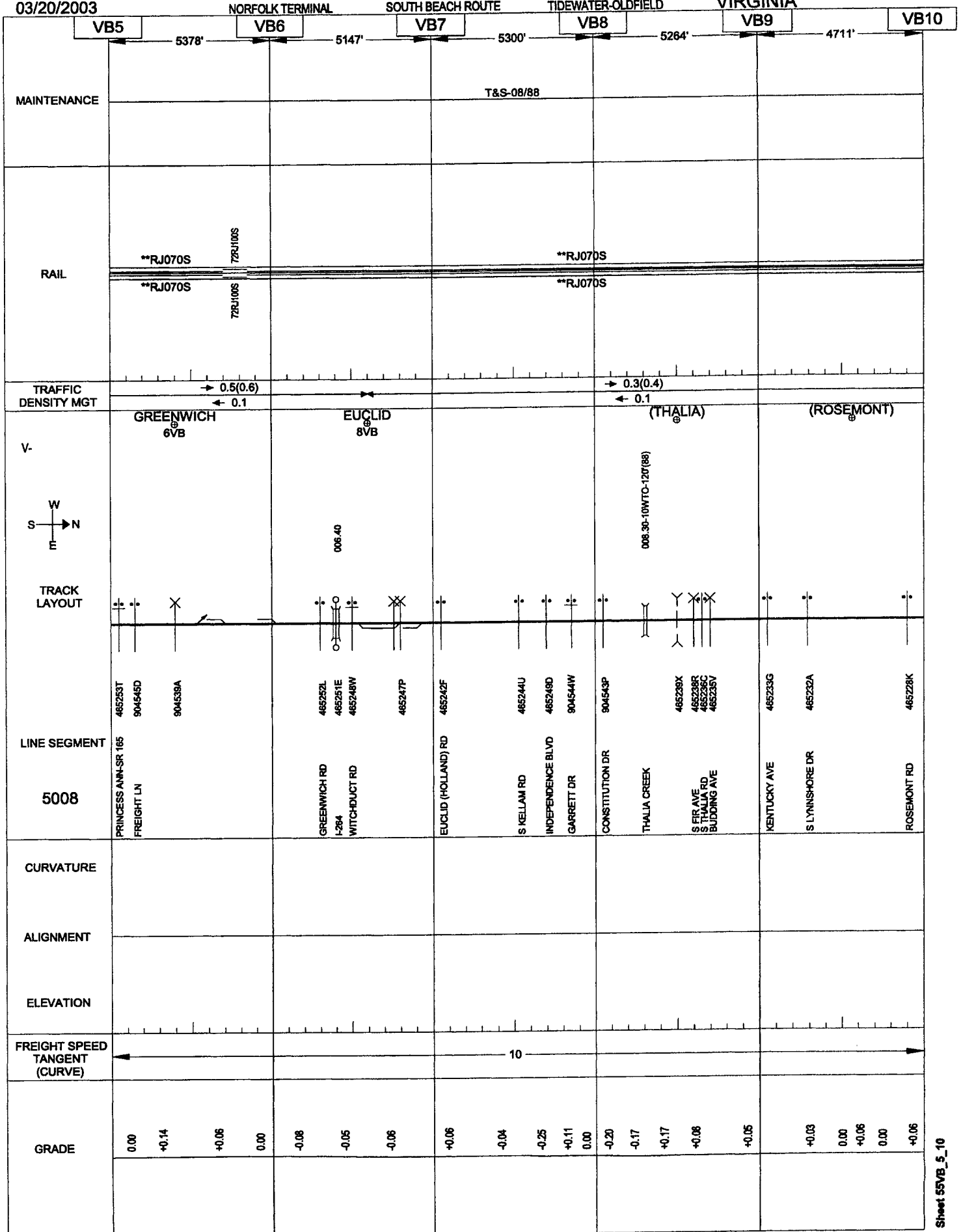
03/20/2003

NORFOLK TERMINAL

SOUTH BEACH ROUTE

TIDEWATER-OLDFIELD

VIRGINIA



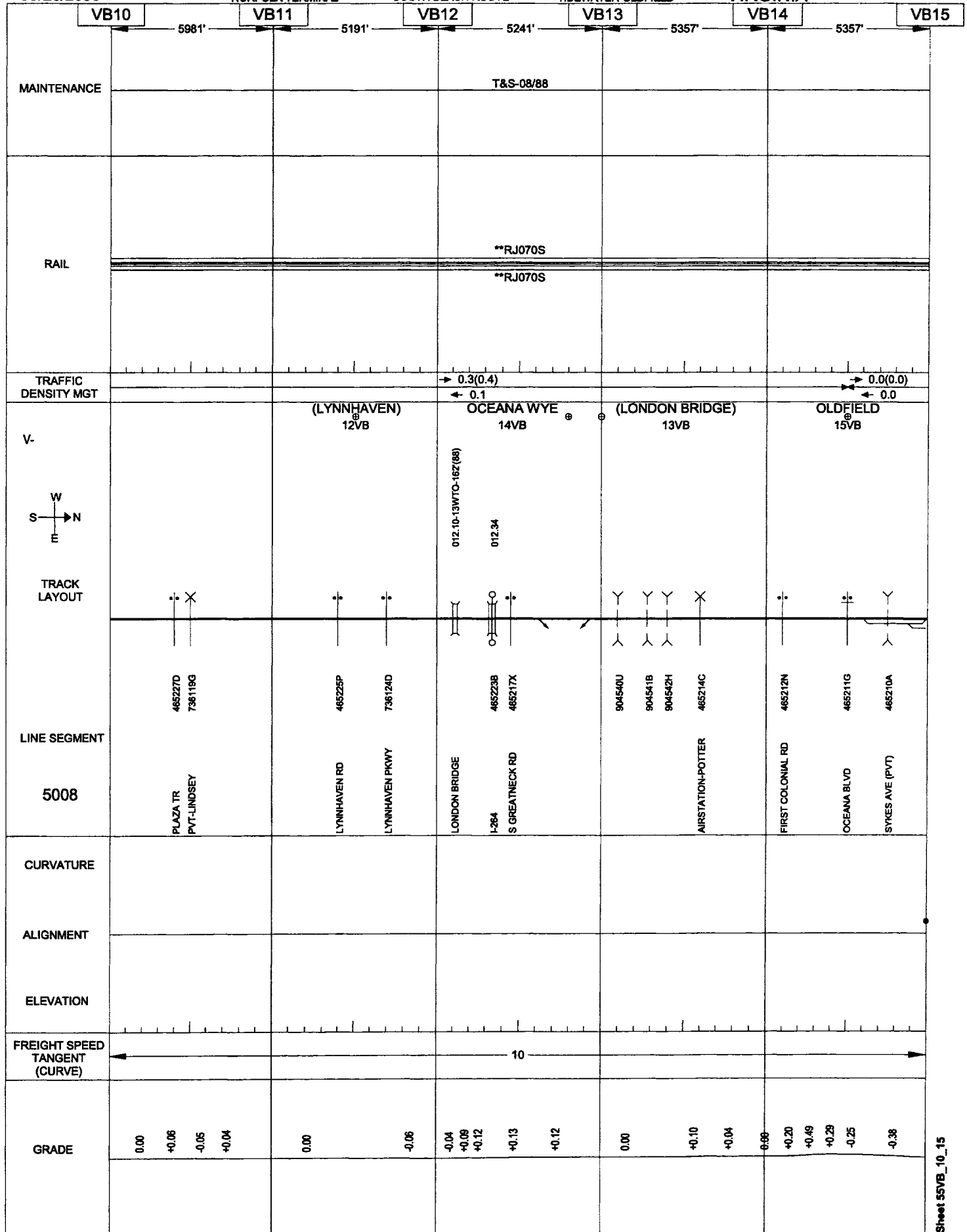
03/20/2003

NORFOLK TERMINAL

SOUTH BEACH ROUTE

TIDEWATER-OLDFIELD

VIRGINIA



03/20/2003

NORFOLK TERMINAL

SOUTH BEACH ROUTE

TIDEWATER-OLDFIELD

VIRGINIA

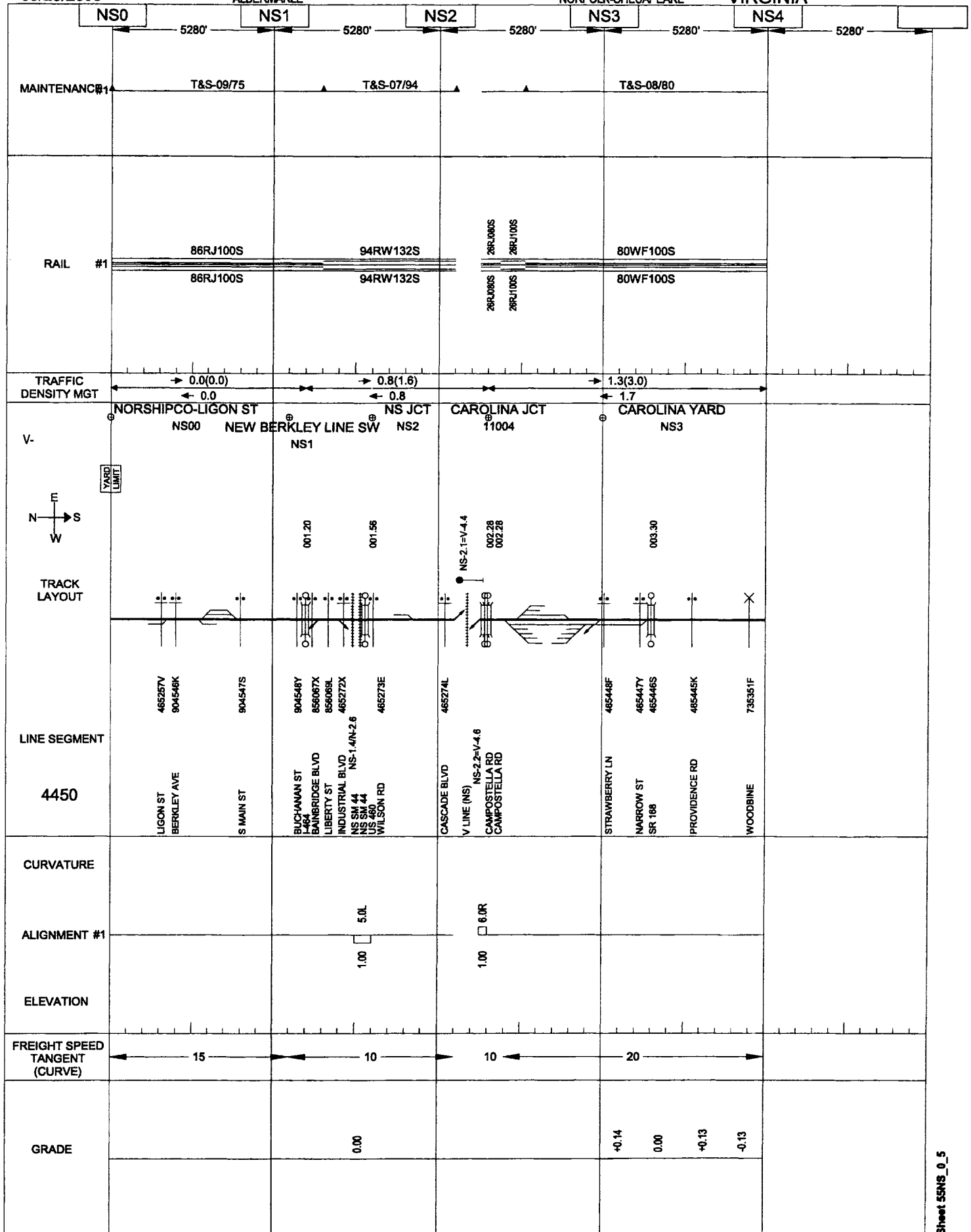
	VB15	3986'					
MAINTENANCE	T&S-08/88▲						
RAIL	**RJ070S **RJ070S						
TRAFFIC DENSITY MGT	0.0(0.0) ← 0.0						
V-	END OF TRACK ⑥						
W S → N E	YARD LIMIT						
TRACK LAYOUT							
LINE SEGMENT	BIRDNECK INDUSTRIAL						
5008							
CURVATURE							
ALIGNMENT							
ELEVATION							
FREIGHT SPEED TANGENT (CURVE)	← 10 →						
GRADE	-0.22 +0.41						

03/20/2003

ALBERMARLE

NORFOLK-CHESAPEAKE

VIRGINIA



333

03/13/2003

DANVILLE

HURT CONNECTION

ALTAVISTA-HURT

PIEDMONT

AC197

AC198

5280'

5280'

MAINTENANCE

T&S-12/82

RAIL

82NWF132S

82NWF132S

TRAFFIC
DENSITY MGT

15.8(21.8)

6.0

V-2-VA



TRACK
LAYOUT

HURT CONN
91200

HURT
197

197.35-3850-3007(00)

197.65

LINE SEGMENT

0709

AC-197.0 = V-200.3

SYCAMORE CREEK

OLD US 29

713824A

AC-198.83 = 197.3

CURVATURE

ALIGNMENT

ELEVATION

4.2R
1.50
3.50
1.00

8.1R

3.6L

FREIGHT SPEED
TANGENT
(CURVE)

30

GRADE

+1.20

+1.40

+0.71

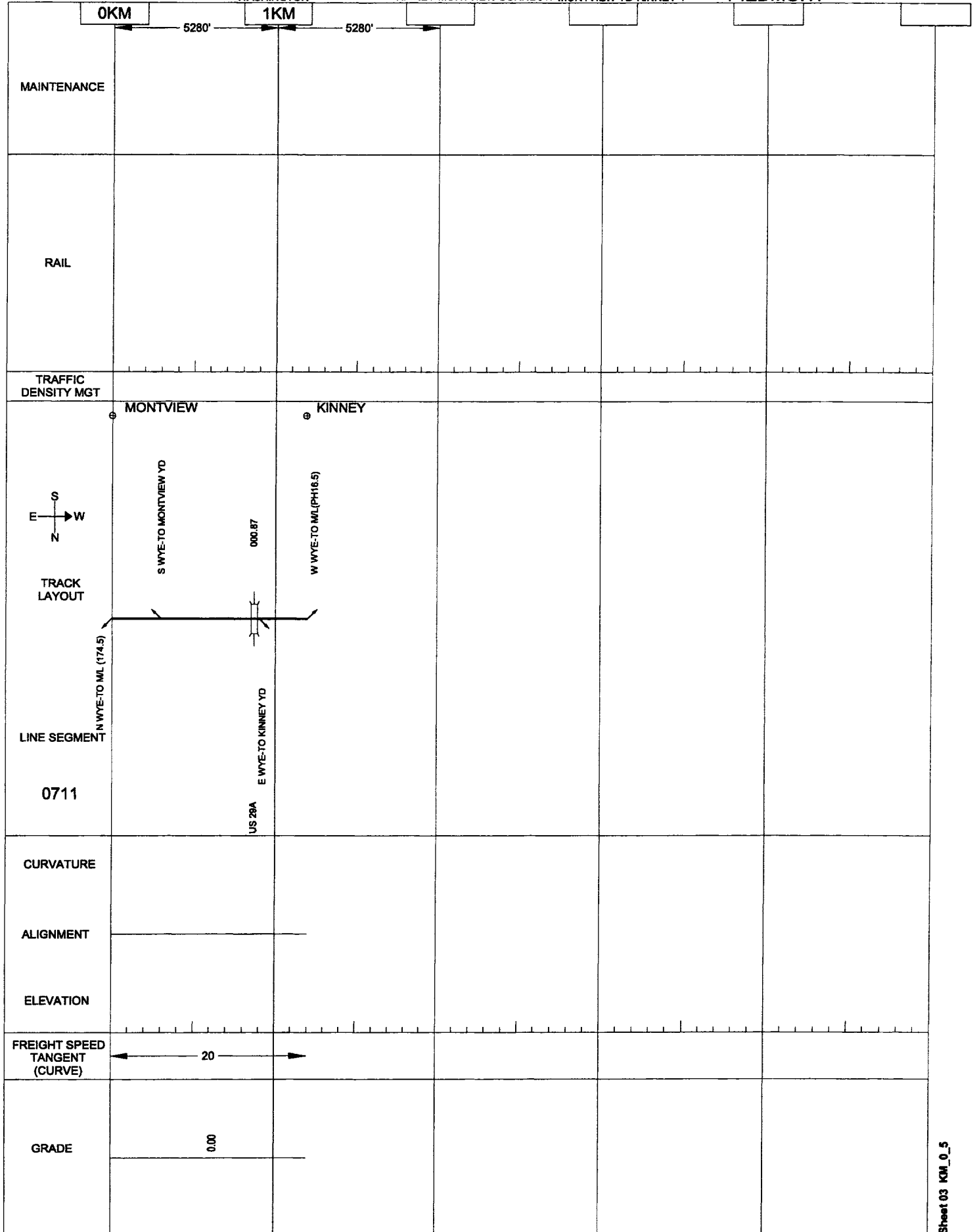
0.00

03/13/2003

WASHINGTON

KINNEY-MONTVIEW CONNECTI MONTVIEW YD-KINNEY Y

PIEDMONT



Explanation of Graphic Display Conventions

Top Margin - (Left to Right)

- 1) Data shown represents when drawing was prepared.
- 2) Items correspond to:
 District, Branch and Spur when applicable. (Original NS System)
 or
 Old Division name, and From-To station names. (Original NS System)
 or
 RDBR, Line Name, From-To station names. (Original CR System)
 or
 District, Line Name, From-To station names. (Shared Asset)
- 3) Operating Division name.
- 4) Sheet number within Operating Division.

Milepost Data Band -

- 1) Current milepost designation with prefix or suffix.
- 2) Actual distance between milepost markers in feet.

Maintenance Section -

T&S date displayed above each main and surfacing data displayed below each main. Surfacing date is suppressed when prior to T&S date. Vertical tick-marks above or below each main show break points for T&S and/or surfacing. To avoid confusion with the year 2000 (or 00) any T&S or Surfacing record with a date of 1920 or earlier is displayed as year 1920 (or 20).

Rail Section -

Data displayed above the graphic for each main represents the left rail and data displayed below the graphic for each main represents the right rail (viewed in increasing milepost direction). Representation gives year laid, rail type, rail weight and premium (P) vs. standard (S) rail. Rail graphic change indicates break point in data values. Rail type codes are shown in Table 1. To avoid confusion with the year 2000 (or 00) any rail with a laid date of 1920 or earlier is displayed as the year 1920 (or 20).

Traffic Density Section -

Annual density displayed in millions of gross tons rounded to tenths. Note this results in density less than 50,000 tons being rounded to zero. Direction of traffic is shown with arrows. Density by track is also shown with track identification.

Track Layout Section -

- 1) Left hand margin gives valuation map I.D., track timetable direction (compass rose) and line segment reporting number.
- 2) Station names and locations are displayed at topmost position in the track layout section (immediately below the traffic density section). Those stations shown in bold print only are timetable stations and those station names which are enclosed in parentheses are non-timetable (reporting) stations. The number shown below the station name is the station code.
- 3) Territory markers (traffic control, test miles, yard limits, state lines, county/incorporated lines) are generally displayed in the area just below the station names. However, on occasion, test miles are displayed below the track line in order to alleviate print congestion problems on some pages.
- 4) Bridge technical specifications are displayed in the top center area between the station names and the track layout. This specification is Bridge number (in milepost format), number of spans, structure construction, and length of structure. See Table 2 for explanation of structure construction codes. For open deck bridge tie replacement dates (year) are provided in parentheses following the bridge technical specifications. Single main track with one redecking date is displayed as (XX). If two dates are available they are displayed as (XX,XX). If three or more dates are available they are displayed as a range (XX-XX) with the first year being the earliest date and the second year being the latest date. This same convention applies to multiple main tracks with the first set of () containing track 1 data, the second set of () containing track 2 data, the third set of () etc.. If page space limitations do not allow the dates to be displayed after the bridge specifications these dates are displayed in any space available adjacent to the specifications. More detailed information for all open deck bridges is provided in Table 3 which follows this Explanation of Graphic Display section.
- 5) Signals, signal structures, AEI scanners, microwave towers, radio base stations, telephones, and the various types of wayside detectors are displayed on the appropriate side of the track and in the area just above or just below the track diagram.
- 6) The track diagram for main line, side tracks, crossovers, leads and yard track including types of turnouts between single main and double main territory is displayed in the center of the track layout section. The heavy lines represent main tracks and their crossovers and switches. The lighter weight lines represent side and other tracks. The heavy arrows indicate junctions with branch lines and connections with other railroads (see item 8). Details of this display are limited to switches on main track and adjacent track. Also shown in this area are the graphic symbols for the various grade crossing types, clearance detectors, overpasses, underpasses, bridges, tunnels, and other overhead structures.

The common names (street, highway, river) associated with these graphic symbols are displayed at the bottom of the track layout section. Note: For railroad crossings at grade, the display convention, when available is: Other Road, Crossing Type, Angle and Maintaining Road. Crossing types are:

SM - Solid Manganese
 MI - Manganese Insert
 RB - Rail Bolted
 XO - Double Crossover
 MP - Movable Point

- 7) The AAR number (DOT number) for each road crossing is displayed in the band which runs just below the track layout display and just above the common name display at the bottom of the track section. This DOT number consists of seven characters (six numbers and a letter) and lines up with the graphic symbol on the track line and its common name at the bottom.
- 8) Other information which may be displayed in the track layout section includes:
 - Industry names associated with various switches and sidings.
 - Equalities which show where two (or more) different lines connect with each other. The format for this display is milepost Junction Point (on the line being displayed) = milepost Junction Point (on the line which is connecting at this point). Note: An = symbol means the lines connect directly and a / symbol means the lines connect indirectly through a yard or side track.)

Alignment Section -

- 1) Graphic representation is given for curve direction and length for each main. (Note: Representation is currently provided for track 1 only. In double track sections track 2 is only a copy of track 1 and may not represent actual conditions.)
- 2) Curvature is specified to tenths of a degree above each main along with left/right indication.
- 3) Location of wheel flange lubricators are given along mains.

Freight Speed Section -

Curve and tangent speed limits taken from the timetables. The curve speed limit (shown within parentheses) is shown under the specific curve to which it applies.

Grade Section -

Grade shown is based on ascending milepost direction.

Ruling grades are based upon determination made by Operations Research and are stated in the uphill direction, e.g. "Bluefield to Roanoke", regardless of whether the uphill direction is ascending milepost or descending milepost.

TABLE 1
RAIL LETTER CODES

N	New rail
R	Relay rail
W	Welded rail
J	Jointed rail
F	Field welded rail
P	Premium Rail (head hardened)
S	Standard Rail (non hardened)

TABLE 2
BRIDGE TYPE CODES

Type of Bridge Structure

BS = Beam Span
BA = Brick Arch
CA = Concrete Arch
CB = Concrete Box
CS = Concrete Span
DG = Deck Plate Girder
DT = Deck Truss
MA = Masonry Arch
MS = Mixed Span
SA = Structural Plate Arch
TG = Through Plate Girder
TT = Through Truss
WT = Timber (Wood) Trestle

Deck Construction

O = Open Deck
B = Ballast Deck
C = Combination

TABLE 3
OPEN DECK BRIDGE INFORMATION

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
322	A-003.20	003.20	01	1983
322	A-003.20	003.20	02	1987
214	C-000.60	000.60	01	1976
214	C-001.30	001.30	01	1987
214	C-001.47	001.47	01	1985
214	C-002.85	002.85	01	1985
214	C-004.63	004.63	01	1987
215	C-005.16	005.16	01	1988
215	C-005.90	005.90	01	1987
215	C-008.48	008.48	01	1985
266	F-036.72	036.70	01	2000
267	F-041.48	041.40	01	1980
267	F-043.10	043.10	01	1982
295	F-110.70	110.80	01	1995
295	F-113.23	113.20	01	1980
297	F-121.13	121.10	01	1998
298	F-128.65	128.60	01	1996
300	F-139.41	139.50	01	1980
300	F-139.47	139.60	01	1980
300	F-139.66	139.80	01	2001
300	F-139.89	139.95	01	1994
301	F-140.80	140.80	01	1992
			01	1978
301	F-141.91	141.90	01	1990
301	F-142.53	142.50	01	1974
301	F-144.56	144.80	01	1999
303	F-152.33	152.40	01	1980
303	F-152.46	152.60	01	1990
304	F-158.98	159.00	01	1993
305	F-160.53	160.50	01	1996
305	F-164.32	164.50	01	1995
306	F-168.62	168.70	01	1985
164	H-005.31	005.28	01	2002
164	H-006.32	006.32	01	1990
164	H-006.62	006.62	01	1990
164	H-007.65	007.66	01	1992
164	H-007.97	007.97	01	1981
164	H-009.21	009.23	01	1992
165	H-011.19	011.19	01	1993
165	H-013.41	013.43	01	2002
166	H-016.01	016.01	01	2002
			01	2001
			01	2000
			01	1980
166	H-016.95	016.98	01	1995
166	H-017.35	017.35	01	1990
167	H-022.27	022.28	01	1981
167	H-022.32	022.31	01	1983
169	H-030.74	030.74	01	1987
169	H-032.05	032.05	01	1987
169	H-032.28	032.28	01	1987
171	H-043.45	043.45	01	1994
173	H-051.39	051.39	01	1986
174	H-057.84	057.86	01	1991
174	H-058.79	058.79	01	1991
175	H-060.47	060.47	01	1985
175	H-061.98	061.99	01	1999

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
175	H-062.59	062.59	01	1991
176	H-066.29	066.32	01	1987
176	H-068.13	068.13	01	1994
176	H-068.53	068.53	01	2002
178	H-075.37	075.37	01	1997
178	H-076.28	076.30	01	1985
178	H-076.98	076.98	01	1986
178	H-077.35	077.35	01	1984
179	H-080.01	080.01	01	1986
180	H-085.20	085.20	01	1991
180	H-086.86	086.86	01	1981
180	H-088.86	088.88	01	1981
181	H-090.07	090.10	01	1992
182	H-096.66	096.66	01	1983
182	H-097.64	097.64	01	2002
182	H-098.63	098.63	01	1996
183	H-102.56	102.56	01	1993
183	H-103.01	103.01	01	1993
183	H-104.20	104.22	01	1995
183	H-104.43	104.44	01	1977
184	H-108.22	108.22	01	1997
185	H-112.48	112.55	01	1996
185	H-114.07	114.07	01	1993
185	H-114.94	114.94	01	1980
186	H-116.63	116.63	01	1995
186	H-118.30	118.32	01	1993
186	H-119.30	119.33	01	1985
187	H-120.13	120.17	01	1980
187	H-123.76	123.76	01	1985
187	H-124.31	124.33	01	1992
188	H-125.48	125.47	01	1983
188	H-126.73	126.73	01	1982
188	H-128.99	128.99	01	1990
188	H-129.56	129.56	01	1983
189	H-130.82	130.82	01	1979
189	H-132.31	132.32	01	1982
189	H-132.86	132.86	01	1980
189	H-133.41	133.41	01	1985
189	H-134.66	134.68	01	1994
190	H-138.73	138.74	01	1985
190	H-139.94	139.93	01	1983
191	H-141.01	141.01	01	1985
191	H-142.56	142.56	01	1978
191	H-143.53	143.55	01	1987
192	H-147.07	147.07	01	1981
192	H-147.32	147.32	01	1993
193	H-150.19	150.19	01	1980
193	H-154.70	154.70	01	1987
194	H-156.40	156.41	01	1985
194	H-157.61	157.61	01	1987
195	H-164.97	164.98	01	1986
196	H-167.27	167.27	01	1986
196	H-167.27	167.27	02	1980
197	H-170.09	170.09	01	1995
197	H-170.27	170.29	01	1994
197	H-172.25	172.27	01	1983
197	H-173.61	173.59	01	1993
197	H-173.83	173.81	01	1993
198	H-177.72	177.74	01	1992
199	H-184.40	184.41	01	1998
199	H-184.44	184.44	01	1982

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
200	H-185.54	185.55	01	1982
200	H-186.65	186.65	01	1992
			01	1985
201	H-191.29	191.31	01	1981
201	H-191.44	191.46	01	1988
201	H-192.18	192.20	01	1979
201	H-192.54	192.55	01	1997
202	H-198.62	198.62	01	1982
204	H-208.73	208.74	01	1987
205	H-213.13	213.11	01	1982
206	H-217.91	217.91	01	1985
207	H-221.05	221.05	01	1992
207	H-222.23	222.23	01	1987
208	H-225.06	225.06	01	1985
208	H-225.74	225.74	01	1994
208	H-229.88	229.88	01	2001
210	H-236.30	236.30	01	1987
247	L-003.48	003.48	01	1995
248	L-005.35	005.35	01	1998
248	L-008.49	008.48	01	1975
253	L-033.57	033.57	01	1991
			01	1990
254	L-036.10	036.10	01	1996
254	L-039.27	039.25	01	1990
257	L-054.48	054.45	01	1990
259	L-061.19	061.19	01	1989
259	L-064.02	064.02	01	1992
261	L-072.60	072.60	01	1996
262	L-078.81	078.81	01	1974
269	M-000.29	000.30	01	1998
3	N-001.27	001.27	01	1985
3	N-001.27	001.27	02	1999
4	N-005.80	005.80	01	1985
4	N-005.80	005.80	02	1985
4	N-006.66	006.66	01	1984
4	N-006.66	006.66	02	2002
8	N-025.52	025.52	01	1990
8	N-025.52	025.52	02	1998
8	N-025.70	025.70	01	1997
8	N-025.70	025.70	02	1999
8	N-025.74	025.74	01	1997
8	N-025.74	025.74	02	1997
11	N-040.62	040.69	01	1986
11	N-040.62	040.69	02	1989
15	N-064.85	064.80	01	1998
15	N-064.85	064.80	02	1992
22	N-080.50	080.52	01	1980
23	N-085.67	085.63	01	1978
33	N-124.38	124.38	01	1995
33	N-124.38	124.38	02	2002
35	N-133.37	133.41	01	2000
35	N-133.37	133.41	02	1987
44	N-133.65	133.75	01	1978
44	N-133.65	133.75	02	1978
46	N-144.87	144.87	01	1980
47	N-149.50	149.50	01	1994
47	N-149.86	149.93	01	1988
55	N-187.55	187.47	01	1990
65	N-222.05	222.05	01	1983
66	N-228.95	228.95	01	1997
67	N-234.04	234.03	01	1993

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
69	N-241.11	241.11	02	2001
70	N-249.38	249.39	01	1987
70	N-249.38	249.39	02	1992
71	N-250.11	250.11	BOTH	1985
71	N-250.44	250.44	01	1992
71	N-250.44	250.44	02	1992
71	N-250.65	250.65	01	1992
71	N-250.65	250.65	02	1992
71	N-250.94	250.94	01	2000
71	N-250.94	250.94	02	1994
71	N-251.90	251.94	01	1996
71	N-251.90	251.94	02	1995
71	N-252.42	252.41	01	1995
71	N-252.42	252.41	02	1993
71	N-253.77	253.77	01	1988
71	N-253.77	253.77	02	1993
71	N-254.87	254.86	01	1994
71	N-254.87	254.86	02	1986
72	N-255.53	255.56	01	1982
72	N-255.53	255.56	02	1983
72	N-256.12	256.12	01	1978
72	N-256.12	256.12	02	2002
73	N-262.76	262.76	01	1984
73	N-262.76	262.76	02	1971
73	N-264.45	264.43	01	1988
73	N-264.45	264.43	02	1983
74	N-266.70	266.70	01	1980
74	N-266.70	266.70	02	1980
75	N-270.45	270.45	01	1981
75	N-270.45	270.45	02	2002
75	N-271.35	271.35	01	1990
75	N-271.35	271.35	02	1994
75	N-271.68	271.68	01	1996
75	N-271.68	271.68	02	1992
75	N-272.61	272.61	01	2002
75	N-272.61	272.61	02	2002
76	N-275.69	275.71	01	1984
76	N-275.69	275.71	02	1985
76	N-276.91	276.93	01	1983
76	N-276.91	276.93	02	1981
76	N-277.35	277.36	01	1990
76	N-277.35	277.36	02	1980
76	N-277.77	277.77	01	1982
76	N-277.77	277.77	02	1993
78	N-285.09	285.09	01	1989
78	N-285.09	285.09	02	1990
79	N-290.74	290.74	01	1991
79	N-290.74	290.74	02	2001
79	N-291.90	291.89	01	1987
79	N-291.90	291.89	02	1985
80	N-295.93	295.93	01	1987
80	N-295.93	295.93	02	1998
80	N-296.21	296.21	01	1997
80	N-296.21	296.21	02	2000
84	N-305.23	305.27	01	1993
84	N-308.75	308.76	01	1995
85	N-310.66	310.66	01	1993
87	N-321.87	321.87	01	1973
87	N-321.87	321.87	02	1988
89	N-333.80	333.82	01	1983
89	N-333.80	333.82	02	1997

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
91	N-340.68	340.68	01	1996
91	N-340.68	340.68	02	1996
91	N-340.79	340.79	01	1996
91	N-340.79	340.79	02	1990
91	N-341.60	341.60	02	2001
91	N-343.04	343.02	01	1985
91	N-343.04	343.02	02	1989
92	N-346.43	346.44	01	1997
92	N-346.43	346.44	02	1997
92	N-346.62	346.62	01	1989
92	N-346.62	346.62	02	1983
92	N-349.14	349.13	01	1997
92	N-349.14	349.13	02	1990
93	N-351.20	351.21	01	1995
93	N-351.20	351.21	02	1989
93	N-352.01	352.00	01	1991
93	N-352.01	352.00	02	1997
93	N-352.14	352.15	01	1992
93	N-352.14	352.15	02	1990
93	N-352.82	352.83	01	1992
93	N-352.82	352.83	02	1997
93	N-354.13	354.13	01	1990
93	N-354.13	354.13	02	1985
94	N-359.76	359.74	01	1993
94	N-359.76	359.74	02	2001
19	P-004.57	004.60	01	1995
20	P-005.55	005.55	01	1999
217	R-003.51	003.51	01	1998
218	R-006.54	006.54	01	1979
219	R-010.25	010.25	01	1993
219	R-014.93	014.93	01	1997
220	R-016.12	016.15	01	1991
220	R-016.75	016.77	01	1979
221	R-022.13	022.15	01	1986
221	R-022.89	022.89	01	1986
222	R-028.35	028.35	01	1990
223	R-030.49	030.49	01	1992
224	R-035.14	035.14	01	1998
224	R-039.57	039.55	01	1981
225	R-040.05	040.02	01	1982
225	R-040.51	040.51	01	1981
225	R-040.78	040.78	01	1987
225	R-041.18	041.20	01	1978
226	R-045.14	045.16	01	1979
226	R-045.30	045.33	01	1995
226	R-049.44	049.48	01	1994
226	R-049.58	049.62	01	1997
227	R-052.78	052.72	01	1991
227	R-053.65	053.48	01	2001
229	R-063.75	063.78	01	1987
230	R-066.02	066.02	01	1985
231	R-070.53	070.55	01	1986
231	R-074.12	074.04	01	1994
			01	1983
234	R-089.65	089.65	01	1981
236	R-096.33	096.36	01	1983
237	R-100.75	100.75	01	1993
237	R-104.26	104.26	01	1984
238	R-108.50	108.50	01	1999
241	R-124.18	124.21	01	1987
241	R-124.18	124.21	02	1983

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
242	R-125.14	125.16	01	1987
242	R-125.75	125.75	01	1999
242	R-125.91	125.91	01	1987
242	R-126.02	126.01	01	1987
242	R-126.06	126.05	01	1988
81	S-002.54	002.54	01	1975
324	V-002.63	002.80	01	1987
325	V-006.80	006.80	01	1974
97	V-149.73	149.90	01	1988
98	V-151.49	151.60	01	1987
99	V-156.25	156.40	01	1999
99	V-157.86	158.00	01	2001
101	V-165.80	165.80	01	2000
101	V-168.00	168.00	01	2001
102	V-173.70	173.70	01	1985
103	V-176.30	176.30	01	1997
103	V-179.58	179.60	01	2002
104	V-182.57	182.50	01	1994
105	V-186.12	186.10	01	1997
106	V-190.10	190.10	01	1997
106	V-194.95	195.00	01	1992
107	V-195.96	196.00	01	1992
108	V-200.00	200.00	01	1986
108	V-204.84	204.90	01	1996
109	V-205.15	205.22	01	1984
109	V-207.10	207.10	01	1985
109	V-208.20	208.20	01	1990
109	V-208.40	208.40	01	1983
110	V-213.68	213.70	01	1988
110	V-214.60	214.60	01	1988
111	V-216.55	216.60	01	2000
113	V-227.50	227.50	01	1993
113	V-229.25	229.40	01	1998
114	V-230.64	230.70	01	1985
114	V-232.00	232.00	01	1986
114	V-232.92	233.00	01	1985
114	V-233.80	233.80	01	1987
116	V-240.30	240.30	01	1998
116	V-241.01	241.00	01	1996
121	V-246.43	246.50	01	1984
121	V-248.90	248.90	01	1990
122	V-250.70	250.70	01	1985
123	V-255.25	255.30	01	1984
123	V-257.43	257.50	01	1991
123	V-257.62	257.70	01	1990
123	V-259.00	259.00	01	1985
123	V-259.82	259.80	01	2002
124	V-262.38	262.40	01	1982
124	V-263.45	263.50	01	1988
124	V-263.75	263.80	01	1988
124	V-263.93	264.00	01	1989
124	V-264.63	264.70	01	1984
125	V-265.10	265.10	01	1993
125	V-265.37	265.40	01	1989
125	V-268.26	268.30	01	1998
126	V-270.62	270.60	01	1997
126	V-271.18	271.20	01	1998
127	V-278.33	278.30	01	1989
127	V-278.47	278.50	01	1989
127	V-278.65	278.70	01	2000
127	V-279.90	279.90	01	1989

<u>PAGE</u>	<u>MILE POST</u>	<u>BRIDGE NUMBER</u>	<u>TRACK</u>	<u>YEAR REDECKED</u>
128	V-280.05	280.00	01	1999
128	V-281.45	281.50	01	2002
129	V-287.60	287.60	01	1988
131	V-295.97	296.00	01	1986
131	V-299.46	299.50	01	1995
133	V-309.22	309.20	01	1991
135	V-316.35	316.30	01	1987
216	W-001.50	001.50	01	1985
82	CA-001.38	001.38	01	1976
315	CW-086.10	086.10	01	1980
315	CW-086.30	086.20	01	1980
315	CW-089.80	089.80	01	1985
316	CW-094.30	094.30	01	1982
316	CW-094.40	094.40	01	1979
317	CW-095.70	095.70	01	1980
317	CW-097.40	097.40	01	1988
317	CW-097.60	097.60	01	1982
317	CW-099.00	099.00	01	1979
318	CW-100.90	100.90	01	1989
318	CW-101.70	101.70	01	1999
319	CW-105.20	105.20	01	1976
319	CW-106.40	106.40	01	1999
244	DW-044.38	044.40	01	NA
245	DW-046.10	046.10	01	1986
245	DW-046.50	046.50	01	NA
309	EB-000.25	000.25	01	1987
309	EB-002.84	002.84	01	1999
311	EB-013.10	013.10	01	1980
312	EB-015.50	015.50	01	1978
312	EB-019.05	019.05	01	1992
312	EB-019.36	019.60	01	1988
313	EB-020.00	020.00	01	1995
313	EB-021.46	021.47	01	1976
273	FD-020.34	020.30	01	1979
273	FD-022.50	022.50	01	1990
275	FD-032.34	032.30	01	1983
276	FD-035.90	035.90	01	1981
276	FD-036.83	036.80	01	1989
277	FD-041.24	041.20	01	1979
277	FD-041.86	041.70	01	1997
277	FD-044.02	044.00	01	1997
278	FD-046.50	046.50	01	1998
			01	1980
278	FD-047.10	047.10	01	1999
279	FD-051.34	051.30	01	1999
280	FD-056.01	056.00	01	1994
280	FD-056.27	056.20	01	2001
281	FD-061.00	061.00	01	1994
281	FD-063.18	063.10	01	NA
282	FD-065.74	065.60	01	NA
283	FD-072.60	072.60	01	1994
285	FD-084.49	084.50	01	2001
287	FD-092.28	092.30	01	1983
287	FD-093.60	093.60	01	1975
321	HS-000.70	000.70	01	1999
321	HS-001.42	001.50	01	1999
321	HS-002.09	002.09	01	NA
321	HS-002.45	002.45	01	NA
321	HS-003.04	003.04	01	1964
270	HY-001.63	001.63	01	1994
271	HY-005.55	005.62	01	1993

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139	NB-302.22	302.22	01	1982
139	NB-302.77	302.77	01	1978
142	NB-316.64	316.64	01	1984
142	NB-316.84	316.85	01	1993
142	NB-316.96	316.95	01	1994
142	NB-317.03	317.03	01	1984
142	NB-317.21	317.21	01	1984
142	NB-317.42	317.42	01	1987
142	NB-317.58	317.57	01	1975
142	NB-317.66	317.66	01	1994
142	NB-318.09	318.05	01	1993
142	NB-318.23	318.23	01	1995
142	NB-318.44	318.44	01	1994
142	NB-319.47	319.47	01	1995
143	NB-320.50	320.50	01	1974
143	NB-321.19	321.19	01	1984
143	NB-321.38	321.38	01	1999
143	NB-321.49	321.49	01	1994
144	NB-327.46	327.46	01	1986
144	NB-328.02	328.02	01	1995
144	NB-328.25	328.24	01	1976
145	NB-331.24	331.24	01	1998
146	NB-339.62	339.62	01	1985
147	NB-341.41	341.41	01	2000
147	NB-341.49	341.49	01	1999
147	NB-342.30	342.26	01	1999
147	NB-342.84	342.75	01	1995
150	NB-355.21	355.21	01	1986
150	NB-359.01	359.01	01	1993
151	NB-361.58	361.59	01	1997
151	NB-362.34	362.34	01	1987
151	NB-362.58	362.59	01	1986
151	NB-364.66	364.66	01	1976
153	NB-370.82	370.84	01	1976
153	NB-372.50	372.49	01	1974
153	NB-373.82	373.82	01	1987
157	NB-394.40	394.40	01	1982
159	NB-401.37	401.37	01	1993
159	NB-403.63	403.63	01	1983
159	NB-403.90	403.90	01	1991
159	NB-404.86	404.86	01	1987
159	NB-404.98	404.97	01	1973
160	NB-405.08	405.08	01	1999
160	NB-405.56	405.56	01	1991
160	NB-406.10	406.10	01	1993
160	NB-406.14	406.12	01	1993
59	PH-008.39	008.39	01	1992
59	PH-009.42	009.42	01	2002
60	PH-014.42	014.42	01	1988
60	PH-014.75	014.73	01	1990
137	PV-000.09	000.00	01	1989
			01	1985
			01	1981
137	PV-003.00	003.00	01	1992
137	PV-004.37	004.37	01	NA
328	VB-000.90	000.90	01	1988
328	VB-002.80	002.80	01	1982
329	VB-008.30	008.30	01	1988
330	VB-012.10	012.10	01	1988
117	VC-000.34	000.35	01	1989
117	VC-001.21	001.21	01	2001